

LOCAL TRAFFIC MANAGEMENT PLAN – LONDON BOROUGH OF CAMDEN

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Work Package No. n/a

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C01	Adrian B.	H. Thomas	M. Woodhouse	n/a	10/05/21	First Issue
C02	Velimir Micic	Vicky Ward	Clive Colman	n/a	28/06/22	Amalgamated Camdenwide LTMP with HS2 and LBC/TFL comments following consultation
C03	Velimir Micic	Tamsin Dale	Clive Colman	n/a	16/09/22	Periodic Update
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STAKEHOLDER REVIEW REQUIRED (SRR)

- ☒ COUNTY/DISTRICT/LONDON BOROUGH COUNCIL
- ☐ LOV
- ☐ LUL
- ☒ NRL
- ☒ TFL
- ☐ UTILITIES COMPANY
- ☐ OTHER

PURPOSE OF SRR

- ☒ ACCEPTANCE
- ☐ APPROVAL
- ☐ NO OBJECTION
- ☐ CONSENT

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Abbreviations and Definitions

Busy periods	The average daily forecast HGV trips (2-way) that exceed 50% of the 'Peak Month' of construction activity.
CLOCS	Construction Logistics and Community Safety
CoCP	Code of Construction Practice
Construction site	Same definition for 'Worksite' below
CSjv	Costain Skanska Joint Venture
CTWG	Community Traffic Working Group
ECRG	Euston Community Residents Group
EEW	Euston Enabling Works (in relation to Network Rail On Network Works)
EMR	Environmental Minimum Requirements – which sets out how the project will be delivered, including the Environmental Statement, the Code of Construction Practice, the Undertakings and Assurances and the Route-wide Traffic Management Plan.
ES	High Speed 2 Environment Statement
EWC	Early Works Contractor
FORS	Fleet Operator Recognition Scheme
GPCN	General Principle Compliance Note
Haul Road	A haul road is internal to the construction site, which may run alongside the trace of the HS2 railway, or which will be used by construction traffic to move from a construction access point to a remote welfare facility or worksite.
Haul road crossing	Where there is a crossing point for pedestrians or other workers
HS2	High Speed 2 Phase 1
HGV	Heavy Goods Vehicles over 7.5T
Key stakeholders	Highway authorities, emergency services and public transport operators
LBC	London Borough of Camden
LEMP	Local Environmental Management Plan
LGV	Large Goods Vehicles over 7.5T
LHA	Lorry Holding Area, also known as Vehicle Holding Area within some other HS2 documentation.
LOD	Limits of Deviation, used to show the limits within which the scheduled works, as listed in Schedule 1 of the High Speed Rail [London - West Midlands] Act 2017, may be constructed, to provide allowances for contingencies, working spaces and similar factors.
Lorry routes	The routes to be used by large goods vehicles agreed in accordance with Paragraph 6 of Schedule 17 to the High Speed Rail [London – West Midlands] Act 2017.
LTMP	Local Traffic Management Plan
LUL	London Underground Limited
MDjv	Mace Dragados joint venture
MWCC	Main Works Civil Contractor
MWSC	Main Works Station Contractor
NRSAWA	New Roads and Street Works Act 1991
ONW	Network Rail – On Network Works
Peak Month	The average daily forecast of LGV trips (two-way) during the peak month of construction activity.
Peak AM	08:00 – 09:00

Peak PM	17:00 – 18:00
PFN	Planning Forum Note
PROW	Public Right of Way
ROMIS	Route Management, Improvement and Safety Plan
RTMP	Route-wide Traffic Management Plan
Schedule 4	Schedule 4 of the High Speed Rail [London - West Midlands] Act 2017
Schedule 17	Schedule 17 of the High Speed Rail [London - West Midlands] Act 2017
Schedule 33 Part 1	Schedule 33 of the High Speed Rail [London - West Midlands] Act 2017
SCS	Skanska, Costain, Strabag Joint Venture
SRN	Strategic Road Network - roads which are Motorways and Trunk Roads managed by the Highways England.
Stakeholders	Local Authorities, community/business/user groups and or the local community, including residents and businesses.
TAS	Tunnel Access Shaft
TfL	Transport for London
TLG	Traffic Liaison Group
TLRN	Transport for London Road Network
TMA	Traffic Management Act 2004
TRO	Traffic Regulations Order
U&A's	Undertakings and Assurances
VMS	Vehicle Management System
WAB	Western Accommodation Building
Worksite	An area wholly under the control and management of the Principal Contractor and may include a part (or whole width) of a highway.

1 Transport Context

1.1 HS2 Phase One – London to West Midlands

- 1.1.1 HS2 will connect 8 out of 10 of the UK's major cities, providing them with a reliable, resilient, and high-capacity train service (London, Birmingham, Manchester, Sheffield, Leeds, Liverpool, Glasgow, and Edinburgh).
- 1.1.2 HS2 will also connect with other key transport interchanges. In addition, it will provide easy access to our motorway network at several stations. All contractors are working with regional transport authorities to integrate their transport plans with HS2. The new HS2 will reduce journey times between London and the northern cities, but also between the northern cities themselves (Birmingham, East Midlands, Sheffield and Leeds will each be connected by journey times of around 20 minutes) and between Scottish and English cities.
- 1.1.3 HS2 The High Speed Two (HS2) Phase One Environmental Statement (ES) was published in November 2013. It has been supplemented by a number of additional volumes as further information has become available, and in light of proposed changes to the project. It is the intention of the Secretary of State to carry out the project so that its impact is as assessed in the ES. The Secretary of State will require the nominated undertaker High Speed 2 Limited to adhere to the arrangements provided for in the Environmental Minimum Requirements (EMRs) in designing and constructing the Phase One of High Speed Two Works.
- 1.1.4 The nominated undertaker will in any event, and apart from the controls and obligations, use reasonable endeavours to adopt mitigation measures that will further reduce any adverse environmental impacts caused by Phase One of HS2, insofar as these mitigation measures do not add unreasonable costs to the project or unreasonable delays to the construction programme.
- 1.1.5 The Code of Construction Practice (CoCP) – Annex 1 of the EMRs – requires contractors to produce a Local Traffic Management Plan (LTMP) prior to works starting. This ensures contractors comply with the HS2 Act.
- 1.1.6 The Route Management, Improvement and Safety Plan (ROMIS) is a summary of any measures required to facilitate the use of the route by heavy goods vehicles (HGVs). It is also a summary of measures required to ensure the safety and free flow of traffic in the proximity of the worksite access points.
- 1.1.7 The HS2 undertakings and assurances (U&As) are commitments made by HS2 to a range of beneficiaries. They are another control used by HS2 to ensure its

contractors comply with the HS2 Act. The relevant U&As to this LTMP are listed as an *Appendix 4* of this document.

1.1.8 The Local Environmental Management Plan (LEMP) sets out site specific control measures to be adopted by HS2 Contractors working within the London Borough of Camden (LBC). The LEMP builds upon, but does not repeat, the HS2 general environmental requirements set out in the CoCP (available online at: https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/593592/Code_of_Construction_Practice.pdf)

1.1.9 The LEMP mirrors the topic headings in the CoCP.

1.2 Camdenwide LTMP Revision Summary

1.2.1 The current revision of the LTMP (Revision C04) incorporates the comments received from TfL following consultation on revision C03 in September 2022 and comments received from LBC in relation to the Interim Schedule 17 lorry route consent application for the HS2 Early Main Station Works submitted in November 2022 (Ref 2022/5079/HS2).

1.2.2 A further revision of the Camdenwide LTMP will be carried out to reflect anticipated changes to construction programme, sequencing, LGV forecasts and required lorry routes for the Main HS2 station works later in 2023. A small update is also likely to be required in relation to SCS Utility Works on Stanhope Street. Any other periodic updates of the Camdenwide LTMP will be carried out whenever significant changes are identified.

1.2.3 The period of this LTMP will cover the works that have been planned from 2023 onwards and those that either have been progressed since last revision C03 or already begun.

1.2.4 For ease of reference changes made as part of C04 revision of this LTMP document in response to comments received from LBC and TfL are highlighted in blue text.

1.3 HS2 in London Borough of Camden

- 1.3.1 HS2 has awarded a number of main works' contracts covering the route from London to Birmingham, divided into areas south, central and north. In Area South, there are four contracts spanning from Euston Station to West Ruislip, where Area South then meets Area Central at the Colne Valley.
- The main works contracts that fall within the London Borough of Camden are:
 - Euston Tunnels and Approaches (MWCC Lot S1) undertaken by Skanska Costain Strabag Joint Venture (SCSJV)
 - Euston Station (MWSC Lot S3) undertaken by Mace Dragados Joint Venture (MDJV).
 - Existing HS2 Network Rail (On Network works) Enabling Works – Euston Station overseen by Network Rail
- 1.3.2 A number of utility contractors will also be carrying out non-contestable works within London Borough of Camden to enable the approved HS2 works.
- 1.3.3 The current expected timeframe, which is subject to change, for S1 project planning and delivery is:



- 1.3.4 The current expected timeframe, which is subject to change, for S3 project planning and delivery is:



- 1.3.5 When the consolidated Single Railway Option scheme has been defined a more detailed programme will be provided for the stakeholder information and circulated via relevant stakeholder forums as well as further reiterations of this document.

1.4 Scope of Works

- 1.4.1 The scope of works for the “Euston Tunnels and Approaches” (S1) extends beyond London Borough of Camden towards London Borough of Brent and includes 7.2km of twin-bored tunnels between Euston Cavern (to be located in the existing railways corridor at the northern end of Park Village East) and the future Old Oak Common HS2 station.
- 1.4.2 The S1 works within the London Borough of Camden can be divided into two main packages outside of the tunnelling operations:
- Euston Approaches: Hampstead Road bridge, Euston Throat Retained Cut (east and west retaining walls), Granby Terrace bridge, Euston Scissor Box, Portal, Crossover tunnels and Cavern. There will also be two headhouses in this section known as Euston Portal Headhouse, Euston Cavern Headhouse.
 - Adelaide Road ventilation shaft: a shaft in the vacant Network Rail Embankment

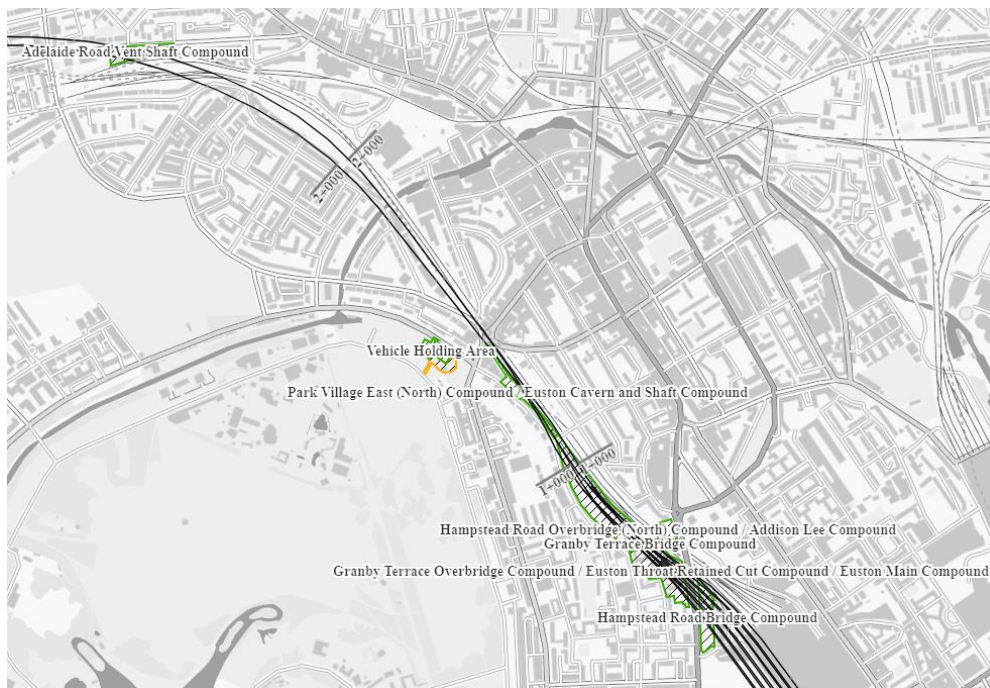


Figure 1: Main work packages outside of tunnelling operations.

- 1.4.3 Some works are adjoining or within the S1 works boundary such as some utility diversions and Euston Station, these works are not within the S1 scope of works and are being constructed by others.
- 1.4.4 The S3 works are entirely based within the London Borough of Camden and consist, for the purpose of this LTMP, the following work packages:
- Utilities - various works (inclusive of Gordon Street works) to realign existing and build new connections as well as protect existing assets where necessary – *works are to have various degree of highway impact*
 - Interim Taxi Rank - London Underground Works– This package of works comprises of the construction of the Interim Taxi Rank (ITR) in the Euston square Gardens East compound followed by the decommissioning of the current taxi rank and stopping up of Melton Street up to the pedestrian crossing with A501 Euston Road to create Euston Square Gardens West Compound. Following decommissioning of the current taxi rank, site enabling work for London Underground interchange box will commence alongside the construction of the permanent Utility corridor works - *works possibly will have highway impact and level of impact is to be confirmed*

- Traction Sub-station – works to relocate the existing London Underground track sub station including, shaft and tunnels works, headhouse construction, fit out and commissioning. Upon commissioning the existing LU Traction Sub-Station will be demolished making way for balance of sub structure works of HS2 Station.
– *minor to no highway impact anticipated with these works*
- Maria Fidelis accommodation – works to build Project welfare accommodation & construction skills centre – *no highway impact anticipated with these works*
- Tower demolition – continuation of early works to demolish buildings in readiness for LUL works – *no highway impact anticipated with these works*
- Ground Investigations – trial holes to determine ground conditions and service locations – *short term highway impact anticipated with these works*
- Instrumentation and Monitoring – install of targets and monitoring devices – *minor to no highway impact anticipated with these works*
- High Speed Station site enabling and advanced works including capping beam works & hoardings, logistics, ground clearance, surveying, early main station piling and capping beam - *no highway impact anticipated with these works*

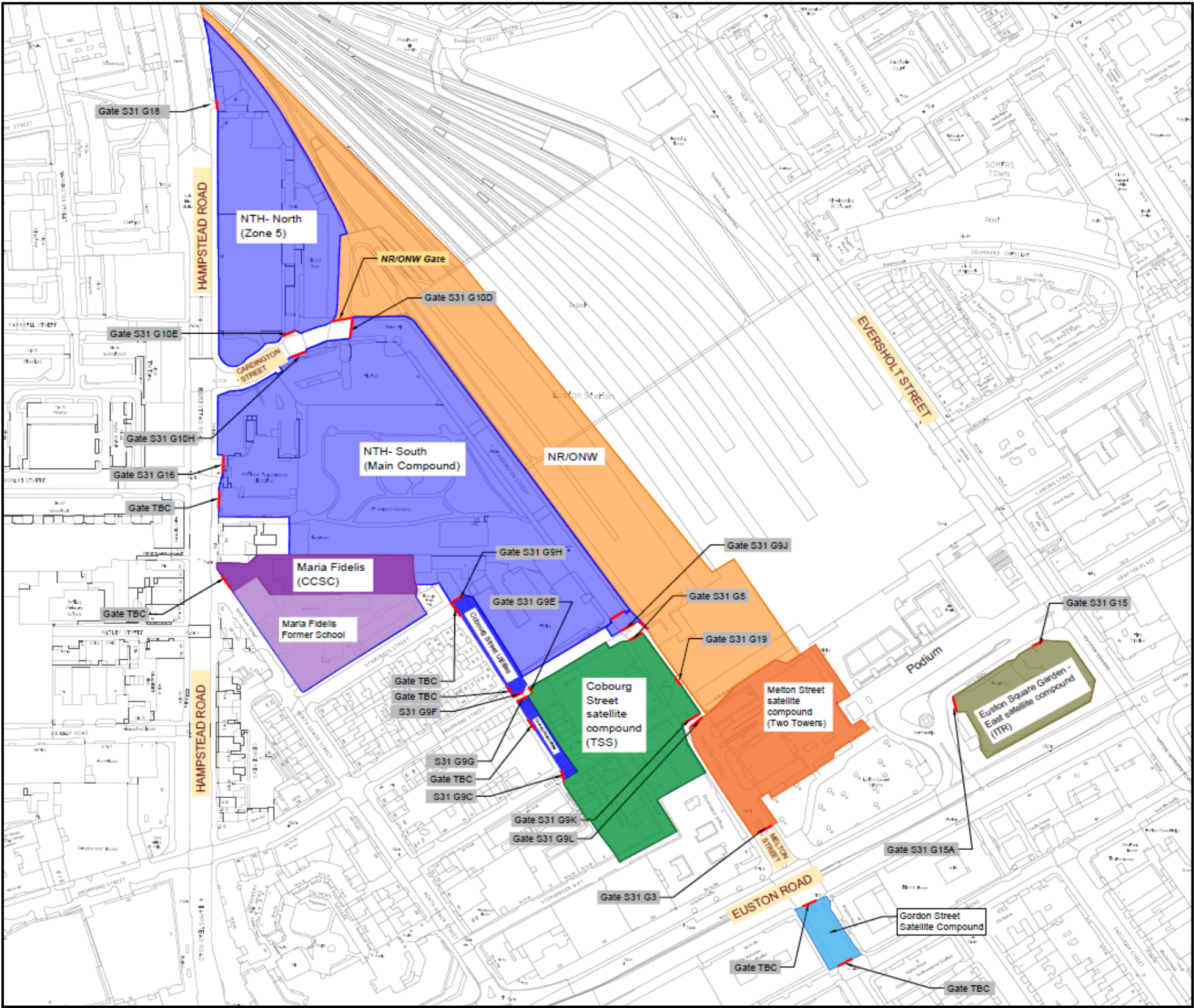


Figure 2: MDJV Site Compounds and Gates Q1/Q2 2023

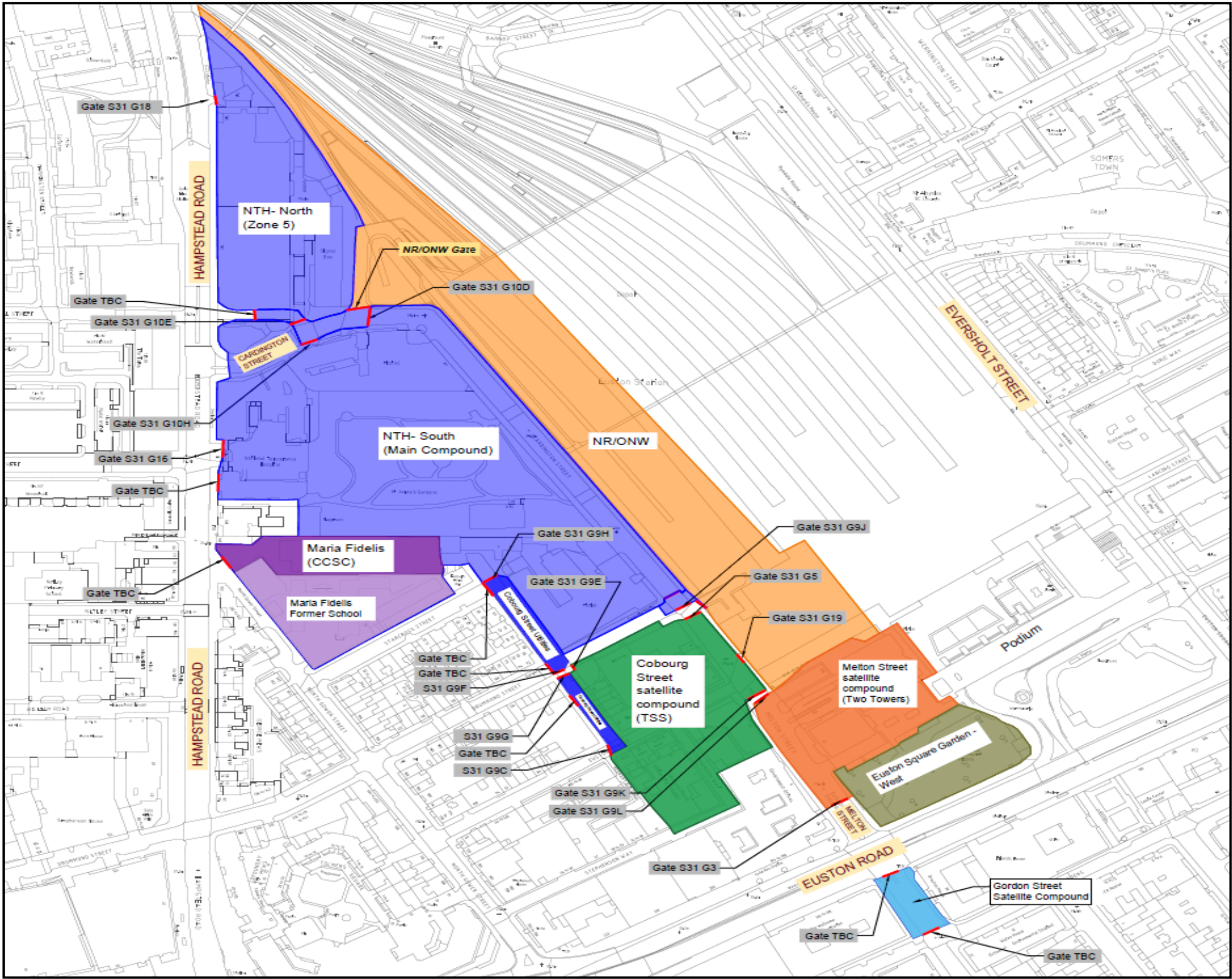


Figure 3: MDJV Site Compounds and Gates Q3&Q4 2023

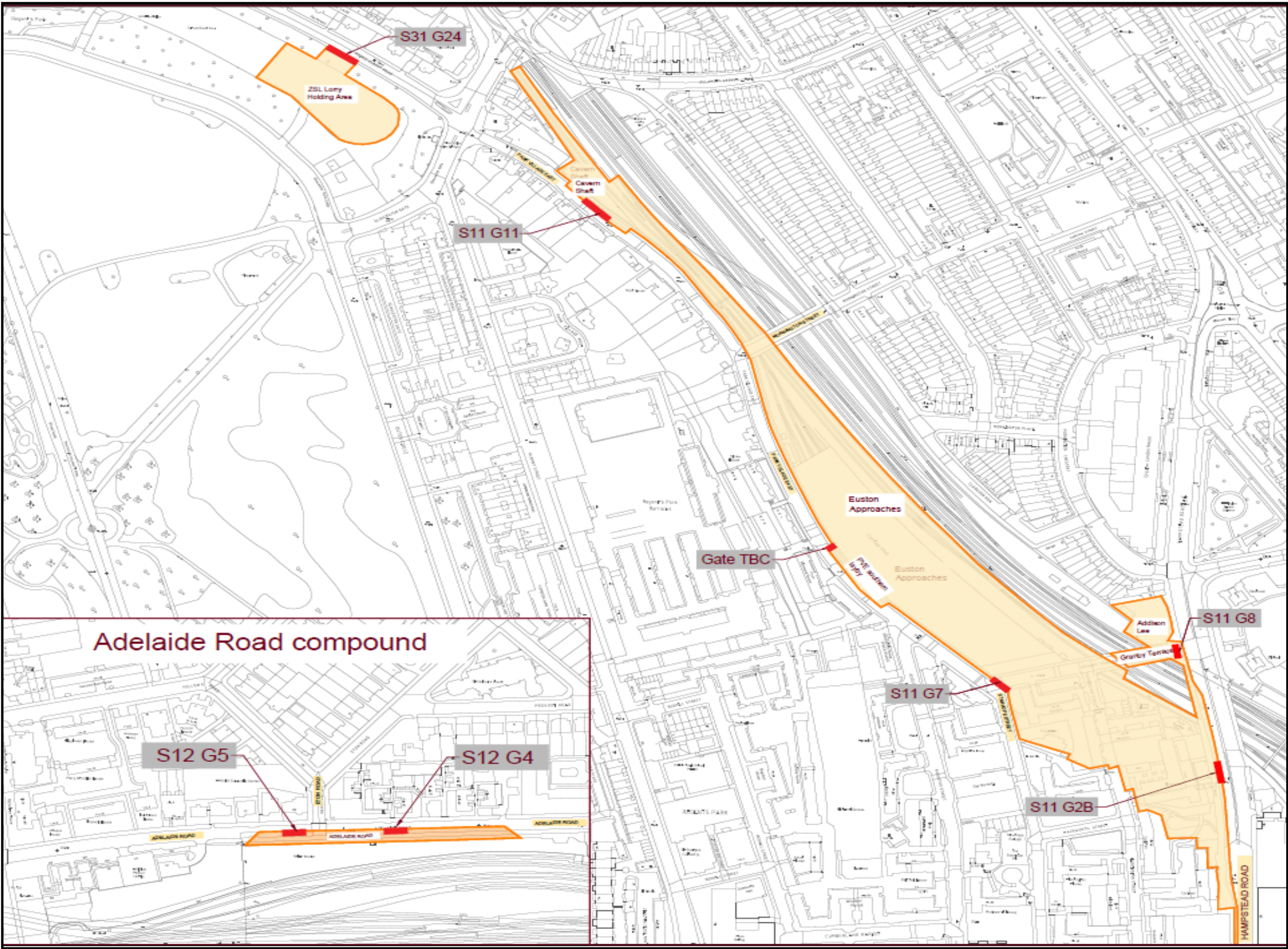


Figure 4: SCS worksite compound/s and gate numbers within London Borough of Camden

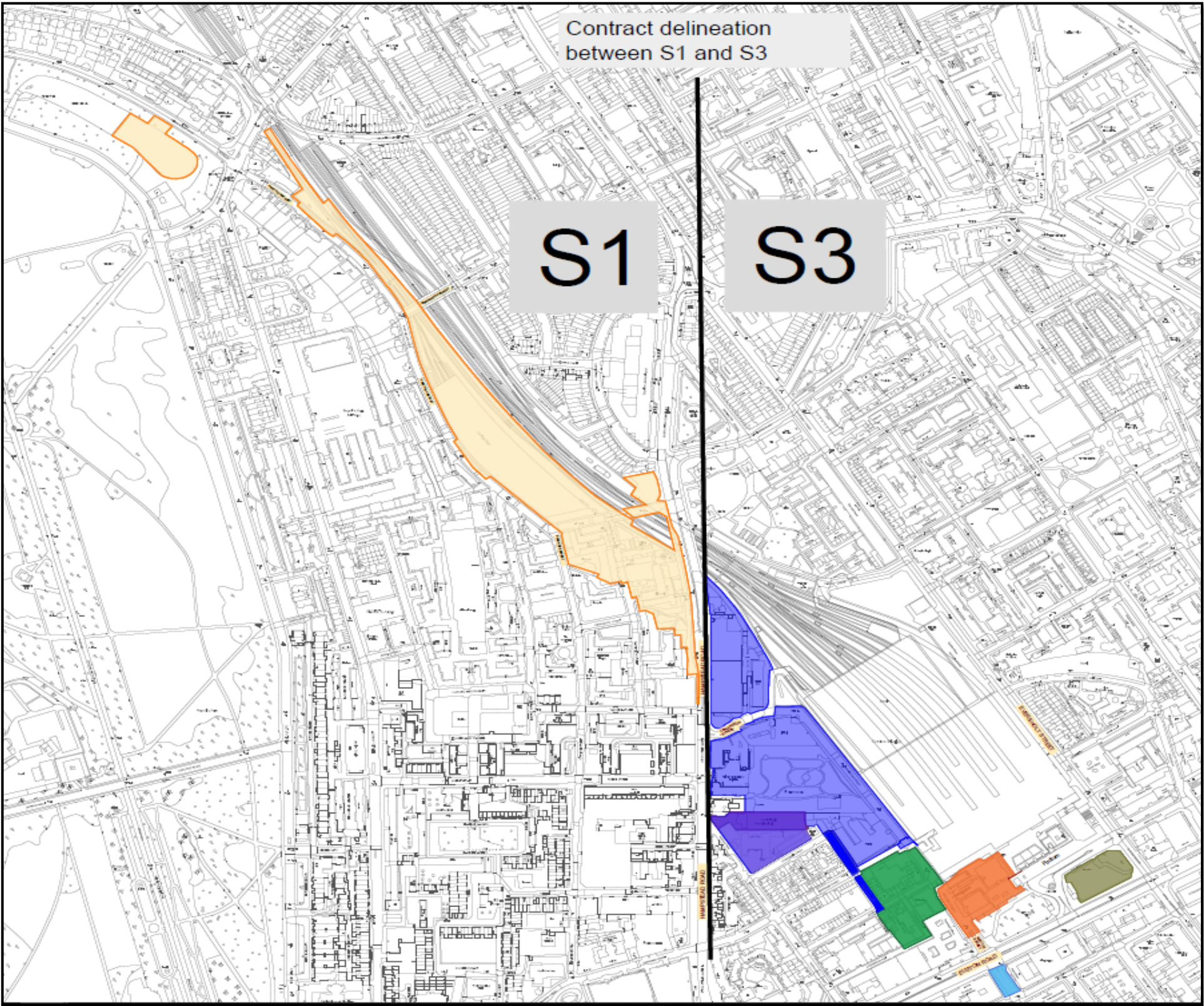


Figure 5: Map showing overview of S1 & S3 main work compounds

2 Local Traffic Management Plan (LTMP)

2.1 Scope of this LTMP

- 2.1.1 HS2 is committed to providing a safe, efficient solution for managing logistics to and from its worksites while doing all it practically can to reduce impacts to its stakeholders including road users, residents, schools, and the community in general.
- 2.1.2 This LTMP outlines the traffic management approach for the roads and streets to be used for construction routes during the main works, as well as for any early preparatory works such as surveys and ground investigations, in the section of S1, S3 and other associated HS2 works within the London Borough of Camden.
- 2.1.3 This LTMP covers the section of Lot S1 & S3 contracts within the London Borough of Camden and will consider cumulative impacts from both based on the latest information available. Discussion between both contractors is continuing and overview of locations where likely interface is anticipated is summarised in *Section 3* of this LTMP.
- 2.1.4 In accordance with the CoCP and the RTMP, the contents of this LTMP includes, where appropriate:
- permitted access routes and accesses for construction traffic
 - site boundaries and the main access/egress points for worksites and compounds
 - temporary and permanent closures and diversions of highways and other PRoW
- a list of roads which may be used by construction traffic in the vicinity of the site including any restrictions to construction traffic on these routes, such as the avoidance of large goods vehicles operating adjacent to schools during drop off and pick-up periods and any commitments set out in the register of Undertakings and Assurances
- phasing of works
 - the proposed traffic management strategy
 - other measures which will affect the highway, such as Lorry Holding areas

- regular operation of traffic liaison groups with key stakeholders to ensure that programmes of HS2 works are shared and which will assist with the highways authorities to carry out their network management duties; and
- a register of applications for consents associated with temporary traffic management measures.

2.1.5 In relation to lorry management, this LTMP includes:

- timing of site operations and timing of traffic movements
- local routes to be used by lorries generated by construction activity
- Lorry Holding areas on- or off-highway, how they will be laid out and operated; and
- weighbridge(s) at a suitable location(s) on site to monitor compliance with vehicle weight restrictions

2.1.6 This LTMP does not demonstrate the outcomes of internal investigations to arrive at which roads will be required for construction traffic and it does not provide vehicle movement numbers per road.

2.1.7 Regular updates will be given to highway authorities of the previous period actual and following period forecasted HGV numbers to each ES compound. Actual data is derived from a VMS database capturing all actuals HGV numbers. Regular forums where this is presented is currently within monthly CTWG meeting, TLG meeting and is always available upon request.

2.1.8 SCS & MDjv have considered feedback from ongoing engagement and contact with key stakeholders and community in the preparation of this plan and will continue to do so for future versions.

2.2 Ongoing review of this LTMP

2.2.1 This LTMP will be reviewed and/or updated each year, or earlier if there is a significant change to design, programme or construction methodology which results in a change to traffic management or lorry routes.

2.2.2 The ongoing review of this LTMP is in accordance with Undertaking and Assurance 1115, which states:

“The Secretary of State will require the nominated undertaker to work with the London Borough of Camden (including through the production and periodic review (with the participation of the London Borough of Camden and TfL) of the Local Traffic Management Plan and through the Traffic Liaison Group) to ensure the Nominated Undertaker supports them as the local highway authority to enable them to carry out their obligations to ensure there is a coordinated approach to traffic management in their area and address local issues as they arise. The Local Traffic Management Plan for Euston will be the subject of regular review to enable it to continue to address local circumstances while recognising HS2's cost and programme constraints.”

- 2.2.3 SCS & MDjv will continue to engage the local community and stakeholders throughout the project, capturing feedback for consideration in each review.

2.3 Relationship to Schedule 17 lorry route applications

- 2.3.1 SCS Railways and MDjv have sought approval through an HS2 Act process for its use of roads for main works construction. This approval is known as a Schedule 17 lorry routes approval.

- 2.3.2 SCS Railways application was approved on 25th August 2020 (Submission Reference: 2019/4700/HS2, Appeal Reference: APP/HS2/4).

- 2.3.3 The appeal decision had the following conditions:

1. The Harrington Square gyratory shall only be used by vehicles egressing the Euston Approaches Worksite in the eventuality that no right-hand turn is available to vehicles exiting the Worksite onto Hampstead Road. If a righthand turn is available, Large Goods Vehicles egressing the Euston Approaches Worksite shall only travel southbound on Hampstead Road.
2. Large Goods Vehicles accessing and egressing the Adelaide Road Worksite shall only use the following roads in the sequence shown in the table below, unless otherwise agreed in writing with the local planning authority or as directed by any road or traffic diversion:

Road Name	Start Point (Junction with)	End Point (Junction with)	Authority
Adelaide Road B509 (eastbound)	A41 Transport for London Road Network (TLRN)	Worksite	Camden
Adelaide Road B509 (eastbound)	Worksite	A502 Haverstock Hill	Camden
Haverstock Hill A502 (eastbound)	B509 Adelaide Road	A502 Chalk Farm Road	Camden
Chalk Farm Road A502 (eastbound)	A502 Haverstock Hill	A502 Castlehaven Road	Camden

Castlehaven Road A502 (northbound)	A502 Chalk Farm Road	A502 Hawley Road	Camden
Hawley Road A502 (eastbound)	A502 Castlehaven Road	A400 Camden Street	Camden
Camden Street A400 (southbound)	A502 Hawley Road	A400 TLRN	Camden
TLRN	Entire Length		TfL

3. No part of Camden High Street or Camden Road (west of its junction with Camden Street) shall be used by any Large Goods Vehicles, unless otherwise agreed in writing with the local planning authority or as directed by any road or traffic diversion.

2.3.4 MDJV works are subject to a number of Schedule 17 lorry route consents, largely due to the staged scope of transfer from site enabling CSJV contractor. These are summarised below as follows:

Schedule 17 Reference Number	Corresponding LTMP Reference Number	Applicable Worksite	Status
2018/0438/HS2	1EW02-CSJ-CL-PLN-S001-000001 C04 (now superseded) & 1CP01-MDS-CL-PLN-SS08_SL20_GF-000004 C02	NTH North (Zone 5) NTH Main Compound Cobourg Street Satellite Worksite (TSS)	<p>The site enabling works for NTH North (Zone 5) transferred to MDJV in 2020 and the balance of these works continue under scope of 2018/0438/HS2.</p> <p>Site enabling works for NTH main works compound transferred to MDJV in 2021 and the balance of these works also continue under scope of 2018/0438/HS2.</p> <hr/> <p>Works associated with Cobourg Street Worksites are superseded by Sch17 lorry route consent 2020/1321/HS2.</p> <hr/> <p><i>New Schedule 17 approval is to be sought for Q1/Q2 2023</i></p>
2020/2355/HS2	1CP01-MDS-CL-PLN-S003—000014 C03	NTH Main Compound	The balance of early works west wall piling (40 further piles) and capping beam are ongoing and continue to be delivered under the scope of 2020/2355/HS2. Upon completion of these works Schedule 17 2020/2355/HS2 will no longer be required.

2020/1321/HS2	1EW02-CSJ-DL-PLN-S003-000034 C02 & LTMP addendum (1CP01-MDS-DL-PLN-SS08_SL20-000001 C02).	Melton Street Satellite Compound (Two Towers)	The demolition and associated works for Two Towers transferred to MDJV in 2020 and balance of these work are ongoing and continue to be delivered under the scope of Schedule 17 lorry route consent 2020/1321/HS2. Upon completion of these works Schedule 17 2020/1321/HS2 will no longer be required.
2021/4406/HS2	1CP01-MDS-CL-PLN-SS08_SL20_GF-000004 C02	Cobourg Street Satellite Compound (TSS)	The balance of main construction works for the LU replacement sub-structure are ongoing and continue to be delivered under the scope Schedule 17 lorry route consent 2021/4406/HS2. This consent will continue to be independent of other Schedule 17 consents.
TBC-work in progress	1CP01-MDS-CL-PLN-S003-000021 C03) dated 21/01/2021 (to be superseded by 1CP01-MDS-CL-PLN-SS08_SL20_GF-000004 C03)	Euston Garden Satellite compound East	Current site enabling works are below the threshold of daily 24 two-way movements and do not require a schedule 17 lorry route consent. To allow for the future uncertainty regarding the proposed scope of utility works in this area a higher two-way HGV movement has been assumed from 2023 onwards.
TBC- outside of MDJV Scope	150248-SKA-PLN-EHW-000002 A03.2	NR-ONW	Current phase 1 site enabling works are below the threshold of daily 24 two-way movements and do not require a schedule 17 lorry route consent. ONW are reviewing further scope and will seek schedule 17 should the numbers be in exceedance of the given threshold.

2.3.5 When the main works commence for the Euston station a further Schedule 17 Lorry Route application will be sought. This will be covered in an updated version of this Camdenwide LTMP inclusive of any modifications that may be required to the highway.

2.3.6 Any additional points of access needed to access the construction compound will be made under Schedule 4 of the High-Speed Rail (London – West Midlands) Act 2017, and this document will be updated periodically to reflect such changes.

2.4 Geographical Area

- 2.4.1 Works on the Main Works Civils Contract and those of the Main Works Station Contractor will follow the alignment set-out within the Act and supporting documentation. The works will be within the Act Limits (Limit of Deviation (LOD) and Limit of Land to be Acquired or Used (LLAU) combined). The following map shows the geographical area of HS2 works within LB Camden.

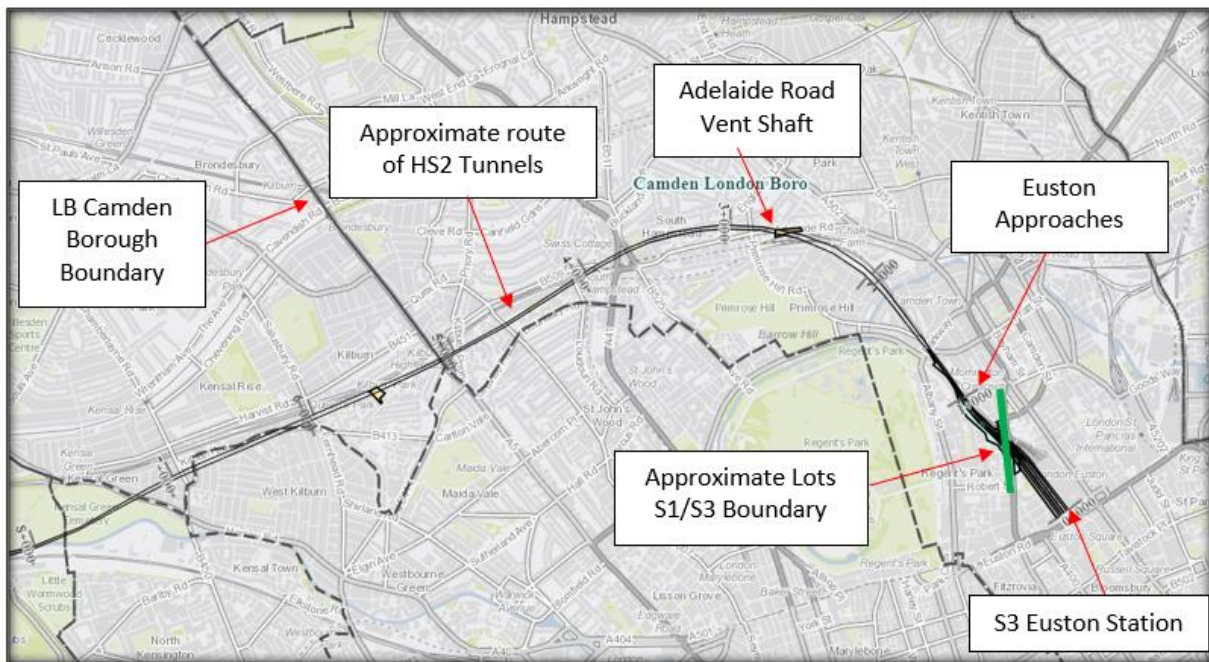


Figure 6: Route overview of map of HS2 works within LB Camden

3 Transport Networks and Services

3.1 Transport Networks and Services

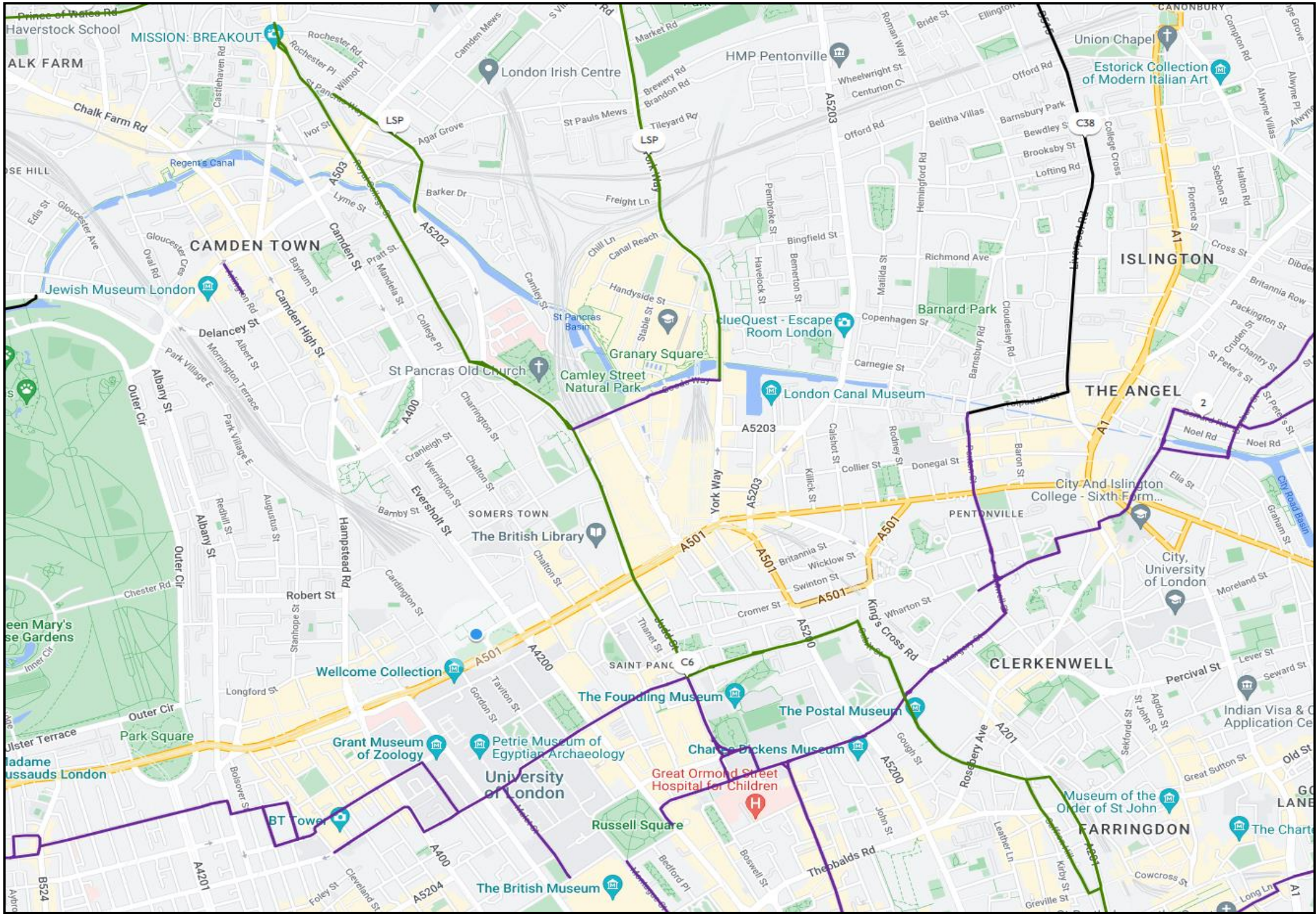
- 3.1.1 The destination and origin of inbound and outbound materials for work packages that have yet to be awarded is unknown and in these instances the same assumptions have been applied as per the ES. Use of TLRN will be utilised to facilitate the movements from the worksite. The use of LB Camden roads will remain as set out in this document, regardless of the destination and origin of inbound and outbound materials.
- 3.1.2 In accordance with PFN 6 (Planning Forum Note 6) *"Submissions will include a general provision that, where a supplier/business is located between the special/trunk road network and a Site, LGVs from that supplier/business to the Site will be required to take the most appropriate route to join the Main Routes. This general provision will include a requirement that these routes, where reasonably identifiable, are discussed at the relevant Traffic Liaison Group"*. Please see Section 6 for proposed concrete routes for information.
- 3.1.3 The existing transport network within the Camden LTMP and surrounding area includes parts of the TLRN serving Central London, including A501 inner ring road, A1, A40, A41, A400, A503 and A5.
- 3.1.4 There are various train and underground stations within the immediate vicinity of the works area, and/or along the proposed lorry routes. These stations are:
- Euston Station is served by London Northwestern Railway, Avanti West Coast London Overground, Northern and Victoria Lines
 - Warren Street underground station is served by Northern and Victoria Lines
 - Euston Square Station is served by the Circle, Hammersmith and City and Metropolitan lines
 - Mornington Crescent, Camden Town and Chalk Farm are all served by the Northern Line
 - Swiss Cottage is served by the Jubilee Line.
- 3.1.5 For SCS operational parking will be provided, normally at welfare facilities, for:
- Site medical team (if any)
 - Canteen staff, who will be arriving earlier than start up activities

- Blue badge holders
- Site security staff, particularly those carrying dogs
- Essential project visitors, carrying out a statutory function
- External operational vehicles (including street sweepers and compliance team vehicles)
- Parking for workers at welfare facilities whose operational vehicles are then continuing into the worksite areas.

3.1.6 MDjv will only provide such operational parking within the site compounds. No provision for parking is to be made at the welfare facility in Maria Fidelis nor on site.

3.1.7 Euston Bus Station lies immediately to the north of Euston Garden East Satellite compound. Minor impacts are anticipated during the construction of footway improvements and betterment of the existing pedestrian accessibility at the crossings leading to interim taxi rank. Surrounding bus routes that may be impacted will be reviewed as part of Schedule 4 consenting regime. Where potential impacts to bus routes are anticipated these are highlighted within relevant work descriptions, see *Section 3.3 to 3.8* of this report.

3.1.8 There are various designated cycle routes within LB Camden. *Figure 7 and Figure 8* below demonstrates designated cycleway routes that consist of former Cycle Superhighways and Quietway's (note: TfL is in the process of updating the legacy names and routes). Included within the *Figure 7* is the proposed development of future Cycleways that are included in those routes. HS2 and its contractors also acknowledge LBC cycle routes based on the information that is available from Camden Cycling Campaign that can be viewed in *Figure 8*. Majority of these routes align with those that are shown in *Figure 7*. Routes within *Figure 7* are routes that link communities, businesses and destinations across London into the cycle network and are of strategic importance. Impacts to routes within *Figure 7* and any required mitigation will be detailed within *Section 3* of this report. Any impacts to less strategic routes that are detailed within *Figure 8* but also feature as part of LBC advisory routes will be mitigated as is reasonably practicable.



Purple/Green - Cycleways

Black – Proposed future routes

*Source data: <https://tfl.gov.uk/modes/cycling/>

Figure 7: Strategic cycleways routes within London Borough of Camden and surrounding area

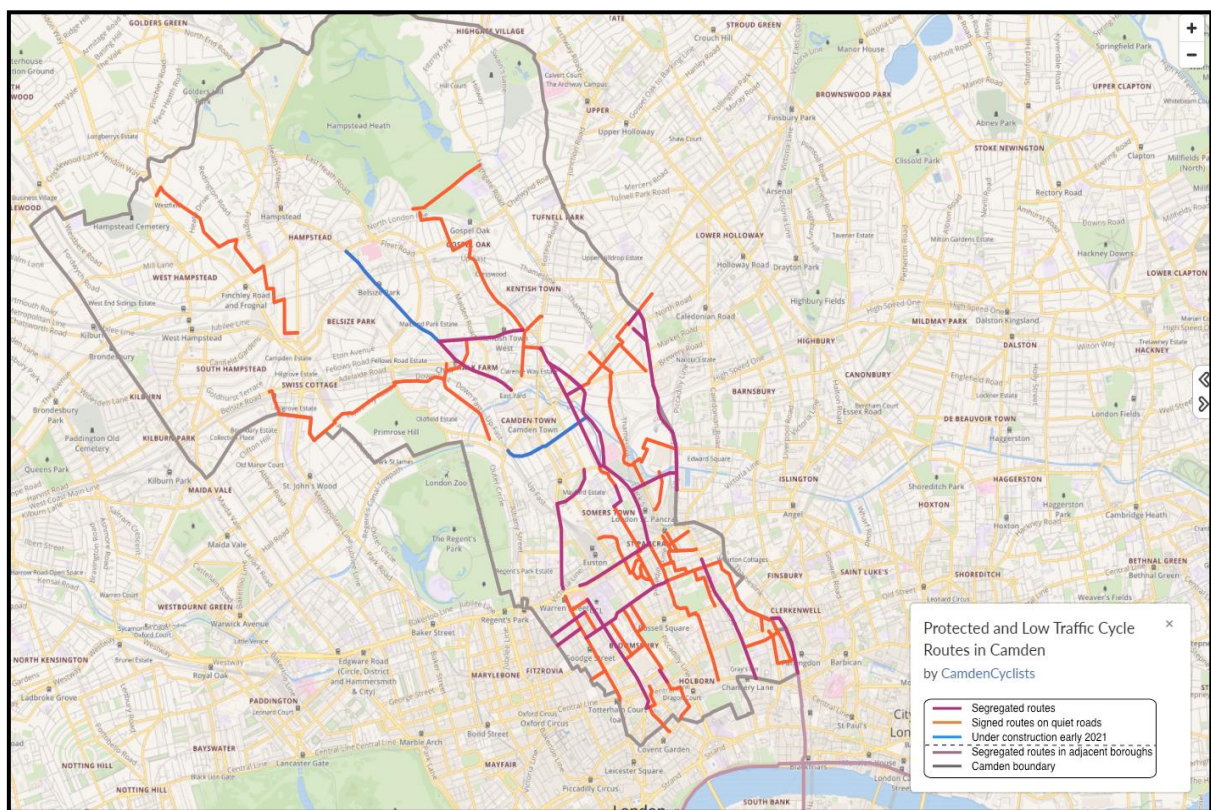


Figure 8: Cycle routes within as per cycle information from London Borough of Camden website
(* Source Data: <https://www.camden.gov.uk/cycle-lanes-and-routes>)

- 3.1.9 The Early Works Contractor had produced and implemented a cycle strategy which the Main Works Contractor has inherited. The EWC had consulted TfL Cycles, Camden Cycling Campaign, and the local community groups on the strategy. The Main Works Contractor will adopt already produced Cycle Signage Strategy document and review the initial proposals. Any improvements will be discussed with relevant stakeholders. There have been changes since creation of initial Cycle Signage Strategy document such as Covid 19 epidemic responses and The Main Works Contractor will try to recognize any such changes within its proposals. Cycle Signage Strategy will be a standalone document that will regularly be reviewed and revised if applicable.
- 3.1.10 Traffic Management will be designed in accordance with the TfL Temporary Traffic Management handbook and the TfL Cyclist at Streetworks.
- 3.1.11 All traffic management will be designed on a case-by-case basis and the appropriate layouts used.

3.2 Road Networks & Significant Construction Changes (Main Civil Works and Utility Works)

- 3.2.1 The details within the sections below are based on current scheme design, programme dates and construction methodology. Project details will change as SCS & MDjv progress with detailed design and construction, allowing us to provide more detail in future versions of this plan.
- 3.2.2 The following Undertakings and Assurances, amongst others, have been considered when planning the works detailed within sections below:

U&A 1106 – “The Secretary of State will require that the Nominated Undertaker will maintain public access along the highway and over other areas where the public has access, where reasonably practicable, and appropriate measures will be implemented to ensure the local community, economy and transport networks in the London Borough of Camden can continue to operate effectively. Where this is not reasonably practicable, alternative measures shall be identified by the Nominated Undertaker to maintain continual public access, especially for pedestrians and cyclists, to routes in the vicinity of the construction sites within the London Borough of Camden. The impact of road based construction traffic will be reduced by implementing and monitoring clear controls on vehicle types, hours of site operation, parking and routes for large goods vehicles. Without prejudice to any other requirements to do so, the Nominated Undertaker will take reasonable steps to ensure that the London Borough of Camden is notified of those controls and informed of any changes to them.”

U&A 1107 - “The Secretary of State will require the nominated undertaker to, where reasonably practicable, retain access for pedestrians and cyclists where safe and appropriate to do so, including where a highway is closed to other traffic under the powers of the Bill.”

3.3 Adelaide Road

- 3.3.1 The temporary Adelaide Road access point required the southern footpath to be closed and hoarded off from use adjacent to the worksite limits. The hoarding extends beyond the footway and into the carriageway. To maintain 2-way traffic along Adelaide Road the central hatching and traffic islands has been removed. To enable safe access and egress from the site, stop works will be in place to manage interface between general traffic and HS2 vehicles.
- 3.3.2 The westbound bus stop (Eton Road) is required to be suspended for the entire duration, of the Adelaide Road works. An alternative location was considered with TfL, however due to various constraints it was not able to relocate the westbound bus stop. There are existing bus stops either side of the suspended bus stop which are 3-4 mins walk (Primrose Hill to the West and Chalk Farm to the East).
- 3.3.3 Pedestrians are diverted to the opposite footway for the duration of the works. The existing crossing locations along Adelaide Road are to be used, these are at the Chalk Farm and Primrose Hill ends. Alternative locations have been considered, but these locations were either deemed unsafe or would require suspending parking bays along the road. A proposal presented by LBC to install a cycle lane along Adelaide Road required the parking on the southern side to be suspended, if this proposal is progressed, then SCS will liaise with LBC regarding any pedestrian crossing points identified within the proposed Camden scheme.
- 3.3.4 Adelaide Road is a popular route for school children, therefore engagement around lorry routes and site/road safety with the local schools is critical and has been and is ongoing. Deliveries will avoid entering the site during the school pick-up/drop-off hours (8:30-9:30 & 15:30-16:30), with the exception of HGV's for piling and concrete activities or in unforeseen circumstances during school term times.
- 3.3.5 Adelaide Road is not a designated cycle route, however there are two cycle routes nearer to Swiss Cottage that cross Adelaide Road. Adelaide Road within Camden Transport Strategy 2019 -2041, as part of The Cycling Action Plan has been identified as a potential section for implementation of primary and secondary cycle network routes. Within the LBC Transport Strategy Phase 2 (2022/23 to 2024/25) there is a potential for LBC to provide provision for separated cycling infrastructure wherever feasible on length of Adelaide Road between Chalk Farm Road and Swiss Cottage. Should LBC implement any cycling schemes along Adelaide Road, the HS2 nominated undertaker would like to review the plans for any future potential clashes. Cyclists will be able to continue to use Adelaide Road and will be accommodated within the proposed traffic management. If a full

closure is ever required at any point a suitable diversion and any mitigation measures will be discussed and agreed with LBC

- 3.3.6 We will engage with LBC regarding their proposals for a dedicated cycle route along Adelaide Road, which is currently planning to be implemented. This will be within TLG meetings, other co-ordination meetings and by maintaining dialogue LBC.
- 3.3.7 The traffic management will be designed and installed in accordance with the Safety at Street Works and Road Works code of practice; and Chapter 8 of the Traffic Signs Manual, and TfL Temporary Traffic Management handbook.
- 3.3.8 The Schedule 17 Lorry Route application was submitted to Camden and has been approved. The application submitted to approach the site from the east (Chalk Farm) and exit towards the west (Swiss Cottage), however this was reversed as part of the Schedule 17 Planning process, as per the routes aforementioned in *Section 2.3*
- 3.3.9 An opportunity has arisen for some deliveries to approach site from the west (Swiss Cottage) and to exit to the west. This opportunity is dependent upon the origin / end destination / size of the vehicles. For example, a deposit site for the excavated material has been identified at Cricklewood, if this route was to be used it will avoid those vehicles travelling towards Camden Town and the Euston area. This will bring positive environmental impact as less vehicles will be travelling through those areas. Prior to this proposed route being used, a new Schedule 17 lorry route application needs to be submitted and approved by LBC.
- 3.3.10 Due to the ground movement anticipated for the construction of the Adelaide Road worksite, the sewer that runs along the middle of Adelaide Road exceeds the acceptance criteria therefore will be required to be lined. To line the sewer a series of new Temporary Access Points (TAP's) will be required to be constructed along the length of the asset and on any connecting sewers, this will allow the section that exceeds the acceptance criteria to be lined. The current proposals and details are being finalised; it is expected that these works will require a full road closure for up to four weeks in duration, subject to further design and constructability development. Due to the U&A 2379 (see below), any closure of Adelaide Road will require approval of the Highway Authority.

U&A 2379 - "During the construction of the Adelaide Road ventilation shaft the Secretary of State will require the Nominated Undertaker to keep one lane of Adelaide Road open to traffic at all times with traffic light control to permit two directional flows, subject to any short term closures (typically expected to be no more than one to two

days) for initial construction works and except in the case of emergency, emergency works, where a closure is necessary due to unforeseen circumstances, or otherwise with the approval or at the direction of the Highway Authority”

- 3.3.11 The full closure mentioned above will impact on the approved lorry route for Adelaide Road site. Impacts on the approved routes affected by the sewer works are being assessed in line with the shaft works access requirement and programme.
- 3.3.12 In addition to the sewer works, short term closures are likely to be required for site establishment and surveying works.
- 3.3.13 Permanent utility connections are required for Adelaide Road Vent Shaft and Headhouse. Further details will be provided once confirmed.
- 3.3.14 For the permanent access point a traffic calming build-out with a pedestrian crossing point to the west of the access may be required to be relocated. The new location of the build-out/crossing point will be discussed with LBC and TfL (if affected).

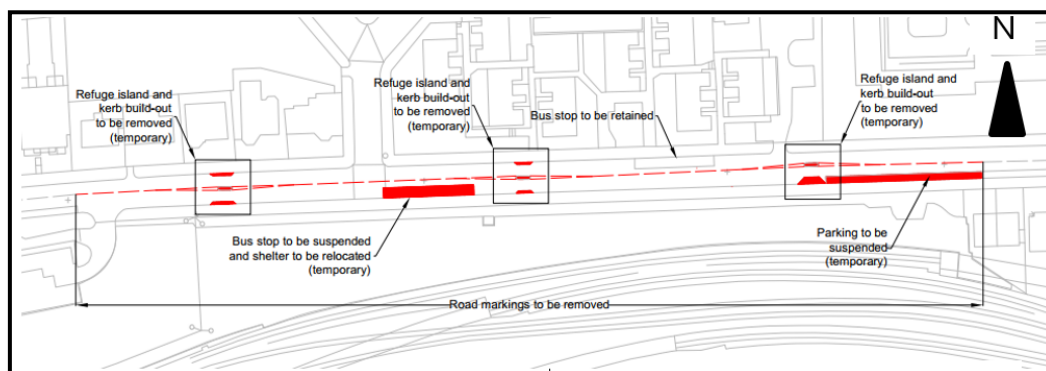


Figure 9: Adelaide Road proposed site clearance

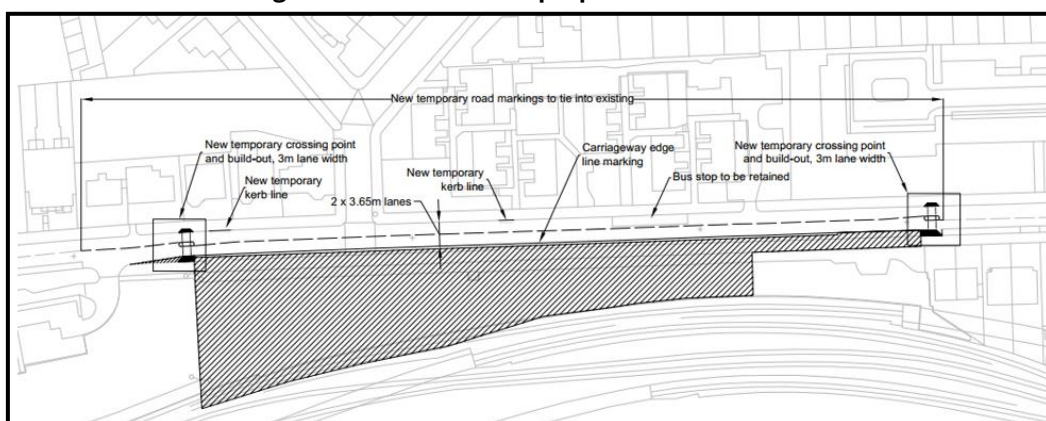


Figure 10: Adelaide Road proposed layout to accommodate site access requirements

3.4 Euston Approaches

Hampstead Road

- 3.4.1 The existing A400 Hampstead Road Overbridge is situated just to the north west of the Euston Main Line Rail Station, within the London Borough of Camden (LBC). The overbridge carries the A400 Hampstead Road across the West Coast Mainline Railway and consists of three traffic lanes (including a bus lane) in northbound and southbound direction. Guard railings are located along the outside kerb on both sides, and the overbridge is lined by parapet walls approximately 1.65m in height on both sides for the entire length. A400 Hampstead Road is to be realigned as part of the wider HS2 works in the Euston Area of London. The realignment will begin at the existing A400 Hampstead Road Overbridge (HRB) in the north and continues approximately 260m south to the existing junction with Robert Street. Before civils works can commence on Hampstead Road Bridge a number of utilities packages need to be delivered, the following is subject to change but current programme outlines, the following is subject to change but current programme outlines:
- Phase 1 utility diversions currently in progress and ending June 2023
 - Phase 2 utility diversions starting June 2023 and ending August 2024
 - HRB/Zone 5 works starting July 2024 and ending July 2026
- 3.4.2 To construct the new Hampstead Road bridge there will be temporary effects to the traffic and buses along the route. The works will be completed in phases, with traffic lanes reduced to 1 lane in either direction, with cyclists to be accommodated within lanes (4-4.25m) when and where reasonably practical, otherwise, optimal lane widths of 3.4m shall be maintained as per TfL Temporary Traffic Management Handbook where cyclists cannot be accommodated. Traffic will be alternated between the northbound and southbound carriageways during differing phases to construct the new bridge. A new temporary road will be constructed to the southeast (Euston Station) side of Hampstead Road to assist in the reduction of closures. At this time, it is planned for two full closures, each 4 weeks in duration. The closures are to pile the bridge piers/abutments and construct the pile caps. See Section 3.11 *Social Impacts* for proposed mitigation during the works in the Euston Approaches area.
- 3.4.3 A400 Hampstead Road is a strategic link and designated red route. As such it falls under the jurisdiction of Transport for London (TfL). The local council is Camden London Borough Council.

- 3.4.4 All works that take place on Hampstead Road including the planned closures will be assessed against the criteria within Section 15 (Traffic and Transport) of the Environmental Statement EIA Scope and Methodology Report.

(https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/260116/HS2_London_to_West_Midlands_EIA_Scope_Methodology_Report_revised_0.pdf). The criteria assessed within Section 15 is set out under the following headings:

- Public Transport Delay
- Disruption at stations/interchanges
- Traffic flows and delays to vehicle occupants
- Parking and loading
- Vulnerable road user delay, amenity, and ambience
- Accidents and safety
- Severance
- Waterways

- 3.4.5 The planned closure does not create any new significant impacts in addition to those that were already assessed within the original ES and any relevant supplementary versions of the ES. Further assessments will be undertaken to ensure that the planned works are coordinated and assessed to ascertain any impact to the network. Should there be an overlap in programme then a further assessment will be undertaken using the criteria mentioned. We will engage highway authorities during any such assessments being undertaken and any traffic management proposals.

- 3.4.6 During the phasing of the works pedestrians will be accommodated by diversion to the opposite footway. For example, when working in the southbound carriageway, pedestrians will be diverted to the northbound footway. North/south pedestrian movements will be accommodated during the full closure periods.

- 3.4.7 As part of the wider HS2 cycle strategy within the Euston area, SCS will ensure advanced signage directs cyclists to use alternative routes – Eversholt Street, Midland Road and Albany Street/Outer Circle. Cyclists who still want to travel through the Hampstead Road works area will be accommodated within vehicular lanes during the lane closure phases (see Section 3.1) and on the temporary road layout, allowing cyclists to comfortably share the carriageway with vehicles. Within

the traffic management, further discussions with TfL and cycle groups will determine the exact layout details for the cycle facilities. The traffic management will be designed and installed in accordance with the Safety at Street Works and Road Works code of practice; and Chapter 8 of the Traffic Signs Manual, and TfL Temporary Traffic Management handbook.

- 3.4.8 Bus routes along Hampstead Road will remain operational during lane closures. The Silverdale bus stops (north (B) and southbound (W)) will be suspended throughout the proposed works including temporary road phase. The Silverdale bus stop north B has been suspended since establishment of S1 worksite along Hampstead Road in the northbound nearside carriage lane. The Silverdale bus stop south W is currently operational and will remain in use as long as is reasonably practicable.
- 3.4.9 Due to the Haul Road access point on Hampstead Road in the existing Silverdale northbound bus stop has been removed, to accommodate access and egress to all of the worksites within the Euston Approaches. Initially this operated as a 'left in' left out' until the removal of the central reservation allowed a right turn exit from the access. The access is now operational and controlled by temporary traffic signals to safely control the access, a Stage 3 Road Safety Audit has been undertaken and the phasing of the lights on along Hampstead Road will be monitored and adjusted accordingly. This access is intended to be in use in lieu of the existing Granby Terrace access.
- 3.4.10 The exact location of the above-mentioned access will alter throughout the construction process. This is to accommodate the piling activity within the Euston Throat area immediately adjacent to the western footway along Hampstead Road. As a result, the works for the access will be designed to accommodate this alternating location. The operation of the access will also be reviewed with other activities taking place on Hampstead Road such as utility works and the operation of Gate 18.
- 3.4.11 The junction at Granby Terrace is proposed to be redesigned to be more robustly aligned with future TfL scheme. During construction period and while Haul Road is in use pedestrian demand only phase will be maintained. Following temporary road construction junction, crossing and signal provisions will be reviewed.

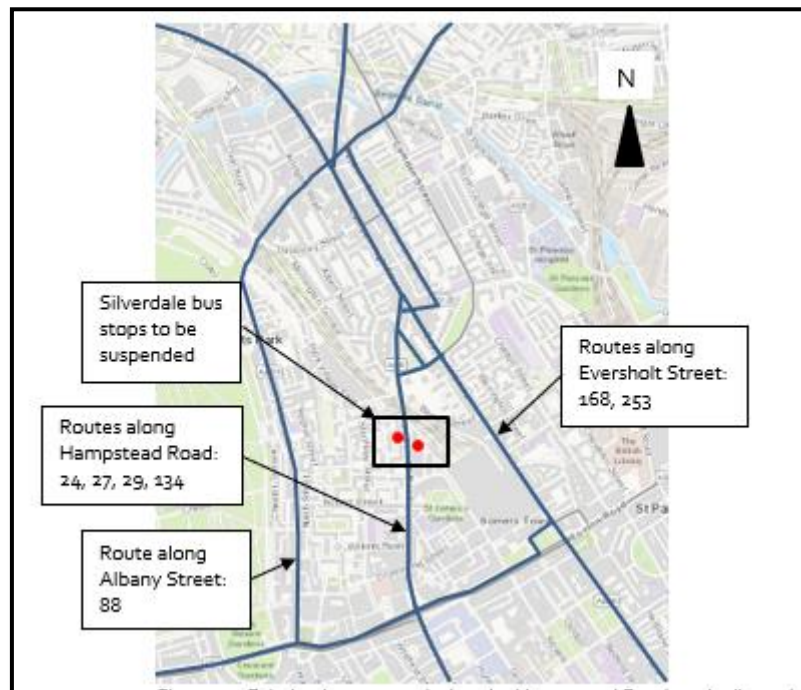


Figure 11: Existing bus routes during the Hampstead Road works (lane closures)

- 3.4.12 A temporary road will be constructed to the east of the existing Hampstead Road, this is to accommodate piling works for the new HS2 Hampstead Road bridge structure. The temporary road is expected to be operational from February 2025 for approximately 2 years – subject to change. Consultation with TFL through Bluebook process and LBC will be undertaken prior to these works.

Park Village East - Cavern Shaft Layby

- 3.4.13 The temporary Park Village East/Euston Cavern access point will require the northern footpath to be closed and hoarded off from use to construct a lay-by at street level. This will enable vehicles to deliver materials to site from street level as the worksite is too constrained to allow vehicles to manoeuvre at track level.
- 3.4.14 The hoarding will extend beyond the footway and into the carriageway. At this location Park Village East is one-way, therefore a lane width in accordance with the Safety at Street Works and Road Works code of practice; and Chapter 8 of the Traffic Signs Manual, and TfL Temporary Traffic Management handbook, will be maintained at all times.
- 3.4.15 With the relatively short linear length of highway works or occupation of the highway, and relatively low volumes of construction traffic expected to use this site access, it is proposed that cyclists will be safe to cycle in the carriageway with general traffic.

- 3.4.16 The hoarding will create an access point from the side of the layby and egress at the south of the layby. Layby footprint will consist of eastern footway and lane incursion. The layby became operational from June 2022 for deliveries, the welfare and site office will be established in second half of 2022. Signage will be displayed in advance advising pedestrians to use the opposite footway at existing crossing at the York and Albany public house and at Mornington Street bridge. No net loss parking will occur as adjacent existing parking bays suspended for these works have been mitigated for by providing new bays to the south of the layby. Access to adjacent property frontages will be maintained and sufficient carriageway width allowed for safe vehicular access/egress. Lighting columns were removed from within the footprint of the site and any mitigation measures were discussed with relevant LBC teams to ensure no loss of highway luminance in comparison to existing.

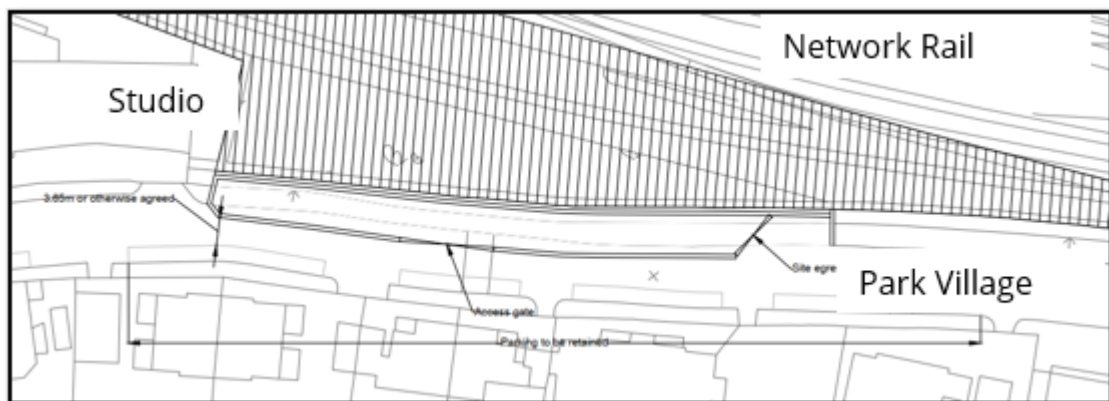


Figure 12: Park Village East proposed layout to accommodate site access requirements

- 3.4.17 As part of the permanent works, it is proposed to construct two permanent headhouse structures along Park Village East, these headhouses will require vehicular access from Park Village East. The proposed access points are envisaged to require, in some areas, to permanently remove parking bay areas, see *Figure 13* and *Figure 14* below. Further details will be produced during detailed design, the proposals once known will be discussed with LB Camden once details are known.

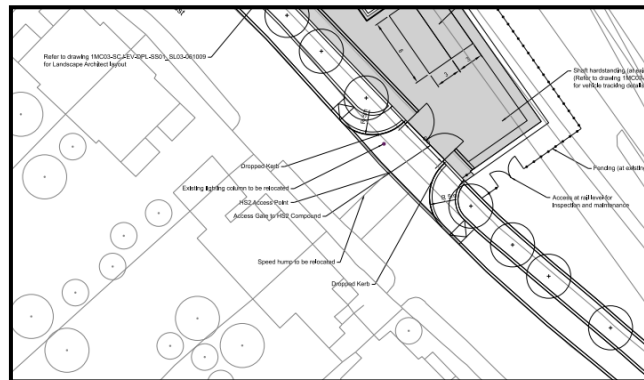


Figure 13: Proposed permanent access location at the Euston Cavern and Shaft

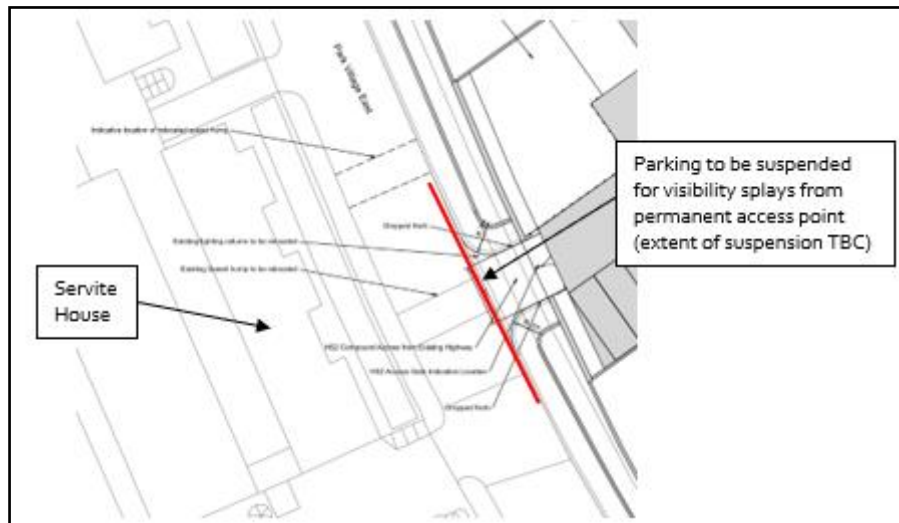


Figure 14: Proposed permanent access location at the Euston Portal Headhouse

Park Village East – Southern Layby

- 3.4.18 At the southern end of Park Village East, a lorry layby is proposed to be installed in Autumn 2022 to serve Tower Cranes D and C. The layby will be delivered in two phases. Phase 1 will be approximately 30m in length and require the suspension of 19 parking bays on Park Village East (8 Pay & Display, 11 Residents bays). Options for mitigating parking bays are being investigated with LB Camden, however, fewer bays are affected than was reported in the ES. Phase 1 will only serve Tower Crane D and will remain in place whilst Park Village East continues to operate two-way for traffic.
- 3.4.19 Phase 2 of the layby is proposed to be delivered when Park Village East is made one way during the closure of Mornington Street Bridge described below. As Park Village East will become one-way for traffic the layby is able to be extended without impacting on the visibility of northbound traffic. The phase 2 layby will be

60m in length and require an additional 5 resident parking bays to suspended. The extended layby will allow Tower Crane C to also be served by the layby.

- 3.4.20 The layby footprint will extend 4m from the existing eastern kerb line with a 3.6m hoarding creating the perimeter of the layby with access gates at either end. During phase 1 the centre line of Park Village East will be realigned to maintain two-way traffic on Park Village East with a remaining carriageway width of 5m past the layby.
- 3.4.21 The layby will operate with vehicles entering through a site gate at the top of the layby then exit the layby heading south on Park Village East as shown in *Figure 15 and 16*.
- 3.4.22 The eastern footpath on Park Village East will be closed whilst the layby is in place and pedestrians will be directed to cross at points to the north and south of the layby
- 3.4.23 The use of the layby is limited by the capacity of the tower cranes which is expected to be 12 movements during phase 1 and 24 during phase 2. More information is provided in the *Section 6*.

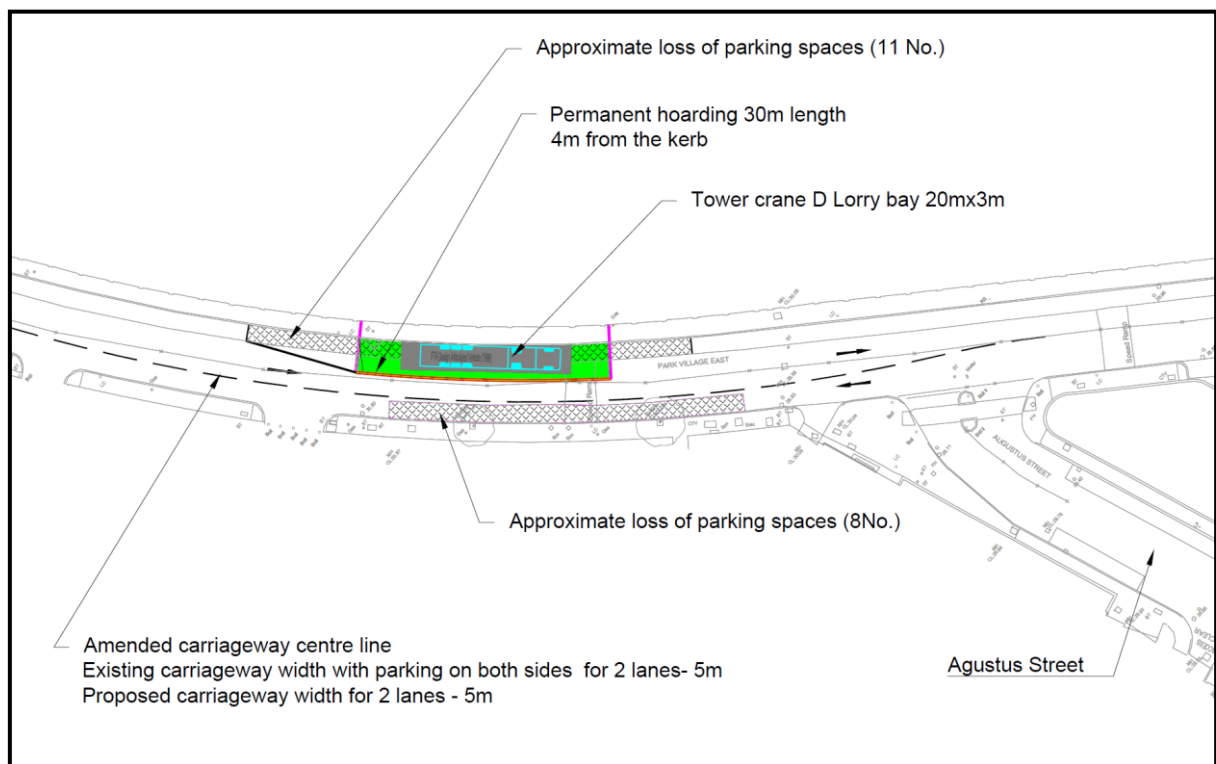


Figure 15: PVE Southern Layby Phase 1

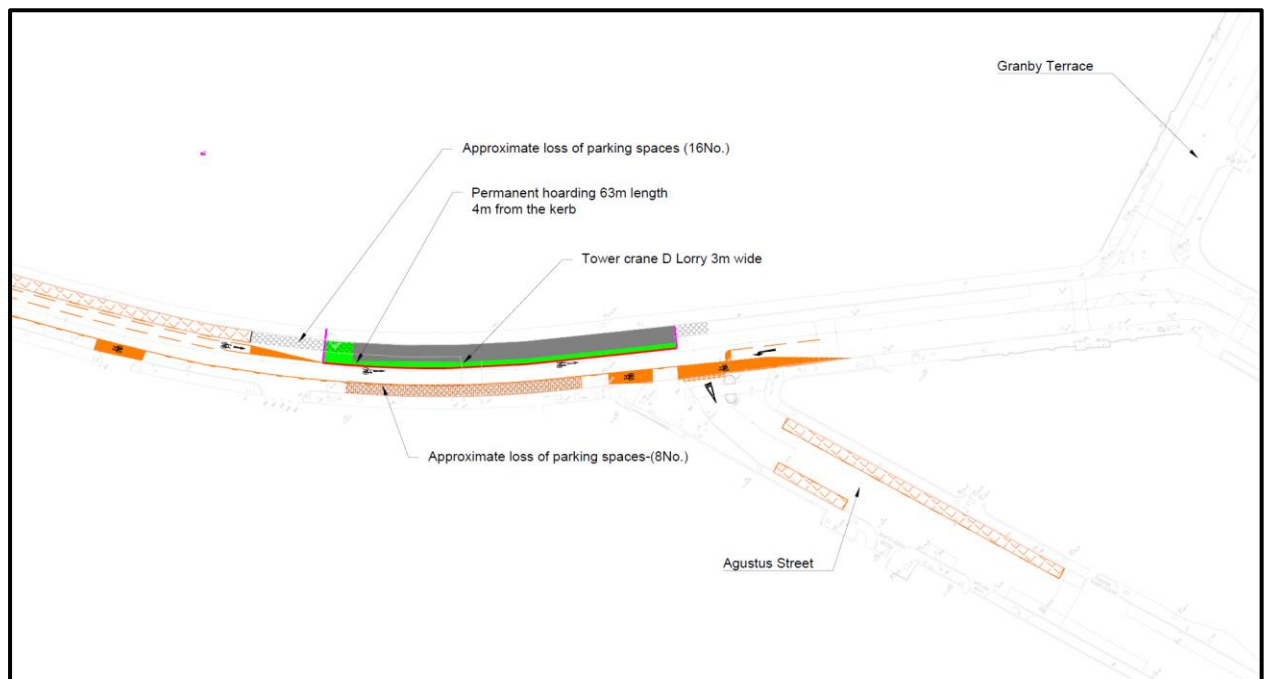


Figure 16: PVE Southern Layby Phase 2

Mornington Street Bridge

- 3.4.24 A closure of Mornington Street Bridge is required to accommodate bridge jacking works on the western abutment, as mitigation for the main works. These works are planned to be undertaken between September 2023 until December 2027 – subject to change and further main works ground assessments. Vehicular traffic will be prohibited to use Morning Street Bridge during this period; however, pedestrians and cyclist will be unaffected. Any mitigation proposals are to be discussed with LB Camden such as extension of directional one-way section along Park Village East.
- 3.4.25 The pilasters on either end of Mornington Street Bridge, a small worksite may be required to expose the bridge joints for the jacking works. Traffic management will be required to undertake the works to pilasters most likely by way of footway closure and storage space, but the exact arrangement are subject to further review.
- 3.4.26 Due to the Euston Approaches work site removing Network Rail track access a number of options are being considered to provide Network Rail track access from Mornington Street Bridge via a Haki staircase and hoist over the bridge parapet wall to be installed before Easter 2023. The proposals are still in the

feasibility stage and will be discussed with LB Camden and the local community as more information becomes available.

- 3.4.27 Due to the closure of Morning Street Bridge, Park Village East is proposed to be made one-way continuing the one-way north of Mornington Street southwards to Augustus Street or a point agreed with Camden. Residents will have to utilise Park Village East northern end junction with Parkway solely for vehicular access. When the bridge is closed construction traffic will not be utilising Mornington Street Bridge, Mornington Terrace or Delancey Street. During the closure of Mornington Street bridge construction traffic serving Euston Cavern Shaft compound will utilise the roads as described in Section 6.
- 3.4.28 As part of the Park Village East wall mitigation measures, it is currently proposed to install permanent soakaways to drain the surface water into the water table beyond the retaining wall. The exact number and location of soakaways is unknown and is currently being designed. To install the soakaways, some short-term impacts to parking and footway/lane closures will be required which will be managed under temporary traffic management.

3.5 MDJV Euston Station (Early Main) Works

NTH Main Works compound including Zone 5

- 3.5.1 The majority of works in this package comprise of test piling, preparation of pile mat areas and working platforms, completion of west wall piles and capping beam, early piling relocation of internal ramp, obstruction removal, utility works and other minor enabling works necessary ahead of starting the main works such as moving around internal site compounds and access roads as well as site storage to suit the delivery teams. From Q1 2023 early main works piling will commence together with Ground Source Heat Pump (GSHP) borehole installation. This will include the installation of ground anchors and associated excavation works. These activities are encompassed within the site boundary and do not have impact to public highway.
- 3.5.2 During Q2/Q3 period of 2023 utility works to Man-hole 1 (MH1) require changes to the position of Cardington Street junction to enable access to the NTH Main Works site. The MH1 is situated in the middle of the existing Cardington Street at its junction with Hampstead Road. *Figure 17* below shows the approximate concept of the new proposed access and relocated position of the junction to facilitate the works to MH1.
- 3.5.3 These works are envisaged to take place during Spring 2023 and new access to be operational from the second half of 2023. The duration of the MH1 works is anticipated to be 6 months after which the temporary relocated access will be reinstated to its pre-existing condition.
- 3.5.4 Traffic impact as a result of these works is expected to be from alternating contraflow arrangement on Hampstead Road that will enable construction of the realigned access.
- 3.5.5 Production of detail design drawings (inclusive of junction control) as well as TM drawings is ongoing and will be consulted upon with the relevant highway authorities.

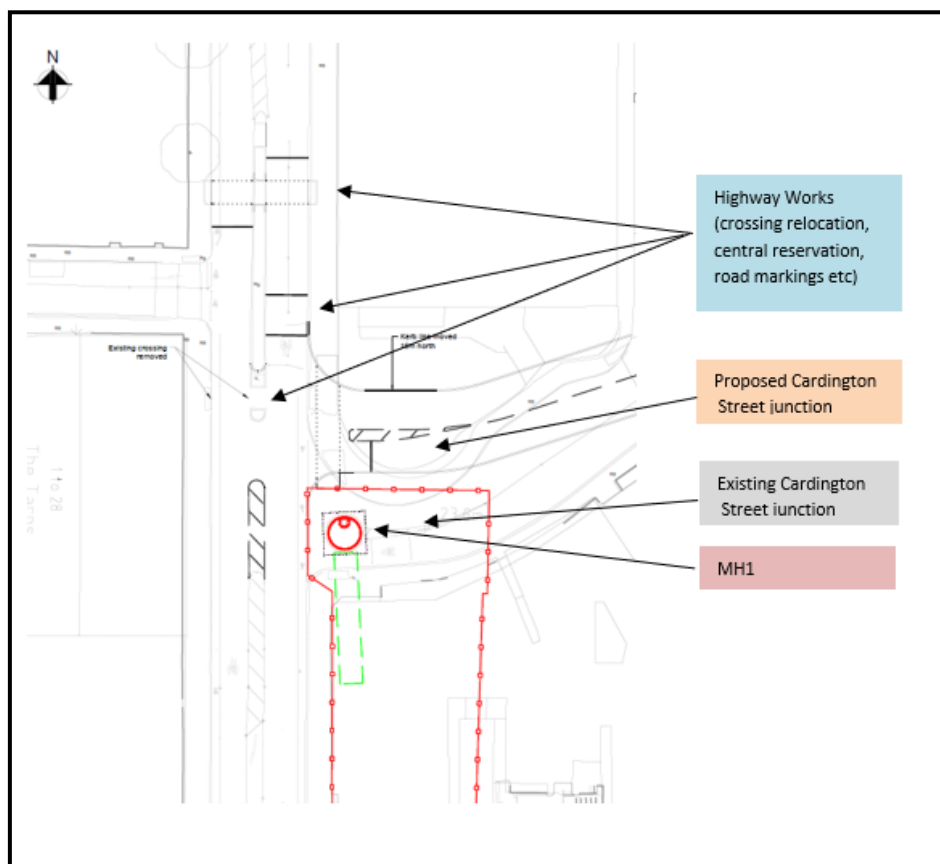


Figure 17: Cardington Street Manhole 1 and new access location sketch

Melton Street Satellite Site & Euston Garden Satellite West

- 3.5.6 The first phase of demolition for Two Towers (1 Euston Square) remains in progress. Following the completion of this first phase, the enabling works will commence such as piling mat preparations and temporary works installation followed by the early piling package in the London Underground interchange section.
- 3.5.7 Upon commissioning of Interim Taxi Rank (ITR) located in Euston Garden East, the current taxi rank situated in Euston Gardens West will be decommissioned. The Melton Street Satellite Site and existing taxi rank (Euston Gardens West Gardens Satellite Site) will be merged into a single site. The enabling works will consist of decommissioning and removal of all taxi rank elements, removal of settlement tanks below ground, vegetation clearance and heritage handrail removal as required and installation of working platform. Some of these works are likely to have minor impacts on the northern footway of A501 Euston Road.

3.5.8 Following the enabling works in the Melton Street/Euston Gardens West compound (merged) the mobilisation and construction for Tunnel Access Shaft (TAS) will commence. This is anticipated to comprise utility diversions, piling, capping beam, excavation followed by utility corridor sewer and water main installation along the perimeter of the new LU interchange box. This compound will see the commencement of piling to form south and west LU interchange retaining walls.

3.5.9 The only element anticipated out of above enabling works that may have traffic impact is associated works with TAS construction due to proximity of the piling activity to the existing footway. The northern footway on this section of A501 Euston Road may be constrained. The degree of footway impact will be confirmed following detailed design and approved via Schedule 4 consenting regime.

Cobourg Street Satellite site (Traction Sub-Station)

3.5.10 This work package is the continuation of the existing construction of the replacement London Underground traction substation building and tunnels which will replace upon completion of the existing Traction Sub-Station on Drummond Street. These works are already consented under Schedule 17 Ref No: 2021/4406/HS2 (see *Section 2* above). The latest programme for the balance of these works is summarised below:

TRACTION SUBSTATION WORKS	START DATE	END DATE
RC substructure and superstructure	Jun 2022	March 2023
Headhouse fit out and envelope, MEPH, power fit out works and power commissioning	April 2023	Jan/Feb 2025

3.5.11 The peak lorry movements with these works (tunnelling and excavation) are now complete. The projected numbers for the balance of works are provided below:

Worksite	Contractor Forecast HGV trips*	Contractor Forecast Estimated period with busy vehicle movements (months)	Contractor Forecast Estimated duration of use (years)
Cobourg Street Compound (TSS)	15 - 60	8	3

**Average daily combined two-way vehicle trips during busy period and within peak month of activity.*

- 3.5.12 Following permanent stopping up of Stephenson Way, between points P5 and P6 on the deposited plans, the remaining public highway is now subject to a Temporary Traffic Regulation Order to allow it to operate as a two-way street. In response to U&As 2669_07 and 2669_08 relating to measures that will allow vehicles to enter and leave the street in forward gear during all periods of the Euston works, as far as reasonably practical, a temporary hammerhead is proposed. This will allow certain categories of vehicle to turn at a location between No 12 Stephenson Way (Magic Circle) and the proposed permanent headhouse currently under construction, see *Figure 18* below. The detailed arrangement for the proposed hammerhead will be subject to Schedule 4 consent.



Figure 18: Proposed TSS hammerhead

- 3.5.13 These mitigation works will be progressed under Schedule 4 consenting regime and are not anticipated to have major traffic impact during construction. If feasible additional parking bays will be provided at this location and discussed with LBC.

Euston Gardens Satellite East

- 3.5.14 The enabling works for this works package have already commenced and the site has been established.
- 3.5.15 Enabling works comprising of elements such as site clearance and preparation within the gardens area, instal of gates for access and egress, cut and fill for formation level, removal of pedestrian crossings on the war memorial island have been completed. Works remain to finalise the taxi rank arrangements and majority of these works will be within the established site. Minor highway works are expected to support the new ITR and will comprise of works within existing private area (internal) that will be subject to agreement with TfL and public highway (external).
- 3.5.16 These works will comprise of:
- Minor crossing improvements within ITR and outside of public highway (around War Memorial and ITR compound exit)
 - Road markings alterations to support new bus and taxi interface once commissioning takes place within ITR and outside of public highway
 - Eversholt Street/Bus Station access and egress junction to the west (south of The Tower One Eversholt Street) slight reconfiguration of the junction by way of island reconfiguration to accommodate both bus and taxi access. Minor nearside short term lane closure is expected.
 - cycle ramp tie-in into the northern footway of Euston Road heading eastbound prior to traffic lights at Euston Road/Eversholt Street junction
 - southern junction access and egress from Euston Road, kerblines and pedestrian crossing alteration and improvement to support new arrangement. Offside lanes adjacent to central reservation on Euston Road at the pedestrian crossing junction are expected to be impacted on Euston Road.
- 3.5.17 Lorry access and egress routes for this work compound are contained in Section 6. Following consultation with TfL, the option of using secondary egress via the war memorial will not be pursued.
- 3.5.18 Works are currently programmed to test and commission the Interim Taxi Rank in April 2023.

Gordon Street Satellite site – Main Works

- 3.5.19 In order to deliver the LU Western Entrance Building the main works at Gordon Street site will comprise of piling the new escalator box, constructing the capping beam, excavating to tunnel level and constructing the RC support structure for the escalator and entrance building.
- 3.5.20 These works are anticipated to commence in Q2/Q3 2024. Further details regarding traffic impacts will be provided during subsequent revisions of this document when information is confirmed.
- 3.5.21 Once the main utility diversions commence (see below *Section 3.7*) with an anticipated start date of Q1 2023, the closure of Gordon Street between A501 Euston Road and Endsleigh Gardens will remain permanent.

Western Haul Road

- 3.5.22 The initial works will follow the completion of main utility corridor works around the perimeter of the new High Speed Station. They will comprise of pavement constriction to support construction vehicle through movement from A400 Hampstead Road to A501 Euston Road (via Cobourg Street east).
- 3.5.23 To support the Western Haul Road access, improvements to the junction with Hampstead Road (east side) will be required at a point immediately south from Robert Street.
- 3.5.24 These works are anticipated in Q1/Q2 2024. Further details regarding traffic impacts will be provided during subsequent revisions of this document when information is confirmed.

Euston Road

- 3.5.25 This package of works is required to facilitate the construction of new London Underground entrance south of Euston Road at Gordon Street. A new subsurface connection between Euston and Euston Square Underground stations is to be provided allowing for accessible underground links between the HS2 station platforms and London Underground. Utility diversions are required to enable the construction of these future underground links and platforms on both northern and southern sides of the road.
- 3.5.26 To accommodate the works there will be impact to A501 Euston Road. The works impacting traffic are expected to start in Q1 2023 and will take approximately six years to complete. The enabling works for Central Section (see below) that accommodate the key central reservation removal and junction signal works is

anticipated for approximately 6 months, starting at Q1 2023 until mid 2023. Current proposals involve lane reductions within a section of A501 Euston Road between North Gower Street and Eversholt Street for which central reservation removal is necessary. The works will move in stages and varying Traffic Management arrangements will be required to implement proposed two traffic lanes in each direction (eastbound and westbound).

3.5.27 Enabling works will consist of:

- Junction signal modification at Melton Street/A501 Euston Road/Gordon Street
- Removal of the central reservation of Euston Road between North Gower Street and Eversholt Street
- Relocation of interim taxi rank from the west side of Euston Square gardens, to the east side. This new interim taxi rank will then remain in this location for the remainder of the HS2 construction until it moves to its permanent location to the north of the new station, adjacent to Hampstead Road. These works are already ongoing as part of the Euston Gardens East Satellite compound (Interim Taxi Rank)
- Permanent stopping up of Melton Street (to support envisaged surrounding pedestrianisation of the HS2 station). This arm of the junction will maintain as an access/egress point for construction traffic only.
- Closure of the junction of Gordon Street with Euston Road to facilitate the works. This will also be permanent as a new entrance pavilion for London Underground will be built in Gordon Street
- Removal of the eastbound cycle lane, relocation of cycle stands and the Santander bike docks on Euston Road, and relocation of E/B and W/B bus stops on Euston Road (in the second phase of TM)

3.5.28 Please see below figures for draft proposals for Euston Road North Section, South Section and Central Section for which Traffic Management proposals are to be drafted. All works are to be undertaken in consultation with the relevant highway authorities.

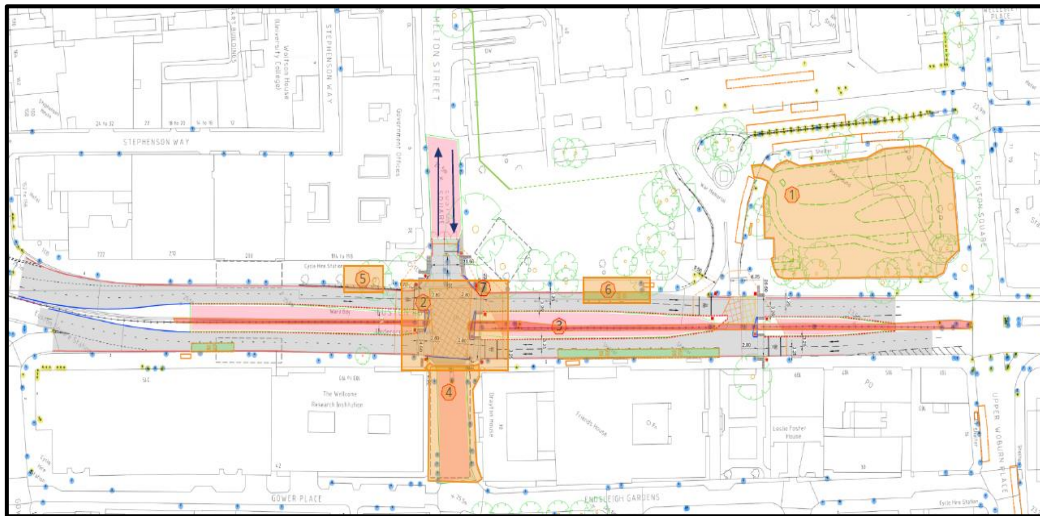


Figure 19: Euston Road Central Section works (primarily consisting of Junction signal modification by TfL and removal of central reserve by MDJV)



Figure 20: Euston Road North Section works (primarily consisting of cycle bay relocations, bus stop removal/relocation by TfL and signal modification)

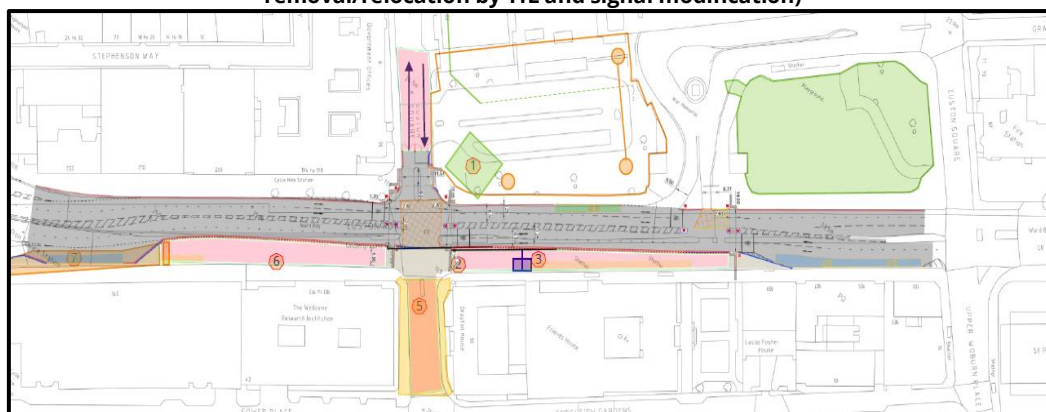


Figure 21: Euston Road South Section works (Primarily consisting of utility diversion and installation)

- 3.5.29 The impact of the works and change in the carriageway arrangement will have some impact to existing bus routes. This impact will be assessed and reviewed with TfL buses as part of the consenting regime to undertake the works.

HS2 Network Rail (On Network Works – ONW) Enabling Works

- 3.5.30 The proposed HS2 station and its approaches requires land that is currently occupied by the operational Network Rail Euston Station and various lineside assets. These will all need to be completely reconfigured to enable the implementation of HS2 services. The project, known as 'HS2 ONW', consists of multiple works packages to be carried out over approximately 5 year period.
- 3.5.31 The first phase of site enabling including the demolition of platform 17 & 18 and associated train shed roof are nearing completion and further site enabling works to support High Speed Station 2 Single Phase station are under way.
- 3.5.32 These works comprise of further demolition works up to platform 15 and include demolition of Western Accommodation Building (WAB). Access to these works is currently assumed north via MDJV site access gate onto Cardington Street. Primary egress will be via Cardington Street and Hampstead Road. Secondary egress will be through MDJv worksite and Melton Street onto Euston Road.
- 3.5.33 HS2 ONW are subject to separate LTMP and are included here for information only. Current revision of the ONW LTMP (A03.2), see *Section 2*, indicates that none of the ONW Skanska worksites will exceed the 24 movements (>7.5t) per day that would require Lorry Route Approval under Schedule 17 of the HS2 Act. In the event that this requirement is predicted to be exceeded, then Skanska and NR will apply for approval in advance from LBC.

3.6 SCS Utility Works

- 3.6.1 See Section 3.3 for SCS utility works taking place in the north of Camden including sewer works on Adelaide Road.
- 3.6.2 There are four main packages of utility works taking place in the Euston Approaches
- **Hampstead Road Phase 1** – Clearing utilities from sections of Hampstead Road in preparation for future civils works
 - **Granby Terrace Mitigation Works** – Diversion of existing networks across Granby Terrace in preparation for civils works on Hampstead Road

- **Parkway utility Mitigation** – Mitigation of utility services in the Parkway area
- **Hampstead Road Phase 2** - Connection of diverted services into the existing networks at either end of Hampstead Road bridge.

- 3.6.3 During the Hampstead Road works, utilities will be temporarily diverted along Granby Terrace, which will allow for the construction of the Hampstead Road bridge extension. During the tunnelling activities, some utilities will be required to be protected, diverted or replaced.
- 3.6.4 Whilst undertaking tunnelling works some utility diversion/protection works will be undertaken, *Figure 22* below shows the areas within LB Camden, the full extent of works is unknown at this stage. Surveys, such as CCTV and Ground Penetrating Radar (GPR) will be undertaken to determine the condition and location of utility assets to inform detailed design.
- 3.6.5 Further details and timings will be provided when known and through appropriate Schedule 4 submissions as well as outlined at the relevant TLG meetings. Further to this collaborative approach on the future utility works led by HS2 has been established via LBC Utilities Coordination meeting. Utilities forum has been established to provide visibility and coordination of the works. GIS tool is being developed by HS2 for the purposes of forward planning and clash detection and greater visibility between HS2 contractors and other parties.
- 3.6.6 Details of the utility works affecting the highway can be found listed within *Appendix 1 and 1A*. Further details of the required works will be developed within detailed design and updated in future versions of this document.

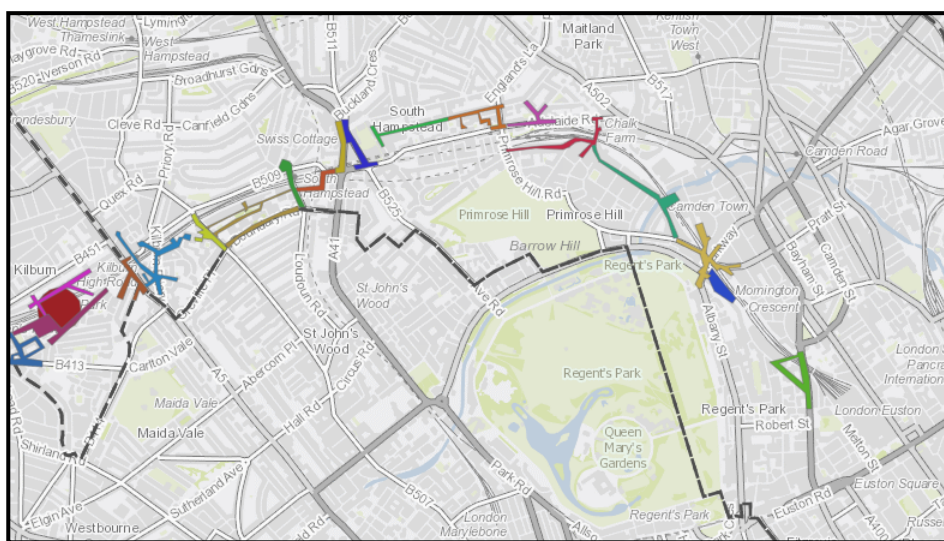


Figure 22: Overview showing SCS areas of works on utilities within LB Camden

3.7 MDjv Utility works

- 3.7.1 As part of the overall HS2 works MDjv will be continuing to divert utilities and connecting to the works already carried out by HS2 Early Works Contractor CSjv. An overview of current known MDjv Utility works is provided in *Figure 24* which is relevant until end of Q4 2023.
- 3.7.2 Surveys, such as CCTV and Ground Penetrating Radar (GPR) will continue to be undertaken should they be required to determine the condition and location of utility assets to inform any detailed design.
- 3.7.3 These works are mainly low impact with regards to traffic management required, short durations, sometimes undertaken overnight and may require temporary suspension of parking for both the surveys and actual works.
- 3.7.4 Details of the more significant utility works affecting the highway can be found below.

Gordon Street Satellite site – utilities

- 3.7.5 Works for this package will commence with a permanent closure of Gordon Street between Endsleigh Gardens and A501 Euston Road. Works will comprise of site compound set up followed by utility works for relocation of all existing utilities to the centre of the road in order to enable the majority of the piling for the proposed Escalator box down to the tunnels connecting to the Metropolitan and Circle lines.
- 3.7.6 The closure of the Gordon Street will prevent through access to A501 Euston Road and Taxi Rank. In addition to the closure of the road to vehicle traffic, some alterations will be required for pedestrians. At least one side of the footway, either eastern or western will be maintained for public and once the first stage of piling is complete, the utilities will be diverted into their final alignments in the footpaths either side of the piled box. Pedestrian movement across Gordon Street junction with Euston Road will be maintained for the majority of these works. Pedestrian diversions may be required at the early phase to support site investigations within the mouth of the junction. We are currently engaging with non-contestable utility asset owners to confirm proposed requirements.
- 3.7.7 To facilitate the closure of this section of Gordon Street, signal modification works will be required at its junction with A501 Euston Road. This is covered under Euston Road Utility Works in this section.
- 3.7.8 Anticipated start of these utility works is beginning of 2023 for approximately one year.

Cobourg Street Utilities

- 3.7.9 The Utilities North work package will require progressive closure of Cobourg Street in various Traffic Management phases culminating in the full closure of the street as part of those works.
- 3.7.10 As part of the worksites, gates will need to be provided to enable access to third parties such as LU and UKPN and potentially refuse collections. Discussions are ongoing regarding access arrangements. General public will not have vehicular access via Cobourg Street at any time during the works but footway access will be maintained (western footway).
- 3.7.11 Upon closure of Cobourg Street, access for large construction vehicles will be via NTH Main Compound to the East of the site. Materials will be moved to and from the site via NTH Main Compound gates at Cardington Street and Melton Street to alleviate the need to utilise any local roads. The hoardings between Cobourg Street and NTH Main Compound may need to be adjusted to facilitate these works.
- 3.7.12 Once the Traffic Management is established, the necessary utility diversions will commence and be followed by the installation of all the shallow and deep elements of the Main utility corridor, gas water, comms, and sewer.
- 3.7.13 Traffic impact due to work site requirements will be to the sections of Starcross Street, Drummond Street and Euston Road between North Gower Street and Cobourg Street. It is currently proposed to change the operation of these streets to two-way working to enable access and egress due to closure of Cobourg Street. This may also have an impact on parking dependant on the duration of the works.
- 3.7.14 These works are anticipated to start in Q4 2022 / Q1 2023 and last approximately for 9 months.

Euston Road Utilities Works

- 3.7.15 Please see *Section 3.5* above regarding proposed works on A501 Euston Road. Enabling works whereby utility diversions that will be executed are to be undertaken as part of North and South section scope of works detailed above. Works are to comprise of diverting a major sewer 1.5 metres in diameter for approximately 750 metres at varying depth between 5-8 metres below ground level.

Hampstead Road / Robert Street junction - Water Main crossing

- 3.7.16 This works package is part of main utility corridor and intrudes into the public highway between Hampstead Road east footway approximately 20 metres south from the junction with Robert Street heading north up to the junction of Hampstead Road/Robert Street.
- 3.7.17 The crossing will be completed in phases in order to maintain traffic flow under traffic management. The first phase is to install the opencut pipework across the southbound lane and eastern footpath and install pipework for the future two 16 inch cross connections. The final phase is to install the pipework within Robert Street junction connecting to existing water main when full closure is expected. For both phases it is anticipated to undertake the works under contraflow arrangement on Hampstead Road. Diversions will be required with associated traffic management phases to facilitate mitigation for Regent Park estate access and egress as well general public
- 3.7.18 These works are anticipated to start in November 2022 and last for approximately 5 months.

Hampstead Road – Asset Protection

- 3.7.19 This work package will involve works to the 16 inch Thames Water trunk main potable water and gas main on Hampstead Road from approximately Robert Street Bus Stop J in northerly direction up to approximately Cartmel Building. As part of the Ground Monitoring Assessment inline replacement has been identified as a mitigation measure to the trunk main and gas main.
- 3.7.20 These works will be completed via opencut method and will be completed in phases to minimise disruption on Hampstead Road. Currently phased contraflow arrangement is envisaged for these works. As part of the traffic management arrangement road closures will be required to Varndell Street and Robert Street during the phasing of the works. Diversions will be required with associated traffic management phases to facilitate mitigation for Regent Park estate access and egress as well general public. Any diversion will be subject to approval by relevant highway authority. Minor to no pedestrian impact is expected.
- 3.7.21 These works are anticipated to start in Q1 2023 and last for 7 months.

Eversholt Street foul connection

- 3.7.22 Following trial hole investigation works in August 2022 to confirm the position of Network Rail foul connection it was determined that initial early works enabling contractor design required further refinement.

- 3.7.23 A re-design of the foul connection is in progress to accommodate the outcome of the trial hole outcome and is anticipated to be complete in Q3 2023 with works commencing Q4 2023 lasting until Q1 2024.
- 3.7.24 The worksite will encompass area just north of One Eversholt Street – The Tower, whereby a single lane past the worksite will be maintained. Northbound traffic will be maintained whereas southbound traffic will require diversion via Hampstead Road and Euston Road.

Eversholt Street - Non-contestable works

- 3.7.25 There will be a number of potential non-contestable works on Eversholt Street during 2022/2023 period, whereby extent of these works is shown in *Figure 23* below by different Utility Asset Owners. These works will be coordinated with HS2 works and local highway authorities through TLG and any other specific fora.

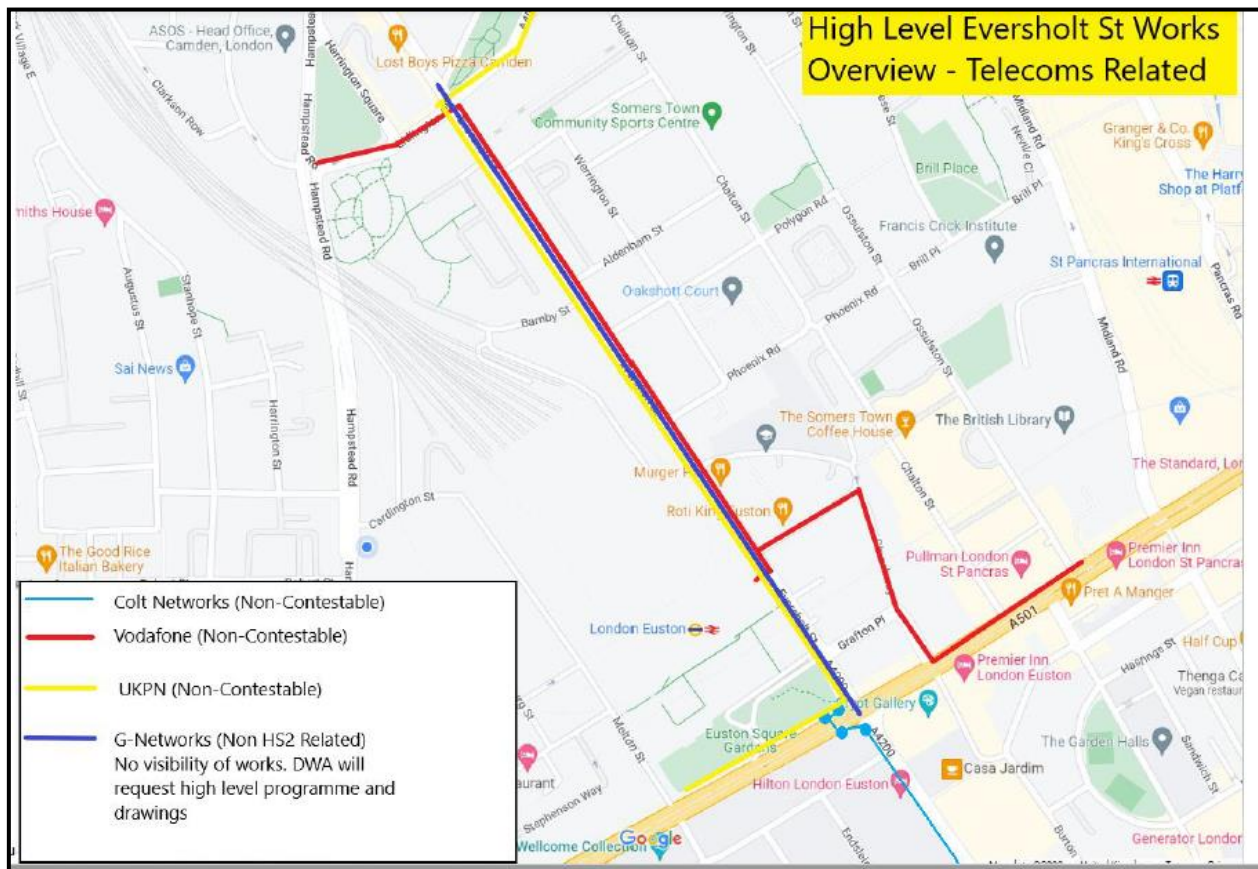


Figure 23: High Level non contestable Eversholt Street Works

Euston Street between Cobourg and North Gower Street

- 3.7.26 This utility works package will comprise of sewer regrading and installation of a new deep sewer between Euston Street and junction of North Gower Street where a new manhole will be constructed at the junction with the existing sewer along North Gower Street.
- 3.7.27 Local traffic impacts are anticipated on Euston Street between Cobourg Street and North Gower Street. The road may need to potentially facilitate two-way movement and parking suspensions may be needed to provide passing points.
- 3.7.28 It is likely that these works cannot be undertaken concurrently with the closure of Cobourg Street due to access/egress arrangement. As such, these works are not anticipated to start before Q2 2023. Duration of these works is anticipated for approximately 9 months.

Starcross Street

- 3.7.29 This utility works package will comprise of sewer re-lining between Cobourg Street and North Gower Street.
- 3.7.30 It is likely that these works cannot be undertaken concurrently with the closure of Cobourg Street due to access/egress arrangement. As such, these works are not anticipated to start before Q4 2023.
- 3.7.31 Current assessment of Ground Monitoring Assessment is being undertaken and these works have potential to be descoped. Further information will be shared when available.

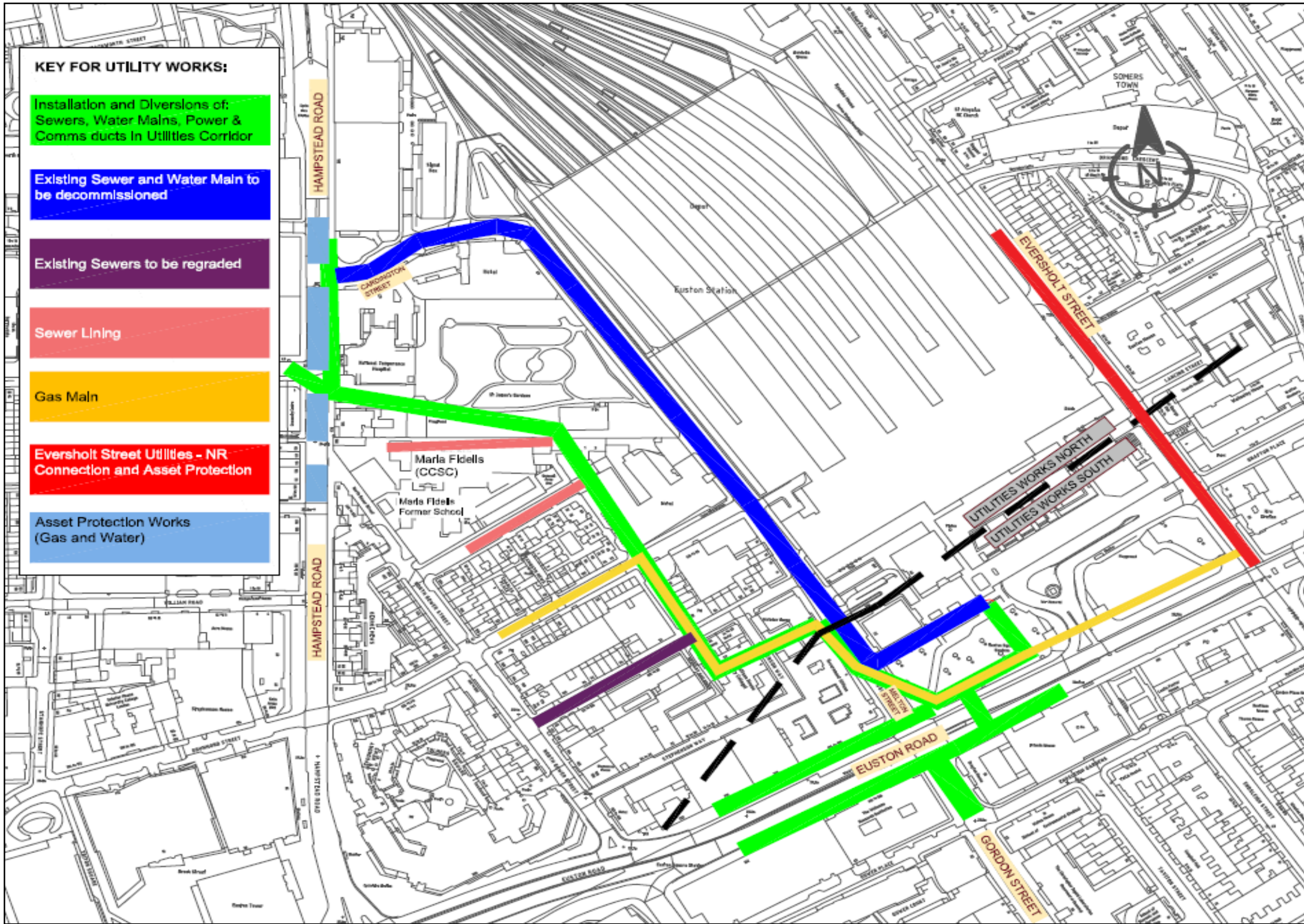


Figure 24: Indicative overview of MDjv Utility work packages until end of Q4 2023 and beyond

3.8 HS2 Concurrent Works Coordination

- 3.8.1 The table below summarises the key works highlighted in the S1 and S3 HS2 contractor Forward where any potential concurrent works coordination may be required or has been identified as likely (See also Appendix 1 and Appendix 1A).

It should be noted that the scope of the analysis provided below does not capture any necessary internal coordination that would be carried out by each contractor before the works are proposed and prior to any forward plans raised.

Concurrent Works Identified		
Period	Identified Forward Plan Reference Number/s	Summary of Concurrent Works Coordination Review
Q1 2023	SCSjv TA011THW-TFL-SCS-5118_01-FP TA011-THW-LBC-SCS-5085-FP MDjv TA02043257462-FP TA02059705644-FP	<p>Proposed Vardnell Street closure (Ref No. TA011-THW-LBC-SCS-5085-FP) by SCS from Feb 2023 for approximately one month has been identified to require further Traffic Management review with proposed Robert Street closure (Ref No: TA02043257462-FP) by MDjv. As a way of mitigation period of the works has been reviewed and ongoing planning and phasing is being undertaken.</p> <p>Potential further works that may require coordination have been identified between above mentioned SCS Vardnell Street closure and proposed MDjv multi way signals traffic management arrangement during utility asset protection works (Ref No: TA02059705644-FP). As SCS works are shorter in duration in comparison to MDjv works phasing will need to be managed to avoid concurrent works.</p> <p>SCS HRB Phase 2 utility works (Ref No: TA011THW-TFL-SCS-5118_01-FP) by potential shift of existing northbound nearside lane uptake to southbound nearside uptake may have dependency on other works in the nearby area by MDjv utility works (Ref nos: TA02043257462-FP and TA02059705644-FP). These works are to be kept under regular review.</p>

Q2 2023	SCSjv TA011-THW-TFL-SCS-5147-FP TA011THW-TFL-SCS-5118_01-FP TA011THW-TFL-SCS-5118_02-FP MDjv TA02059705644-FP TA02048593472-FP	Potential further works that may require coordination have been identified between MDjv Cardington Street access realignment (Ref No: TA02048593472-FP) as well as utility asset protection works (Ref No: TA02059705644-FP) and SCSjv HRB Phase 2 utility works close to Varndell Street (Ref No: TA011-THW-TFL-SCS-5147-FP) and worksite establishment in the southbound nearside lane (Ref No: TA011THW-TFL-SCS-5118_01-FP). Phasing will be reviewed to minimise concurrent working impact. Non-contestable utility works as part of SCSjv scope have been identified that may potentially require further coordination with above works (Ref No: TA011THW-TFL-SCS-5118_02-FP)
Q3 2023	N/A	No works anticipated to require coordination during this quarter.
Q4 2023	SCSjv TA011-THW-TFL-SCS-5147-FP TA011THW-LBC-SCS-5086_02-FP MDjv TA02059705644-FP	There is a small likelihood in this quarter of further coordination required between MDjv utility asset protection works that would be close to completion in this period between SCSjv works which also would be towards end of their programme (Ref No: TA011-THW-TFL-SCS-5147-FP and TA011THW-LBC-SCS-5086_02-FP)

3.9 Coordination of other works

- 3.9.1 Works within the London Borough of Camden area will be co-ordinated within the Traffic Liaison Group and Camden Coordination Group meetings, with the aim to manage impacts on the surrounding road networks.
- 3.9.2 Co-ordination meetings are also being held with some of the developments/schemes that are under construction or that are near to starting construction with the aim of minimising impacts on the highway network, and to the wider community.
- 3.9.3 We will assist the highway authority with their network co-ordination duty as far as reasonably practicable, including attending regular NRSWA meetings if invited.

3.10 Preparatory Activities

- 3.10.1 Other works such as surveys, ground investigations and monitoring activities will be required to be undertaken prior to the start of and during the construction works.
- 3.10.2 Some of these activities are described within *Section 1.3* to inform detailed design of the proposed utilities works.

3.10.3 Further examples of preparatory for all assets/works are, but not limited to:

- Installing equipment, such as survey points
- Ground investigations
- Monitoring, including installing instruments
- Site establishment, including/installing hoarding
- Surveys

3.11 Social Impacts

- 3.11.1 We have limited options for locating our worksite access and exit points for our worksites, resulting in access – and therefore construction traffic – being close to schools and people’s homes. We understand there is significant community and stakeholder concern about accessing these worksites from residential streets and we are committed to keeping construction traffic to roads designed to take heavy goods vehicles whenever possible and reducing construction traffic on local road streets whenever we are required to use them.

Regents Park Estate impacts and mitigation

- 3.11.2 For SCS the Regents Park Estate lays to the south and to the west of the Euston Approaches worksite; and consist of high-density housing. The Euston Approaches worksite has 4 access points, one of which is at the northern most end of Stanhope Street where Stanhope Street meets Park Village East and Granby Terrace. In order to use this access point it is proposed to use local roads which includes the use of Stanhope Street and Robert Street, as a result of proposing to use the local roads within the Regents Park Estate, there are various local impacts that we are committed to reducing as far as reasonably practicable.
- 3.11.3 Local impacts include:
- the proximity of Netley Primary and Christ Church CE Primary Schools, and the Regents Park Children’s Centre to the proposed lorry route through the estate.
 - Volume of residents within the Estate, of which some are considered vulnerable.
 - Noise and air quality of the works and the use of roads within the estate to access the worksites.
 - Safety; and
 - Traffic – free flow of traffic and also “rat-running” through the estate
- 3.11.4 For worksites access points and indicative programme of use within the Regents Park estate and along Park Village East, please see *Section 3.4 and Section 4*, which minimises the use of internal estate roads. Key to mitigating is the reduction of HGV’s using local roads via the use of a haul road, management of the access points, school time restrictions, driver and vehicle safety, and the use of the Vehicle Management System. Others include the management of crossings, deploying customer experience reps, speed restriction and localised signage.

- 3.11.5 MDjv works do not require access through the Regents Park Estate but must be cognisant of the local impacts as noted above. During utility works described in *Section 3.7* some impact could occur by way of road closures to Varndell Street and Robert Street at junctions with Hampstead Road. MDjv will work with relevant stakeholders and highway authorities to minimise the disruption as reasonably as possible.
- 3.11.6 Possible mitigations for vehicles that are required to use Regents Park estate roads by SCS are:
- Use of Customer Experience Representatives or redeploying gate/ security staff if the need arises along the lorry route within the estate. These personnel will have similar duties to those deployed by the EWC contractor in the Euston area, with the additional focus on pedestrians/cyclists during school pick-up/drop-off hours.
 - Avoidance of school pick-up/drop-off hours (8:30-9:30 & 15:30-16:30) along Robert Street and Stanhope Street, with the exception of HGV's for piling and concrete activities or in unforeseen circumstances.
- 3.11.7 Abnormal loads under an Abnormal Load License to be agreed with the Police and/or Local Authority.
- 3.11.8 In spring 2021, SCS installed 10mph advisory speed signs along the approved lorry routes, which drivers of HS2 vehicles will be trained to observe even though the limit is not enforceable. These signs have been installed at following roads:
- Robert Street, entire length
 - Stanhope Street, from its junction of Robert Street to its junction with Park Village East
 - Park Village East, entire length
- 3.11.9 'Local access only' signs on the main entrances to Regents Park Estate and Park Village East.
- 3.11.10 Hampstead Road Haul road
- The use of this haul road would help reduce lorries on residential streets in the Regent's Park Estate because lorries could enter and exit directly from and

onto Hampstead Road (left in and right out). The haul road will predominantly service the area between and including the extensions of Granby Terrace Bridge and Hampstead Road. Challenges to extending the use of the haul road are the space constraints within the worksite limiting safe movement of vehicles and workers around large-scale equipment requiring exclusion zones and bulk excavation that will be under way, in addition to the works on the extension of Granby Terrace dissecting the site and not allowing safe passage. The duration of the haul road will be kept under review and opportunities to extend the use is being pursued, updates will be stated within future versions of this document. Please see *Section 4.1* below, which details the use and duration of access points.

- Haul Road access point on Hampstead Road was brought into use in March 2021, the access operated as left in/left out until June 2022. In late summer 2021 due to internal clash this access point was suspended and temporary Haul Road access 2 created to facilitate internal works. Since June 2022 the Haul Road access has been operating as a left in and right out onto Hampstead Rd, avoiding the use of the Harrington Square loop. Following a successful trial period, the right turn will remain in place whilst the access point remains in its current location, and it does not conflict with other works taking place on Hampstead Rd.

- 3.11.11 Granby Terrace bridge is currently stopped up and is not being used as an access point whilst bridge construction and utility works take place. Once the bridge is complete Granby Terrace Bridge could be used as an site access but this will be detailed in future revisions of the Camden LTMP.

Grafton Loop and Osnaburgh Loop

- 3.11.12 SCS vehicles that approach from the east of the works on A501 Euston Road that require access to Hampstead Road, are approved to use the Grafton Way Loop. An alternative is for vehicles to continue along A501 Euston Road to Great Portland Street, where vehicles will double-back using the Osnaburgh Loop, turning left onto Hampstead Road.

- 3.11.13 The proposal is to continue the use of the Grafton Way Loop for vehicles to and from Hampstead Road for SCS work sites, and vehicles travelling to the Lorry Holding Area (LHA) or are accessing the Regents Park Estate via Albany Street will use the Osnaburgh Loop.
- 3.11.14 For MDjv vehicles wishing to access Hampstead Road/Cardington Street and Melton Street from the east, the proposal is to only use the Osnaburgh Loop as per existing consented Schedule 17 lorry route. Any MDjv vehicles that are unable to utilise the Melton Street egress point and have to egress via Cardington Street and head eastbound on TLRN (A501 Euston Road) approaching from A400 Hampstead Road will also need join the A501 Euston Road westbound traffic and utilise the Osnaburgh loop to double back and head eastbound.

Harrington Square Gardens route mitigation

- 3.11.15 SCSjv Schedule 17 (Ref No: 2019/4700/HS2) see *Section 2*, allows Harrington Square Gardens to form part of the lorry routes for the Euston Approaches Worksite. SCS has in May 2022 implemented a right turn out of their worksite compound in agreement with TfL, hence, mitigating and reducing the need to utilise the Harrington Square Gardens as a loop. This is described in above for the Haul Road sub-section within the Regents Park Estate impacts and mitigation section.
- 3.11.16 MDJV has also requested approval for the limited use of the Harrington Square gyratory to access Gate 18 as part of the Interim Schedule 17 lorry route consent application (Ref No: 2022/5079/HS2). MDJV can confirm the following: LGV lorry movements travelling northbound on A400 Hampstead Road to the northern access of the NTH Main Worksite via Gate 18, to what is known as Zone 5, shall not exceed 12 LGVs per day. The capacity of the Gate shall not exceed 12 LGVs per day (24 two-way movements).
- 3.11.17 No other HS2 LGVs would use the Harrington Square gyratory in relation to the Interim Schedule 17 Lorry Route approval (Ref No: 2022/5079/HS2). This is with exception of occasional usage of the Harrington Square gyratory to enable LGVs to turn around, in the unlikely event that they miss or are unable to make the right hand turn into the NTH Main Worksite from A400 Hampstead Road, in which case there would be no reasonable, safe and approved means of turning around.
- 3.11.18 For vehicles accessing Gate 18 of the NTH Main Worksite, the Harrington Square gyratory shall only be used, in the eventuality that no right-hand turn is available to vehicles accessing Gate 18 from A400 Hampstead Road heading northbound. If a right-hand turn is available, Large Goods Vehicles accessing the NTH Main

Worksite, Gate 18 from Hampstead Road heading northbound shall not use the Harrington Square gyratory.

- 3.11.19 The limited additional use of Gate 18 will not introduce new or additional significant adverse environmental effects and is deemed to be EMR compliant.
- 3.11.20 For further information on the options considered for accessing Gate 18, please see *Appendix 5*.

Other mitigations considered within LB Camden that have been rejected or are under consideration

- 3.11.21 Investigation of opening the left turn from Hampstead Road onto Euston Road eastbound, to avoid the use of Grafton Way loop.

SCS has considered and rejected the implementation of the left-hand turn onto Euston Road for the following reasons:

- This arrangement was not assumed in the ES and could potentially create additional significant effects.
- Introducing a left turn will either require a loss of a protected pedestrian stage or an all-red phase, which will lose green time for traffic to exit from Tottenham Court Road and therefore back up traffic from around the Grafton Way loop, interfering with the hospital A&E.
- Works to signals cannot be carried out without TfL agreement/procurement of the equipment. TfL have sent a letter to HS2 objecting to the said left hand turn.
- UCH have stated that they would have concerns with the left hand turn due to safety concerns for its staff located in Tomler's Square area which is at the eastern corner of the Hampstead Road/Euston Road junction.

- 3.11.22 Right turn from Robert Street onto Hampstead Road

- 3.11.23 Previously it was proposed to open the right turn from Robert Street onto Hampstead Road. However, the proposals to create a signalised access from Hampstead Road into the site could create traffic impacts to the Hampstead Road corridor, therefore, in favour of utilising a right-turn from the Hampstead Road access, hence, the proposal to signalise the right turn from Robert Street will not

be progresses. This proposal will be kept under review, updates will be stated within future versions of this document.

Route-wide mitigation

- 3.11.24 Other mitigation will be adopted to help further mitigate any local impacts; these are to be adopted throughout our works within other areas these are:
- Driver vehicle safety and awareness training, teamed with education for schoolchildren around construction traffic and vehicles
 - Potential use of larger, environmentally friendly lorries, which can take higher volumes of excavated materials away from worksites, meaning fewer loads and vehicles.
 - Use of hi-direct vision lorries.
- 3.11.25 As we progress with our design and construction planning prior to starting on site, we will continue to search for alternatives which will help reduce construction traffic impacts on residential streets. We will address any new detail in future versions of this plan.
- 3.11.26 We will continue to engage TfL, London Borough of Camden and other stakeholders.
- 3.11.27 In accordance with U&A 1108 in the development of future versions of this LTMP, to help minimise and/or manage potential impacts of our works and to build a better understanding of the community.

3.12 Engagement

- 3.12.1 We have been engaging stakeholders in Camden, which includes community members, to understand the ways in which the community in these areas operates and functions. Feedback is considered in the ongoing development of our local traffic management planning.
- 3.12.2 Engagement includes a series of workshops, briefings, presentations, community events, drop-in sessions, individual meetings, walk-arounds and HS2-led cross-contractor forums. Key stakeholder groups engaged to date include:
- London Borough of Camden officers
 - TfL

- HS2, CSJV, MDJV
- Camden Community Traffic Working Group (CCTWG) & Camden Traffic Liaison Group (TLG)
- Origin Housing
- Local cycle groups
- Nearby schools
- Local residents, resident associations and groups
- Businesses groups including Stephenson Way (SWG), Drummond Street Traders DST), University College London (UCL)

- 3.12.3 Within the Traffic Liaison Group (TLG) meetings, the project is consulting the highways and emergency services on the current and future works, including routes to be used to and from sites. Emergency and blue light services are invited to attend the TLGs, however, we are undertaking additional engagement with them to focus on main works traffic matters.
- 3.12.4 The project's community engagement team ensure the local community is informed of any works within the area. Communication takes place via correspondence such as advanced notification letters to individual properties; updates at the Community Traffic Working Group (CTWG) meetings; pop-ups and other events and responding to enquiries.
- 3.12.5 We continue to engage all relevant stakeholders through a variety of activities to ensure we have a current understanding of the communities in which we are working. We welcome feedback at any time via the range of engagement channels. We take feedback into account as we continue to seek ways to reduce the impacts of our works.

Key issues

- 3.12.6 Key issues identified through ongoing engagement in earlier revisions of this document are believed to have been addressed and all responses provided.
- 3.12.7 Any further key issues that are to be identified during engagement of this revision will be captured here. We consider all feedback from other engagement plans, community and stakeholders in assessing options for our construction traffic routes.
- 3.12.8 Sensitivity around certain routes

- Regents Park Estate and surrounds
 - Stanhope Street
 - Robert Street
 - Regents Park Children's Centre (under 5 years)
 - Netley Primary School
 - Christ Church CE Primary School
 - Harrington Square
 - Harrington Street
- Prince Albert Road
 - North Bridge House Prep School (aged 7-13 years)
- Adelaide Road
 - UCL Academy (Year 7+)
 - Sarum Hall School (aged 3-11 years)
 - Haverstock School
- Park Village East
 - Concerned residents
 - Concerned business (Park Village Ltd)
- MDJV Northern Concrete Route
 - Proximity to Richard Cobden Primary School
 - Proximity to Brecknock Primary School
- Other
 - Delancey Street
 - Mornington Terrace
 - Mornington Crescent
 - Mornington Street
 - Arlington Road
 - Albert Street
 - capacity at Gloucester Gate
 - TfL and LBC support the idea of considering the 'Healthy streets' strategy in the final design of Hampstead Road

3.12.9 Safety, related to children and schools in particular

- regular walking routes, particularly through the Regents Park Estate
- busy traffic and pedestrian times at school access points
- pick-up/ drop-off areas and crossing points (consider traffic marshals)
- general numbers of children and parents in the area

- proximity of schools and nurseries to HS2 sites and construction traffic
- safe segregation of schools and public play areas and facilities

3.12.10 Re-use materials as often as possible

- Include onsite concrete batching plant if it helps to reduce deliveries

3.12.11 Avoid local roads as much as possible

- Use TLRN and Strategic Regional Network roads as much as possible
- Install and use haul road for use when site conditions allow, with access/egress from Hampstead Road
- Use right turn onto Hampstead Road from Euston Approaches worksites when possible
- Take shortest/fastest route to TLRN and SRN
- Restrict use of minor roads wherever possible
- Give reassurance that contingency routes on minor roads will only be used when necessary
- Lorry Holding area locations relative to site access
- Compliance checks and weighing at site rather than LHA

3.12.12 Materials by Rail

Material by Rail (MbR) would consider moving a proportion of excavated material by rail from Euston station, in addition to the use of the road network. The feasibility of utilising MbR for works in the Euston Station area is, at the time of writing, under consideration by Lord Oakervee – a final decision on the implementation (to an as-yet undetermined level) of MbR is anticipated in late 2022.

All of the Environmental Minimum Requirements (EMR) compliance analysis to date has excluded MbR and considers movement by road only, as outlined in the Environmental Statement (ES).

The 'Interim' phase of works for Euston Station are for early site enabling works (including pile mat construction) and early works piling between January 2023 and December 2023. These works will utilise routes that are stated within *Section 6* and will form part of Schedule 17 Lorry Route Approval submission for this period.

3.12.13 The outcome of the decision on MbR will have little to no effect for the MDjv 'Interim' phase of works, as the project would not be in a position to utilise rail transport. This is due to the lead time (design, procurement, consenting and construction) required for a suitable MbR facility, which could not be ready in 2023.

3.12.14 Hampstead Road closure

- restrict duration of any closures
- aim for closure during August, school holidays or quieter periods
- early communication about road closures (up to 18 months with TfL)
- consider how to avoid rat running during closure

3.12.15 Site access

- understand access points and frequency of access requirements at each
- temporary v permanent access points (and size of vehicles using permanent access)

3.12.16 Parking

- Parking is critical to residents
- Clarity about relocated parking if any

3.12.17 Public transport

- Relocation of bus-stops and bus diversions requires extensive engagement with TfL
- Delay to bus services

3.12.18 Cyclists

- Safety concerns of cyclists mixing with HGVs

3.12.19 Coordination with other closures and works

- TfL's Cycle Superhighway 11, which would close Outer Circle (project not proceeding)
- LBC's cyclist/ pedestrian scheme – reduces capacity in favour of pedestrians and cyclists
- phase works to avoid programme conflict

Response to key issues

- 3.12.20 Key issues from previous engagement and any new issues that are raised during the planned period will be outlined within this section.
- 3.12.21 We are highly conscious of the sensitivity of using residential streets, particularly those in the Regents Park Estate. However, as our site access is at the end of one of these streets, we have no alternative than to use these streets. We have succeeded in keeping construction traffic to strategic routes, designated for heavy goods vehicles in most cases whilst our site permits the use of the haul road. It is necessary to use Stanhope Street via Robert Street to travel from the strategic route to our site access for extended periods when it is not feasible, safe or possible to continue use of the haul road within site.
- 3.12.22 We continue to provide local schools with offers of road safety campaigns as well as skills and educational workshops. Schools receive regular communications about our works so schoolchildren, teachers, parents, and road users are safe and aware of the construction in the nearby area.
- 3.12.23 LBC has confirmed the acceptability of the northern concrete route in their comments on the Interim Schedule 17 lorry route consent application (Ref: 2022/5079/HS2), but raised concerns relating to the safety of vulnerable users, particularly at junctions and around primary schools which are located on the route or nearby.
- 3.12.24 In addition to the recent highway improvement measures that have been introduced as part of the Council's response to the COVID-19 pandemic, we note that a number of other safety measures have been implemented to protect the two primary schools in the vicinity of the northern concrete route. Brecknock Primary School forms part of the Council's Healthy School Streets Scheme and a permanent timed road closure is now in place on Cliff Villas Monday to Friday during school term time between the hours of 08:00 - 09:15 am and 15:00 - 16:00.

- 3.12.25 Richard Cobden Primary School situated on Camden Street, which forms part of the TLRN network, is protected by existing street furniture (railings) that extend some 50m north of the main school entrance and the full length of the footway south to the junction with Crowndale Road to the south. The school's operational policy also requires parents to drop off and pick up children from within the school playground and to remain with the children until they have been collected.
- 3.12.26 The School's Engagement Sub-framework for Camden already includes Richard Cobden Primary School. MDJV will engage with both schools identified by Camden to establish whether they wish to participate in HS2's traffic safety education programmes, in partnership with the Metropolitan Police Commercial Unit. We welcome LBC's offer to support with any engagement with these schools.
- 3.12.27 We will be working in accordance with the following Undertaking and Assurances:

U&A 1160 – “The Secretary of State will require the Nominated Undertaker to seek reasonably practicable measures to further reduce or avoid significant effects on schools, in doing so the Promoter will continue to consult the London Borough of Camden and the schools. Such measures could include but are not limited to:

12.2.1 the provision of acoustic installations, including glazing, and associated ventilation

12.2.2 the provision of air handling equipment

12.2.3 the provision of high hoardings

12.2.4 no noisy works being carried out in the vicinity of the school during examination periods

12.2.5 the use of additional lorry control methods and banksmen.”

U&A 1162 – “The Secretary of State will require the Nominated Undertaker and/or its contractors, in accordance with paragraph 6(2) of Schedule 4 to the Bill, to maintain pedestrian access to and from the relevant schools during term time and normal operating hours of the relevant schools save where it is necessary to temporarily suspend access in the case of an emergency or for the implementation of traffic and pedestrian measures.”

U&A 1165 – “The Secretary of State will require the Nominated Undertaker to engage with the London Borough of Camden and schools to fully understand any impacts that may occur and identify and reasonable measures to mitigate as identified within the Code of Construction Practice.”

U&A 1166 – “The nominated undertaker shall engage with the London Borough of Camden prior to completion of the detailed design of the traffic management

associated with the construction work at Euston station and elsewhere, with a view to reducing as far as reasonably practicable any disruption caused by such works to the efficient arrival and departure of pupils, staff and other visitor to and from school premises. "

U&A 1167 – "The nominated undertaker shall, in accordance with the Code of Construction Practice, give reasonable advance notice of the construction works taking place around schools to the London Borough of Camden.

4 Temporary Highway Works

4.1 Worksite & Access Points for Construction Activities

- 4.1.1 MDJV anticipate the use of the gates as identified in *Figure 2 & Figure 3*. Further new gates are not anticipated at this stage for new access or egress points for the sites currently identified. Small alterations may be required from time to time to facilitate any site works. The proposed access point for the construction of Construction Skills Centre at the former Maria Fidelis School site is as per the consented scheme 2021/3796/P (decision dated 13/12/2021) and makes use of HS2 gate S31G9H. A such, relevant traffic and transport detail is included in that consenting regime and is outside the scope of this document.
- 4.1.2 For MDJv works, there is one access and one egress point for the early works, Traction substation and Two Towers demolition being the entrance from Hampstead Road into Cardington Street and the latter being from Melton Street onto the Euston Road. Secondary access and egress for the section of NTH north (Zone 5) may be required due to internal site constraints.
- 4.1.3 For MDJv works for the Interim Taxi Rank access is within the bus station reached from Eversholt Street whilst egress is from within the bus station onto the Euston Road (right hand turn only).
- 4.1.4 A minor alteration to the position of Cardington Street gate is envisaged at the end of Q2 2023 when remaining section of Cardington Street will be permanently stopped up. Similarly, the remaining section of Melton Street is anticipated to be permanently stopped up at the start of Q2 2023 subject to ITR commissioning, closure of current taxi rank and Gordon Street.
- 4.1.5 Table below summarises MDJV main location of access points for construction vehicles or where interface with general public is significant (please also see *Section 1*).

Compound	Location of access point required	Additional comments
NTH North (Zone 5)	Situated on Hampstead Road eastern footway some 140 metres south of the junction with Granby Terrace	This gate (Gate S31 G18) will only be used should there be serviceability issues of vehicles reaching northern apex of the site as described in other sections of this document
	Situated along the northern kerbline within Cardington Street some 50 metres east of the junction with Hampstead Road	This gate (Gate S31 G10E) will be the main gate serving the NTH North compound.
NTH South (Main Compound), TSS, Two Towers and Cobourg Street Utilities, Euston Square Gardens (West) Satellite compound	Situated along the southern kerbline within Cardington Street some 50 metres east of the junction with Hampstead Road	This gate (Gate S31 G10H) will be the main gate serving the NTH South, TSS and Two Tower compounds via internal site Haul Road. This gate will also service Cobourg Street Utilities site when it gets established.
	Situated along Cardington Street some 100 metres east of the junction with Hampstead Road	This gate (Gate S31 G10D) will be the main gate serving TSS and Two Tower compounds via former Cardington Street. This gate will also service Cobourg Street Utilities site when it gets established as well as Euston Gardens West Satellite compound.
	Two gates situated at the far end of stopped up section of Drummond Street to facilitate pedestrian walkway	Two gates (Gate S31 G9J and S31 G5) to allow public walkway from Drummond Street to Euston Station to be maintained.
	Two gates situated some 100 metres on Melton Street north of junction with Euston Road	Two gates (Gate S31 G9K and S31 G9L) to allow public walkway from Euston Station to Euston Road to be maintained.
	Situated some 40 metres on Melton Street north of junction with Euston Road	This gate (Gate S31 G3) is the main egress point for vehicles arriving from the Cardington Street. Access via this gate is anticipated as secondary only on must need basis.

	Multiple Gates along Cobourg Street at junctions with Starcross Street, Drummond Street and Euston Street	Two main gates (Gate S31 G9F and S31 G9G) at Drummond Street junction with Cobourg Street are required to allow public walkway from Drummond Street to Euston Station to be maintained. Balance of gates will be confirmed and established for emergency and third-party access only.
Euston Square Garden (East) Satellite compound - ITR	Access and egress gate within TfL bus station	Two gates (S31 G15 and S31 G15A) are situated within private access to existing bus station. Access is via first gate from Eversholt Street and egress via second gate onto Euston Road.
Gordon Street Satellite compound	Two gates situated either end of the site at junctions with Euston Road/Gordon Street and Gordon Street/Endsleigh Gardens	The gates are to be confirmed. Main gate will be from the south of the compound at junction of Gordon Street/Endsleigh Gardens.

- 4.1.6 In order to construct the works, it is proposed there will be four main access points within the Euston Approaches. These are located at the northern end of Park Village East; Granby Terrace at the junction of Stanhope Street/Park Village East, Granby Terrace at the junction with Hampstead Road; and on Hampstead Road.
- 4.1.7 The access point on Park Village East – northern end will allow for deliveries to the site at street level and materials will be lowered down to track level for construction. This access point came into use in June 2022.
- 4.1.8 The proposed southern layby on Park Village East will only serve Tower Cranes C and D to lift bulk materials into and out of site.
- 4.1.9 The worksite access located on Granby Terrace (corner of Stanhope Street/Park Village East) is proposed for the construction of the Euston Approaches. This access will allow track level access for the construction of the Euston Throat Retained Cut (east and west retaining walls), Euston Scissor Box, Portal, Crossover tunnels, and remove excavated material from the Cavern site. However, this will have varying levels of use throughout the construction programme.
- 4.1.10 Hampstead Road Haul Road access was installed in March 2021 (as described in *Section 3.4*). This access point was signalled to enable right turn out of Euston Approaches worksite in June 2022. By enabling the right turn from the access for construction traffic to travel southbound along Hampstead Road this will avoid

the need to use Harrington Square. This will have varying levels of use throughout the construction programme, see below section.

- 4.1.11 Once the signals for the Hampstead Road access are in use, it is proposed to bag the signals at the Granby Terrace junction, which will operate as a left in only, the central median will be closed to prevent southbound traffic turning right onto Granby Terrace. This will have varying levels of use throughout the construction programme, see below section.
- 4.1.12 Refer to *Section 6* for Environmental Assessment and contractor forecast HGV movements.

Compound	Location of access point required
Adelaide Road Vent Shaft Main Compound	Adelaide Road
Park Village East (North) Satellite Compound	Park Village East
Carriage Shed and Park Village Eat Satellite Compound	Junction of Park Village East / Stanhope Street / Granby Terrace
Granby Terrace Overbridge Satellite Compound	Junction of Park Village East / Stanhope Street / Granby Terrace; and Hampstead Road
A400 Hampstead Road Overbridge (north) Satellite Compound	Junction of Park Village East / Stanhope Street / Granby Terrace
Hampstead Road worksite	Hampstead Road
ZSL Lorry Holding Area	Prince Albert Road

*See Figure 4 for SCS Map showing the site access locations

- 4.1.13 As described in *Section 3* further improvements that may occur and any changes to the work access points will be reflected in a future version of this document.
- 4.1.14 Both contractors will undertake regular reporting through the TLG to keep Camden officers informed of any emerging work and regarding any potential changes to the work access points.
- 4.1.15 Any physical changes to work site accesses will be subject to a revised schedule 4 submission.

4.2 Temporary Highway Works to Deliver the Project

- 4.2.1 A list of works that affect the highway from both contractors are included within *Appendix 1*. The works listed within this schedule will be consented under Schedule 4 of the High Speed 2 Act. The list comprises of both civil and utility works.
- 4.2.2 Coordination of these works will be discussed at the relevant TLG meeting/ Area Works meeting. Key highway works that are anticipated further TM review following coordination between HS2 contractors are summarised in *Section 3* above.
- 4.2.3 For any works affecting the Statutory Undertakers (Utility Provider Assets) that will be affected by the works to these assets will be undertaken with agreement of their respective owner and Schedule 4/33 of the High Speed 2 Act.
- 4.2.4 Any abnormal loads will be managed on a case-by-case basis, all relevant authorities will be consulted in accordance with the current Legislation and guidance.

4.3 Temporary works required to accommodate the delivery of the TBM

- 4.3.1 The TBM tunnelling activity for the Euston Tunnels will consist of two TBMs commencing at Old Oak Common, heading eastwards towards Euston, once the tunnel boring has been completed at the Euston Cavern area, it is proposed to remove the machines by road back to the Atlas Road site in North Acton.
- 4.3.2 Each TBM machine will be broken down into smaller segments, it is anticipated that each TBM will require a maximum 40 HGVs.
- 4.3.3 In order to deliver the TBM to site, street furniture is required to be temporarily removed. Figure below summarises the route to Atlas Road.
- 4.3.4 Further details, temporary highway works and timings will be provided when known and through appropriate Schedule 4 submissions as well as outlined at the relevant TLG meetings.



Figure 25: Overview of TBM route from Euston Cavern to Atlas Road

4.4 Road Safety Audit Scheduling

- 4.4.1 For significant temporary traffic management, road safety audits will be undertaken in accordance with standard GG 119. These are expected to be required where:
- exceptional temporary traffic management alters a highway to the extent that a driver will not be familiar with the route/direction to take (see section 2 of GG 119).
 - the temporary traffic management is not a standard layout conforming to Chapter 8 of the Traffic Signs Manual.
- 4.4.2 All permanent works will be subject to a stage 1-3 road safety audits in accordance with GG 119.
- 4.4.3 Any works that affect the TLRN, TfL's guidance document SQA 170 Road Safety Audits will be used instead of GG 119.

4.5 Local Direction Signing Requirements for Emergency Services & Deliveries

- 4.5.1 Emergency and blue light services are invited to attend the TLGs, however, we are undertaking additional engagement with them to focus on main works traffic matters.
- 4.5.2 Site specific measures to allow access to properties will be included within the consents, consultation will be made in advance of the consent being submitted to the Local Authority for approval.

- 4.5.3 Signage to sites will be from the TLRN or at an appropriate location, the highway authority will be consulted on and, as necessary, consent will be sought.
- 4.5.4 Any additional signage will be implemented on a site-by-site basis and approved through the consents process.

5 Road Network Management

5.1 Monitoring Highway Conditions

- 5.1.1 Measures will be implemented to monitor the highway condition for mud/debris brought onto the road by the site traffic. These measures will be implemented prior to commencement of the works. Should mud/debris make its way to the highway, we it shall be immediately cleared, by use of road sweeper, in accordance with the Secretary of State Use Class Approval.
- 5.1.2 Measures will be reviewed to ensure effectiveness in preventing mud/debris is not on the highway. Measures will be implemented based on the site conditions and constraints, measures such as the use of wheel washes/baths, jet washing, Agitator Roads, and bitumen surfacing will be considered and used where appropriate.
- 5.1.3 During operational times, daily tracking and recording of “Mud on the Road” will be undertaken. SCS is currently reviewing the option to track and record via DataScope app to ensure the cleanliness of the local roads servicing the construction area/gate. The system allows staff to provide real time reports of the state of roads around sites. Database that is generated is then used to identify any issues and trends which in turns triggers review of the measures and their effectiveness. Currently in development “Mud Cam” an AI based software system should provide additional element to system of detection and warning by real-time monitoring and warning that assesses images and alerts when mud reaches unacceptable levels.
- 5.1.4 MDjv strive where possible to instal proactive solutions for mud management into layouts and design of the worksites, using the “Mud Matrix” as a starting point for the correct solution. During operational times monitoring, daily tracking and recording of the gate access and egress will take place, this is a mechanism by which action for escalation process will be triggered.
- 5.1.5 Any issues with regard to mud/debris on the highway will be reported to TLG or other appropriate means and the results of the monitoring periodically reported to TLG/shared with Camden, as necessary.
- 5.1.6 A condition survey has been produced by HS2 for ES identified lorry routes of B-classification roads and lower, A-roads were not surveyed as they are distributor roads and is assumed that the build-up and condition of the road will be constructed and maintained to not require any improvements. This information

will be distributed to the relevant authority by HS2 detailing the condition of their road network.

- 5.1.7 Any additional conditional surveys required will be undertaken prior to works commencing. These areas are likely to be at site access points that adjoin A-roads or on lorry routes that deviate from those identified within the ES. Contractors will be responsible for the restoration of the highway in the immediate vicinity of site accesses in accordance with the RTMP.

5.2 Safety Management

- 5.2.1 Safety management in this context refers to vehicle compliance and safety management that all vehicle operators, vehicles, and drivers working on or associated with the main works section S1 & S3 of the HS2 Project should adhere to and as stated within the RTMP section 5 and the relevant items within the Code of Construction Practice (CoCP).
- 5.2.2 At the first point of contact with the HS2 project, be it the vehicle holding area, and or the site access gate, a vehicle safety equipment check, in line with RTMP requirements, will take place.
- 5.2.3 Vehicle safety equipment checks will be undertaken for the 1st visit each working day.
- 5.2.4 The Logistics System will capture but is not limited to the following:
- FORS Status
 - Driver training status, as required.
 - Vehicle safety equipment
 - Vehicle Type
 - Data to formulate vehicle CO2e emissions
- 5.2.5 As part of ensuring safety and compliance along the approved roads identified under Schedule 17 various measures have either been adopted, are under consideration and trial or are being proposed. These measures are:
- Erection of Gate Signage for better route identification and visibility
 - Duty Access Managers (DAMs) and Logistics Managers foot patrol for on the ground observations

- Regular monthly engagement between S1 and S3 contractor and other stakeholders to discuss any upcoming works that could affect approved roads and any notable incidents to date
- Regular monthly haulier engagement has been established where known route issues are communicated in advance such as roadworks and reinforced by the Control Tower
- Police commercial vehicle unit engagement assisting with stopping lorries to identify any deviation from approved roads
- Trial of the Haulier App has been utilised for single excavated material work package to date and will be reviewed further.
- Schedule 17 route signage
- SCS are implementing ANPR Cameras which will be installed in key locations capturing vehicle routing to and from Site, alert the gate staff and control tower to the status of the vehicle.
- MDjv plan for Schedule 17 lorry route adherence is currently under review and approval and will be shared in future updates of this document or relevant fora.

5.2.6 Drivers on HS2 business are generally required to display HS2 visible identifiers and as per Undertaking and Assurance 2136 mentioned below. The identifiers are sent to the drivers within their driver packs identifying the routes to be used.

U&A reference 2136

All vehicles over 3.5t employed on construction will be required to display an A4 size identifier, stating 'HS2', inside the cab windscreen in a position that does not obscure the driver's visibility. It should only be in use when the vehicle is on HS2 business.

The purpose of the identifier is to allow emergency services, stakeholders and the public to identify HS2 vehicles when off permitted routes, parked inappropriately, badly driven or when involved in a road traffic incident.

5.2.7 All vehicles working on behalf of HS2 will usually display their identifier. A vehicle is regarded as being on HS2 work when:

- The vehicle is wholly employed on HS2 work for the entirety of the driver's shift.
- A vehicle wholly employed on HS2 work leaves its operating centre until it returns to its operating centre.
- A vehicle wholly employed on HS2 work is parked away from its operating centre and including when the driver is on either daily or weekly rest and the vehicle is parked on the public highway or in an area accessible to the public.
- A driver on shift and engaged on other, non HS2 work, is instructed to go to any HS2 site.
- Returning to its operating centre after completing HS2 work.
- A vehicle on mixed (HS2 and Other), multiple deliveries and/or collections initially leaves an 'Other' delivery or collection point and commences its journey to the HS2 site. Conversely, a vehicle departing an HS2 site for an 'Other' delivery or collection will no longer be on HS2 work."

6 Managing Construction Movement

6.1 Lorry routes

- 6.1.1 Site specific lorry routes are detailed in this section and includes the combined construction traffic on routes utilised by the different work compounds.
- 6.1.2 Outlined below are the construction lorry routes to and from each compound site and includes the road numbers and names between each compound and the Strategic Road/Motorway Network. Maps are also included within *Appendix 2*.
- 6.1.3 We will seek to reduce, as far as reasonably practicable, the use of the TLRN by HGVs within the Inner Ring Road within weekday AM peak hours (8am to 9am), except in circumstances beyond the control of the contractor or where the use of the road is necessary to minimise effects on cost and programme.
- 6.1.4 Where a supplier/business is located between the special/trunk road network and a site, HGVs from that supplier/business to the site will be required to take the most appropriate route to join the main routes. This general provision will include a requirement that these routes, where reasonably identifiable, are discussed at the relevant Traffic Liaison Group.

SCS Lorry routes

- 6.1.5 Adelaide Road Vent Shaft Main Compound
- To site: TLRN – B509 Adelaide Road – compound access
 - From site: compound access – B509 Adelaide Road – A502 Chalk Farm Road / Castlehaven Road / Hawley Road – A400 Camden Street – A503 Camden Road (TLRN) – TLRN.
- 6.1.6 Adelaide Road Vent Shaft Main Compound – Proposed alternative route
- To site: TLRN – B509 Adelaide Road (eastbound) – compound access
 - From site: compound access – B509 Adelaide Road (westbound) – TLRN.
- 6.1.7 Park Village East (North) Satellite Compound
- To site: TLRN – A4201 Albany Street/Gloucester Gate – Park Village East.

- From site (when Mornington Street Bridge is open): Park Village East – Mornington Street – Mornington Terrace – A503 Delancey Street – A4201 Albany Street/Gloucester Gate/Osnaburgh Terrace/Street – Great Portland Street – TLRN.
- From site (when Mornington Street Bridge is closed: Park Village East – Stanhope Street, Robert Street, – A4201 Albany Street - Osnaburgh Terrace/Street – Great Portland Street – TLRN.

6.1.8 Carriage Shed and Park Village East Satellite Compound

- To site: TLRN – Great Portland Street – A4201 Albany Street – Robert Street – Stanhope Street.
- From site: Stanhope Street – Robert Street – A4201 Albany Street/Osnaburgh Terrace/Street – Great Portland Street – TLRN.

6.1.9 Granby Terrace Overbridge Satellite Compound

- To site: TLRN – A400 Gower Street – Grafton Way – A400 Tottenham Court Road – A400 Hampstead Road (TLRN) – site access/Granby Terrace
- From site: A400 Hampstead Road (TLRN) – A400 Gower Street – Grafton Way – A400 Tottenham Court Road – TLRN.

6.1.10 A400 Hampstead Road Overbridge (north) Satellite Compound

- To site: TLRN – A400 Gower Street – Grafton Way – A400 Tottenham Court Road – A400 Hampstead Road (TLRN) – site access/Granby Terrace
- From site: A400 Hampstead Road (TLRN) – A400 Gower Street – Grafton Way – A400 Tottenham Court Road – TLRN.

6.1.11 ZSL Lorry Holding Area

- To LHA: TLRN – Great Portland Street – A4201 Albany Street/Gloucester Gate – A5205 Prince Albert Road.
- From LHA: A5205 Prince Albert Road – (Park Village East¹) – A4201 Albany Street/Gloucester Gate/Osnaburgh Terrace/Street – Great Portland Street – TLRN.

¹ If required to deliver to Park Village East (North) Satellite Compound

MDjv lorry routes

6.1.12 National Temperance Hospital

- To site – TLRN to Cardington Street (anticipated until mid 2023) – (primary)
- To site² - *TLRN to re-aligned Main NTH Site Access off Hampstead Road (anticipated from mid 2023 to facilitate the works)* – (primary)
- To Site³ (NTH North compound to Gate S31 G18) – (secondary access when required)
 - From south: TLRN northbound (A400 Hampstead Road), Harrington Square and TLRN southbound (A400 Hampstead Road) into site
 - From north: TLRN southbound (A400 Hampstead Road) into site
- From site – Melton Street to TLRN (primary)
- From site⁴ – Cardington Street via A400 Hampstead Road southbound continuing onto TLRN westbound (A501 Euston Road) and utilising Osnaburgh loop (A4201 Great Portland Street /A4201 Albany Street/Gloucester Gate/A4201 Osnaburgh Terrace/ A4201 Osnaburgh Street) if required for eastbound journeys (secondary)
- From Site ⁵ (NTH North compound Gate S31 G18) – NTH north via S31 G18 to TLRN southbound (A400 Hampstead Road)

6.1.13 Cobourg Street Satellite Compound (Traction Sub-station)

As per the above National Temperance Hospital route

6.1.14 Melton Street Satellite Compound (Tower demolition)

- As per the above National Temperance Hospital route

² Cardington Street Main Gate Access is to be re-aligned during middle of 2023. Vehicles will enter the Main NTH Site directly from TLRN (A400 Hampstead Road). Cardington Street will be either stopped up in entirety or closed off. Cardington Street access and gate will be re-instated approximately after 6 months.

³ Number of constraints have been identified whereby serviceability to the small area of NTH North (Zone 5) compound may not be possible apart from the use of Gate S31 G18. MDjv will ensure that no more than 12 lorries (24 two-way movement) will use this gate daily.

⁴ Majority of the MDJV vehicles will egress the NTH site via Melton Street (primary) preference. Some MDJV vehicles might egress back via Cardington Street (secondary) preference (current assumption has the approximate 70-30% split between the Melton Street and Cardington Street)

⁵ Any vehicles that access via Gate S31 G18 should egress back via the same gate due to site constraints identified.

6.1.15 Euston Gardens West Satellite compound (taxi rank)

- As per the above National Temperance Hospital route

6.1.16 Euston Gardens East Satellite compound (Interim Taxi Rank)

- To site – TLRN westbound⁶ – A4200 Churchway – A4200 Grafton Place
- From site – Bus station to TLRN westbound

6.1.17 ZSL Lorry Holding Area (LHA)⁷

- To LHA : TLRN – Great Portland Street – A4201 Albany Street/Gloucester Gate – A5205 Prince Albert Road.
- From LHA: A5205 Prince Albert Road – A4201 Albany Street/Gloucester Gate/Osnaburgh Terrace/Street – Great Portland Street – TLRN

6.1.18 Gordon Street Satellite Compound

- To Site: TLRN – A4200 Upper Woburn Place – Endsleigh Gardens
- From Site: Gower Place – A400 Gower Street – Grafton Way – A400 Tottenham Court Road – TLRN eastbound

6.1.19 On-Network Site Enabling Phase 2 Works (Delivered by Network Rail)

- These works do not form part of MDJV scope and are subject to a separate LTMP and consenting regime. Their worksite access is dependent on utilising Cardington Street.

Closure of lorry routes/alternative temporary routes

6.1.20 In the event that any of the above mentioned routes become subject to closure or any issues arise general requirements will be followed as set out in Route-Wide

⁶ The access to A4200 Churchway from A501 Euston Road is only possible from westbound direction. Any vehicles arriving from the west, travelling eastbound will have to utilise TRLN Kings Cross loop.

⁷ MDJV will not use of ZSL lorry holding area during the 2023 period. This will be reviewed and updated in further revisions of this document as well as for the Main Works.

Traffic Management Plan under “Closures of Lorry routes/alternative temporary routes”.

6.2 Site & Flows Forecasts & Comparison with Environmental Statement

6.2.1 Refer to *Section 4* for site access points.

6.2.2 Overall reduction of HGV movements is a result of construction methodology changes and efficiency in design.

SCS Forecasts

*Average daily combined two-way vehicle trips during busy period and within peak month of activity.

Worksite	SES2 / AP3	SES2 / AP3 Period with busy vehicle movements (months)	SES2 / AP3 Duration of worksite use (years)	Contractor Forecast	Contractor Forecast Estimated period with busy vehicle movements (months)	Contractor Forecast Estimated duration of use (years)
	HGV trips*			HGV trips		
Adelaide Road Vent Shaft Main Compound	90-100	4-6	5	60-80	5	4
Park Village East (North) Satellite Compound	90-136	14	7	20-30	1	4
Carriage Shed and Park Village East Satellite Compound	160-250	23	11	300-450	18	6
Granby Terrace Overbridge Satellite Compound	260-280	4	11		4	6
A400 Hampstead Road Overbridge (north) Satellite Compound	30-40	15	11		15	6
ZSL Lorry Holding Area	No forecast of HGV's given within ES (as amended)			130-190	16	6
Mornington Street Overbridge Satellite Compound	20-30	14	5	Worksite not proposed to be used		
Mornington Terrace Sidings Satellite Compound			12	Worksite not proposed to be used		
A400 Hampstead Road Overbridge (south) Satellite Compound	16	2	18	Worksite not proposed to be used		

N.B. These forecasts are likely to change as more detail becomes available

MDjv Forecasts

Worksite	SES2 / AP3	SES2 / AP3 Period with busy vehicle movements (months)	SES2 / AP3 Duration of worksite use (years)	Contractor Forecast	Contractor Forecast Estimated period with busy vehicle movements (months)	Contractor Forecast Estimated duration of use (years)
	HGV trips*			HGV trips*		
Euston Square Gardens (west)	140 - 200	10	18	80 - 160	5	18
National Temperance Hospital **	300 - 450	27	18	275 - 400	10	18
Euston Square Gardens (east)	20 - 36	13	18	24	6	18
Gordon Street***	20-30	3	2 (2017- 2018) & 6 (2021-2026)	24	15	8

*Average daily combined two-way vehicle trips during busy period and within peak month of activity.

**Includes movements for Cobourg Street (Traction Sub-Station), NTH northern worksite (Zone 5), Melton Street (Two Towers), Cobourg Street utilities. For the purposes of Schedule 17 (2021/4406/HS2) lorry route consent the numbers had been presented separately at the time of the submission.

N.B. These forecasts are likely to change as more detail becomes available

*** The numbers included are for Gordon Street enabling and utility works only. Numbers are to be revised for the Main Works.

6.3 Local Worksite Capacities, Holding Areas & Other Local Measures

- 6.3.1 All vehicles as necessary are to be managed through the Vehicle Management System (VMS), as described within Section 4 of the Route-wide Traffic Management Plan (RTMP). SCS propose to use a Lorry Holding Area within the ZSL London Zoo car park to hold vehicles and to carry out vehicle compliance checks. A new access/egress has been constructed (by the EWC) from A5205 Price Albert Road to avoid the use of Outer Circle.
- 6.3.2 The holding area may also need to be used to regulate vehicle movements to avoid vehicles parking on the highway network waiting to enter a worksite.
- 6.3.3 An example of vehicles using the LHA are deliveries would be steel reinforcement, plant, import material deliveries. Deliveries such as time sensitive materials (wet concrete and bitumen) and excavated material will be sent direct to site.
- 6.3.4 MDJv will engage with SCS to agree access arrangements to their Lorry Holding Area on an as and when required basis. For the current planned works that been captured in the current revision of the LTMP, MDJv do not require the use of a Lorry Holding Area in this location. MDJV will undertake future review to understand whether this option may be suitable to support the Main Station Works.
- 6.3.5 MDJv is currently reviewing further options for vehicle holding areas (VHA) and construction consolidation centres (CCC) to control the vehicle and material flow to and from Euston. This is intended to ensure steady flow of construction traffic to site compounds.
- 6.3.6 The vehicle holding areas will seek to conduct mandatory compliance checks prior to arriving at the site entrances, reducing time at the gates, and preventing potential backlogs. In addition, they will conduct controlled releases of vehicles to regulate those that arrive early trying to access the site at the same time.
- 6.3.7 The consolidation centre(s) will seek to reduce part loads arriving at site. Intention is to amalgamate these loads away from Euston and only bring full loads to site. This has further potential at reducing the number of vehicles utilising the road network to and from site at the start of Main Station Works or sooner.
- 6.3.8 A report from the VMS will be shared and discussed with LBC and TfL within the TLG or at other fora if required.

Local Concrete routes for Euston Station

- 6.3.9 In accordance with the guidance set out in Planning Forum Note 6, suppliers/businesses located between the special/trunk road network and the HS2 Site should use the most appropriate route between the supplier/business site and the Main Route(s). At the time of preparing this LTMP, MDJV has yet to confirm which concrete supplier will be used to support the Early Main works piling and Ground Source Heat pump installation phase of work, but it is considered reasonably likely that a proportion of concrete supplies will be sourced from one of the concrete batching plants located in Kings Cross.
- 6.3.10 At the request of Camden Council, MDJV has identified preferred lorry route options and their alternatives between Kings Cross and the main access point to the NTH works compound for discussion at the Traffic Liaison Group held in August 2022. Criteria that were applied when considering what constitutes an appropriate route include:
- Use TLRN and Strategic Regional Network roads as much as possible
 - Makes use of routes assumed and assessed under Environmental Statement where feasible
 - Where alternative routes are identified, the use of these routes remains EMR compliant
 - Takes shortest/most direct route to TLRN and SRN
 - Construction requirements and constraints
 - Avoids alterations to the existing road network where practicable
 - Utilises local suppliers
- 6.3.11 Below table summarises the preferred routes to and from site, see Appendix 6 for the full concrete route sift matrix.

Preferred Route from North to access site	Preferred Route from South to access site	Preferred route for Egress
<ol style="list-style-type: none"> 1. A5200 York Way (LBC) 2. A5200 Camden Park Road (LBC) 3. A503 Camden Road (TLRN) 4. A400 Camden Street (TLRN) 5. A400 Oakley Square (TLRN) 6. A400 Lidlington Place (TLRN) 7. A400 Harrington Square (TLRN) 8. A400 Hampstead Road (TLRN) 9. Cardington Street (LBC) 	<ol style="list-style-type: none"> 1. A5200 York Way (LBC) 2. A5203 Wharfdale Road (LBI) 3. A5203 Caledonian Road (LBI) 4. King's Cross Bridge (TLRN) 5. A501 Grays Inn Road (TLRN) 6. A501 Euston Road - west (TLRN) 7. Great Portland Street (WCC) 8. A4201 Albany Street (LBC) 9. A4201 Osnaburgh Terrace (LBC) 10. A4201 Osnaburgh Street (LBC) 11. A501 Euston Road - east (TLRN) 12. A400 Hampstead Road (TLRN) 13. Cardington Street (LBC) 	<ol style="list-style-type: none"> 1. Melton Street (LBC) 2. A501 Euston Road - east (TLRN) 3. A5200 York Way (LBC)
Review Summary		
This is the MDJv's preferred concrete route from the north. It captures all criteria review favourably and comprises of least number of LBC roads from the north.	This is the MDJv's preferred concrete route from the south. In comparison to the northern route it doesn't score favourably on three aspects, construction constraints i.e. critical concrete pours, overall number of roads on the network and overall distance, it being 0.5 mile longer.	Although there are two options for egress for majority of vehicles, this option is the most direct and hence the most favourable for exit of all or high majority of traffic.

6.3.12 LBC Officers subsequently asked HS2 to include the proposed local concrete routes as part of the Interim Schedule 17 lorry route consent application (Ref No: 2022/5079/HS2). Following comments received from Camden post submission, MDJV has confirmed that the combination of the proposed northern and southern routes can accommodate the forecasted number of construction vehicles for the site enabling and early works packages. No other proposed concrete routes as per Appendix 6 will be sought as part of the current Schedule 17 lorry route consent application.

6.3.13 At post submission stage, LBC officers confirmed the acceptability of the 'northern' concrete lorry route but raised concerns relating to the safety of vulnerable users, particularly at junctions and around primary schools which are located on the route or nearby.

- 6.3.14 All HS2 Principal Contractors are required to meet the vehicle safety standards set out in Section 5.7 of the High Speed 2 Phase One and 2a Route-wide Traffic Management Plan. Principal Contractors are also required to consider changes in related technology, and reasonably require adoption of new technology by the Principal Contractor and their supply chain during their vehicle fleet renewal programme, where technology standards and reliability permit. The Principal Contractors' response to these requirements is set out in Section 5.2 of the LTMP.
- 6.3.15 HS2 will work with its supply chain to operate LGVs with direct vision cabs for use on the northern 'concrete' lorry route and will aim to increase the proportion of 5* DVS vehicles that are in use by local concrete suppliers as part of their normal fleet renewal programmes."
- 6.3.16 All LGV drivers working on HS2 are required to undertake Safe Urban Driving (SUD) or equivalent training courses. This is also a standard requirement of the Fleet Operator Recognition Scheme (FORS). Further details are provided in Section 5.6 of HS2 Phase One Routewide Traffic Management Plan (HS2-HS2-CL-PLN-000-000002).
- 6.3.17 All LGV drivers working on the HS2 project are required to adhere to national and local highways legislation, including speed limits.
- 6.3.18 MDJV proposals for school engagement are covered in *Sections 3.12.22 to 3.12.26* of this LTMP.

6.4 Proposed working hours

- 6.4.1 In accordance with the Code of Construction Practice, core working hours will be from 08:00 to 18:00 on weekdays (excluding bank holidays) and from 08:00 to 13:00 on Saturdays. To maximise productivity within the core hours a period of up to one hour before and up to one hour after normal working hours for start-up and close-down of activities will be permitted to include (but not be limited to) deliveries, movement to place of work, unloading, maintenance and general preparation work. In some cases, works will be undertaken 24/7. HGV movements will primarily be restricted to the core working hours, however some deliveries – such as concrete – will be made outside of these hours. All works will be agreed with the local authority prior to undertaking the work under Section 61 of the Control of Pollution Act 1974.
- 6.4.2 Certain other specific construction activities will require extended working hours for reasons of engineering practicability. These activities include, but are not limited to, major concrete pours and piling/diaphragm wall works. Surveys (e.g.

for wildlife or engineering purposes) may also need to be carried out outside core working hours.

6.4.3 Repairs or maintenance of construction equipment that is required to be carried out outside core working hours will normally be carried out on Saturday afternoons between 13.00 and 18.00 or Sundays between 10:00 and 17:00. Only essential repairs or maintenance works will be undertaken on Sundays.

6.4.4 In the case of work required in response to an emergency or which, if not completed, would be unsafe or harmful to the works, staff, the public or the local environment, the relevant local authority will be informed as soon as reasonably practicable of the reasons for the works and their likely duration.

6.5 U&As Restriction Vehicle Flows

6.5.1 HS2 Ltd entered into several commitments (Undertakings and Assurances) in relation to construction traffic and transport during the passage of the Bill. Following Royal Assent of the High-Speed Rail (London - West Midlands) Act 2017 any nominated undertaker is contractually obliged to comply with all relevant U&As made by the Secretary of State/ the Promoter regarding HS2.

6.5.2 U&As associated with the Camdenwide LTMP area are included in *Appendix 4* and also set out route wide safety, capacity, and traffic management related U&As respectively.

6.5.3 The Logistics system has been implemented, limitations have been put on during the AM and PM peak hours which corresponds to the modelling assumptions in the HS2 Traffic Assessment. The VMS Logistic system capability restricts the available booking slots hence, should ensure the limitations are not exceeded in line with Traffic Assessment. This will be applicable to both contractors.

6.5.4 The AM and PM peak periods are in accordance with the hours stated within the HS2 ES. The AM and PM peak hours have had further limitations set; the limitations are as follows:

- AM Peak period - 10% to site, 10% from site
- PM Peak period - 10% to site, 10% from site

The percentages represent the amount of booking slots available of the booking capacity

6.5.5 Regular reporting of the previous period actuals HGV numbers and the next period forecasted HGV movements will be presented to Camden and the

Community Stakeholders within the Traffic Liaison Group and Community Traffic Working Group on a regular basis.

6.6 Lorry Route Approvals & Monitoring

- 6.6.1 All LGVs delivering for the works will be booked onto the Logistics System, Only vehicles booked onto the system will be allowed to enter the sites. All drivers will receive a driver pack with the approved routes. Drivers will not deviate from the approved Schedule 17 lorry routes, unless directed by the emergency services or the highway authority or are to follow approved and established diversions.
- 6.6.2 SCS are implementing ANPR Cameras which will be installed in key locations capturing vehicle routing to and from Site, alert the gate staff and control tower to the status of the vehicle.
- 6.6.3 Current MDJV vehicle off route process is outlined below in Figure 25. Once the vehicle has been rejected at the gate the off-route process for consequences is deployed. SCS processes are almost aligned with those of MDjv
- 6.6.4 MDjv plan for Schedule 17 lorry route adherence is currently under review and approval and will be shared in future updates of this document or relevant forums.
- 6.6.5 For effective action to be undertaken either photographic evidence or vehicle registration or both is required combined with route detail and timing.

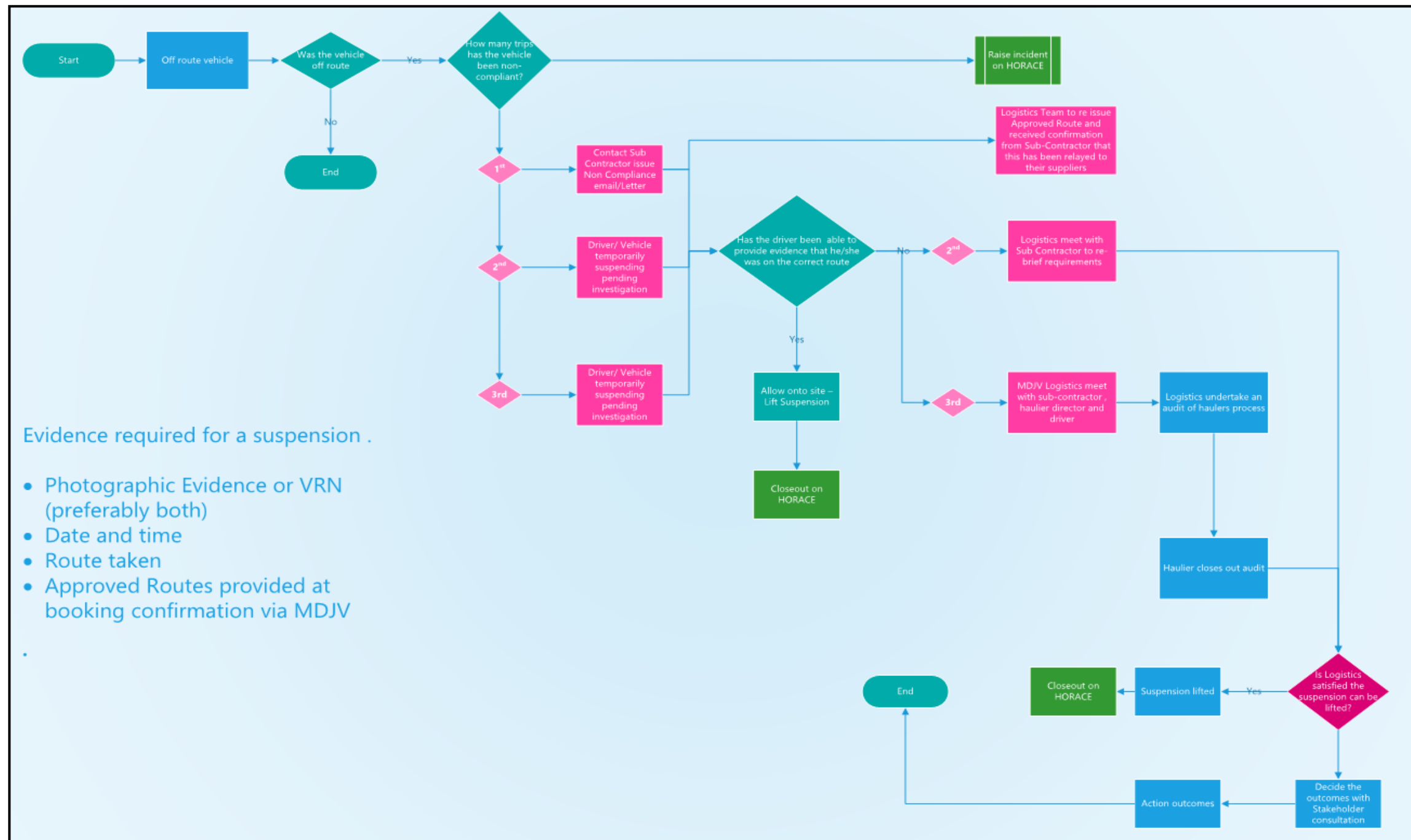


Figure 26: MDJV Non-compliance Off Route vehicle process (Note: for Clarity, please print on A3)

7 Workforce Transport

7.1 Local travel plans

- 7.1.1 A workforce travel plan has been produced by both SCS (Doc Ref No: 1MC03-SCJ-TM-PLN-S001-000004 C02) and MDJV (Doc Ref No: 1CP01-MDS-CL-PLN-SS08_SL20-000010). The plans take account of all travel arrangements for each site compound and the documents have been issued to the relevant authority for engagement in line with the RTMP.
- 7.1.2 For SCS Operational parking will may be provided, as required normally at welfare facilities, for:
- Site medical team (if any)
 - Canteen staff, who will be arriving earlier than start up activities
 - Blue badge holders
 - Site security staff, particularly those carrying dogs.
 - Essential project visitors, carrying out a statutory function
 - External operational vehicles (including street sweepers and compliance team vehicles)
- 7.1.3 Parking provision may be restricted in suburban and urban areas, commensurate with public transport provision. **Within inner urban areas, only essential operational parking will be provided.**
- 7.1.4 MDjv will only provide such operational parking within the site compounds. No provision for parking is to be made at the welfare facility in Maria Fidelis nor on site
- 7.1.5 Within LB Camden, all staff will be expected to use public transport. Shift patterns will be arranged to avoid any 'black spots' within the public transport timetable.
- 7.1.6 COVID 19 is being monitored in line with government guidelines and WHO recommendation. All HS2 contractors are monitoring the guidance and will implement any measures as necessary.

Appendix 1: London Borough of Camden StreetManager forward plans

Below is the summary of the Forward Plans raised on StreetManager at the time of writing this report. The list is regularly reviewed and any new Forward Plans raised or amended are available to be viewed via StreetManager by local highway authority.

Nominated Undertaker	Forward plan reference number	Road/Street	USRN	Highway authority	Works location	Proposed start date	Proposed end date	Utility Works	Description of work	Traffic management type
Sector 1 - Central	TA011THW-LBC-SCS-4232-FP	KILBURN HIGH ROAD	20400395	LONDON BOROUGH OF CAMDEN	Kilburn High Road j/o Oxford Road	15/08/2022	26/08/2022	Yes	Utility trial holes on Oxford Road / Kilburn High Road	Multi-way signals
Sector 1 - Central	TA011THW-LBC-SCS-4296-FP	ADELAIDE ROAD	20400577	LONDON BOROUGH OF CAMDEN	Adelaide Road from j/o Beaumont Walk to opposite the Medical Centre	31/10/2022	25/11/2022	Yes	Utilities duct installation works (Non-contestable works)	Lane closure
Sector 1 - Central	TA011THW-LBC-SCS-4295-FP	ADELAIDE ROAD	20400577	LONDON BOROUGH OF CAMDEN	Adelaide Road from j/o Eton Road to the outside of Blashford	29/08/2022	28/10/2022	Yes	Utilities Duct Installation works	Lane closure
Sector 1 - Central	TA011THW-LBC-SCS-4242-FP	ALEXANDRA ROAD	20400570	LONDON BOROUGH OF CAMDEN	Alexandra Road	20/01/2023	10/08/2023	Yes	WP26.9- Cadent MP 76.2MM SI	Some carriageway incursion
Sector 1 - Central	TA011THW-LBC-SCS-4259-FP	LOUDOUN ROAD	20400568	LONDON BOROUGH OF CAMDEN	Loudoun Road	06/12/2023	04/06/2024	Yes	Utility Diversion Works	Some carriageway incursion
Sector 1 - Central	TA011THW-LBC-SCS-4256-FP	LANGTRY WALK	20402812	LONDON BOROUGH OF CAMDEN	Langtry Walk	28/04/2023	03/11/2023	Yes	Utility Diversion Works	Some carriageway incursion
Sector 1 - Central	TA011THW-LBC-SCS-4155-FP	SPRINGFIELD LANE	20400678	LONDON BOROUGH OF CAMDEN	Springfield Lane j/o Kilburn Place	09/12/2022	16/02/2023	Yes	Utilities sewer lining works	Some carriageway incursion
Sector 1 - Central	TA011THW-LBC-SCS-4154-FP	KILBURN PRIORY	20400684	LONDON BOROUGH OF CAMDEN	Kilburn Priory	09/12/2022	07/09/2023	Yes	Utilities sewer lining works (WP26.5a)	Some carriageway incursion
Sector 1 - Central	TA011-THW-LBC-SCS-5097-FP	OVAL ROAD	20400628	LONDON BOROUGH OF CAMDEN	Works located near Prep School	01/08/2022	23/09/2024	Yes	WP26.1B Parkway Utilities Diversions works - UKPN cable diversion.	Some carriageway incursion
Sector 1 - East	TA011-THW-TFL-SCS-5147-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Carriageway adjacnet to Varndell Street	01/06/2023	30/11/2023	Yes	HRB Phase 2 utilities southern area	Multi-way signals
Sector 1 - East	TA011-THW-LBC-SCS-5146-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Hampstead Rd at the junction with Granby Terrace	01/03/2023	30/06/2023	No	Traffic Management required on Hampstead Rd will be for access purposes to Granby Terrace worksite whilst bridge is constructed,	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5143-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington St bridge between Park Village East and Mornington Terrace	07/11/2022	16/12/2022	No	Structural survey across the full width of bridge	Road closure
Sector 1 - East	TA011-THW-LBC-SCS-5131-FP	PARK VILLAGE EAST	20400723	LONDON BOROUGH OF CAMDEN	ECS Layby approximately 100 metres south of Parkway/Park Village East junction	24/10/2022	23/12/2022	No	Substation installation - hoarding removal and adjustment	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5105-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge - site team trial holes	01/01/2023	30/01/2023	No	Site Team trail holes for permanent design - Jay Master	Road closure

Sector 1 - East	TA011-THW-TFL-SCS-5129-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Regular hoarding maintenance on Hampstead Road section between Cardington Street and Granby Terrace	11/04/2022	14/04/2022	No	Monthly regular hoarding and other maintenance	Lane closure
Sector 1 - East	TA011THW-LBC-SCS-5086_02-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Junction of Granby Terrace and Hampstead Road	15/12/2023	25/01/2024	Yes	utility diversion GTB/P03	Lane closure
Sector 1 - East	TA011THW-TFL-SCS-5118_02-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	non contestable works	24/05/2024	16/08/2024	Yes	utility non contestable works	Lane closure
Sector 1 - East	TA011THW-TFL-SCS-5118_01-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Phase 2 utility works northern area	09/01/2023	26/04/2024	Yes	HRB Phase 2 utility contestable works	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5108-FP	GRANBY TERRACE	20400773	LONDON BOROUGH OF CAMDEN	Granby Terrace Bridge from Hampstead Road to Stanhope Street	01/06/2023	30/12/2023	No	GTB extension or alteration to the bridge	Road closure
Sector 1 - East	TA011THW-LBC-SCS-5053-FP	GLOUCESTER AVENUE	20499040	LONDON BOROUGH OF CAMDEN	Sunny Mews on Gloucester Avenue	21/12/2022	28/02/2023	Yes	WP26.5a - Utility sewer lining works	Some carriageway incursion
Sector 1 - East	TA011THW-LBC-SCS-4152-FP	GLOUCESTER AVENUE	20499040	LONDON BOROUGH OF CAMDEN	Gloucester Avenue	21/12/2022	14/08/2023	Yes	Utilities diversion works - Sewer Lining works (WP26.5a)	Some carriageway incursion
Sector 1 - East	TA011-THW-LBC-SCS-5104-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	DH Road closure	01/01/2024	01/12/2024	No	DH Road closure	Road closure
Sector 1 - East	TA011-THW-LBC-SCS-5096-FP	PARKWAY	20400733	LONDON BOROUGH OF CAMDEN	Lane closures required for access to Thames Water manholes	03/10/2022	23/12/2022	Yes	WP26.1B Parkway Utilities Diversions works - TW Sewer lining	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5092-FP	ALBERT STREET	20400728	LONDON BOROUGH OF CAMDEN	Albert Road where it meets parkway	12/12/2022	08/02/2023	Yes	Install of two new valves and monitoring equipment on the 36" water main located beneath Albert Street	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5091-FP	PARKWAY	20400733	LONDON BOROUGH OF CAMDEN	Outside north bridge house prep school in footway and carriageway	03/01/2023	30/01/2023	Yes	WP26.1B Parkway Utilities Diversions works - Thames Water main monitoring.	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5090-FP	DELANCEY STREET	20400724	LONDON BOROUGH OF CAMDEN	Parkway & Delancey Street north and westbound carriageway	04/10/2022	21/12/2022	Yes	Cable trenching & installation - Phase 1-5 Excavation of joint and pulling pit, joining of cables, backfill & reinstatement - Phase 6	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5089-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Works are located on the North and Southbound lanes on Hampstead Road between Harington Square and Granby Terrence Bridge.	10/07/2023	14/09/2023	Yes	WP26.12 GTB Permanent Utilities Reinstatement	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5086-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	North and Southbound lanes on Hampstead Road between Harington Square and Granby Terrence Bridge.	10/07/2023	14/09/2023	Yes	WP26.12 GTB Permanent Utilities Reinstatement	Road closure

Sector 1 - East	TA011-THW-LBC-SCS-5085-FP	VARNDELL STREET	20400832	LONDON BOROUGH OF CAMDEN	Traffic Management to consist of Road Closure on Varndell Street Works are located on the full carriageway of Vardell Street between Hampstead Road and Stanhope Street.	21/02/2023	03/04/2023	Yes	Continuation of Utilities permanent reinstatement as part of WP26.12 GTB Utilities.	Road closure
Sector 1 - East	TA011THW-LBC-SCS-4048-FP	PARK VILLAGE EAST	20400723	LONDON BOROUGH OF CAMDEN	Mornington Street to Augustus Street	01/12/2022	30/12/2026	No	Relocation of roadspace for one way system.	Some carriageway incursion
Sector 1 - East	TA011THW-LBC-SCS-4044-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Lane closure on Mornington street bridge to allow for the installation of a Haki staircase from street level down to track.Lane closure on south side of Mornington street bridge.	25/09/2023	13/12/2027	No	installation of a Haki staircase	Lane closure
Sector 1 - East	TA011-THW-LBC-SCS-5042-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge	25/09/2023	13/12/2027	No	Removal of listed piers both SE and NE on Mornington Street Bridge	Multi-way signals
Sector 1 - East	TA011-THW-LBC-SCS-5041-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge	25/09/2023	13/12/2027	No	Removal of NW listed Pier	Multi-way signals
Sector 1 - East	TA012-THW-LBC-SCS-5040-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge	25/09/2023	13/12/2027	No	Footway and lane closure required to facilitate lifting and removal of SW Listed Pier	Multi-way signals
Sector 1 - East	TA011-THW-LBC-SCS-5039-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge	25/09/2023	13/12/2027	No	Removal of Listed Piers	Some carriageway incursion
Sector 1 - East	TA011-THW-LBC-SCS-5038-FP	MORNINGTON STREET	20400770	LONDON BOROUGH OF CAMDEN	Mornington Street Bridge	25/09/2023	13/12/2027	No	Closure of Mornington Street Bridge	Road closure
Sector 1 - East	TA011-THW-LBC-SCS-5019-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Hampstead Road section between junction of Varndell Street and Granby Terrace	08/07/2024	21/02/2025	No	Southbound carriageway closure between junction of Varndell Street and Granby Terrace for continued HRB construction.	Contra-flow
Sector 1 - East	TA011-THW-LBC-SCS-5016-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Hampstead Road section between junction of Varndell Street and Granby Terrace bridge	22/10/2024	17/11/2024	No	Four week closure of Hampstead Road bridge to facilitate ongoing new Hampstead Road Bridge construction	Road closure
Euston Station - MDjv	TA02049440058-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Between Robert St and North Gower St	31/01/2024	30/03/2024	Yes	West Utilities Road new junction	Multi-way signals
Euston Station - MDjv	TA02048593472-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Junction Cardington St	01/05/2023	30/06/2023	No	New Road Alignment for Cardington Street	Multi-way signals
Euston Station - MDjv	TA02043257462-FP	ROBERT STREET	20400825	LONDON BOROUGH OF CAMDEN	Robert Street between Hampstead Rd and Stanhope Street	01/11/2022	31/03/2023	Yes	Utility Works	Road closure
Euston Station - MDjv	TA02059705644-FP	HAMPSTEAD ROAD	20400780	TRANSPORT FOR LONDON (TFL)	Between Granby Terrace and Drummond St	01/10/2022	31/08/2023	Yes	Utilities (including asset protection)	Multi-way signals
Euston Station - MDjv	TA02026774071-FP	STARCROSS STREET	20400841	LONDON BOROUGH OF CAMDEN	Entire street	01/10/2023	31/12/2023	Yes	Sewer relining	Some carriageway incursion

Euston Station - MDjv	TA02016557719-FP	EUSTON STREET	20400950	LONDON BOROUGH OF CAMDEN	Entire Street	01/04/2023	31/01/2024	Yes	Utilities	Some carriageway incursion
Euston Station - MDjv	TA02064987133-FP	COBOURG STREET	20400843	LONDON BOROUGH OF CAMDEN	Entire Street	01/10/2022	30/12/2023	Yes	Utilities	Some carriageway incursion
Euston Station - MDjv	TA02072801781-FP	EVERSHOLT STREET	20400782	LONDON BOROUGH OF CAMDEN	Euston Rd to Drummond Cres	01/01/2023	30/04/2023	Yes	Gas Main Replacement	Multi-way signals
Euston Station - MDjv	TA02081357788-FP	EUSTON ROAD	20400889	TRANSPORT FOR LONDON (TFL)	Between North Gower St and Eversholt St	01/01/2023	30/06/2023	No	Enabling Works	Lane closure
Euston Station - MDjv	Forward plan awaiting review and submission	GORDON STREET	20400962	LONDON BOROUGH OF CAMDEN	Between Euston Road and Endsleigh Gardens	01/04/2024	01/04/2030	No	LU Western Entrance Building main works	Road closure
Euston Station - MDjv	Forward plan awaiting review and submission	GORDON STREET	20400962	LONDON BOROUGH OF CAMDEN	Between Euston Road and Endsleigh Gardens	01/01/2023	31/12/2023	Yes	Utility enabling works for the main works	Road closure

Appendix 1A: London Borough of Camden HS2 Concurrent Works Coordination

FP Reference Number	>>>	SCS		TA011-THW-TFL-SCS-5147-FP	TA011-THW-LBC-SCS-5146-FP	TA011-THW-LBC-SCS-5143-FP	TA011-THW-LBC-SCS-5131-FP	TA011-THW-LBC-SCS-5105-FP	TA011THW-LBC-SCS-5086_02-FP	TA011THW-TFL-SCS-5118_02-FP	TA011THW-TFL-SCS-5118_01-FP	TA011-THW-LBC-SCS-5108-FP	TA011THW-LBC-SCS-5053-FP	TA011THW-LBC-SCS-4152-FP	TA011-THW-LBC-SCS-5104-FP	TA011-THW-LBC-SCS-5096-FP	TA011-THW-LBC-SCS-5092-FP	TA011-THW-LBC-SCS-5091-FP	TA011-THW-LBC-SCS-5090-FP	TA011-THW-LBC-SCS-5089-FP	TA011-THW-LBC-SCS-5086-FP	TA011-THW-LBC-SCS-5085-FP	TA011THW-LBC-SCS-4048-FP	TA011THW-LBC-SCS-4044-FP	TA011-THW-LBC-SCS-5042-FP	TA011-THW-LBC-SCS-5041-FP	TA012-THW-LBC-SCS-5040-FP	TA011-THW-LBC-SCS-5039-FP	TA011-THW-LBC-SCS-5038-FP	TA011-THW-LBC-SCS-5019-FP	TA011-THW-LBC-SCS-5016-FP	
MDJV	v v v	Road	TM Type	Multi-way signals	Lane closure	Road closure	Lane closure	Road closure	Lane closure	Lane closure	Lane closure	Road closure	Some carriageway incursion	Some carriageway incursion	Road closure	Lane closure	Lane closure	Lane closure	Lane closure	Lane closure	Road closure	Road closure	Some carriageway incursion	Lane closure	Multi-way signals	Multi-way signals	Multi-way signals	Some carriageway incursion	Road closure	Contra-flow	Road closure	
				Start Date of Works	Jun-23	Mar-23	Nov-22	Oct-22	Jan-23	Dec-23	May-24	Jan-23	Jun-23	Dec-22	Dec-22	Jan-24	Oct-22	Dec-22	Jan-23	Oct-22	Jul-23	Jul-23	Feb-23	Dec-22	Sep-23	Sep-23	Sep-23	Sep-23	Sep-23	Sep-23	Jul-24	Oct-24
				Duration Months	6	4	1	2	1	1	3	16	7	2	8	11	3	2	1	3	2	2	1	50	51	51	51	51	51	51	8	1
TA02049440058-FP		HAMPSTEAD ROAD	Multi-way signals	Jan-24	2																											
TA02048593472-FP		HAMPSTEAD ROAD	Multi-way signals	May-23	2																											
TA02043257462-FP		ROBERT STREET	Road closure	Nov-22	5																											
TA02059705644-FP		HAMPSTEAD ROAD	Multi-way signals	Oct-22	11																											
TA02026774071-FP		STARCROSS STREET	Some carriage way incursion	Oct-23	3																											
TA02016557719-FP		EUSTON STREET	Some carriage way incursion	Apr-23	10																											
TA02064987133-FP		COBOURG STREET	Some carriage way incursion	Oct-22	15																											
TA02072801781-FP		EVERSHOLT STREET	Multi-way signals	Jan-23	4																											
TA02081357788-FP		EUSTON ROAD	Lane closure	Jan-23	6																											
Forward plan awaiting review and submission		GORDON STREET	Road closure	Apr-24	73																											
Forward plan awaiting review and submission		GORDON STREET	Road closure	Jan-23	12																											

Key:

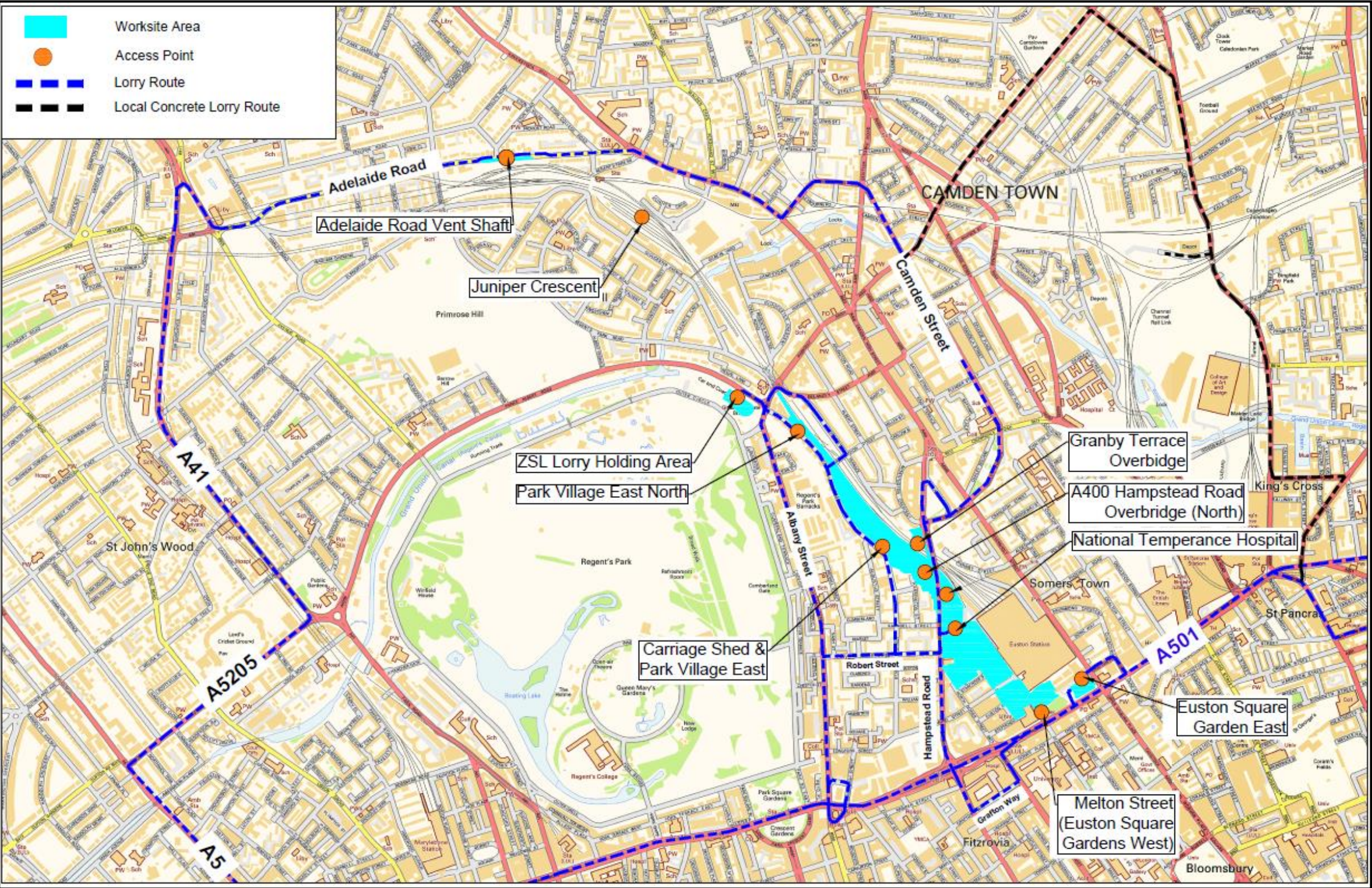
TM to be reviewed - requires further coordination

Potential further review required

No coordination required

* Above matrix is the review of the HS2 contractor highway works namely SCSjv and MDjv and any significant Forward Plans raised on Street Manager. For further details please see Appendix 1

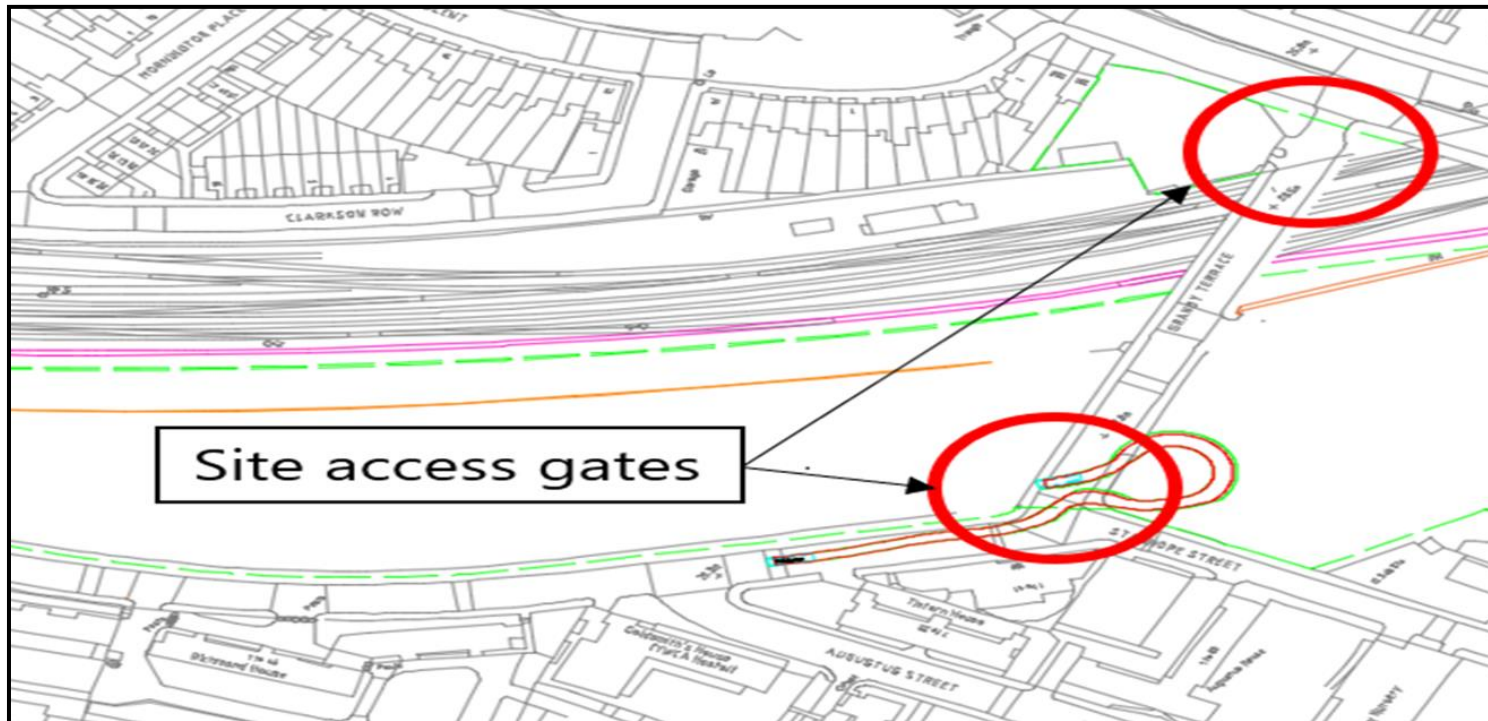
Appendix 2: Maps of proposed lorry routes



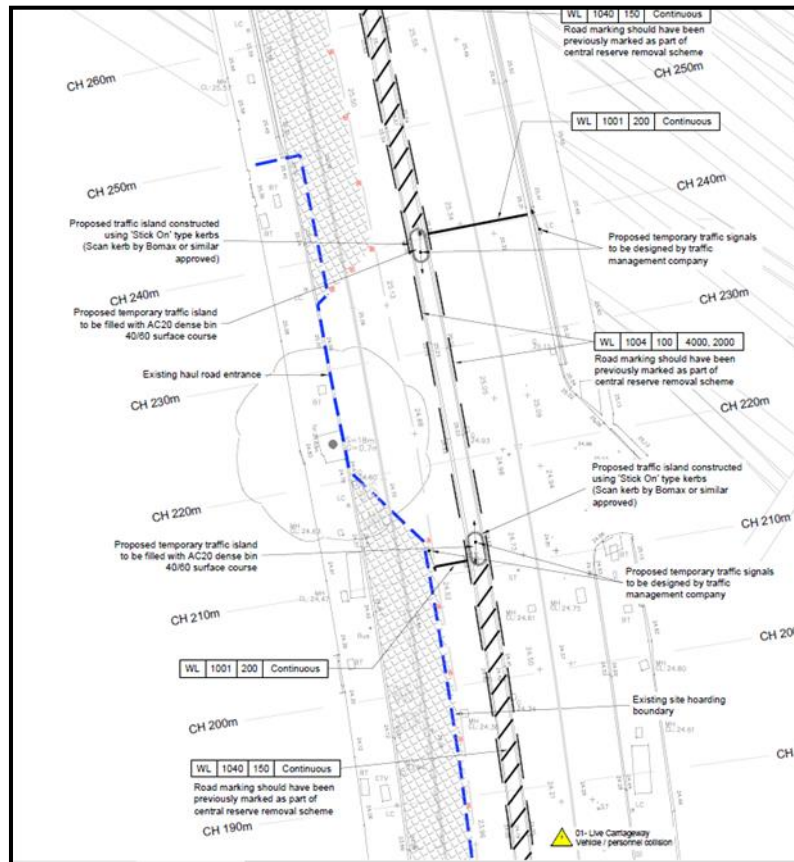
* Map is to be read in conjunction with Section 6

Appendix 3: SCS Proposed site access locations

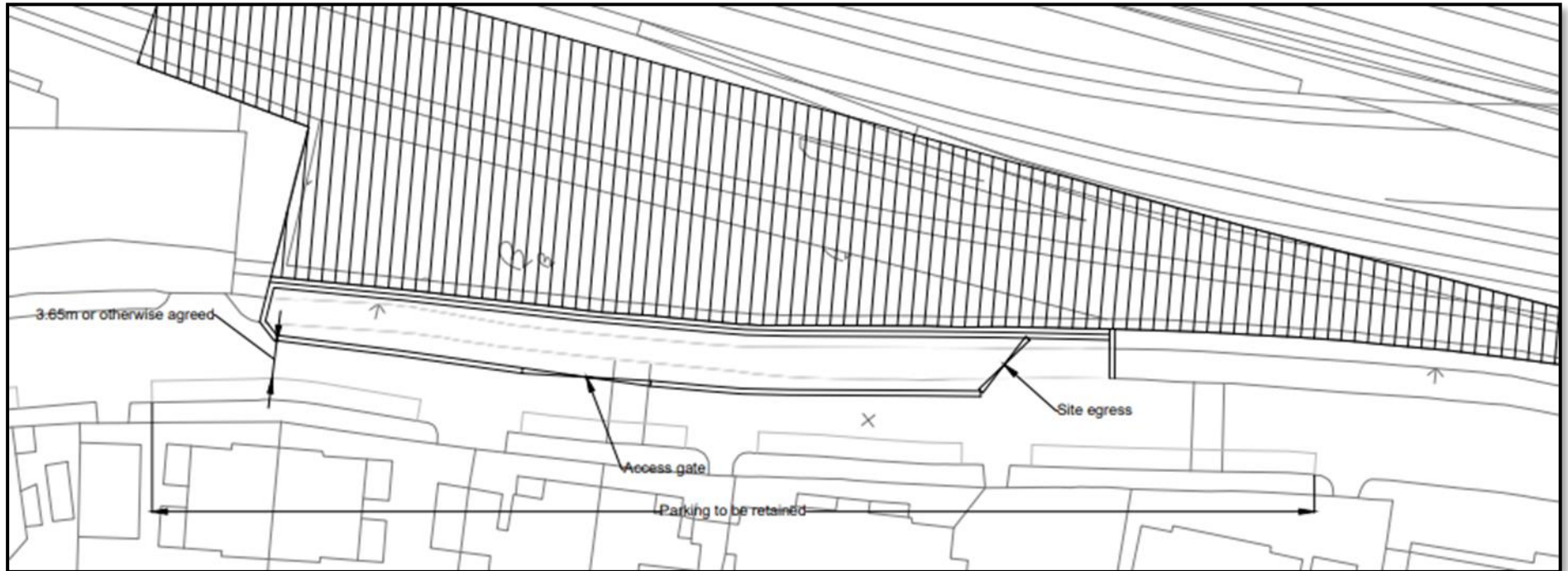
Euston Approaches worksite access points



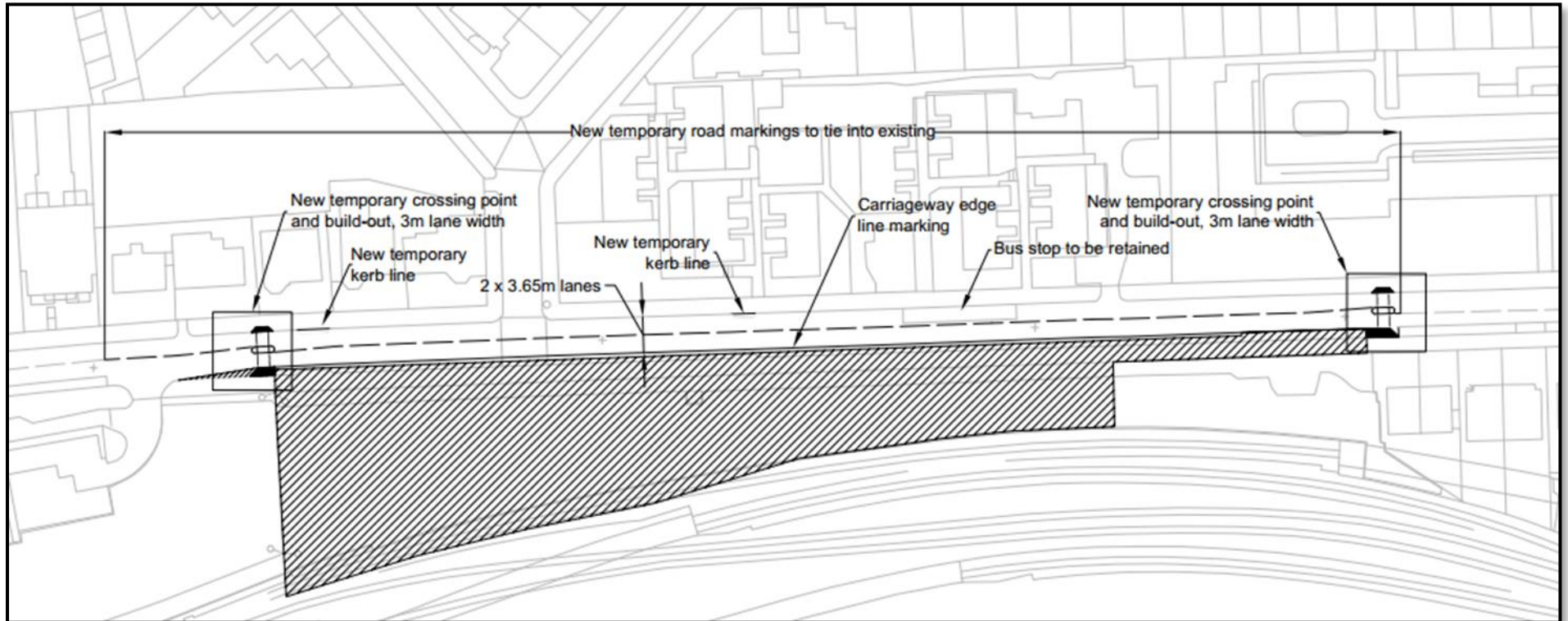
Hampstead Road access



Park Village East (North) Satellite Compound and proposed road layout during construction.



Adelaide Road Vent Shaft Main Compound access and proposed road layout during construction.



Appendix 4: Undertakings and Assurances

U&A_1108	<p>The Secretary of State will require the Nominated Undertaker, to produce Local Traffic Management Plans (LTMPs) within the London Borough of Camden prior to the commencement of any works under the Bill and keep them updated, in consultation with the highway and traffic authorities, the emergency services and other relevant key stakeholders. The LTMP(s) will include, as appropriate:</p> <ul style="list-style-type: none"> • details of permitted access routes and accesses for construction traffic • details of site boundaries and the main access/egress points for worksites and compounds • details of temporary and permanent closures and diversions of highways and other public rights of way (including timing) • a list of roads which may be used by construction traffic in the vicinity of the site including any restrictions to construction traffic on these routes, such as the avoidance of large goods vehicles operating adjacent to schools during drop off and pick-up periods and any commitments set out in the register of Undertakings and Assurances • details of phasing of works • the proposed traffic management strategy • other measures which will affect the highway, such as Lorry Holding areas (including timing of use) • proposals for the regular operation of traffic liaison groups with key stakeholders to ensure that programmes of HS2 works are shared and which will assist with the highways authorities to carry out their network management duties • a register of applications for consents associated with temporary traffic management measures. <p>In relation to lorry management, LTMPs will include details of the following, where appropriate:</p> <ul style="list-style-type: none"> • timing of site operations and timing of traffic movements • local routes to be used by lorries generated by construction activity; Lorry Holding areas • Lorry Holding areas on- or off-highway, how they will be laid out and operated (including timing of operation);
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	<ul style="list-style-type: none"> weighbridge(s) at a suitable location(s) on site to monitor compliance with vehicle weight restrictions.
U&A_2356	<p>3.1 The Secretary of State will require the Nominated Undertaker to seek to further mitigate the construction traffic impacts of the Scheme in the Camden area by reducing the number of construction related Large Goods Vehicles and maximising the amount of materials moved by rail in so far as reasonably practicable. To this effect, the Secretary of State will require the Nominated Undertaker to:</p> <p>(a) continue to explore, during detailed design, additional and/or alternative opportunities to those referred to in Clause 3.3 including but not limited to the Backing Out Road 2 siding, for removing material by rail and will consider their implementation in so far as they are reasonably practical, within Bill powers and are within the scope of the environmental assessment carried out in connection with the Bill</p> <p>(b) identify and place an obligation on its contractors to identify opportunities to reduce the number of vehicles on the road, including but not limited to bringing forward options to move material by rail</p> <p>(c) give due consideration to proposals made by the London Borough of Camden and other members of ESSRB related to further reducing traffic impacts in the London Borough of Camden, including consideration of an on-site concrete batching plant.</p> <p>3.2 To support the delivery of paragraph 3.1 the Promoter will require the Nominated Undertaker to continue to work collaboratively with the London Borough of Camden during further project development and detailed design, both bilaterally and through the ESSRB and EIPB. The Promoter will require the Nominated Undertaker to keep the Euston Strategic Board (ESB) updated regarding its work to meet its obligations under paragraph 3.1 including regular reports on progress.</p>
U&A_1308	<p>The Secretary of State will only exercise powers under the Bill in relation to Albert Street north (land parcels 165 and 166) for the purpose of, and in connection with, the diversion of utilities. After the completion of these works, it will no longer be used as a construction route unless the Promoter is required to do so by the London Borough of Camden under Schedule 17 of the Bill.</p>

U&A_2292	The Promoter will require the nominated undertaker to ensure that vehicles over 3.5t axle weight regularly attending a worksite(s) used for the purpose of the Works display a notice that identifies the vehicle as being operated as for the purposes of the Proposed Scheme.
U&A_2038_04	<p>In the exercise of the powers conferred under paragraph 1 of Schedule 2 to the Bill the Nominated Undertaker shall seek to maintain open Dumpton Place from its junction with Gloucester Avenue for traffic purposes to the Property throughout the construction period for the works to be authorised by the Bill. Use of Dumpton Place by vehicular traffic may be restricted from time to time in connection with temporary works and utilities works to be undertaken. The Nominated Undertaker shall give as much notice in advance to the Petitioner as is reasonably practicable in circumstances where vehicular access along Dumpton Place is required to be temporarily prevented.</p> <p>12.1 Any notice in writing that is to be given by the Promoter to the Petitioner shall be deemed effectively given if it is sent by recorded delivery or by registered letter addressed to English Rose Estates (Primrose Hill) Limited For the Attention of Scott Levy care of 70 Charlotte Street London W1T 4QG or any other address notified in writing to the Promoter from time to time, or otherwise served in a manner acceptable under the Bill.</p>
U&A_2103	The Promoter will take forward this recommendation through the development of the Local Environmental Management Plan (LEMP) and Local Traffic Management Plans, which it will develop with input from the local authority. Working hours will be agreed with the relevant local authority on behalf of local residents as part of securing the necessary consents under Section 61 of the Control of Pollution Act 1974.
U&A_1190	Subject to any necessary order or consent from the London Borough of Camden to allow the use of Park Village East by two-way traffic, the Promoter will require the Nominated Undertaker to maintain vehicular access to No. 2 Park Village East, London, NW1 7PX for the duration of works at Park Village East for the construction of the Proposed Scheme. Vehicular access to the driveway of No. 2 Park Village East may be restricted from time to time in connection with deliveries, construction traffic and utilities work to be undertaken for the purposes of the project, and any other unforeseen circumstances, although reasonable efforts will be made to provide access wherever practicable.

U&A_2357	The Promoter will require the Nominated Undertaker to require its contractors, in constructing the Proposed Scheme in the London Borough of Camden, to use ready-mix concrete lorries with a minimum capacity of 8 cubic metres, except in the case of an emergency or where it is necessary in order to ensure the safe construction of the works to use vehicles of a lesser capacity.
U&A_2364	The Nominated Undertaker will continue to keep the requirement for and design of the replacement Hampstead Road Bridge under review during the detailed design stage with a view to further mitigating the impacts on the community in so far as is reasonably practicable, within Bill powers and is within the scope of the environmental assessment carried out in connection with the Bill.
U&A_2379	During the construction of the Adelaide Road ventilation shaft the Secretary of State will require the Nominated Undertaker to keep one lane of Adelaide Road open to traffic at all times with traffic light control to permit two directional flows, subject to any short term closures (typically expected to be no more than one to two days) for initial construction works and except in the case of emergency, emergency works, where a closure is necessary due to unforeseen circumstances, or otherwise with the approval or at the direction of the Highway Authority.
U&A_1102	<p>The Secretary of State will require that the Nominated Undertaker prepare construction workforce travel plans, through engagement with the relevant highways authority, with the aim of encouraging the use of sustainable modes of transport to reduce the impact of workforce travel on local residents and businesses in the London Borough of Camden.</p> <p>The Nominated Undertaker's objectives to support these aims will include:</p> <ul style="list-style-type: none"> • effective management of construction worker traffic to minimise damage to the environment, impact on the surrounding road network, danger to road users and disturbance to neighbouring properties; and • the introduction of measures to reduce single occupancy car journeys by staff working on construction sites through the encouragement of car-sharing, use of available public transport, cycling and walking to work where reasonably practicable.
U&A_1103	The Secretary of State will require that the Nominated Undertaker during construction works, will ensure, insofar as is reasonably practicable that

	<p>the impacts from construction traffic on the local community in the London Borough of Camden (including all local residents and businesses and their customers, visitors to the area, and users of the surrounding transport network) are mitigated by its contractors where reasonably practicable.</p> <p>For the avoidance of doubt, the Secretary of State confirms that transport assurances provided to the London Borough of Camden shall be subject to the Nominated Undertaker securing all requisite consents under Schedule 4 and 17 of the Bill.</p>
U&A_1104	<p>The Secretary of State will require the Nominated Undertaker to prepare Local Traffic Management Plans in liaison with the London Borough of Camden, Transport for London and the emergency services.</p> <p>For the avoidance of doubt, the Secretary of State confirms that transport assurances provided to the London Borough of Camden shall be subject to the Nominated Undertaker securing all requisite consents under Schedule 4 and 17 of the Bill.</p>
U&A_1105	<p>The Secretary of State will require the Nominated Undertaker to ensure that Traffic Liaison Group (TLG) meetings will take place to enable consultation on the temporary management programme, enable the highway authority to carry out its obligations to ensure there is a co-ordinated approach to traffic management in its area and ensure that local authorities, emergency services and bus operators are aware of programme construction activities that could have an impact on the local strategic road network. The TLG meetings will take into account concurrent construction activities from other schemes.</p>
UA&_1106	<p>The Secretary of State will require that the Nominated Undertaker will maintain public access along the highway and over other areas where the public has access, where reasonably practicable, and appropriate measures will be implemented to ensure the local community, economy and transport networks in the London Borough of Camden can continue to operate effectively. Where this is not reasonably practicable, alternative measures shall be identified by the Nominated Undertaker to maintain continual public access, especially for pedestrians and cyclists, to routes in the vicinity of the construction sites within the London Borough of Camden. The impact of road based construction traffic will be reduced by implementing and monitoring clear controls on vehicle types, hours of site operation, parking and routes for large goods vehicles.</p>

	Without prejudice to any other requirements to do so, the Nominated Undertaker will take reasonable steps to ensure that the London Borough of Camden is notified of those controls and informed of any changes to them.
U&A_1107	The Secretary of State will require the nominated undertaker to, where reasonably practicable, retain access for pedestrians and cyclists where safe and appropriate to do so, including where a highway is closed to other traffic under the powers of the Bill. Prior to any formal application under the Bill relating to traffic or highways proposals, site specific measures will be discussed with highway authorities and emergency services through the Traffic Liaison Group meetings established in accordance with the Code of Construction Practice and the Route-wide Traffic Management Plan. Examples of the measures will include:
U&A_1109	Lorry routes will be set out in the LTMPs and as set out in the Planning Memorandum, the Nominated Undertaker will have forward discussions, where reasonably practicable, on lorry route applications prior to submission.
U&A_1110	<p>The Secretary of State will require the Nominated Undertaker to produce and update (where reasonably required) site specific traffic management measures within the London Borough of Camden and where reasonably practicable to discuss the site specific traffic management measures with the highways authorities and the emergency services. The site specific traffic management measures will include the following, as appropriate:</p> <ul style="list-style-type: none"> • details about specific traffic management, within site specific plans • road traffic management layouts and signage including works necessary for site access for construction traffic, which will be subject to consultation with the relevant highway authority • installation of appropriate signage indicating all temporary and permanent diversions of PRow • measures to be implemented to reduce construction traffic impacts, or impacts associated with parking on residential streets • measures to minimise impact on highway users • retaining access for cyclists and pedestrians, where safe and appropriate • timing of traffic management operations, if their scope can be undertaken during off-peak, night or weekend working • parking controls

	<ul style="list-style-type: none"> • measures to ensure that construction vehicles do not cause damage to highways, and measures to ensure that any damage to grass verges is repaired and reinstated • requirements relating to the movement of traffic from business and commercial operators of road vehicles, including goods vehicles • controls to reduce environmental impacts to nearby receptors and consideration of temporary reduced speed limits around worksites • co-ordination with utility companies and service diversions; and • - winter gritting plans, which will complement those of the relevant highway authority.
U&A_1111	<p>The Secretary of State shall require that the Nominated Undertaker and its contractors shall carry out such monitoring as is necessary to ensure compliance with the requirements of the CoCP, and this will include the maintenance of records of traffic management measures. The monitoring programme, the approach to regular consultation with highway authorities and emergency services and the control processes will be required to be set out in the Environmental Minimum Requirements. The Secretary of State will require the nominated undertaker to set out within the Route-wide Traffic Management Plan, in accordance with the Code of Construction Practice, appropriate measures to require monitoring for the purpose of identifying deviation of Large Goods Vehicles from authorized construction routes , and where there has been an identified deviation, this may result in the issue of a Traffic Enforcement Notice (TEN) and the principal contractor will be alerted to this breach.</p>
U&A_1113	<p>The Secretary of State will require the nominated undertaker to prepare and maintain a Route Management, Improvement and Safety Plan (ROMIS) as set out within the Route-wide Traffic Management Plan prepared in accordance with the Code of Construction Practice. The objective of the ROMIS plan will be to demonstrate that the traffic grounds for refusal of a route as set out in Schedule 16 to the Bill have been appropriately considered. The ROMIS plan shall:</p> <ul style="list-style-type: none"> • set out the construction routes to be approved in accordance with Schedule 17 to the Bill • consider what physical changes to the highway and other land are necessary to enable use by Large Goods Vehicles • confirm that the measures related to safety and free flow of traffic have been considered and, as necessary, mitigated in the proximity of the construction access points.

U&A_1114	The Secretary of State will require the nominated undertaker to comply, as far as reasonably practicable, with the Construction Logistics for Cyclist Safety (CLOCS) standards and the First Operator Recognition System (FORS) or such equivalent or better standards or system that may replace them.
U&A_1115	The Secretary of State will require the nominated undertaker to work with the London Borough of Camden (including through the production and periodic review (with the participation of the London Borough of Camden and TfL) of the Local Traffic Management Plan and through the Traffic Liaison Group) to ensure the Nominated Undertaker supports them as the local highway authority to enable them to carry out their obligations to ensure there is a coordinated approach to traffic management in their area and address local issues as they arise. The Local Traffic Management Plan for Euston will be the subject of regular review to enable it to continue to address local circumstances while recognising HS2's cost and programme constraints.
U&A_1156	The Secretary of State will require the nominated undertaker so far as reasonably practicable to consider opportunities for joint use of construction sites with other construction projects within the London Borough of Camden area insofar as it does not impact the timely, economic and safe delivery of the Proposed Scheme.
U&A_1158	The Secretary of State will require the nominated undertaker so far as reasonably practicable to coordinate activities on and relating to the various construction sites in the London Borough of Camden so as to reduce the combined impact of HS2 construction within the London Borough of Camden area insofar as it does not impact the timely, economic and safe delivery of the Proposed Scheme. This will include, but not be limited to, the coordination of applications for consent under section 61 of the Control of Pollution Act 1974.
U&A_1160	<p>The Secretary of State will require the Nominated Undertaker to seek reasonably practicable measures to further reduce or avoid significant effects on schools, in doing so the Promoter will continue to consult the London Borough of Camden and the schools. Such measures could include but are not limited to:</p> <ul style="list-style-type: none"> • the provision of acoustic installations, including glazing, and associated ventilation • the provision of air handling equipment

	<ul style="list-style-type: none"> the provision of high hoardings no noisy works being carried out in the vicinity of the school during examination periods the use of additional lorry control methods and banksmen.
U&A_1162	The Secretary of State will require the Nominated Undertaker and/or its contractors, in accordance with paragraph 6(2) of Schedule 4 to the Bill, to maintain pedestrian access to and from the relevant schools during term time and normal operating hours of the relevant schools save where it is necessary to temporarily suspend access in the case of an emergency or for the implementation of traffic and pedestrian measures.
U&A_1163	The Secretary of State will, during detailed design and during construction planning of the HS2 works in the London Borough of Camden, require the Nominated Undertaker and/or its contractors to consult the local community, the London Borough of Camden and other Stakeholders in the London Borough of Camden in order to develop the Local Traffic Management Plans (LTMPs). These LTMPs will develop mechanisms and requirements for the management of protecting school pupils and other vulnerable road users (including all pedestrians).
U&A_1164	The Promoter will continue to consult the local community, local schools and the Local Education Authority to understand the potential impact of the scheme on schools and what mitigation might be provided in the event that the construction of HS2 does significantly impact the school or impact on the viability of the school.
U&A_1165	The Secretary of State will require the Nominated Undertaker to engage with the London Borough of Camden and schools to fully understand any impacts that may occur and identify and reasonable measures to mitigate as identified within the Code of Construction Practice.
U&A_1166	The nominated undertaker shall engage with the London Borough of Camden prior to completion of the detailed design of the traffic management associated with the construction work at Euston station and elsewhere, with a view to reducing as far as reasonably practicable any disruption caused by such works to the efficient arrival and departure of pupils, staff and other visitor to and from school premises.
U&A_1167	The nominated undertaker shall, in accordance with the Code of Construction Practice, give reasonable advance notice of the construction works taking place around schools to the London Borough of Camden.

U&A_1271	The nominated undertaker shall engage with the Petitioners prior to completion of the detailed design of the traffic management associated with the utility works with a view to reducing, as far as reasonably practicable, any disruption caused by utility works to The Dublin Castle.
U&A_1272	The nominated undertaker shall, having regard to representations made during the engagement referred to in paragraph 1 and in consultation with the relevant highway authority, give due consideration to the following factors during the detailed design of the traffic management associated with the utility works with a view to reducing any significant adverse impact of those works on The Dublin Castle, so far as reasonably practicable and subject to the requirements of the construction programme and the powers authorised in the Bill, timing any particularly disruptive short term utility works to avoid hours of peak activity; and maintaining access for customers, staff, deliveries and bands to The Dublin Castle
U&A_1273	The nominated undertaker shall, in accordance with the Code of Construction Practice, give reasonable advanced notice of the utility works to the Petitioners.
U&A_1808	The nominated undertaker shall engage with the Petitioner prior to completion of the detailed design of the traffic management associated with the Relevant Scheme Works with a view to reducing, as far as reasonably practicable, any disruption caused by the Relevant Scheme Works to the access to and egress from Regents Park Terrace.
U&A_1809	<p>The nominated undertaker shall, having regard to representations made during the engagement referred to in paragraph 1 (U&A Ref ID 1808), and in consultation with the relevant highway authority, give due consideration to the following factors during the detailed design of the traffic management associated with the Relevant Scheme Works with a view to reducing any significant adverse impact of those works on access to and egress from Regents Park Terrace, so far as reasonably practicable and subject to the requirements of the construction programme and the powers authorised in the Bill:</p> <ul style="list-style-type: none"> • providing reasonable access for pedestrians to Regents Park Terrace • maintaining vehicular access and egress to Regents Park Terrace • (save for unplanned closures due to unforeseen circumstances).

U&A_1810	The nominated undertaker shall, in accordance with the Code of Construction Practice, give reasonable advanced notice of the Relevant Scheme Works to the Petitioner. (Regent's Park Rd)
U&A_2060	Except in the case of utility works, the Promoter does not intend construction traffic required in connection with the authorised works to be routed via Regent's Park Road and will not seek to have traffic diverted along that road unless directed to do so by the local highway authority.
U&A_2061	The Promoter will require that its contractors must produce advance information sheets that set out any parking bay suspensions in Regent's Park Road, King Henry Road, Gloucester Avenue and Berkley Road required in consequence of the carrying out of the authorised works; and will require that, where reasonably practicable to do so, the information sheets must be circulated at least 2 weeks before the parking bay suspensions take effect and be made available online.
U&A_2062	The Promoter will, where reasonably practicable, ensure that vehicular access to the Properties will be maintained. (Regents Park Road)
U&A_1192	Subject to any necessary order or consent from the London Borough of Camden to allow the use of Park Village East by two-way traffic, the Promoter will require the Nominated Undertaker to maintain vehicular access to No. 4 Park Village East, London, NW1 7PX for the duration of works at Park Village East for the construction of the Proposed Scheme. Vehicular access to the driveway of No. 4 Park Village East may be restricted from time to time in connection with deliveries, construction traffic and utilities work to be undertaken for the purposes of the project, and any other unforeseen circumstances, although reasonable efforts will be made to provide access wherever practicable.
U&A_1253	The nominated undertaker shall engage with the Petitioners prior to completion of the detailed design of the traffic management associated with the utility works with a view to reducing as far as reasonably practicable any disruption caused by the utility works to the efficient arrival and departure of pupils, staff and other visitors to and from the school premises. (North Bridge House Prep School)
U&A_1254	The nominated undertaker shall, having regard to representations made during the engagement referred to in paragraph 1, and in consultation with the relevant highway authority, give due consideration to the

	<p>following factors during the detailed design of the traffic management associated with the utility works with a view to reducing any significant adverse impact of those works on the school so far as reasonably practicable and subject to the requirements of the construction programme and the powers authorised in the Bill:</p> <ul style="list-style-type: none"> • timing utility works to avoid seasonal peak periods of activity for the school, for example by carrying out works during school holidays; • timing any particularly disruptive short term utility works to avoid hours of peak activity for the school; • co-ordinating any separate elements of the utility works on the relevant roads; • phasing utility works so as to minimise the overall impact due to associated traffic restrictions in the London Borough of Camden; and • taking into account other construction activities relating to the Proposed Scheme in the vicinity of the utility works. <p>(North Bridge House Prep School)</p>
U&A_2305	<p>In the period prior to commencement of construction of the Works and for the duration of the construction of the Works which result in the loss of vehicular parking along Park Village East, the Promoter will require the nominated undertaker to explore, with the London Borough of Camden as the relevant local highway authority, potential measures to mitigate the impact of any such loss of parking on the residents of Park Village East.</p> <p>(Park Village East Heritage Group)</p>
U&A_2306	<p>The Promoter will require the nominated undertaker to, when preparing the relevant Local Traffic Management Plan(s) in accordance with the HS2 Phase 1 Route-wide Traffic Management Plan, consider the requirements for maintaining access for emergency services to the properties on Park Village East during the Works. (Park Village East Heritage Group)</p>
U&A_2400	<p>The Secretary of State will require the Nominated Undertaker to work to reduce the impact of utility works undertaken on Park Village East (as part of or in connection with the HS2 Phase One construction works) on the northern vehicular access to Park Village Limited, and specifically to use reasonable endeavours to maintain vehicular access to Park Village</p>

	Limited via the northern vehicular access for the duration of these utility works.
U&A_2342	<p>The Promoter will require the Nominated Undertaker to, in accordance with HS2 Information Paper D11 (as updated from time to time) use reasonable endeavours to avoid obstructing (so as to prevent functional vehicular access) insofar as it is within the Nominated Undertaker's remit and control at least one of the separate Park Village Limited vehicle and pedestrian accesses ("the Park Village Access") during the operational hours of the Property, during the construction period of the Specified Works SUBJECT TO any necessary orders and consents being obtained to permit the use by two-way traffic of the highway known as Park Village East and PROVIDED THAT the Nominated Undertaker may restrict the Park Village Access from time-to-time for the purpose of allowing deliveries, construction traffic movements and utilities works relating to the construction of the Specified Works and in the case of emergencies.</p> <p>The Promoter acknowledges that Park Village Limited may be entitled to a claim for compensation for injurious affection under section 10 of the Compulsory Purchase Act 1965 in relation to the disruption to access as a result of utility works to be carried out in the highway known as Park Village East.</p>
U&A_1643_01	The Promoter will require the nominated undertaker to maintain vehicular access to the garages of Silsoe House, No. 50 Park Village East, London, NW1 for the duration of those part of the Works at Park Village East save that such access may be restricted from time to time in connection with deliveries, construction traffic and utilities work to be undertaken for the purposes of the Works, and any other unforeseen circumstances, although reasonable efforts will be made to provide access wherever practicable.
U&A_1217	The Promoter will require the Nominated Undertaker to design the replacement Hampstead Road Bridge with 4 vehicular lanes plus segregated cycle lanes and pedestrian walkway.
U&A_2333	<p>The Promoter will require the Nominated Undertaker (either directly or through its contractors) to seek to work collaboratively with the Zoological Society of London and the Royal Parks to further develop the</p> <p>Promoter's proposals for appropriate ecological mitigation to reduce the effects on the Regent's Park hedgehog population from the proposed</p>

	HS2 Vehicle Holding area and any replacement parking created within the Gloucester Slips car park site ('the ecological mitigation').
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Appendix 5: MDJV Gate 18 Justification and Mitigation Review

Background

- 7.1.7 The 'Interim' phase of works for Euston Station are for early site enabling works (including pile mat construction) and early works piling between January 2023 and December 2023. These works will utilise routes that are stated within *Section 6* and will form part of Schedule 17 Lorry Route Approval submission for this period. Gate 18 as shown in Figure 2 is situated on Hampstead Road (TLRN) on the eastern side of the Hampstead Road footway approximately 140 metres south of Granby Terrace junction. To access the Gate 18 from the south, heading northbound on Hampstead Road, use of Harrington Square Gardens as a loop is required to access the Gate 18. The movement at this gate is proposed to be left in during access and left out during egress.
- 7.1.8 The proposed use of the Harrington Square loop has been discussed on a number of occasions with LBC officers during pre-application meetings and in paragraphs 2.2.11 and 2.2.28 of the submitted Written Statement for Information (ref: 1CP01-MDS_ARP-TP-REP-SS08_SL20-990009 C01). These confirm that the use of Harrington Square is required for vehicles to turn around and gain access into the NTH Main Worksite via Gate 18, for vehicles travelling on A400 Hampstead Road (northbound). This would be for occasional, low usage (maximum 12 vehicles per day). A review of all options considered throughout consultation with LBC is set out in the sections below.

Gate 18 Scope Justification

- 7.1.9 As part of Schedule 17 submission that will support the "Interim" phase of works for Euston Station, in addition to the use of Cardington Street gates MDJV would like to retain the ability to use aforementioned Gate 18.
- 7.1.10 Cardington Street main site entrance caters for most of the vehicles entering the NTH compound inclusive of northern end known as Zone 5. This area to the north is longer than it is wide. The works involved in piling and installation of various piling mats in addition to other enabling works may cut the access from Cardington Street to the north of the site.
- 7.1.11 Please see Figure AP6.1 and AP6.2 below that demonstrate Early Works Piling Zone 5 constraints. These constraints comprise from:

- Site triangular shape constraints the construction vehicle movement at northern apex of the site
- Various piling mat level difference impose further limitations to construction vehicle movement within site
- Piling sequence is likely to obstruct the construction vehicle movement from Cardington Street for short periods
- Works within southern section have the potential to impede access to northern section of the site (zone 5)

7.1.12 Constraints associated with the northern end of the site within Zone 5 are shown in Figure AP6.1.

Gate 18 Mitigation Considered

1. *Access to Gate 18 by means of a right turn from Hampstead Road heading in the northbound direction.*

SCS has confirmed that they wish to extend their consent duration of the right hand turn and temporary traffic light signal arrangement well beyond December 2022. Whilst such traffic management arrangement is in place it is not feasible to turn right into Gate 18.

For vehicles accessing Gate 18 of the NTH Main Worksite, the Harrington Square gyratory shall only be used, in the eventuality that no right-hand turn is available to vehicles accessing Gate 18 from A400 Hampstead Road heading northbound. If a right-hand turn is available, Large Goods Vehicles accessing the NTH Main Worksite, Gate 18 from Hampstead Road heading northbound shall not use the Harrington Square gyratory.

2. *Utilising previous access arrangement to Zone 5*

Previous arrangement consisted of use of access "Gate 17" that was situated just off Hampstead Road on the access road that was connected to Cardington Street and adjacent to the railway tracks. Historically due to safety reasons the access road during the project had a weight limit of 26t imposed. This access road has been removed and is no longer in existence and 2 metre margin exclusion zone is established off the back of the retaining wall to the railway tracks. As such, Gate 18 has to serve as an access and egress.

3. Access from SCS worksite Gate S11 G2B

SCS Gate S11 G2B is situated some 55 metres to the north of Gate 18 on western side of Hampstead Road, please see Figure 4 & AP.06. The mitigating proposal consisted of exploring turnaround of MDjv construction vehicles within Gate S11 G2B and then utilise the right turn out of the SCS site heading southbound and turn left into Gate 18 removing the need to send vehicles around the Harrington Square Gardens loop. Following discussions with SCS and MDjv the implications identified as a result of this proposal in hierarchical order are:

- Due to spatial constraints at the current SCS Hampstead Road access there is no onsite capacity to handle MDjv works vehicles, in addition to SCS own logistics activities that take place on entry and exit, such as compliance checks and wheel washing. Space constraints illustrated in Figure below:

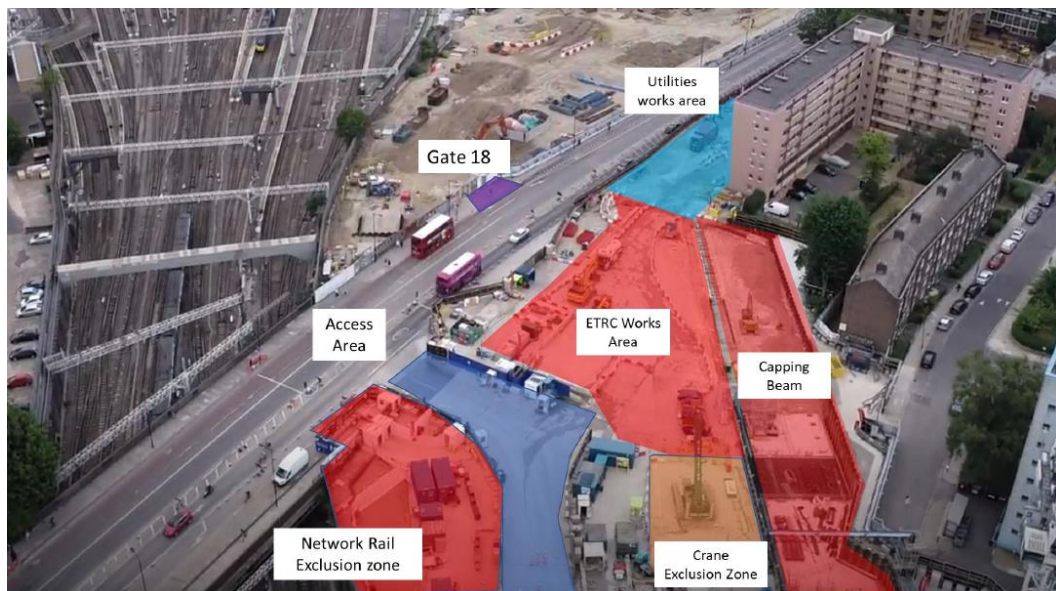


Figure AP.06: SCS Site Access Constraints

SCS vehicles are booked into site using a vehicle management system which details the number, estimated arrival time and type of vehicles. MDjv do not have access to the SCS vehicle management system therefore SCS would have no visibility of the number, arrival times and type of MDjv vehicles which would impact on the safe operation of the access. The management of, and identification of MDjv vehicles in addition to SCS activities has the potential to cause vehicles to queuing back onto Hampstead Rd creating a road safety risk in breach of the SCS EMRs set out in the HS2 Phase 1 Route Wide Traffic management plan regarding measures to minimise queuing on the highway.

SCS are seeking to reduce the people plant interface wherever possible. Rejected vehicles egress from site under site traffic management operative supervision, and MDjv vehicles would be controlled in the same way and this would increase the people plant interface in the already space constrained access area.

- Traffic signals that are in use at the access point allow for a left turn in and right turn out of SCS site. For MDjv vehicles to access Gate 18 a new straight across movement would have to be included in the traffic signal method of control which would change the inter-green times and impact on network performance. Reviewing the location of the Gate 18 access it would be approximately 55m from the southbound stop line. Therefore, to give the adequate clearance for works vehicles to exit the SCS site and into Gate 18 this would increase the all red phasing of the traffic signals from the current time of 5 seconds to 10 seconds, doubling the lost time at the junction each cycle. There is also a potential for MDjv vehicles needing a separate phase to safely exit from Gate 18 under this arrangement which could further reduce the green time given to Hampstead Road. It is also important to note that any changes to the operation of the junction would need to be agreed with TfL in addition to the current letter of consent issued to SCS as well undertaking of a Road Safety Audit. Neither contractor supports this alteration to the junction.
- MDjv have safety concerns that MDjv vehicles associated movement with this proposal from SCS gate are to be in the blind spot for any buses moving away from Silverdale Bus Stop W as per below Figure AP6.
- SCS anticipate change of position to their current main access gate due to works sequences from mid-2023 onwards. This anticipated gate position is south of the Gate 18. Access from SCS new gate position would mean traveling north to Gate 18 and would not be possible.

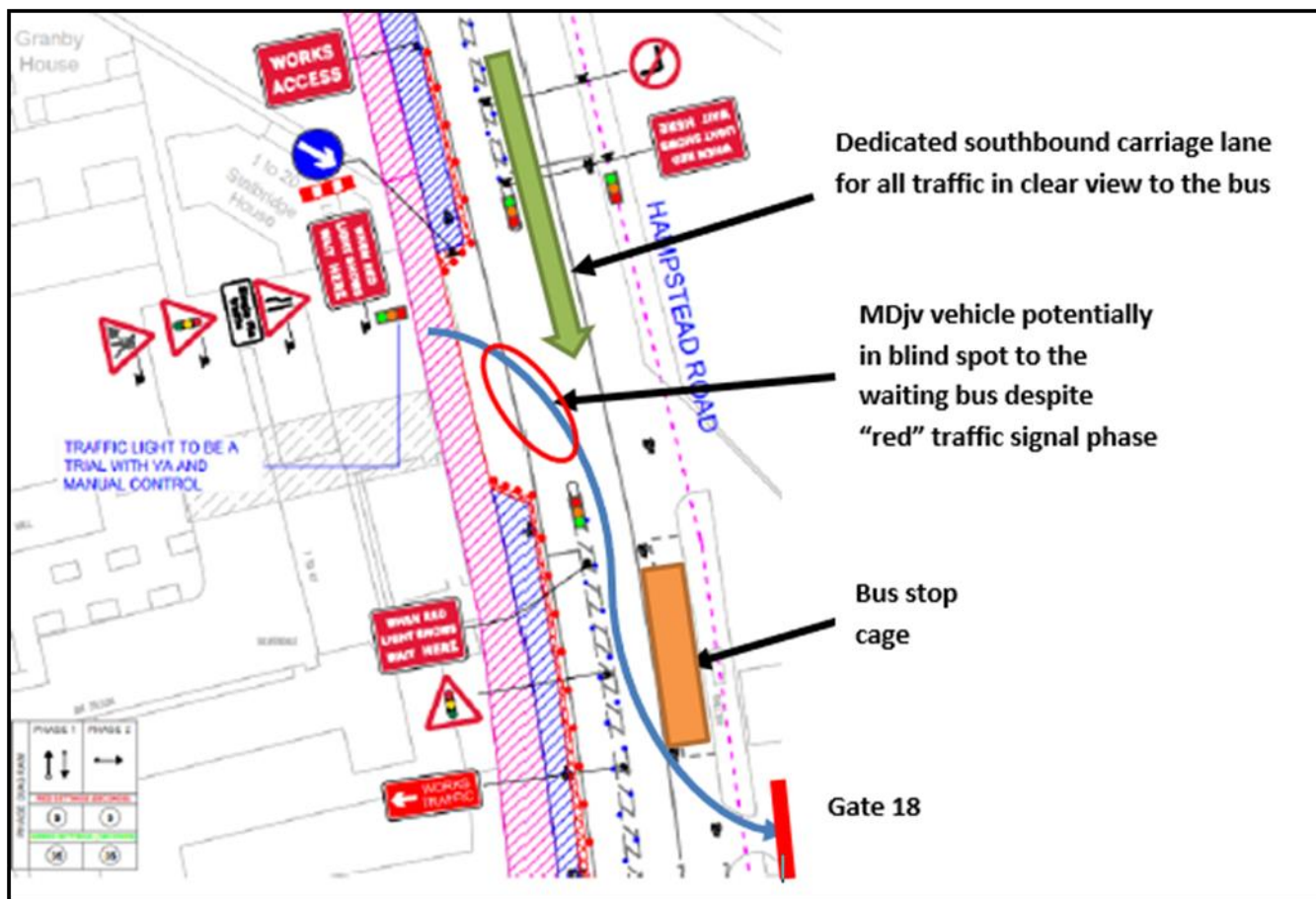


Figure AP6: Proposed MDJv manoeuvre and construction vehicle blind spot to the bus stop on southbound lane

Conclusion

- 7.1.13 Upon consideration of the proposal to utilise SCS Hampstead Road access gate for MDJv vehicle to turnaround and access Gate 18 removing the need to send vehicle around Harrington Square Gardens it was concluded that this proposal was not safe. SCS due to primary reason identified (internal S1 site constraints) do not believe it is feasible for MDJV vehicles to use the SCS Hampstead Road access for MDJV turnaround to Gate 18. In addition, the combined cumulative risk from the implications identified associated with this proposal to the safety of the workforce and general public is not supported and accepted by neither contractor.

Figure AP6.1: NTH Early Works Piling Northern Compound (Zone 5) constraints

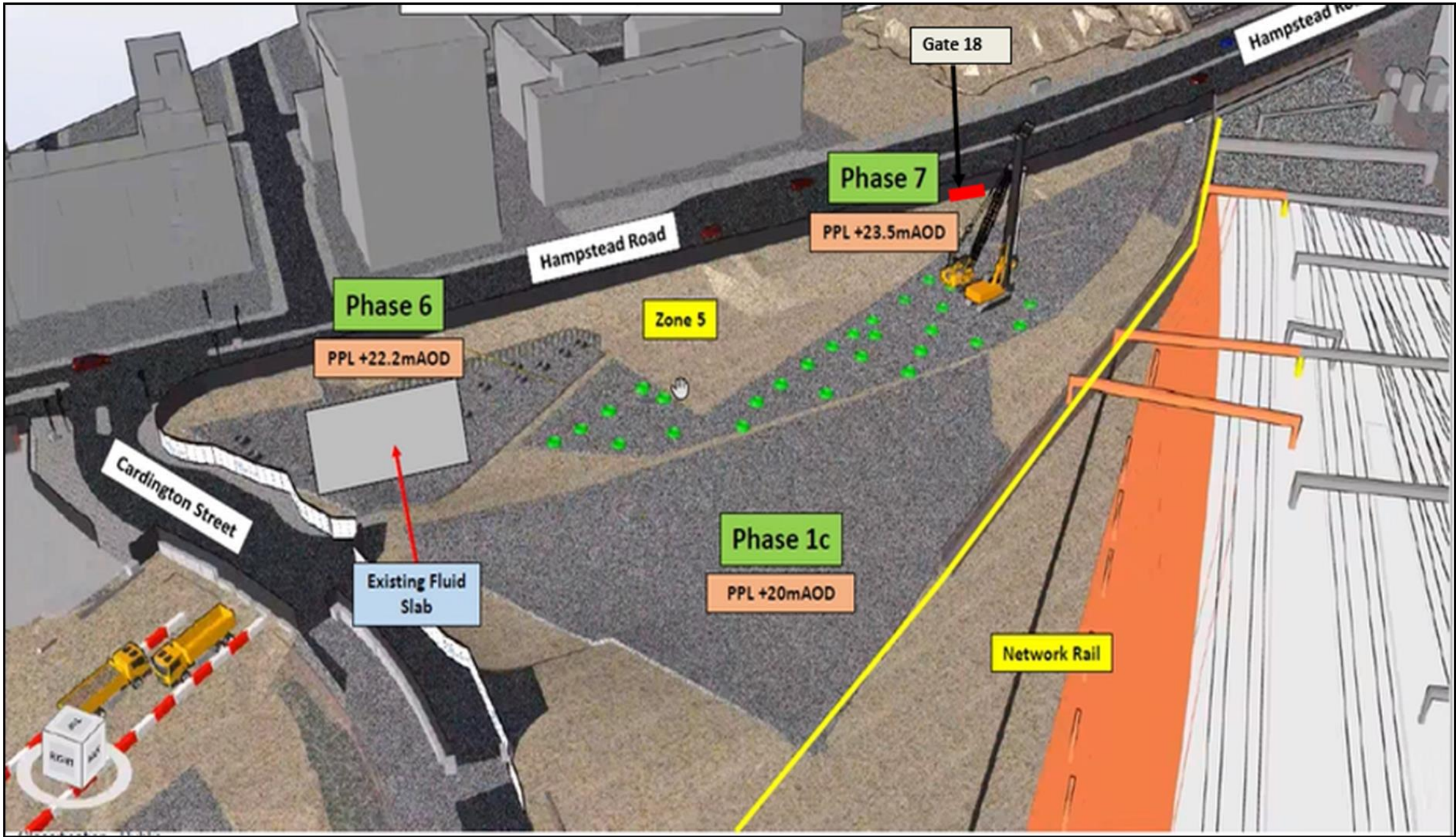
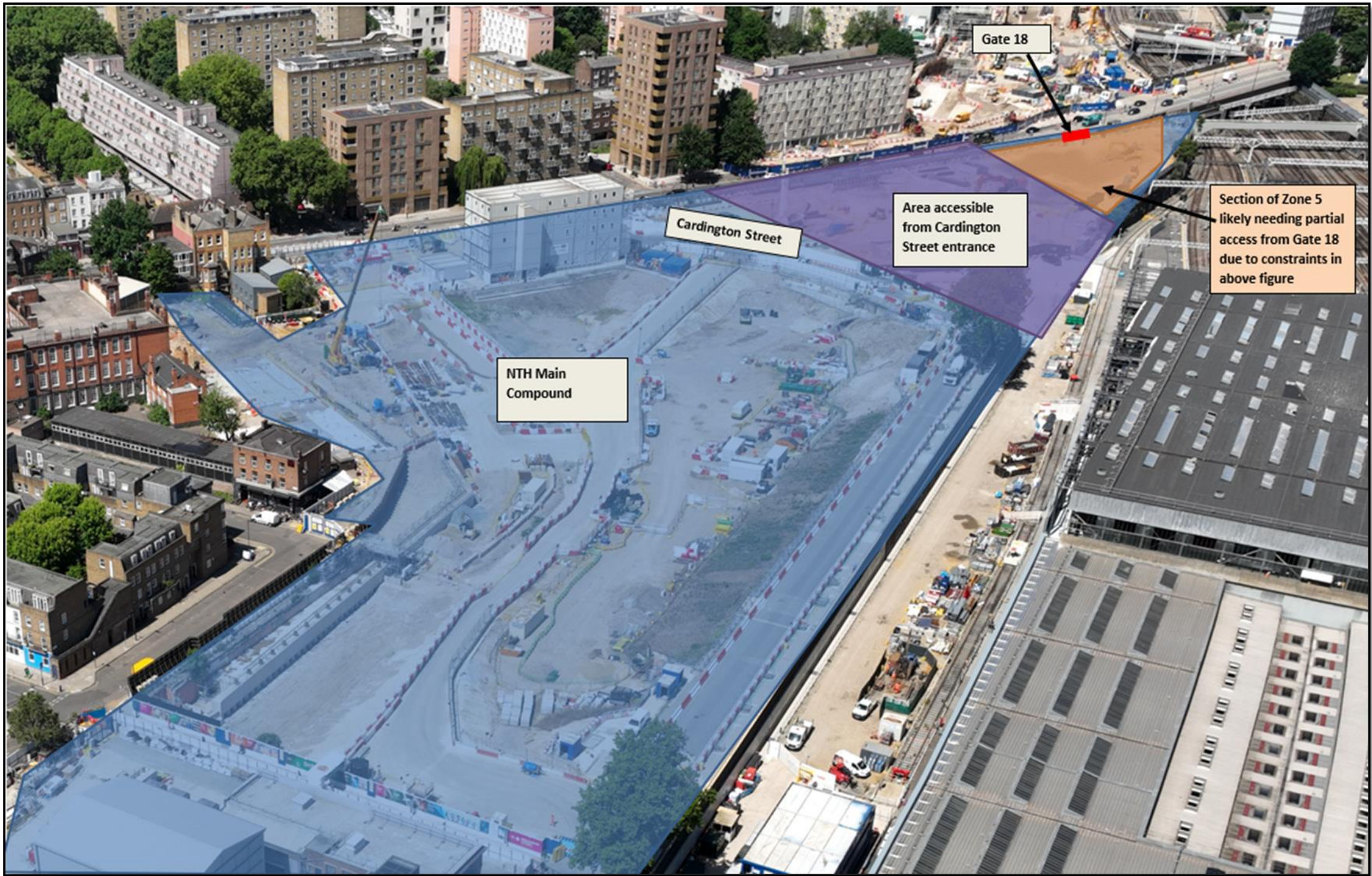






















































Figure AP6.2: NTH Early Works Piling northern compound (Zone 5) Gate 18 approximate serviceability area (sketch for illustrative purpose only)



Appendix 6: MDJV Concrete Route Sift Matrix

Concrete Route Options Considered	North Access 1	North Access 2	North Access 3	North Access 4	South Access 1	South Access 2	South Egress (primary)	South Egress (secondary)
Roads and No. of roads								
1	A5200 York Way (LBC)	A5200 York Way (LBC)	A5200 York Way (LBC)	A5200 York Way (LBC)	A5200 York Way (LBC)	A5200 York Way (LBC)	Melton Street (LBC)	Cardington Street (LBC)
2	A5200 Camden Park Road (LBC)	A5200 Camden Park Road (LBC)	Agar Grove (LBC)	Goods Way (LBC)	A5203 Wharfedale Road (LBI)	A5203 Wharfedale Road (LBI)	A501 Euston Road - east (TLRN)	A400 Hampstead Road (TLRN)
3	A503 Camden Road (TLRN)	A503 Camden Road (TLRN)	A502 St. Pancras Way (LBC)	Pancras Road (LBC)	A5203 Caledonian Road (LBI)	A5203 Caledonian Road (LBI)	A5200 York Way (LBC)	A501 Euston Road - west (TLRN)
4	A400 Camden Street (TLRN)	A502 St. Pancras Way (LBC)	Pancras Road (LBC)	B512 Crowndale Road (LBC)	King's Cross Bridge (TLRN)	King's Cross Bridge (TLRN)		Great Portland Street (WCC)
5	A400 Oakley Square (TLRN)	Pancras Road (LBC)	B512 Crowndale Road (LBC)	A400 Oakley Square (TLRN)	A501 Grays Inn Road (TLRN)	A501 Grays Inn Road (TLRN)		A4201 Albany Street (LBC)
6	A400 Lidlington Place (TLRN)	B512 Crowndale Road (LBC)	A400 Oakley Square (TLRN)	A400 Lidlington Place (TLRN)	A501 Euston Road - west (TLRN)	A501 Euston Road - west (TLRN)		A4201 Osnaburgh Terrace (LBC)
7	A400 Harrington Square (TLRN)	A400 Oakley Square (TLRN)	A400 Lidlington Place (TLRN)	A400 Harrington Square (TLRN)	Great Portland Street (WCC)	A400 Gower Street (LBC)		A4201 Osnaburgh Street (LBC)
8	A400 Hampstead Road (TLRN)	A400 Lidlington Place (TLRN)	A400 Harrington Square (TLRN)	A400 Hampstead Road (TLRN)	A4201 Albany Street (LBC)	Grafton Way (LBC)		A501 Euston Road - east (TLRN)
9	Cardington Street (LBC)	A400 Harrington Square (TLRN)	A400 Hampstead Road (TLRN)	Cardington Street (LBC)	A4201 Osnaburgh Terrace (LBC)	A400 Tottenham Court Road (LBC)		A5200 York Way (LBC)
10		A400 Hampstead Road (TLRN)	Cardington Street (LBC)		A4201 Osnaburgh Street (LBC)	A400 Hampstead Road (TLRN)		
11		Cardington Street (LBC)			A501 Euston Road - east (TLRN)	Cardington Street (LBC)		
12					A400 Hampstead Road (TLRN)			
13					Cardington Street (LBC)			
Applicable review Criteria	Positive 	Negative 	Opportunity 	Neutral 				
Use TLRN and Strategic Regional Network roads as much as possible								
Makes use of routes assumed and assessed under Environmental Statement where feasible								
Take shortest/fastest route to TLRN and SRN								
Construction constraint								
Avoids alterations to the existing road network where practicable								
Utilises local suppliers								
Distance to site (miles)	2.2	2.3	1.8	1.6	2.7	2.2	1.3	2.5
No. of roads total	9	11	10	9	13	11	3	9
No. of LBC roads total	3	6	5	5	5	5	2	5

Appendix 7: TfL Comment and Response Sheet

Comment Sheet Rev No:	C02	Comment Response Key
Document No:	1CP01-MDS-CL-PLN-SS08_SL20_GF-000004_C03	MDJV
Document Title:	Local traffic Management Plan - London Borough of Camden	SCS
Reviewed Doc Revision:	C03	Combined
Date Issued:	21/09/2022	

			Stakeholder Comments			HS2/NU Response		
No.	Page No.	Document Ref	Reviewer	Position	Comment	Response from NU/HS2	Notes	Open/Closed
1	8	1.3.2	TFL - Michael Beaver	Principal Network Impact Manager	A number of utility contractors will also be carrying out non-contestable works within London Borough of Camden to enable the approved HS2 works. Comment – the nature of the works isn’t important if it affects a TfL service	Comment is noted regarding effect of the non-contestable works to TfL.	No amendment required	Closed
2	31	3.4.4	TFL - Michael Beaver	Principal Network Impact Manager	All works that take place on Hampstead Road including the planned closures will be assessed against the criteria within Section 15 (Traffic and Transport) of the Environmental Statement EIA Scope and Methodology Report. Comment – when is this closure expected to take place	Appendix 1 captures Street Manager Forward Plan where any significant road closures are outlined and will be reviewed during future revisions. Currently closures on Hampstead Road associated with temporary road are anticipated to take place in 2026 and 2028.	Changes to be adopted in future revision	Closed
3	40	3.5.17	TFL - Michael Beaver	Principal Network Impact Manager	Lorry access and egress routes for this work compound are contained in Section 6. The secondary egress route that would utilise the loop around the war memorial is subject to further discussion and agreement with TfL. Comment – this has been rejected by TfL	Comment is acknowledged and paragraph 3.5.17 has been amended accordingly.	paragraph and section amended	Closed
4	49	3.7.6	TFL - Michael Beaver	Principal Network Impact Manager	Traffic impact will be in terms of closure of the Gordon Street preventing through access to A501 Euston Road and Taxi Rank. In addition to the closure of the road to vehicle traffic, some alterations will be required for pedestrians. Once the first stage of piling is complete, the utilities will be diverted into their final alignments in the footpaths either side of the piled box. Pedestrian access should be enabled between Endsleigh Gardens/Gower Place and A501 Euston Road. Comment – should this include pedestrian assessment	The Gordon Street utility works will be phased to mitigate severance impacts to pedestrians between Euston Road and Endsleigh Street. At least one side of the footway, either eastern or western will be maintained for public and will only constitute a minor impact to be dealt via HS2 Schedule 4 submission hence not warranting pedestrian assessment. Pedestrian assessment will be carried out for the main works on Gordon Street.	paragraph and section amended	Closed

5	49	3.7.7	TFL - Michael Beaver	Principal Network Impact Manager	To facilitate the closure of this section of Gordon Street, signal modification works will be required at its junction with A501 Euston Road. This is covered under Euston Road Utility Works in this section. Comment – no mention of footway closures ? will east west movements for peds on the southside be maintained	Comment acknowledged and section 3.7.6 has been updated accordingly.	paragraph and section amended	Closed
6	52	3.7.24	TFL - Michael Beaver	Principal Network Impact Manager	Trial hole works are anticipated to start in Q4 2022/Q1 2023 followed by start of main works in Q2 2023 and last approximately for 16 weeks. Similar traffic management is expected for both works, detail to be confirmed when available and during future updates of this document or relevant fora. Comment - TfL have said that these works cannot happen at the same time as the Central reservation works on Euston Road and diverting via Hampstead Road whilst the water main works continue. Q1/ Q2 dates are unachievable.	Comment acknowledged and section for Eversholt Street foul connection works amended with latest anticipated dates for the works. The programme for these works no longer coincide with central reservation works on Euston Road	paragraph and section amended	Closed
7	63	3.12.2	TFL - Michael Beaver	Principal Network Impact Manager	Engagement includes a series of workshops, briefings, presentations, community events, drop-in sessions, individual meetings, walk-arounds and HS2-led cross-contractor forums. Key stakeholder groups engaged to date include: Comment - should blue light services be mentioned in the below list as routes are affected. <ul style="list-style-type: none"> London Borough of Camden officers <ul style="list-style-type: none"> TfL HS2, CSJV, MDJV Camden Community Traffic Working Group (CCTWG) & Camden Traffic Liaison Group (TLG) <ul style="list-style-type: none"> Origin Housing Local cycle groups Nearby schools Local residents, resident associations and groups Businesses groups including Stephenson Way (SWG), Drummond Street Traders (DST), University College London (UCL) 	This paragraph refers to the engagement with stakeholders for seeking feedback and any improvements for ongoing development of local traffic management planning. Following paragraph 3.12.3 discusses and acknowledges the emergency services.	No change	Closed
8	64	3.12.14	TFL - Michael Beaver	Principal Network Impact Manager	Hampstead Road closure <ul style="list-style-type: none"> restrict duration of any closures <ul style="list-style-type: none"> aim for closure during August, school holidays or quieter periods early communication about road closures (up to 18 months with TfL) consider how to avoid rat running during closure Comment – alternative cycle routes need to be agreed and any works associated with that prior to closure taking place.	Comment noted. Mitigation measures associated with Hampstead Road closure will be detailed in the future revisions of the document.	Changes to be adopted in future revision	Closed
9	70	4.1.3	TFL - Michael Beaver	Principal Network Impact Manager	For MDJv works for the Interim Taxi Rank access is within the bus station reached from Eversholt Street whilst egress is from within the bus station onto the Euston Road. Secondary egress will still be from within the bus station comprising of an internal loop around back to Eversholt Street. Comment – clarification that this does not include the war memorial loop	Comment acknowledge and adopted, reference to the secondary egress has been removed.	paragraph and section amended	Closed

10		5.2.5 & 6.6.2	TFL - Michael Beaver	Principal Network Impact Manager	SCS are implementing ANPR Cameras which will be installed in key locations capturing vehicle routing to and from Site, alert the gate staff and control tower to the status of the vehicle - Comment: Estimated dates for delivery for ANPR cameras required and how data will be shared with TfL/ LB's.	It is anticipated the SCS ANPR system for vehicle monitoring will be tested mid-2023 and operational soon thereafter. Discussions on data sharing are ongoing and it is envisaged that TLG will be used to share pertinent data. Once confirmed details will be incorporated in future revisions.	Changes to be adopted in future revision	Closed
11	79	5.2.4	TFL - Michael Beaver	Principal Network Impact Manager	FORS Status (Bronze, Silver or Gold – Comment - clarification required)	Comment noted. Further information on HS2 FORS requirements are set out in RTMP section 5.6.2.9	No change	Closed

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