

2021/6222/P
St Pancras Bridge, 2-6 St Pancras Way



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2021/6222/P



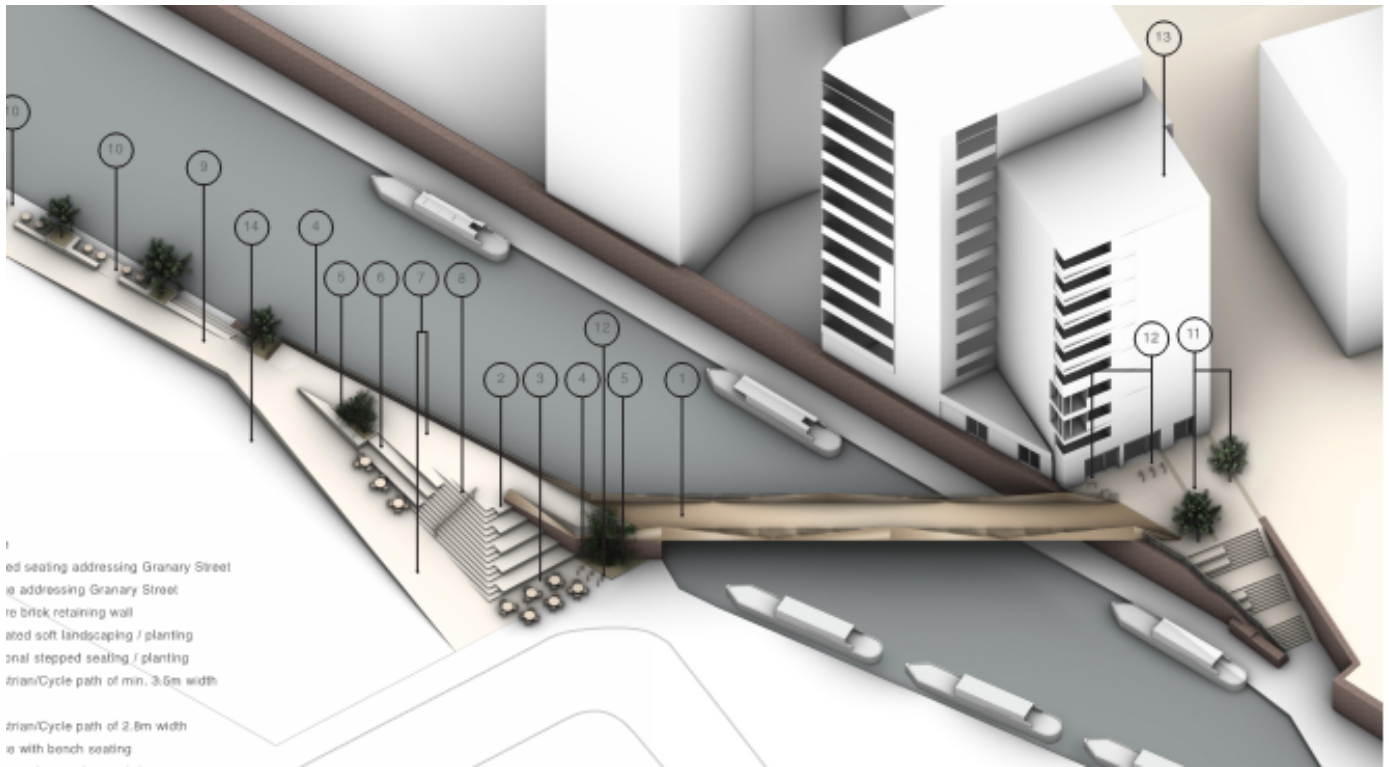
The location of the bridge when viewed from Camley Street



The view along the canal with the residential moorings on the left



The existing view of the where the bridge steps are proposed to approach Granary Street



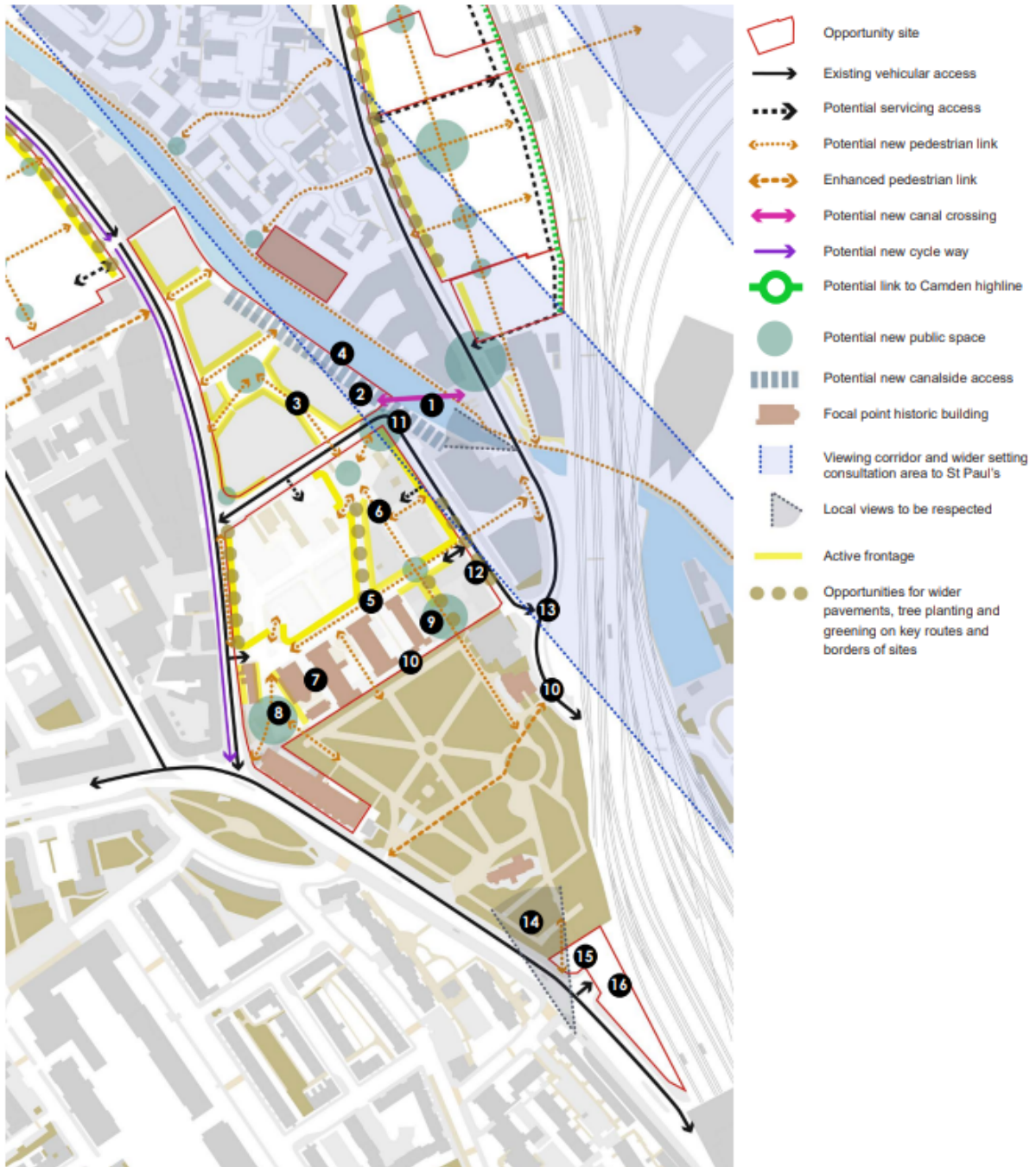
The proposed view of the bridge



The proposed view across the bridge from Granary Street



Proposed CGI of the Bridge with the completed UBB development



Extract from the Canalside to Camley Street Supplementary Planning document dated November 2021 highlighting the new canal crossing within the wider context

Delegated Report		Analysis sheet	Expiry Date:	16/02/2022
(Members Briefing)		N/A / attached	Consultation Expiry Date:	12/02/2022
Officer			Application Number(s)	
Jennifer Walsh			2021/6222/P	
Application Address			Drawing Numbers	
St Pancras Bridge 2-6 St Pancras Way London NW1 0PB			Please refer to draft decision notice	
PO 3/4	Area Team Signature	C&UD	Authorised Officer Signature	
Proposal(s)				
Creation of pedestrian and cycle bridge over the Grand Union Canal, linking Granary Street and Camley Street spanning from 2-6 St Pancras Way to 103a Camley Street				
Recommendation(s):	Grant condition planning permission			
Application Type:	Full Planning Permission			

Conditions or Reasons for Refusal:	Refer to Draft Decision Notice					
Informatives:						
Consultations						
Adjoining Occupiers:	No. notified	00	No. of responses	00	No. of objections	00
Summary of consultation responses:	Site Notices were displayed around the site from 15 th January to 12 th February 2022. A press notice was published on 13 th January 2022. No responses have been received.					
CAAC/Local groups comments:	<p>Canals and River Trust commented on the application as follows:</p> <ul style="list-style-type: none"> - The Trust has always maintained some concern regarding the need of this footbridge, considering the proximity of the existing Camley Street Road Bridge. However, the Trust was also consulted during pre-application stage process and acknowledges the amendments seeks to address some of the concerns. The main issues relevant to the Trust as a Statutory consultee on this application are: <ul style="list-style-type: none"> A) The impact on the structural integrity of the Regent's Canal and its towpath, and navigational safety; B) The impact on the character and appearance of the Regent's Canal Conservation Area; C) The impact on the adjacent moorings; D) The impact on the ecology of the Regent's Canal. <p><i>Officers Response: Within their response the Canals and River Trust have proposed conditions which they feel are necessary to address these matters. The report has been written to address these issues in order. The recommendation accepts the proposed conditions and it forms part of the consideration and the assessment in making the recommendation.</i></p>					

Site Description

The location of the bridge is to fall within the Ugly Brown Building Site to the North West and the land outside of the Co-op Supermarket to Camley Street to the North East. The bridge will span across the Regent's Canal. The site is located within the Regent's Canal Conservation Area. Whilst the buildings on the Ugly Brown Building site are defined within the Conservation Area Appraisal and Management Strategy as buildings which make a negative contribution to the Conservation Area, the site benefits from recent planning permission to redevelop the site which has been implemented through the works to Plot A.

The canal and towpath are designated Open Space, a Habitat Corridor, a Site of Nature Conservation Importance (SNCI) and a Metropolitan Walk.

Relevant History

Ugly Brown Building, 2-6 St Pancras Way

2017/5497/P: Demolition of the existing building (Class B1 and B8) and erection of 6 new buildings ranging in height from 2 storeys to 12 storeys in height above ground and 2 basement levels comprising a mixed use development of business floorspace (B1), 73 residential units (C3) (10 x studio, 29 x 1 bed, 27 x 2 bed 7 x 3 bed), hotel (C1), gym (D2), flexible retail (A1 - A4) and storage space (B8) development with associated landscaping work. **GRANTED 17/03/2020**

101 Camley Street, London NW1 0PF:

2014/4385/P: Planning permission (ref. 2014/4385/P and subsequent MMA - 2018/3682/P) was **granted subject to s106 legal agreement** for the: Demolition of existing building and redevelopment for a mixed-use building ranging from 6 -13 storeys comprising 2,220sqm employment floorspace (Class B1), 121 residential flats, the provision of a pedestrian footbridge with disabled access over the Regent's Canal, and associated landscaping and other works relating to the public realm. **This permission is being implemented and is nearing completion, at the time of writing.**

2020/5530/P: Non-material amendment (NMA) to vary condition 8 (landscaping works must be carried out prior to first occupation of residential flats) of planning permission 2016/6311/P dated 14/07/2017 (variation of 2014/4385/P dated 18/03/2015 for: new 6-13 storey buildings with employment space, residential units and associated works) TO VARY the implementation from prior to first occupation of residential units to within six months of occupation. **GRANTED 17/12/2020**

103 Camley Street, London NW1 0PF;

2011/5695/P: Demolition of existing industrial buildings (Class B1c & B8) and the erection of a building ranging from 4-12 storeys to create a mixed use development comprising 307 x student units accommodation (Class Sui-Generis) including student cycle store; 14 x 2-bed, 15 x 3-bed and 11 x 4-bed self contained residential flats (Class C3); incubator business units comprising 1,653sqm floorspace (Class B1); 2 x retail units of 406sqm (Class A1/A3) and associated works and improvements to public realm including canal footpath. **Granted Subject to a Section 106 Legal Agreement 30/03/2012**

Relevant policies

National Planning Policy Framework 2021
The London Plan 2021

Camden Local Plan 2017

Policy G1 – Delivery and location of growth
Policy A1 – Managing the impact of development
Policy A2 – Open Space
Policy A3 – Biodiversity
Policy C5 – Safety and security
Policy C6 – Access for all
Policy D1 – Design
Policy D2 - Heritage
Policy DM1 – Delivery and monitoring
Policy CC1- Climate change mitigation
Policy CC2 – Adapting to climate change
Policy T1 – Prioritising walking, cycling and public transport
Policy T3 – Transport infrastructure

Camden Supplementary Planning Guidance

CPG – Access for All 2019
CPG - Design 2021
CPG - Amenity 2021
CPG – Public Open Space 2021
CPG – Transport 2021

Regents Canal Conservation Area Appraisal and Management Plan 2008

Canalside to Camley Street Supplementary Planning Document (SPD) 2021

Assessment

1.0 History

1.1 This report relates to the proposals to deliver a new canal bridge between Camley Street and Granary Street/“Ugly Brown Building” (UBB) site. This project is a key part of longstanding objectives to improve walking and cycling connectivity around the Camley Street area and between Camden Town and King’s Cross.

1.2 This location for additional connectivity was first explored back in 2011 when the Council initiated work looking at the Camley Street area as part of the wider King’s Cross Placeshaping work and it explored the longer term potential to explore improved routes and connections through the area. Since this time, development within the area has been contributing towards a ‘bridge’ option within this vicinity. Back in 2012, the committee report for 103 Camley street (the proposed landing location) discusses the opportunity of a bridge landing in this location and “being the only location for such a bridge”. Whilst the proposals did not include provision for the bridge itself, consideration was had to the suitable landing points being on the open space adjacent to the steps down to the canal. This application also had a plan which showed the proposed landing location.

1.3 Looking to the other side of the canal, within the 101 Camley Street application, a footbridge was proposed in 2014. The location of which however, was not supported by the Canals and River Trust primarily due to the visual clutter, a the displacement of 3 residential moorings and the impact on the canal and towpath. The bridge was then within a subsequent application and alternative provisions were explored within the surrounding sites.

1.4 Following on from pre application discussions, whilst the planning approval for the Ugly Brown Building, 2-6 St Pancras Way did not include a bridge, it did include a commitment that the developers would lead on delivering one. A section 106 legal agreement included a financial contribution of £1m paid towards the delivery of the bridge. This contribution has added to the funds which have already been allocated from 101 Camley Street, 102 Camley Street and 103 Camley Street. As this bridge is being applied for by the developers of this site, consideration of the routes has been an important design feature. The orientation of the bridge links to routes through the site north along St Pancras Way from Camley Street as well as South along Granary Street.

1.5 This application seeks permission for the installation of the bridge which has been discussed for many years and is one which aligns with a number of Council Plans and strategies.

2.0 Proposal

2.1 This application seeks permission for the construction of a new bridge, lighting access, ramps, stairs, seating areas and associated landscaping to integrate within the wider redevelopment on the site of the Ugly Brown Building (UBB).

3.0 Assessment

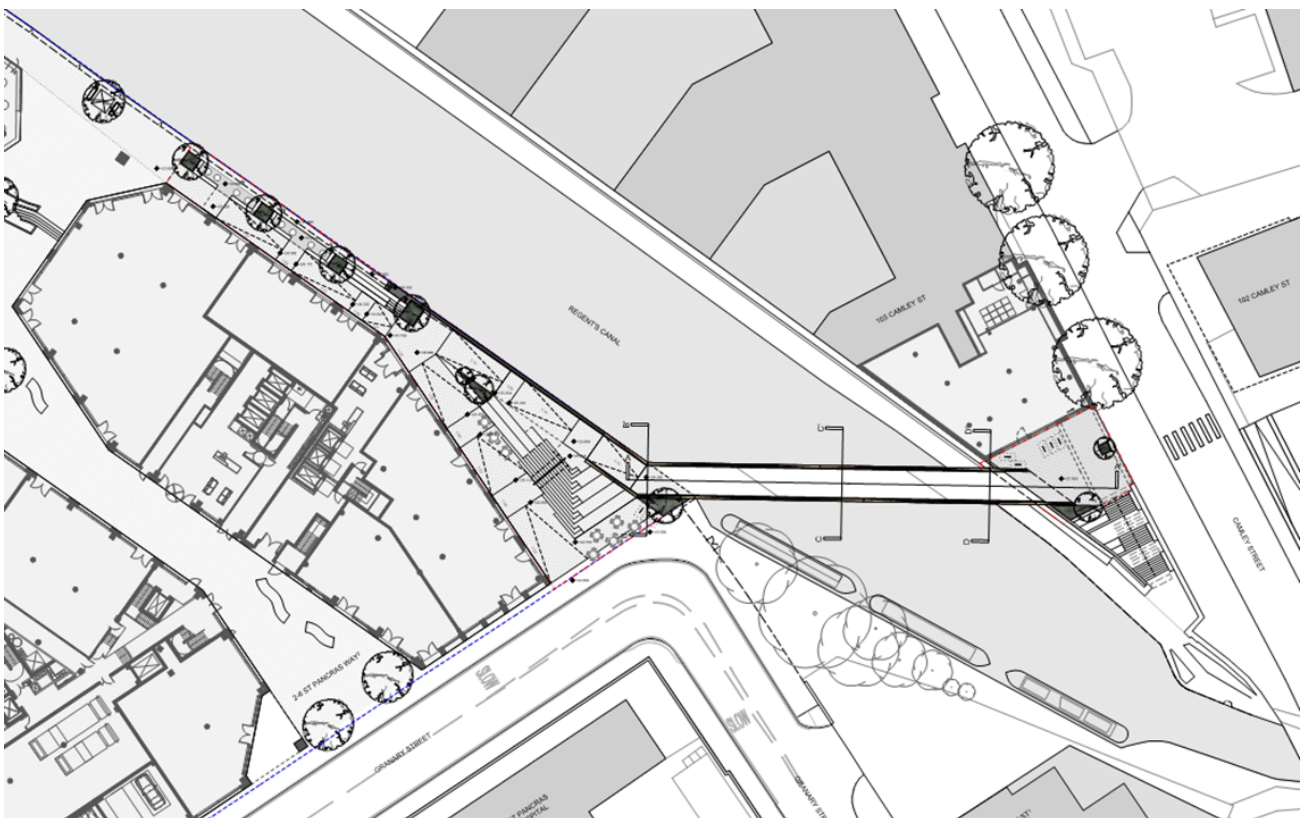


Figure 1: Proposed Site Plan of the bridge and the landings

Design

3.1 The design of the bridge has been well considered and has gone through a period of consultation with the Council and the Canals and Rivers Trust to ensure that all parties concerns have been reasonably addressed.

3.2 The revised location of landing on the space outside 103 Camley Street as previously envisaged and then landing within the Ugly Brown Building site to engage with the site and the wider Granary Street is considered acceptable. The proposed bridge and its ramps are considered to allow for further connections and routes through this area which has been a Councils aspiration for many years.

3.3 There is a 3m level difference on the Granary Street Side to Camley Street and therefore the proposed bridge has been designed to create a safe accessible crossing for all. The application seeks to provide a straight cycle and pedestrian bridge which avoids oversailing any residential moorings which was a concern from the Council and Canals and Rivers Trust within the previous design. The landing to the UBB site then wraps around into the site but provides further connections to Granary Street as well as the main site itself. The bridge design includes a set of steps coming down to the corner of Granary Street. These include wider steps which can be used as a seating area. There are desire lines from the St Pancras Hospital site which is welcomed.

3.4 The impact and appearance on the Regents Canal Conservation Area is of a key concern to both the Council and the Canals and Rivers Trust. The appearance of the bridge has been fully explored during the consultation process. The materials of the bridge are well considered due to their association to the surrounding structures as well as the new development on the UBB site. The bridge is pre-fabricated metal bridge which is read as one due to its light weight appearance coming from the detail and alternation between the solid parapet and space balusters.



Figure 2: The proposed Bridge looking south

3.5 From below, the bridge will form a V-shape with the lintels being well considered. These have been designed to be a bow tie shape pre cast concrete where it meets the brick work. To the Camley Street side, this will adjoin the existing brick wall and to the UBB side, a new red brick wall will be built to create the ramp down or the steps which front Granary Street.

3.6 The balustrade is set at a height of 1.4m which is 30cm higher than a standard balustrade. The height reflects a need to protect cyclists and provides better security. The drainage for the bridge has also been well considered and is well integrated into the design by drainage outlets which are connected to the wider UBB sites drainage system.

3.7 Within Canal's and River's Trust response, they have considered that even though the brick wall has been amended to try to address their previous concerns, that the brickwork of the approach ramp should be further detailed through a planning condition to add some visual interest and animation. A condition is therefore recommended to be added to the permission. The Trust have suggested exploring English Bond as a contextual response which the applicants are open to investigating.

3.7 In considering these proposals, special attention has been paid to the desirability of preserving or enhancing the character or appearance of the Regent's Canal Conservation Area, under s.72 of the Listed Buildings and Conservation Areas Act 1990 as amended by the Enterprise and Regulatory Reform Act (ERR) 2013. Special regard has also been attached to the desirability of preserving the listed buildings, their setting, and features of special architectural or historic

interest, under s.16 and s. 66 of the Listed Buildings and Conservation Areas Act 1990 as amended by the Enterprise and Regulatory Reform Act (ERR) 2013.

3.8 The aesthetic and structural approach to the bridge design is considered to be appropriate to the character of the conservation area and associated heritage assets preserving and enhancing its character and appearance. It is considered that the palette of materials are well considered and the views of the canal will be maintained. The design of the bridge also ensures that the space beneath is maximised with views of the canal remaining as open as possible thereby preserving its setting.

Access and lighting

3.9 The design of the bridge has been to incorporate a gradient of no less than 1:15 and retain a 3.5m clear width of the bridge. The proposed width at 3.5m would provide sufficient space for pedestrians, wheelchair users and cyclists. An Access report has been submitted in support of the application. Metal studs are proposed together with a difference colour texture of the surfaces are considered to make the landings and slopes visually apparent. Due to the detail coming forward at the next stage, a condition is recommended to be added to ensure the detail of these finishes are submitted once known.

3.10 The bridge will be open at all times and will be lit during darker hours. Lighting on the ramp and bridge will be provided by fittings integrated into the handrail and concealed in the bridge upstands and will be switched on during darker opening hours. Visibility will therefore be maintained during the earlier or later hours in the winter, making users feel safer when using this route. Two new wall lights are proposed to light the bridge underside over the towpath which will enhance the safety of users.

3.11 A lighting report was submitted in support of the application. The report states the measures which have been considered to reduce night time light pollution which is welcomed. Within the report, it states in order to protect the local ecology, considerations have been made to the type of lighting, the locations of the lighting and the times the lights are on. Therefore, a condition is recommended to ensure that the times of the lighting is suitable and also details for the dimmable lighting presence and/or timed lighting options are fully explored.

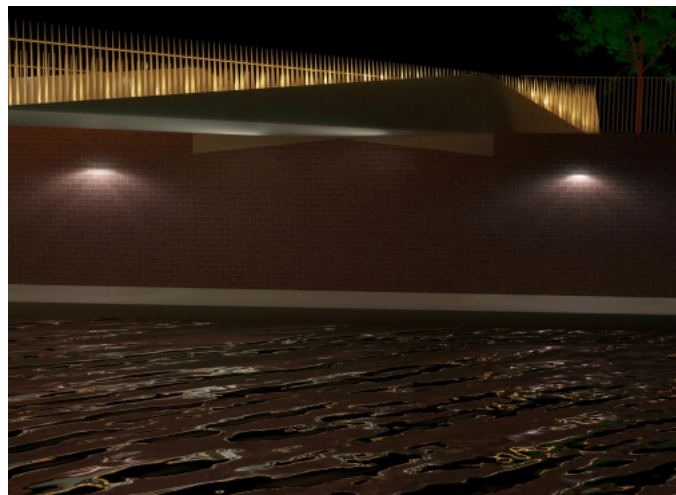


Figure 3: Proposed view under the bridge of the towpath

3.12 The proposed lighting for the bridge has been carefully designed to strike a balance between the security and visibility for users of the bridge and to ensure that the setting of the canal is maintained however, the finer detail of the proposal needs further work. Therefore a condition is recommended to submit the finer details to ensure that the illumination is contained within the body of the bridge and ramp ensuring there is no overspill onto the canal thereby protecting the ecology.

3.13 The Council's Transport team have assessed the application and are satisfied with the details in the proposal. The 3.5m wide bridge will provide a new pedestrian and cycle access across the canal which is welcome. The landing area on the Canley Street side (adjacent to the Co-op supermarket) appears to be public highway as maintained by the council. Remedial works to this area will need to be carried out by the council and its contractor. As it is the intention that the council takes over maintenance and ownership of the bridge once it is complete, details around the works to this landing area should be included in any Highways Agreement between the developer and the council, which includes the requirement for the council to have the final say on the design of this area and to carry out the work via its contractor, unless agreed otherwise. An informative is recommended to be added to ensure that any works which connect to the public highway are discussed with the council first prior to works commencing.

Impact on the Adjacent Moorings

3.14 As previously mentioned, one of the main objections from Canals and Rivers Trust to the previous location of the bridge was the impact of the proposed bridge on the existing residential moorings. These are located to the south of the

UBB site adjacent to Granary Street. During the pre-application process, the applicants met with the canal boat residents to discuss their concerns in relation to safety and security.

3.15 The changes to relocate the landing so as not to oversail the moorings were welcomed. Further discussions are also continuing with the Council and the owners of 101 Camley Street to ensure the safety and security of the site is protected and improved during the process of the application.

Conclusion

4.0 A new bridge in this location has been a longstanding Council objective as expressed in the adopted Canalside to Camley Street Supplementary Planning Document (SPD) and S106 funds have specifically for that purpose. The detailed design of the bridge is considered to add architectural interest to the canal and the bridge design is considered to respond sensitively to its setting. Its form is considered to take account of the wider area, the ecological value of the canal and the wider conservation area whilst providing this important strategic route for pedestrians and cyclists further improving the permeability of the wider Camley Street area.

The decision to refer an application to Planning Committee lies with the Director of Regeneration and Planning. Following the Members Briefing panel on Monday 27th June 2022, nominated members will advise whether they consider this application should be reported to the Planning Committee. For further information, please go to www.camden.gov.uk and search for 'Members Briefing'.

Application ref: 2021/6222/P
Contact: Jennifer Walsh
Tel: 020 7974 3500
Email: Jennifer.Walsh@camden.gov.uk
Date: 22 June 2022

Development Management
Regeneration and Planning
London Borough of Camden
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Judd Street
London
WC1H 9JE

Phone: 020 7974 4444

planning@camden.gov.uk
www.camden.gov.uk

DP9
100 Pall Mall
London
SW1Y 5NQ

DRAFT

Dear Sir/Madam

DECISION

Town and Country Planning Act 1990 (as amended)

Full Planning Permission Granted

Address:

St Pancras Bridge
2-6 St Pancras Way
London
NW1 0PB

DECISION

Proposal:

Creation of pedestrian and cycle bridge over the Grand Union Canal, linking Granary Street and Camley Street spanning from 2-6 St Pancras Way to 103a Camley Street

Drawing Nos: Site Location Plan; 1914-PL-215; 1914-PL-210; 1914-PL-201; 1914-PL-021; 1914-PL-020; 1914-PL-800; 1914-PL-228; 1914-PL-227; 1914-PL-226; 1914-PL-225; 1914-PL-223; 1914-PL-222; 1914-PL-221; 1914-PL-200; 1914-PL-216; Design and Access Statement December 2021;

The Council has considered your application and decided to grant permission subject to the following condition(s):

Condition(s) and Reason(s):

- 1 The development hereby permitted must be begun not later than the end of three years from the date of this permission.

Reason: In order to comply with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).

- 2 The development hereby permitted shall be carried out in accordance with the following approved plans
Site Location Plan; 1914-PL-215; 1914-PL-210; 1914-PL-201; 1914-PL-021; 1914-PL-020; 1914-PL-800; 1914-PL-228; 1914-PL-227; 1914-PL-226; 1914-PL-225; 1914-PL-223; 1914-PL-222; 1914-PL-221; 1914-PL-200; 1914-PL-216; Design and Access Statement December 2021;

Reason:

For the avoidance of doubt and in the interest of proper planning.

- 3 Before the relevant part of the work is begun, detailed drawings, or samples of materials as appropriate, in respect of the following, shall be submitted to and approved in writing by the local planning authority:

a) Manufacturer's specification details of all facing materials (to be submitted to the Local Planning Authority) and samples of those materials (to be provided on site).

The relevant part of the works shall be carried out in accordance with the details thus approved and all approved samples shall be retained on site during the course of the works.

Reason: To safeguard the appearance of the premises and the character of the immediate area in accordance with the requirements of policies D1 and D2 of the London Borough of Camden Local Plan 2017.

- 4 Prior to the commencement of the development hereby permitted, a Risk Assessment and Method Statement outlining all works to be carried out adjacent to the water, and moorings, must be submitted to and approved in writing by the Local Planning Authority, in consultation with the Canals and Rivers Trust and implemented as agreed.

Reason: To ensure that the works have no adverse impact on the adjacent moorings or other users of the towpath and waterspace in line with Policies A1, A3 and D2 of London Borough of Camden Local Plan 2017.

- 5 Prior to the commencement of the development hereby approved, a survey of the condition of the waterway walls on both the north and south side of the canal, in the vicinity of the proposed abutments, and a method statement and schedule of works identified shall be submitted to and approved in writing by the Local Planning Authority, in consultation with the Canals and Rivers Trust. All repair work required to ensure that the structural integrity of the canal walls is protected during the construction and operational phases of the development shall be carried out in accordance with the agreed method statement and repairs schedule, by a date to be confirmed in the repairs schedule.

Reason: To ensure that the structural integrity of the Regent's Canal is retained in line with Policies A1 and A3 of London Borough of Camden Local Plan 2017.

- 6 No development shall take place until full details of hard and soft landscaping and means of enclosure of all un-built, open areas have been submitted to and approved by the local planning authority in writing. Such details shall include details

of the finished floor materials for the bridge deck the steps and the ramps, surface treatments (including on the southern abutment wall) together with a means of on-going maintenance. The relevant part of the works shall not be carried out otherwise than in accordance with the details thus approved.

Reason: To ensure that the development achieves a high quality of landscaping which contributes to the visual amenity and character of the area in accordance with the requirements of policies A2, A3, D1 and D2 of the London Borough of Camden Local Plan 2017.

- 7 Prior to the occupation of the development hereby permitted, full details of the proposed lighting and CCTV scheme, including a lux plan of the proposed light spill, and details of operation and ongoing maintenance, shall be submitted to and approved in writing by the Local Planning Authority in consultation with Canals and Rivers Trust, and implemented as agreed.

Reason: To ensure that there are no adverse impacts on the adjacent moorings or the ecology of the Regent's Canal in line with policies A1, A3 and D1 of London Borough of Camden Local Plan 2017.

Informative(s):

- 1 Your proposals may be subject to control under the Building Regulations and/or the London Buildings Acts that cover aspects including fire and emergency escape, access and facilities for people with disabilities and sound insulation between dwellings. You are advised to consult the Council's Building Control Service, Camden Town Hall, Judd St, Kings Cross, London NW1 2QS (tel: 020-7974 6941).
- 2 This approval does not authorise the use of the public highway. Any requirement to use the public highway, such as for hoardings, temporary road closures and suspension of parking bays, will be subject to approval of relevant licence from the Council's Streetworks Authorisations & Compliance Team, 5 Pancras Square c/o Town Hall, Judd Street London WC1H 9JE (Tel. No 020 7974 4444). Licences and authorisations need to be sought in advance of proposed works. Where development is subject to a Construction Management Plan (through a requirement in a S106 agreement), no licence or authorisation will be granted until the Construction Management Plan is approved by the Council.
- 3 Access to, and oversailing of, the Canal and River Trust's land and water during the construction and operation of the development must be agreed in writing with the Canal and River Trust before development commences. Please contact Bernadette McNicholas in the Canal and River Trust's Estate Team at Bernadette.mcnicholas@canalrivertrust.org.uk to discuss the necessary agreements.
- 4 The applicant/developer should refer to the current Canal and River Trust "Code of Practice for Works affecting the Canal and River Trust" to ensure that and necessary consents are obtained, and liaise with the Trust's Third Party Work's Engineer: <http://canalrivertrust.org.uk/about-us/for-businesses/undertaking-works-on-our-property>

- 5 Any surface water discharge to the waterway will require prior consent from the Canal and Rivers Trust. Please contact Liz Murdoch from the Canal and River Trust Utilities team (Liz.Murdoch@canalrivertrust.org.uk)."
- 6 All works should be conducted in accordance with the Camden Minimum Requirements - a copy is available on the Council's website (search for 'Camden Minimum Requirements' at www.camden.gov.uk) or contact the Council's Noise and Licensing Enforcement Team, 5 Pancras Square c/o Town Hall, Judd Street London WC1H 9JE (Tel. No. 020 7974 4444)

Noise from demolition and construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You must secure the approval of the Council's Noise and Licensing Enforcement Team prior to undertaking such activities outside these hours.

- 7 This consent is without prejudice to, and shall not be construed as derogating from, any of the rights, powers, and duties of the Council pursuant to any of its statutory functions or in any other capacity and, in particular, shall not restrict the Council from exercising any of its powers or duties under the Highways Act 1980 (as amended). In particular your attention is drawn to the need to obtain permission for any part of the structure which overhangs or connects to the public highway (including footway). Permission should be sought from the Council's Engineering Service Network Management Team, Town Hall, Argyle Street WC1H 8EQ, (tel: 020 7974 2410) or email highwayengineering@camden.gov.uk.

In dealing with the application, the Council has sought to work with the applicant in a positive and proactive way in accordance with paragraph 38 of the National Planning Policy Framework 2021.

You can find advice about your rights of appeal at:

<http://www.planningportal.gov.uk/planning/appeals/guidance/guidancecontent>

Yours faithfully

Chief Planning Officer