

**95 AVENUE ROAD,  
LONDON,  
NW8 6HY.**

**DESIGN & ACCESS STATEMENT**

FOR PRIOR APPROVAL

***/ 289-PD-DAS V9***

Rev: 27/02/2023



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# 2.0 THE SITE - CONTEXT

27/02/2023

## 2.1 OVERVIEW

The locality comprises a mixture of uses, with education, civic, retail and residential. The predominant use within the immediate site surroundings is residential in the form of purpose built flats in multi-storey blocks. Opposite the site, on the other side of Avenue Road are education buildings.

There are two grade II listed buildings on the other side of Adelaide Road opposite the site. These are indicated by yellow dots on Fig.4 opposite. Swiss Cottage Library and Regency Lodge.

The site is not listed and does not form part of any conservation areas.

The site is a triangular shaped plot with a cross shaped tower building and a lower linear garage block flanking the southern boundary. The buildings are surrounded by areas of landscaping, a northerly garden, footpaths and driveways.

There are existing trees and a hedgerow that line the property boundaries along St John's Wood Park and Avenue Road. The southern boundary is a brickwork wall that forms the enclosure to the existing garage.

The site has both pedestrian and vehicular access off Avenue Road & St John's Wood Park. The main entrance into the building is from Avenue Road, with stepped and ramp access into the main entrance hall.

There is a secondary pedestrian access on the western elevation that leads into the common stairwell between the ground and lower ground floor levels.

The garage block occurs on the southern edge of the site with individual enclosed garages that are accessed from the existing cross over off St John's Wood Park. There is a disused secondary garage access from the forecourt off Avenue Road.

### KEY:

Application Site      ● ● ● ● ● ● ● ●

Grade II Listed Buildings      ●

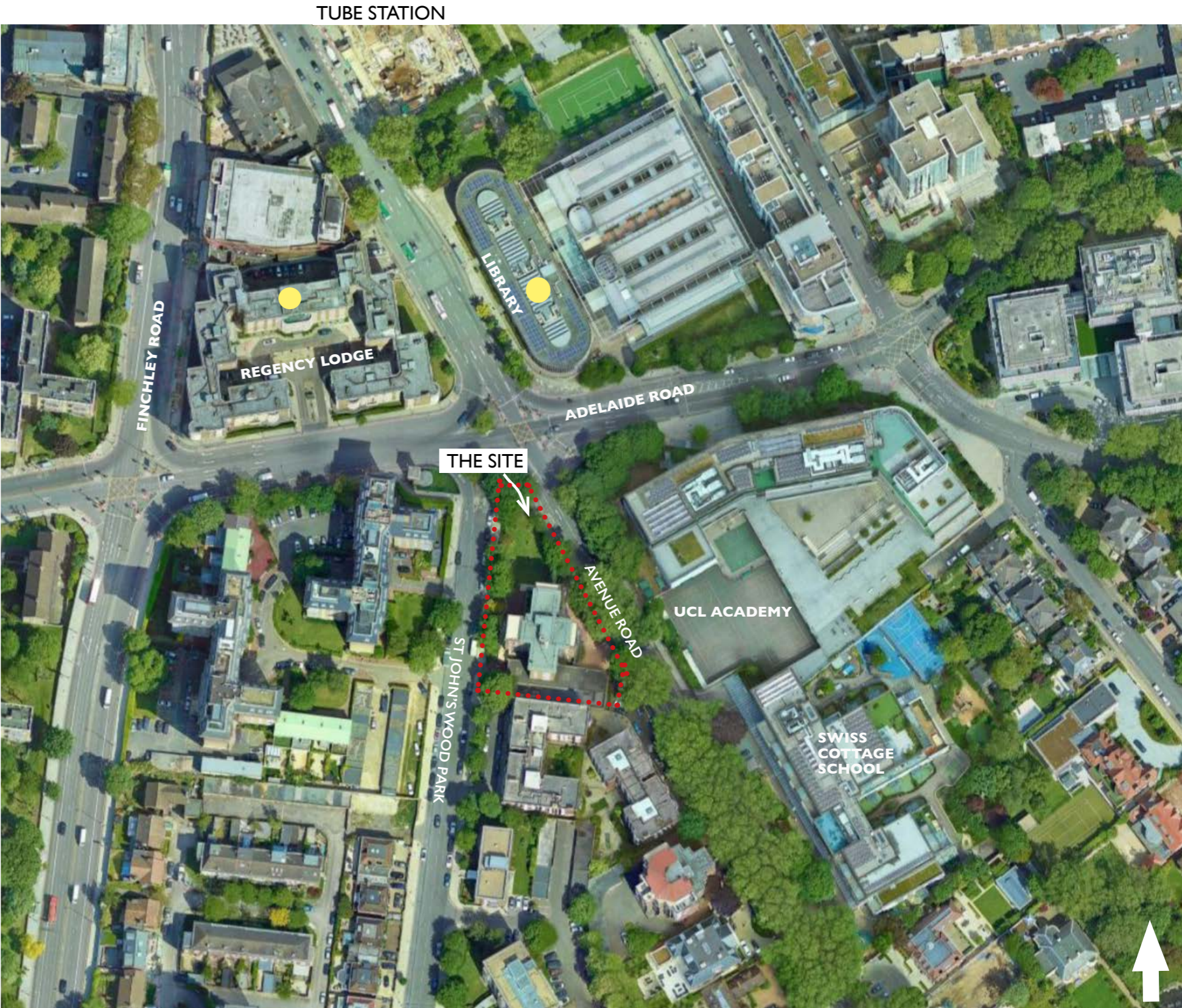


Fig4. Contextual overview of the main roads surrounding the site, the surrounding buildings and the listed buildings nearby.

# 2.0 THE SITE - CONTEXT

27/02/2023

## 2.2 CAMDEN POLICIES MAP CONTEXT

As illustrated in the extract of the Camden Policies Map 2021 August v3 at Fig 5.

- The site is not within a Conservation Area.
- There are no designated views across the site.

### KEY

The Site

Built environment

Conservation Area

Ancient Monument

Archaeological Priority Area

Designated Views:

Viewing Corridor

Lateral Assessment Area

Background Assessment Area

Natural environment and open space

Open Space

Metropolitan Open Land

Site of Special Scientific Interest

Ancient Woodland

Metropolitan Walk

Habitat Corridor, missing link

Regent's Canal

Sites of Importance for Nature Conservation:

Local

Borough Grade 1

Borough Grade 2

Metropolitan

Local Green Space

This is a detailed map extract from the Camden Policies Map 2021 August v3. It shows a section of North London, including areas like West Hampstead, South Hampstead, Kilburn, St John's Wood, Maida Vale, and Primrose Hill. A red dot marks the 'The Site' location in the South Hampstead area. The map is overlaid with various colored and patterned boundaries representing different planning designations. Yellow areas indicate Conservation Areas, including West End Green, Fitzjohn's, Park Hill, Eton, and Elsworth. Green areas represent Open Space and Metropolitan Open Land. Blue lines show the Regent's Canal. Dashed lines indicate designated views. The map also shows numerous streets, parks like Regent's Park, and other landmarks. A north arrow is located in the bottom left corner of the map area.

Fig 5. Extract from Camden Policy Map 2021 August v3. The red dot indicates the site location.

5

HUB  
Architecture  
Interior Design









2.3 FLOOD MAP FOR PLANNING

The Environment Agency Flood Map as identified in Fig.6 indicates that the site is located in flood zone 1, an area with low probability of flooding.

The site is within Camden's Critical Drainage Area, referenced as 'Group3\_005' on the Critical Drainage Areas / Local Flood Risk Zones plan dated 04/06/2014.

For further details refer to the *Screening Study Flood Risk Assessment* prepared by **RIDA** which is submitted as part of our application.  
*Ref: 325 FA-001*

KEY

-  Selected point
-  Flood zone 3
-  Flood zone 3: areas benefitting from flood defences
-  Flood zone 2
-  Flood zone 1
-  Flood defence
-  Main river
-  Flood storage area

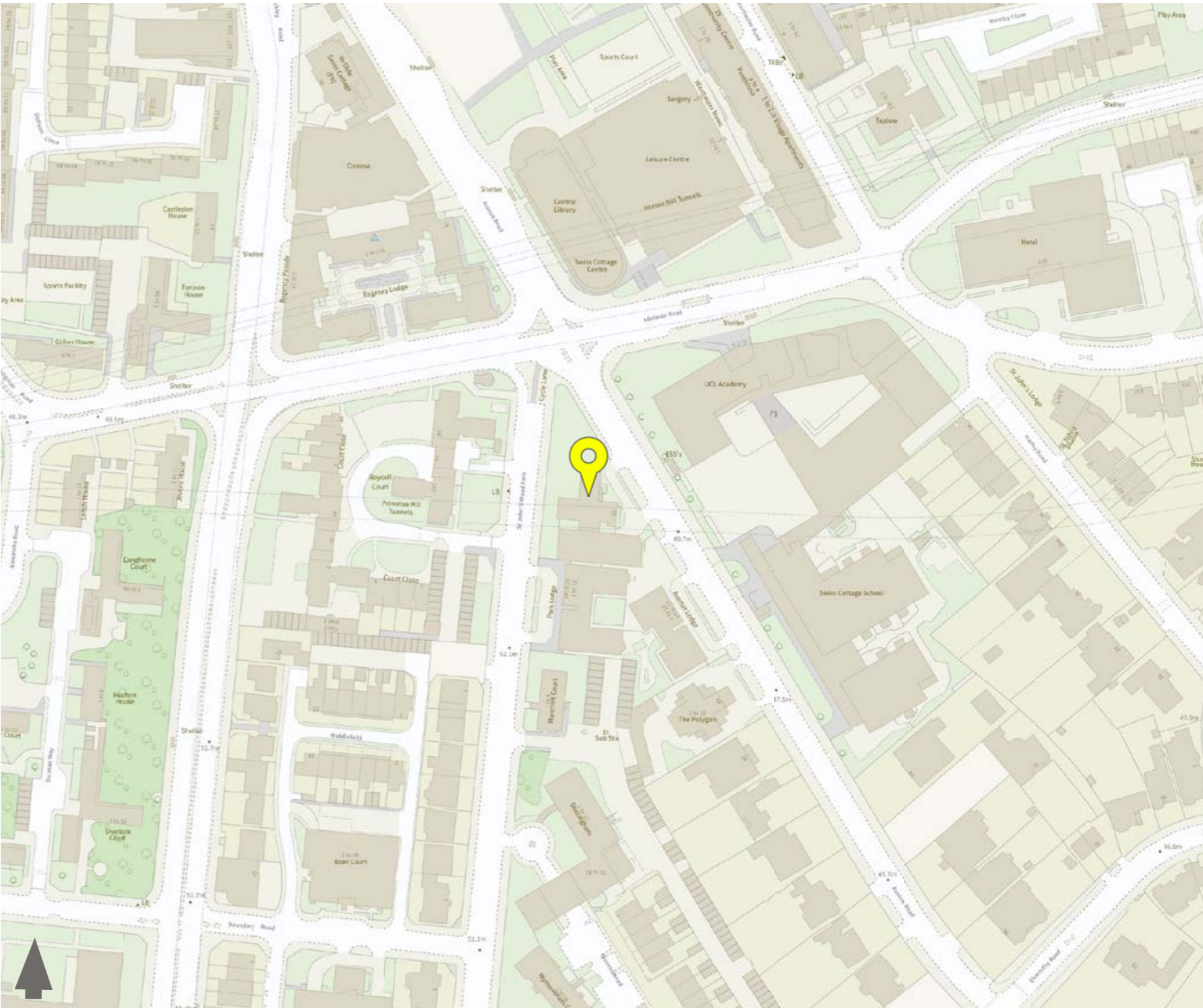


Fig 6. Extract from the Environment Agency Flood Map.

2.4 ACCESSIBILITY

In terms of public transport accessibility, the site is considered to be in a highly sustainable location being only approximately 0.2 miles (6-minute walk) to Swiss Cottage Tube Station (London Underground) which provides good connectivity to central London and 0.3 miles (7-minute walk) to South Hampstead Train Station (London Overground).

There are numerous local bus connections nearby all offering frequent services in and around the city.

As indicated on Fig.7 opposite, the site has a PTAL rating of 6A (second best) due to the proximity of the surrounding transportation links.

KEY

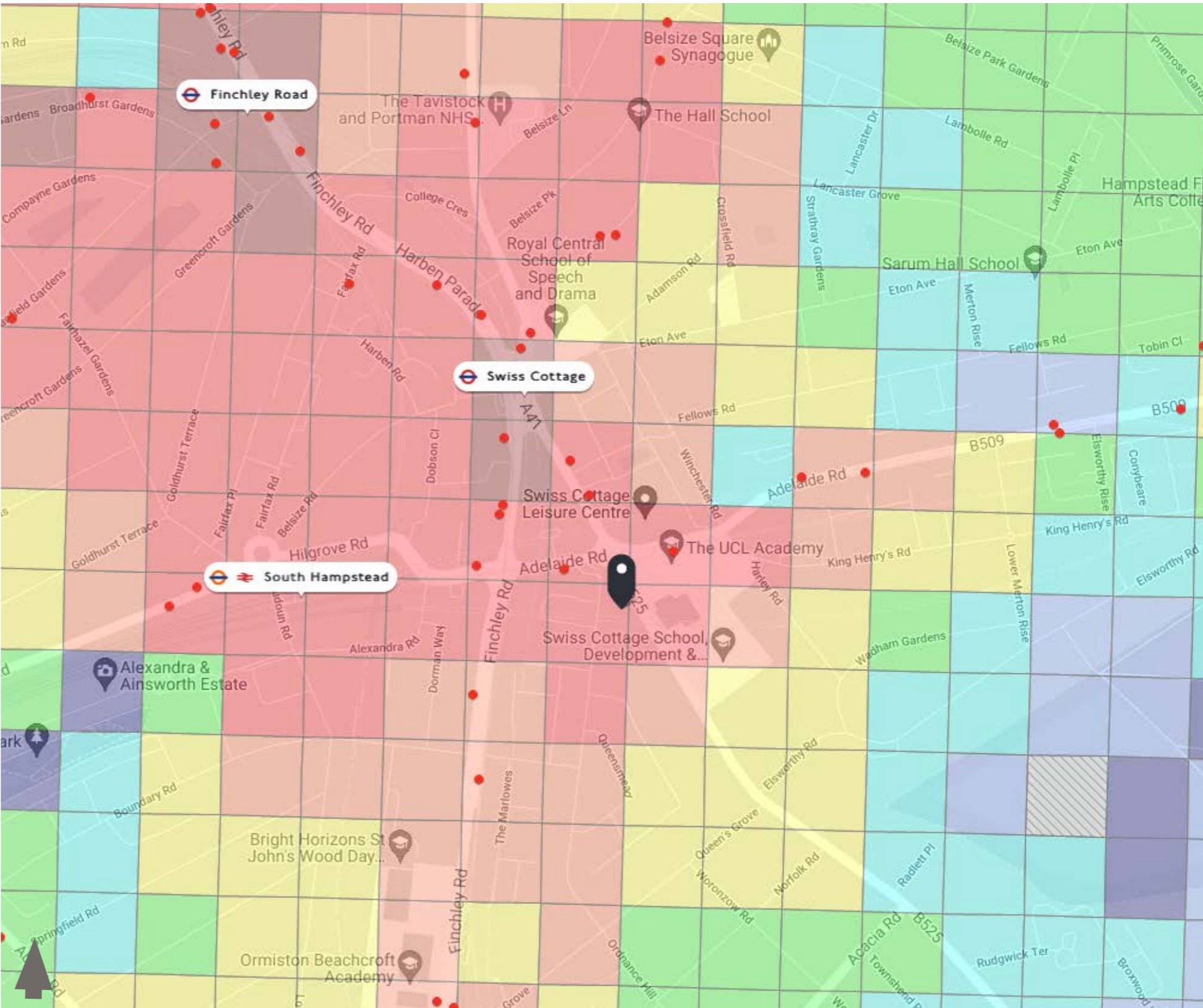
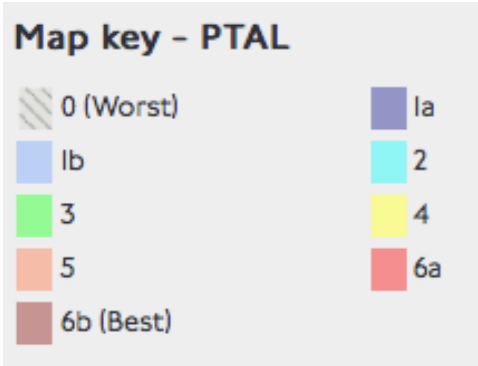


Fig 7.TFL for London PTAL Map.

## 3.0 SITE PHOTOGRAPHS

27/02/2023

### 3.1 SITE PHOTOGRAPHS The Site Surroundings



Fig 8. View of the site from Avenue Road, looking towards the northern elevation.



Fig 9. View of the site when approaching in a southerly direction along St John's Wood Park, with a partial view of the northern & western elevations.

## 3.0 SITE PHOTOGRAPHS

27/02/2023

### 3.1 SITE PHOTOGRAPHS The Site Surroundings



Fig 10. View of the western elevation with an access way off St John's Wood Park that leads to the rear entrance of the property.



Fig 11. View of the site when approaching in a northerly direction along St John's Wood Park. The neighbouring building Park Lodge is on the right.

## 3.0 SITE PHOTOGRAPHS

27/02/2023

### 3.2 SITE PHOTOGRAPHS The Building



Fig 12. View of the existing building fronting Avenue Road, with the main approach to the apartment block.  
The contrasting brickwork is noted on the various building wings.

- A - View of the eastern elevation with the garage on the left.
- B - The main entrance with a stepped and ramped approach.
- C - View from the main entrance looking south towards the garage.

## 3.0 SITE PHOTOGRAPHS

27/02/2023

### 3.2 SITE PHOTOGRAPHS The Building



Fig 13. View of the northerly and westerly elevations of the existing building. Contrasting brickwork is noted on the various building wings.

- A - Partial view of the garden on the northern portion of the site.
- B - Access way off St John's Wood Park that leads to the garage area.
- C - View from the garage roof looking towards the eastern and southern portion of the building.

## 3.0 SITE PHOTOGRAPHS

27/02/2023

### 3.3 SITE PHOTOGRAPHS The Building - Roof level



Fig 14. Roof level views of the existing services enclosure and lift overrun.

A - Partial view of eastern elevation of the existing services enclosure with the clerestory windows at low level that serve the flat below on the 7th floor.

B - South eastern elevation of the existing services enclosure, with the lift overrun projection.

C - Northern elevation of the existing services enclosure.

D - Existing access door onto the roof via the existing services enclosure.

### 3.3 SITE PHOTOGRAPHS The Building - Roof level



Fig 15. Existing roof level view - looking south-west.



Fig 16. Existing roof level view - looking north-west.



Fig 17. Existing roof level view - looking north-east.



Fig 18. Existing roof level view - looking north.



Fig 19. Existing roof level view - looking east.



Fig 20. Existing roof level view - looking north-west.

THE TOWN AND COUNTRY PLANNING (GENERAL PERMITTED DEVELOPMENT) (ENGLAND) ORDER 2015 (AS AMENDED)

Part 20, Class A - New dwelling houses on detached blocks of flats

Permitted development

A. Development consisting of works for the construction of up to two additional storeys of new dwelling houses immediately above the existing topmost residential storey on a building which is a purpose-built, detached block of flats, together with any or all—

- a) **engineering operations** reasonably necessary to construct the additional storeys and new dwelling houses;
- b) works for the replacement of **existing plant or installation of additional plant** on the roof of the extended building reasonably necessary to service the new dwelling houses;
- c) works for the construction of **appropriate and safe access to and egress from** the new and existing dwelling houses, including means of escape from fire, via additional external doors or external staircases;
- d) works for the construction of **storage, waste or other ancillary facilities** reasonably necessary to support the new dwelling houses.

ASSESSMENT OF THE PERMITTED DEVELOPMENT CRITERIA

Development not permitted if: (A.1)

a) The permission to use any building as a dwelling house has been granted only by virtue of Class M, MA, N, O, P, PA or Q of Part 3 of this Schedule;

The existing building is a purpose built apartment block with residential accommodation, no part of the dwelling house has been consented by virtue of Class M, N, O, P, PA or Q.

b) Above ground level, the building is less than 3 storeys in height;

The existing building is 8 storeys above ground level.

c) The building was constructed before 1st July 1948, or after 5th March 2018;

The block was constructed in the 1960's.

d) The additional storeys are constructed other than on the principal part of the building;

The additional storeys over the existing roof level will be constructed on the principal part of the building.

e) The floor to ceiling height of any additional storey, measured internally, would exceed the lower of— (i) 3 metres; or (ii) the floor to ceiling height, measured internally, of any storey of the principal part of the existing building;

The proposed floor to ceiling height measured internally will not exceed the lower of 3 metres or the floor to ceiling height measured internally of any storey of the principal part of the existing building.

f) The new dwelling houses are not flats;

The proposal is for 2 no. new flats that are located within the two additional storeys. There will be one flat per new floor.

g) The height of the highest part of the roof of the extended building would exceed the height of the highest part of the roof of the existing building by more than 7 metres (not including plant, in each case);

The additional storeys will not exceed 7 metres in height, measured from the existing roof level. Refer to the existing and proposed elevations/sections for details.

h) The height of the highest part of the roof of the extended building (not including plant) would be greater than 30 metres;

The height of the extended building will not exceed 30 metres in height. Refer to the existing and proposed elevations/sections for details.

i) Development under Class A.(a) would include the provision of visible support structures on or attached to the exterior of the building upon completion of the development;

The proposed structure for the additional storeys will not be visible externally. The structure has been designed so that it will be integrated within the building envelope. Refer to the Structural Engineer's Feasibility Report prepared by MBP.

Ref: MBP 8538 Structural Engineer's Feasibility Report Issue 2  
MBP 8538 Structural Drawings Appendix A 220512

j) Development under Class A.(a) would consist of engineering operations other than works within the existing curtilage of the building to— (i) strengthen existing walls; (ii) strengthen existing foundations; or (iii) install or replace water, drainage, electricity, gas or other services;

The existing building has been assessed by a structural engineer to establish that the existing building is capable of supporting the additional proposed floors. The structural design has been developed with the existing structure in mind to enable the transfer of the additional loads downwards through the existing structure in addition to consideration of other elements such as wind loads. Refer to the Structural Engineer's Feasibility Report prepared by MBP.

Ref: MBP 8538 Structural Engineer's Feasibility Report Issue 2  
MBP 8538 Structural Drawings Appendix A 220512

k) In the case of Class A.(b) development there is no existing plant on the building

N/A

l) In the case of Class A.(b) development the height of any replaced or additional plant as measured from the lowest surface of the new roof on the principal part of the extended building would exceed the height of any existing plant as measured from the lowest surface of the existing roof on the principal part of the existing building;

The new plant is no higher than the existing plant on the building.

m) Development under Class A.(c) would extend beyond the curtilage of the existing building

The development is within the curtilage.

n) Development under Class A.(d) would— (i) extend beyond the curtilage of the existing building; (ii) be situated on land forward of a wall forming the principal elevation of the existing building; or (iii) be situated on land forward of a wall fronting a highway and forming a side elevation of the existing building;

The proposals do not align with any of these points.

o) The land or site on which the building is located, is or forms part of— (i) article 2(3) land; (ii) a site of special scientific interest; (iii) a listed building or land within its curtilage; (iv) a scheduled monument or land within its curtilage; (v) a safety hazard area; (vi) a military explosives storage area; or (vii) land within 3 kilometres of the perimeter of an aerodrome.

The site has none of these designations.

## Conditions: (A.2)

Section A.2 of the GDPO sets out certain conditions that will be imposed on development under Class A.

Set out below are the main conditions which relate to the design along with relevant details for each item:

(1) Where any development under Class A is proposed, development is permitted subject to the condition that before beginning the development, the developer must apply to the local planning authority for prior approval of the authority as to—

a) Transport and highways impacts of the development;

Refer to the Transportation report for further details.

Report Ref: R01-SB-Transport Statement-F1 (220527)

The site is well connected to the surrounding public transport connections and benefits from a PTAL rating of 6A which is the second best.

Cycle spaces have been incorporated for the new flats. These are situated on the ground floor within the existing garage and will encompass 4 no. spaces via individual bike lockers. An allowance for 2 spaces per new dwelling.

There is no existing provision for secure external cycle storage on the site.

b) Air traffic and defence asset impacts of the development;

Not required.

c) Contamination risks in relation to the building;

Refer to the Desktop Contamination Report prepared by GeoSmart.

Report Ref: 7645 IRI Geosmart Report 95 Avenue Road

d) Flooding risks in relation to the building;

For further details refer to the Screening Study Flood Risk Assessment prepared by RIDA which is submitted as part of our application. Report Ref: 325 FA-001

The site falls within a critical drainage area as defined on the Camden Critical Drainage Areas / Flood Risk Zones Plan dated 04/06/2014.

The incorporation of green/brown roofs will reduce surface water run off, which will be an improvement from the current felt roof.

e) The external appearance of the building;

Refer to the design response under the section covering appearance & materiality on page 19.

f) The provision of adequate natural light in all habitable rooms of the new dwelling houses;

Refer to the Daylight Consultant's report for further details.

Report Ref: 95 Avenue Road Daylight Sunlight Report June 2022

g) Impact on the amenity of the existing building and neighbouring premises including overlooking, privacy and the loss of light;

The two storey rooftop extension has been designed to ensure the proposal does not impact on the amenity of the existing building and of the nearby neighbouring properties.

The proposed extension aligns with the character and style defined by the existing building. The proposed rooftop extension incorporates purposeful breathing spaces where the proposed external envelope is set back and inset from the existing building edges, particularly on the southern facade where there is an existing neighbouring building / Park Lodge.

h) Whether because of the siting of the building, the development will impact on a protected view identified in the Directions Relating to Protected Vistas dated 15 March 2012 issued by the Secretary of State, and

The site does not fall within the lines of any protected views or vistas. Refer to the Camden Policies Map 2021 August v3 at Fig 5. on page 5.

i) Where the existing building is 18 metres or more in height, the fire safety of the external wall construction of the existing building; and (j) where the development meets the fire risk condition, the fire safety impacts on the intended occupants of the building, and the provisions of paragraph B (prior approval) of this Part apply in relation to that application.

Refer to the Fire Consultant's Fire Safety Strategy Report for further details.

Report Ref: FSS 23128 01(1.0)

Fire\_statement\_form\_-\_95 Avenue Road

5.1 THE EXISTING BUILDING

The Structure and Facade

The existing building is a concrete framed cross shaped structure that is 8 storeys above ground with a dual toned brickwork outer skin, with casement windows, with double glazed doors leading to cantilevered balconies with obscured glazing for the upper floor flats. The felt finished flat roof has projecting eaves set over the existing 7th floor. The 7th floor flat has inset terraces on the east, north and western elevations.

The proposed footprint of the new added floors will follow the primary facade lines where the floor plates are not inset.

Refer to the Structural Engineer’s Feasibility Report prepared by MBP.  
*Ref: MBP 8538 Structural Engineer’s Feasibility Report*

The existing plant area situated over the existing roof will be dismantled and removed. The water tanks within will be relocated.

The lift shaft will be retained and extended upwards.

The existing east facing clerestory windows that occur at the base of the existing plant room enclosure over the roof level will be retained. These are associated to the flat on the 7th floor below and as a result our proposed footprint does not extend into this eastern portion of the existing roof.

5.2 THE EXISTING BUILDING IMPROVEMENTS & ANCILLARY FACILITIES

The 2no. new proposed dwellings will utilise the existing waste/recycling storage and disposal facilities that exist on the site.

Current facilities:  
The building waste and refuse is managed by the live-in caretaker.

There is a waste storage area that is on the north western corner of the ground floor level with current capacity for 2no Euro Style Bins.

Recycling Waste Storage is within a purpose built brickwork enclosure in the garden area on the northern elevation. With current capacity for 2no Euro Style Bins.

Car Parking.

The proposal for the new added dwellings will be car free in-line with Camden’s car-free policy for all new developments. No changes are proposed to the existing parking arrangements.

Cycle Store Facilities

The existing building does not include a dedicated cycle storage area for the existing flats.

Our proposals allow for the addition of 4no. cycle storage spaces, within bicycle lockers. These will be located within the existing garage area with access via the entrance off St John’s Wood Park.

In accordance with the cycle parking standards set out within the London Plan 2021 for C3 residential development our proposal will provide a minimum total of 4no. cycle storage spaces, which is 2 spaces for each new flat.

Existing Lift

The existing lift will be replaced with a new machine room less lift that will serve all the existing floors and the two new added floors over the main roof level. The existing lift shaft will be retained. A new lift overrun will be situated over the new rooftop extension flat roof at the head of the extended lift shaft.

Plant & Services

New roof top plant for the 2no. new apartments will be situated over the eastern portion of the existing roof that is over the 7th floor and will be contained behind a screened acoustic enclosure.

The proposed drainage will be connected to the existing drainage system and the existing outlet terminals on the existing roof will be adapted to suit the proposed layout configuration. The proposed design incorporates a void between the existing roof level over the 7th floor and the new structure for the proposed 8th floor to facilitate the passage of services.

The existing cold water tanks that occur within the existing brickwork plant room that is situated over the existing roof will be removed. New tanks will be located within the lower ground floor level of the existing building.

New heating, cooling and electrical supplies will be provided for the new apartments.

5.3 HERITAGE

The roof top extension proposal is some distance from any affected heritage asset, and because the extension is in design terms similar to the existing building, the proposed addition sits comfortably in its setting and will not have any significant impact in this regard.

Refer to fig.4 on page 4, which indicates that the two listed buildings are situated to the north of the proposal site, on the other side of the main arterial roadway – Adelaide Road.

5.4 THE TWO STOREY EXTENSION - ADDITIONAL FLATS

Amount

The proposal is for the creation of 2 additional dwellings at the application site which currently comprises of 29 self-contained units. The new dwellings will occupy the new 8th and 9th floors which will be situated on top of the existing building.

Layout

Refer to pages 21-24 for the proposed floor plan layouts for each apartment.

API GIA = 173 m²	3 Bedroom 6 Person	8th Floor Level
AP2 GIA = 140 m²	3 Bedroom 6 Person	9th Floor Level

Access to the proposed new roof extension will be via the existing common stairwell core that is accessed off the main entrance lobby on the ground floor. The common stairwell core, staircases and the lift core will all be extended upwards to enable access to and from the new flats.

The proposed flats will provide self-contained accommodation which is in-keeping with the other units in the existing building. The new flats will include south facing private terraces, allowance for natural daylight and cross ventilation.

The proposed development will provide apartments that are dual aspect and which provide good quality accommodation that surpasses minimum space standards.

The proposed built envelope follows the existing external wall line of the 7th floor below for coherence of the newly proposed structure with the existing structure below. Some areas are purposefully designed to be inset to maintain privacy and limit overlooking.

Massing & Scale

The proposed mass and scale of the proposed extension is proportionate to the existing building and complies with the conditions set out in the GPDO Part 20, Class A - New dwelling houses on detached blocks of flats.

In addition, the extent of the proposed built mass also takes into account the daylight, sunlight, overshadowing impacts.

Refer to the submitted Daylight report for further details.



Fig 21. The existing Avenue Road elevation.

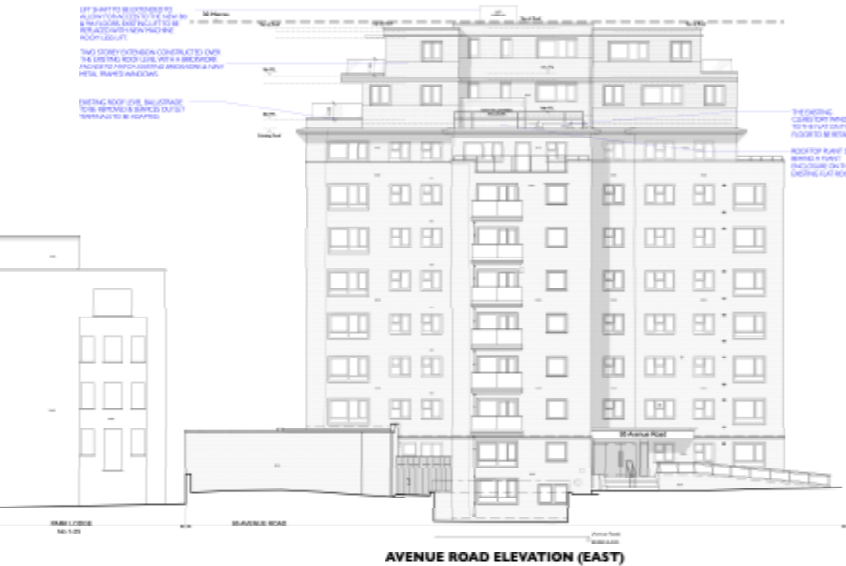


Fig 22. The proposed Avenue Road elevation.

Privacy & Overlooking

The proposed fenestration for the new 8th and 9th floors in most cases / where applicable replicates the formation, proportions and positions of the those within the existing facade below. There are some instances where this differs due to the design of the internal apartment layouts.

The new windows typically sit within the same vertical plane of the existing windows below or are set-back where the proposed floor plate is inset.

Amenity

The proposed layouts for the new added floors have been designed to allow for the incorporation of private outdoor amenity space for each new dwelling.

The existing site also has a communal garden on the northern portion of the site and Primrose Hill is a short walk from the site enabling easy access to open green space.

Sustainability

Green or brown sedum roof finishes are proposed for the new flat roof that forms the enclosure over the proposed rooftop extension. The retained areas of the existing roof over the 7th floor will be adapted to enable a green or brown roof finish. This will enhance the biodiversity of the site.

The incorporation of green/brown roofs will reduce surface water run off, which will be an improvement from the current felt roof.

5.5 ACCESS

Existing Access

No changes are proposed to the pedestrian or vehicular access into and around the existing property from the street level.

The existing circulation for the existing apartments and other ancillary areas around the building will remain unchanged.

Common Stairwell

The existing common stairwell will be extended upwards to allow for new staircase flights to be inserted to serve the proposed 8th and 9th floor levels.

The proposed staircases will sit directly above the existing common stairwell forming a continuation of the existing circulation core.

Existing Lift Extension

The lift shaft will be extended upwards to allow for lift access to the additional proposed floors. New lift lobbies will be added on the 8th and 9th floors to facilitate access into each new flat.

5.6 APPEARANCE & MATERIALITY

Proposed Facade, Appearance

The rooftop extension will match the style and appearance of the existing building to maintain coherence with the existing architectural language.

Proposed Materials

The proposed materials will match the existing building. The facade will comprise of brickwork to match the brickwork on the existing building. New windows will be steel and will match the appearance of those on the existing building.

Green / Brown sedum roof is proposed for the new flat roof over the rooftop extension, together with replacement of the existing felt roof finish over the 7th floor with a green/ brown sedum roof finish.

Proposed Structure

Refer to the Structural Engineer’s Feasibility Report prepared by MBP.  
*Ref: MBP 8538 Structural Engineer’s Feasibility Report*



Fig 23. The existing elevation with the dual toned brickwork skin.



Fig 24. Proposed bricks to match the bricks on the existing building.

5.7 AMENITY

General Amenity.

In evolving the design, we have been mindful of the need to maintain appropriate levels of amenity for our neighbours. The existing building is a ‘point-block’, located centrally within the site which means that it is naturally separated from the neighbouring properties. Consequently, it is possible to extend the existing building without encroaching in any significant way.

Sunlight and Daylight.

Refer to the separate detailed sunlight and daylight report by Avison Young which shows no significant negative impact on the surrounding neighbours. Report Ref: 95 Avenue Road Daylight Sunlight Report June 2022.

Overlooking & Privacy.

Once again, the central location of the existing block, and the fact the only two additional units are proposed means that the potential for further overlooking or loss of privacy is very limited, particularly given the distance to the boundary and the open spaces gardens and windows of neighbouring occupiers. The proposed extension will typically have windows in the same location as the existing floors below, at either the same vertical plane or are intentionally designed to be set back from the primary façade line.

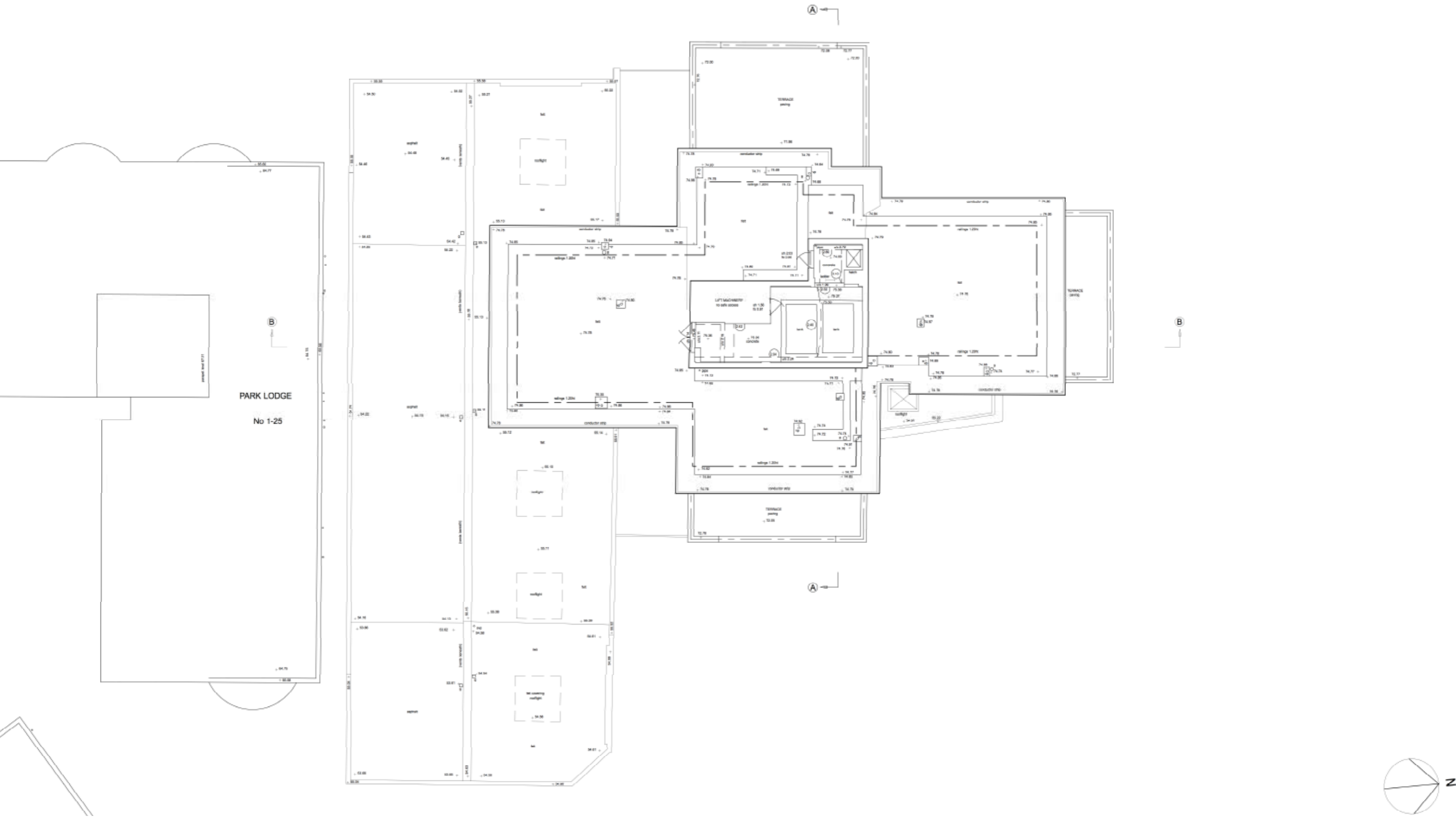
Sense of enclosure

Once more the distances between buildings are already very generous by London standards. The limited amount of additional development, concentrated on the central part of the existing roof means that the potential for an increase in sense of enclosure is necessarily inherently very limited. Our view is that this change will have no material effect in this regard.

Noise.

The proposed extensions are set over the existing centrally located apartment building which is well separated from the neighbouring buildings as a consequence of the urban layout and building forms surrounding the site. The existing apartment building already includes terraces and balconies, therefore the proposals are not dissimilar to the current arrangement, resulting in a modest change to the current conditions, which is unlikely to have any real impact on neighbouring occupiers given the distances involved.

5.8 EXISTING ROOF PLAN



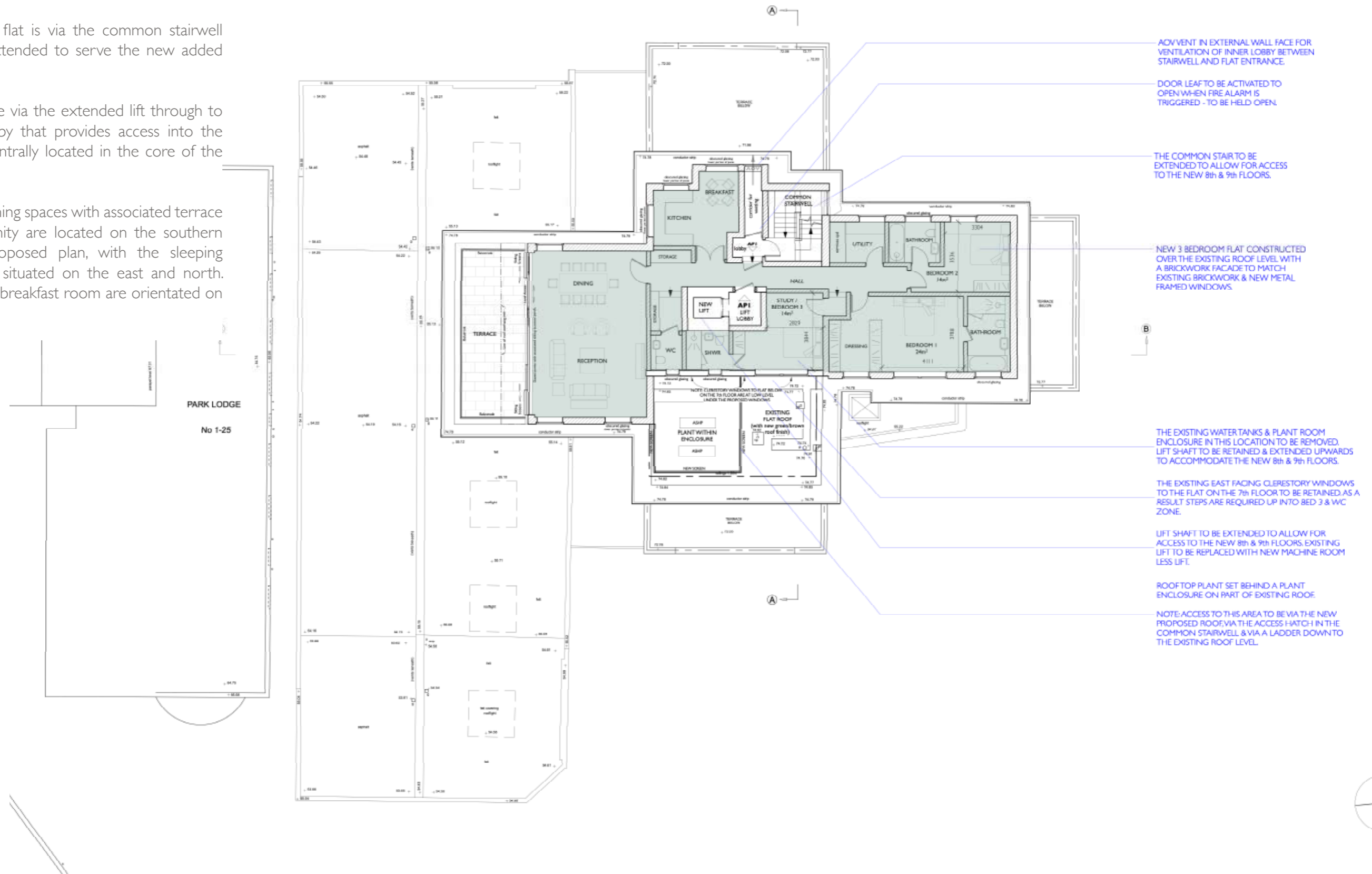
### 5.9A PROPOSED PLAN - 8th Floor

The proposed 8th floor plan shown in context with the existing building and nearby neighbouring buildings.

Access into the flat is via the common stairwell which will be extended to serve the new added floors.

Lift access will be via the extended lift through to the new lift lobby that provides access into the flat. The lift is centrally located in the core of the building.

The living and dining spaces with associated terrace for private amenity are located on the southern part of the proposed plan, with the sleeping accommodation situated on the east and north. The kitchen and breakfast room are orientated on the west.



 Apartment I  
 New Walls  
 New Terrace



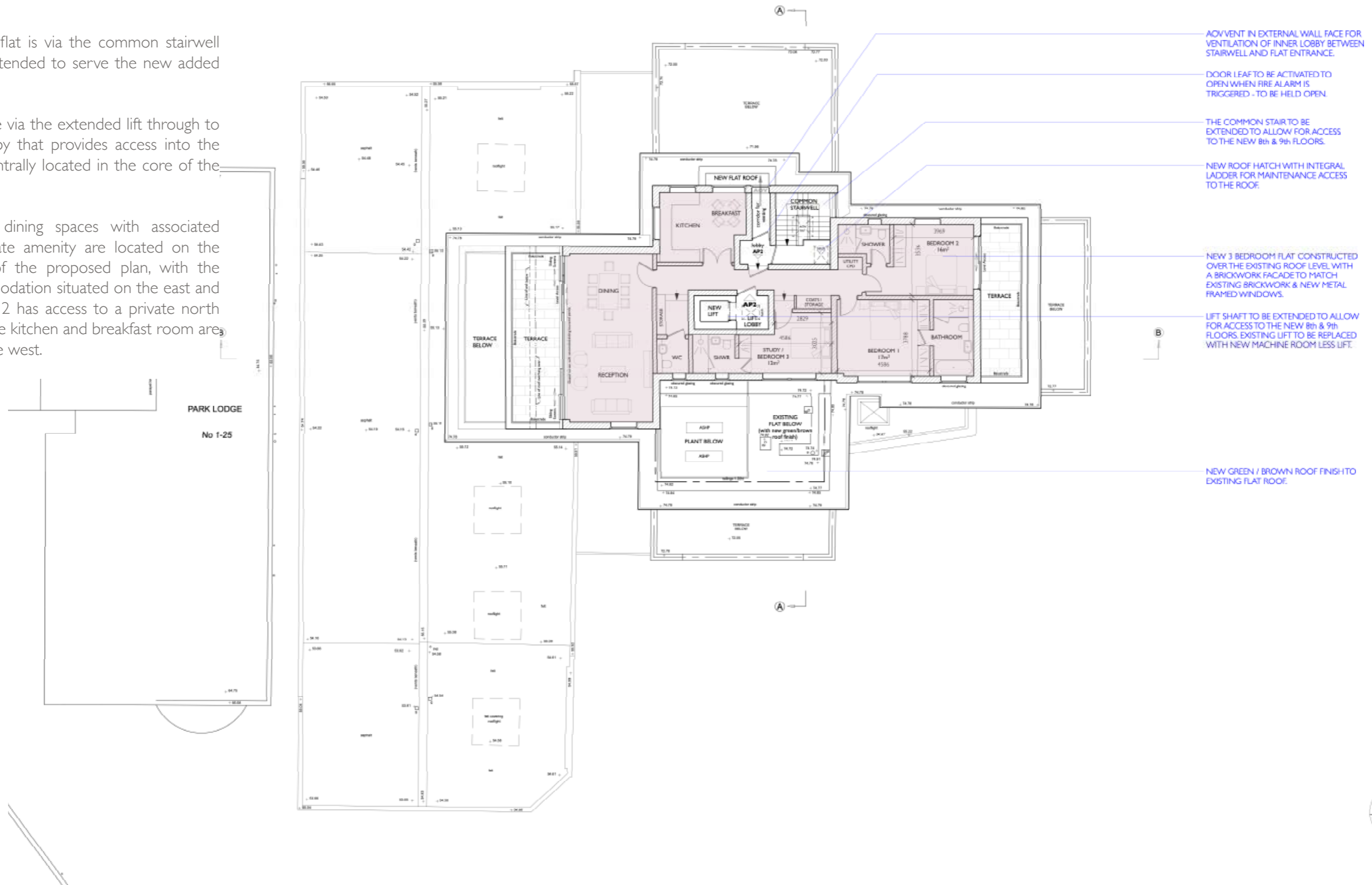
## 5.10A PROPOSED PLAN - 9th Floor.

The proposed 9th floor plan shown in context with the existing building and nearby neighbouring buildings.

Access into the flat is via the common stairwell which will be extended to serve the new added floors.

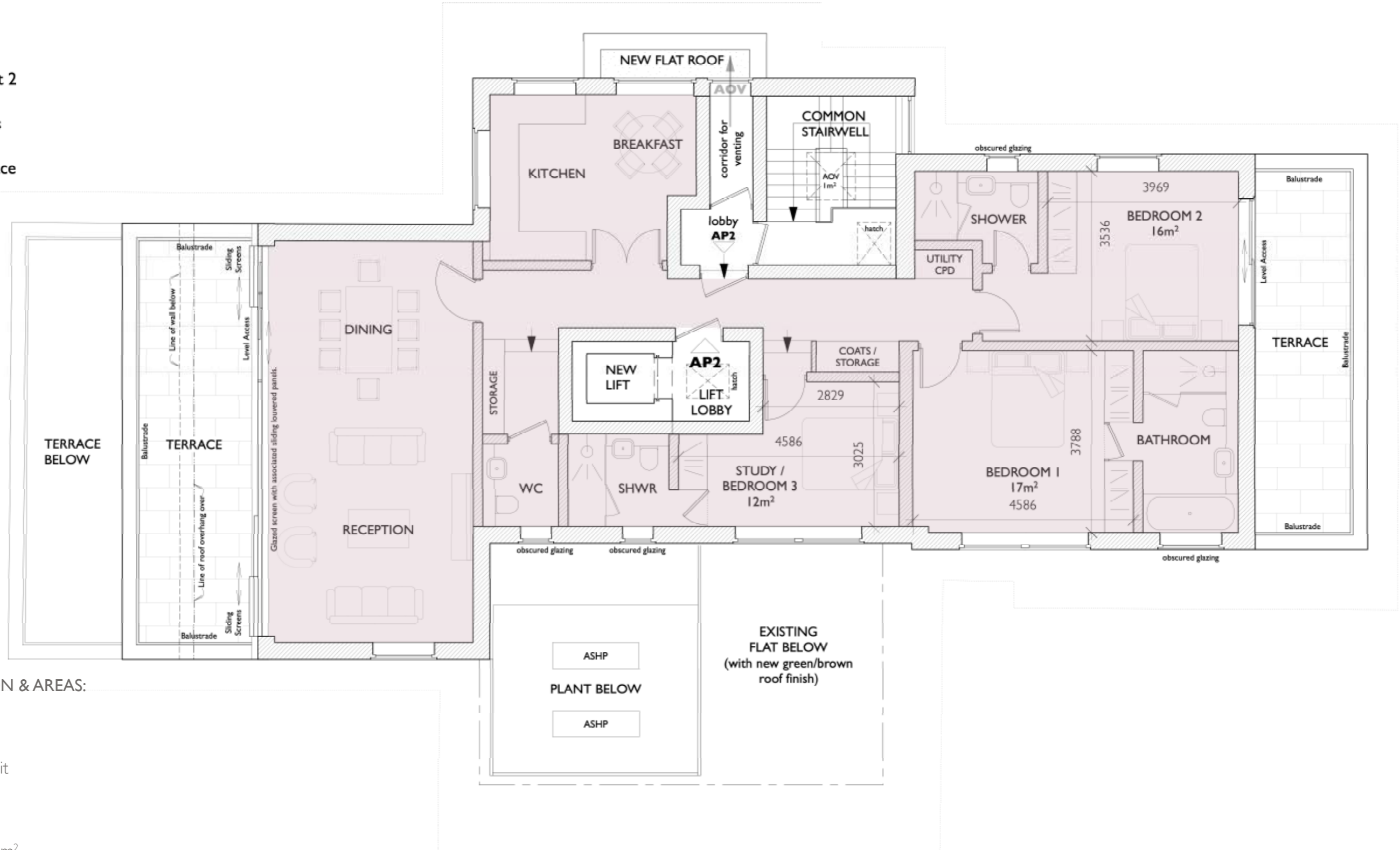
Lift access will be via the extended lift through to the new lift lobby that provides access into the flat. The lift is centrally located in the core of the building.

The living and dining spaces with associated terrace for private amenity are located on the southern part of the proposed plan, with the sleeping accommodation situated on the east and north. Bedroom 2 has access to a private north facing terrace. The kitchen and breakfast room are orientated on the west.



## 5.10B PROPOSED APARTMENT 2 LAYOUT - 9th Floor:

- Apartment 2
- New Walls
- New Terrace



### FLAT CONFIGURATION & AREAS:

TOTAL GIA: 140m²

3 Bedroom 6 Person Unit

Bedroom 1 = 17m²

Bedroom 2 = 16m²

Bedroom 3 / Study = 12m²

Storage Provision = 2.7m²

Private Outdoor Amenity = 35m²



**New Flat Roof**  
with green / brown roof finish



5.12 AVENUE ROAD ELEVATION - EXISTING & PROPOSED



EXISTING

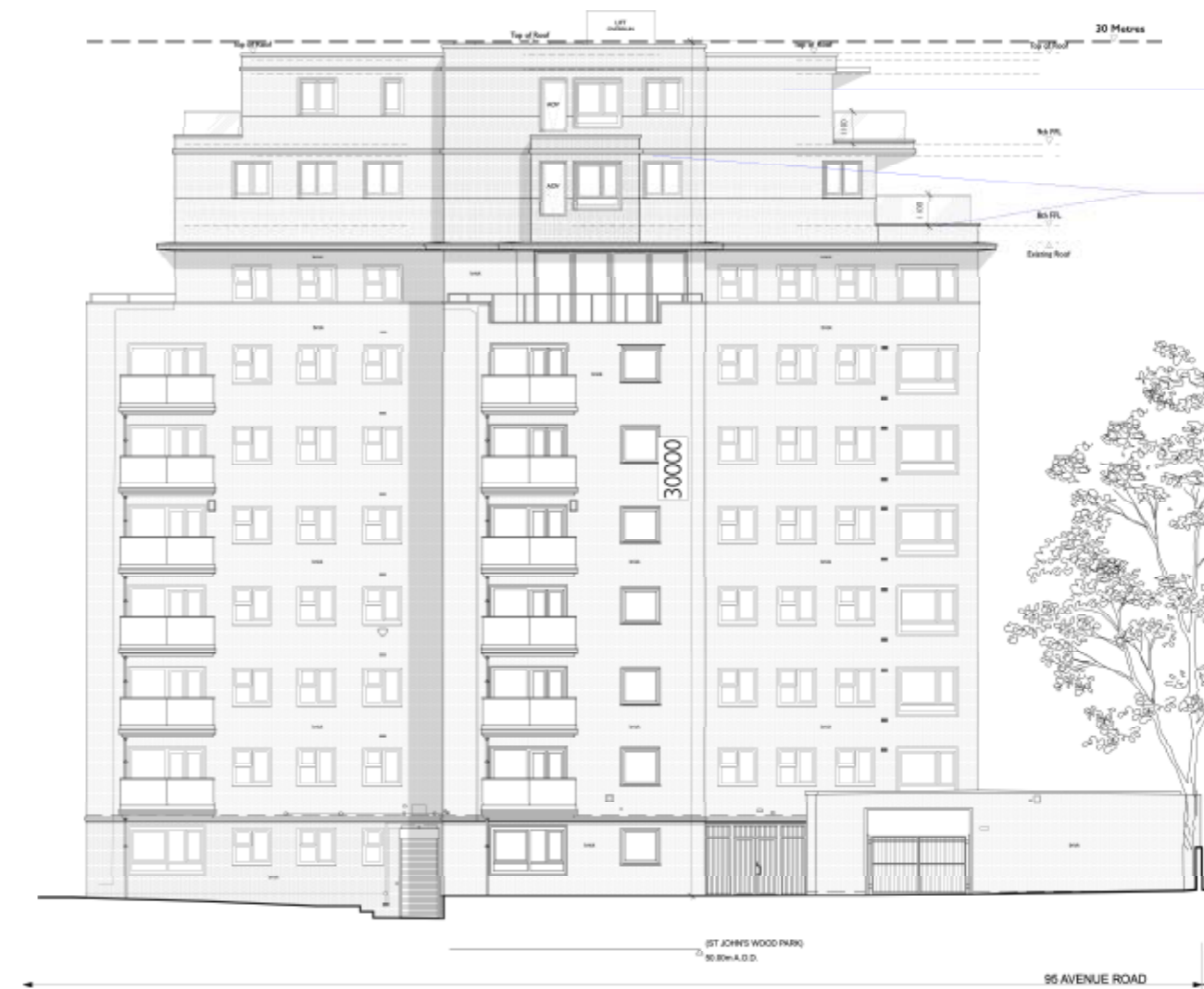


PROPOSED

5.13 ST JOHN’S WOOD PARK ELEVATION - EXISTING & PROPOSED



EXISTING

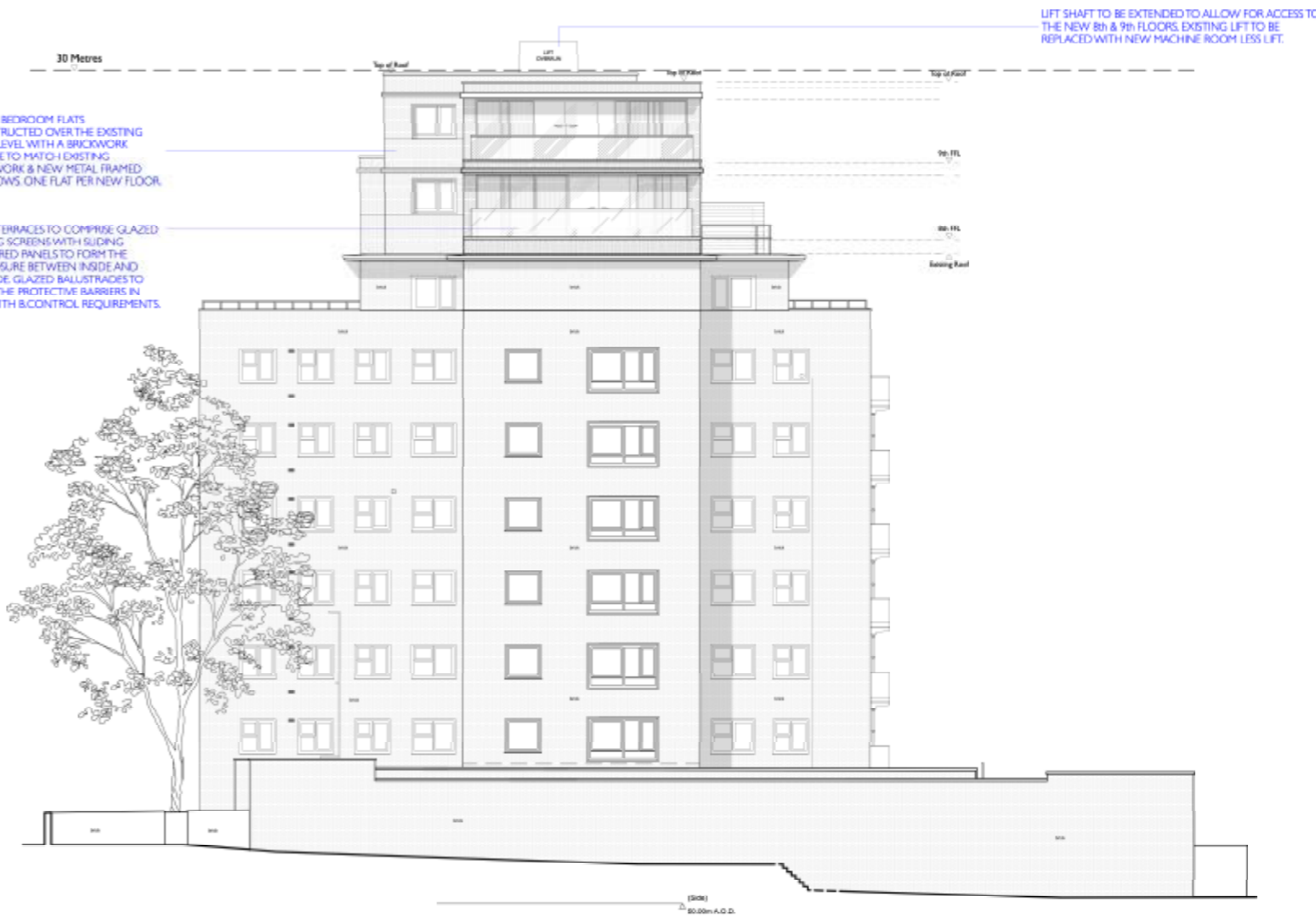


PROPOSED

5.14 SOUTH ELEVATION - EXISTING & PROPOSED

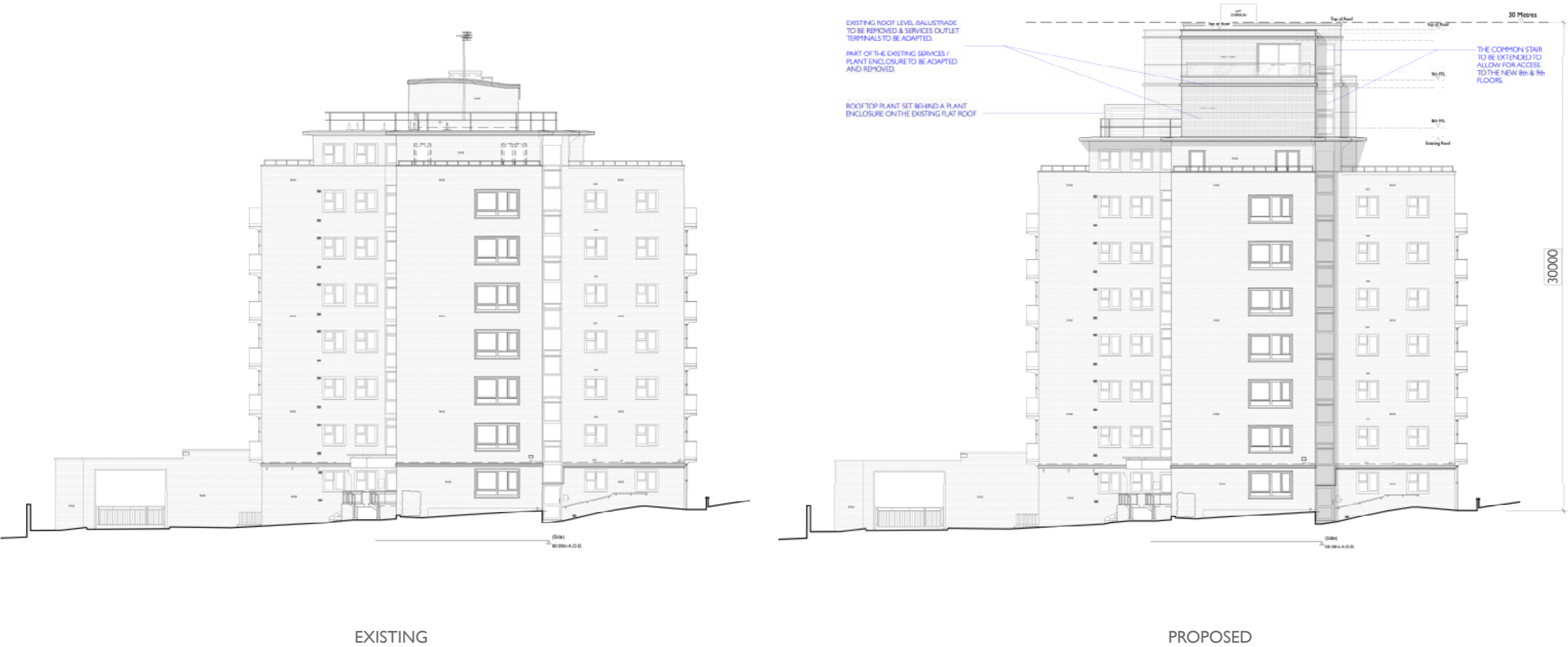


EXISTING

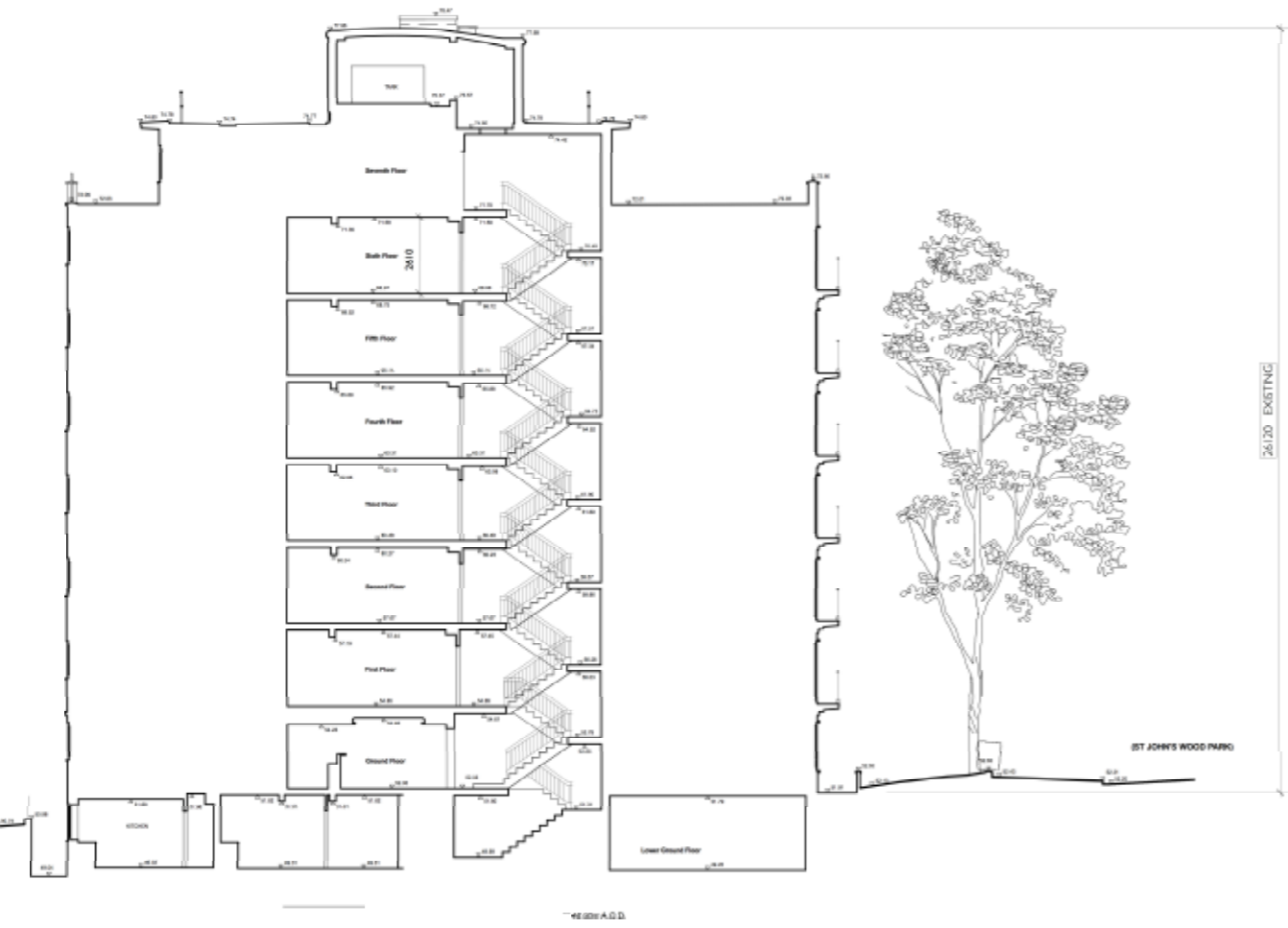


PROPOSED

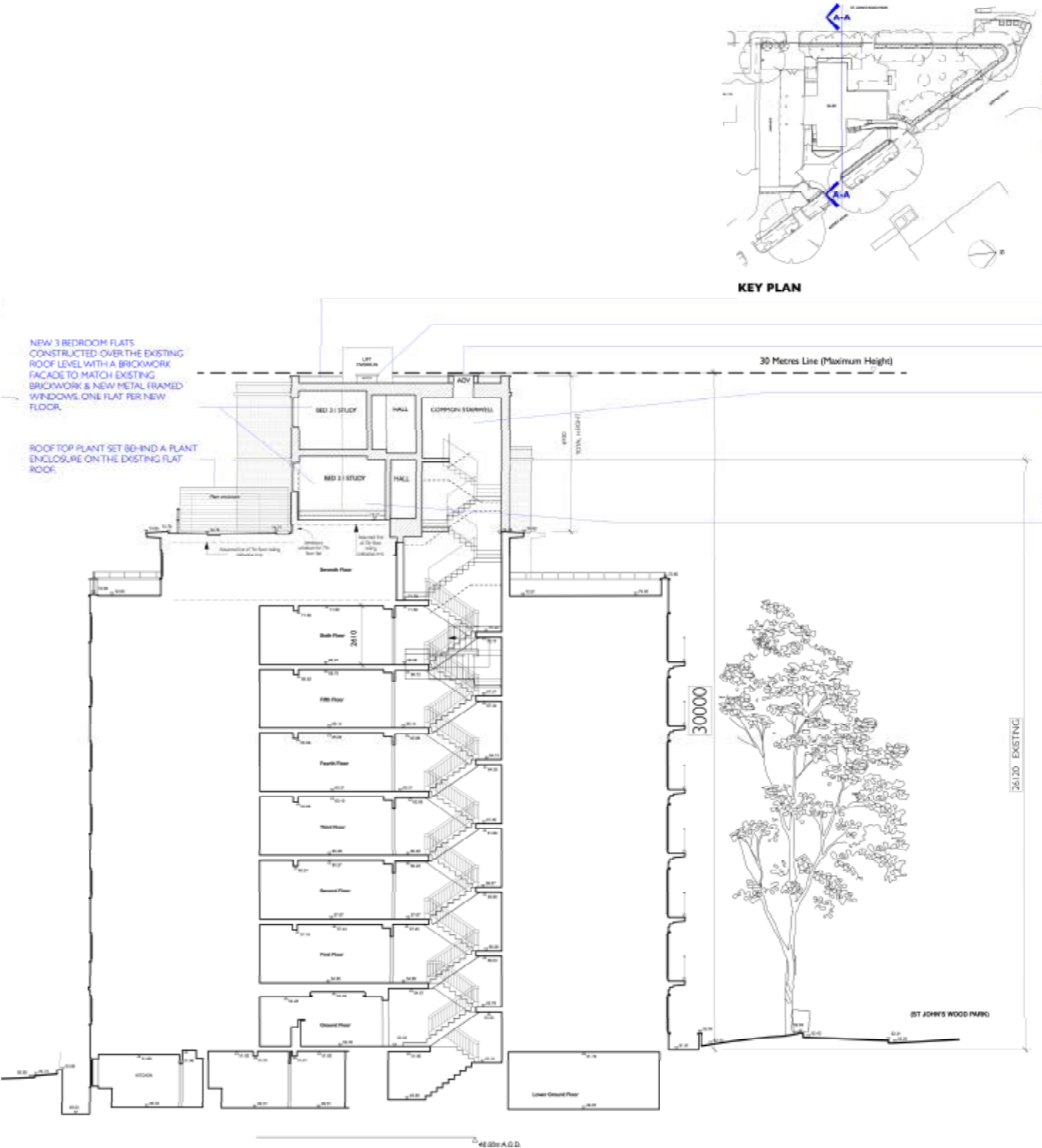
5.15 NORTH ELEVATION - EXISTING & PROPOSED



5.16 SECTION A-A - EXISTING & PROPOSED

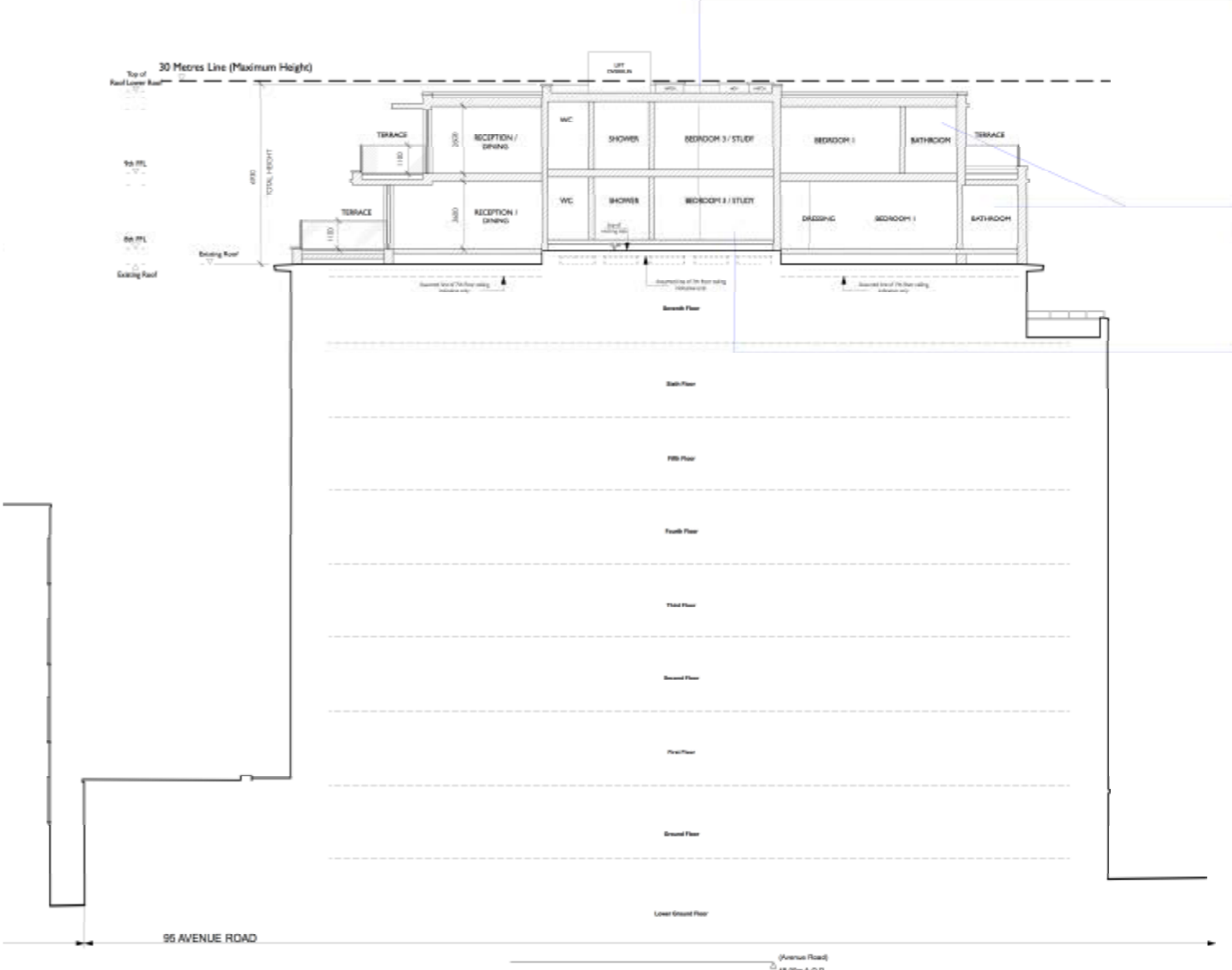
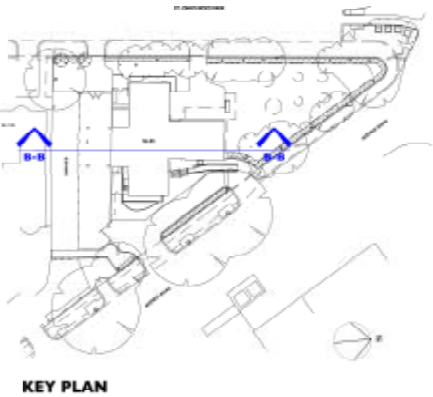


EXISTING



PROPOSED

5.17 SECTION B-B - EXISTING & PROPOSED



5.18 DEMOLITION PLAN - EXISTING ROOF

