

Former Hampstead Police Station Stables

London NW3 1PD



Design and Access Statement

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14/05/2022
Project code
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Planning

01_INTRODUCTION:

This design and access statement is submitted in support of a Planning application submission for:

Potential Redevelopment of the Police Stables, The Hampstead Police Station, 26 Rosslyn Hill, NW3 1PD.

This Design and Access Statement should be read in conjunction with:

- Cover Letter
- Existing Set
- Proposed Set
- Block Map
- Site location Plan

02_CONTEXT:

The site is located in the north area of the Borough of Camden, on the north side of Rosslyn Hill at the junction with Downshire Hill in Hampstead.

The site is occupied by a Grade II listed building which was a former Police Station and Magistrates Court with associated stable and outbuildings to the rear of the site.

The site, together with other Listed buildings, is part of the Hampstead Conservation Area.

The entry point is on Downshire Hill. This was the vehicular entry point to the curtilage of the former Police station which was used as a car park in the more recent years of the building’s use and previously for stables under the Metropolitan Police.

The Police Stables is a unique and important site, sitting at the bottom of The Hampstead Police Station parking area.

The site has been carved out from The Hampstead Police Station, which was recently sold to a professional developer by the Department for Education. The site sits within the curtilage of the Grade II listed Police Station, itself within a Conservation Area.

The site comprises the original Police Stables building as well as a more modern attached Annex building. The entirety of the site is in a state of disrepair: The buildings are unsafe and, in some areas, inaccessible due to health and safety concerns. The Police Stables sit entirely within the back parking area of the Police Station, only accessible via a private gate and right of way, and do not face or abut public roads or paths. There is no public access to the area and the site is visible to only a limited number of neighbouring properties.

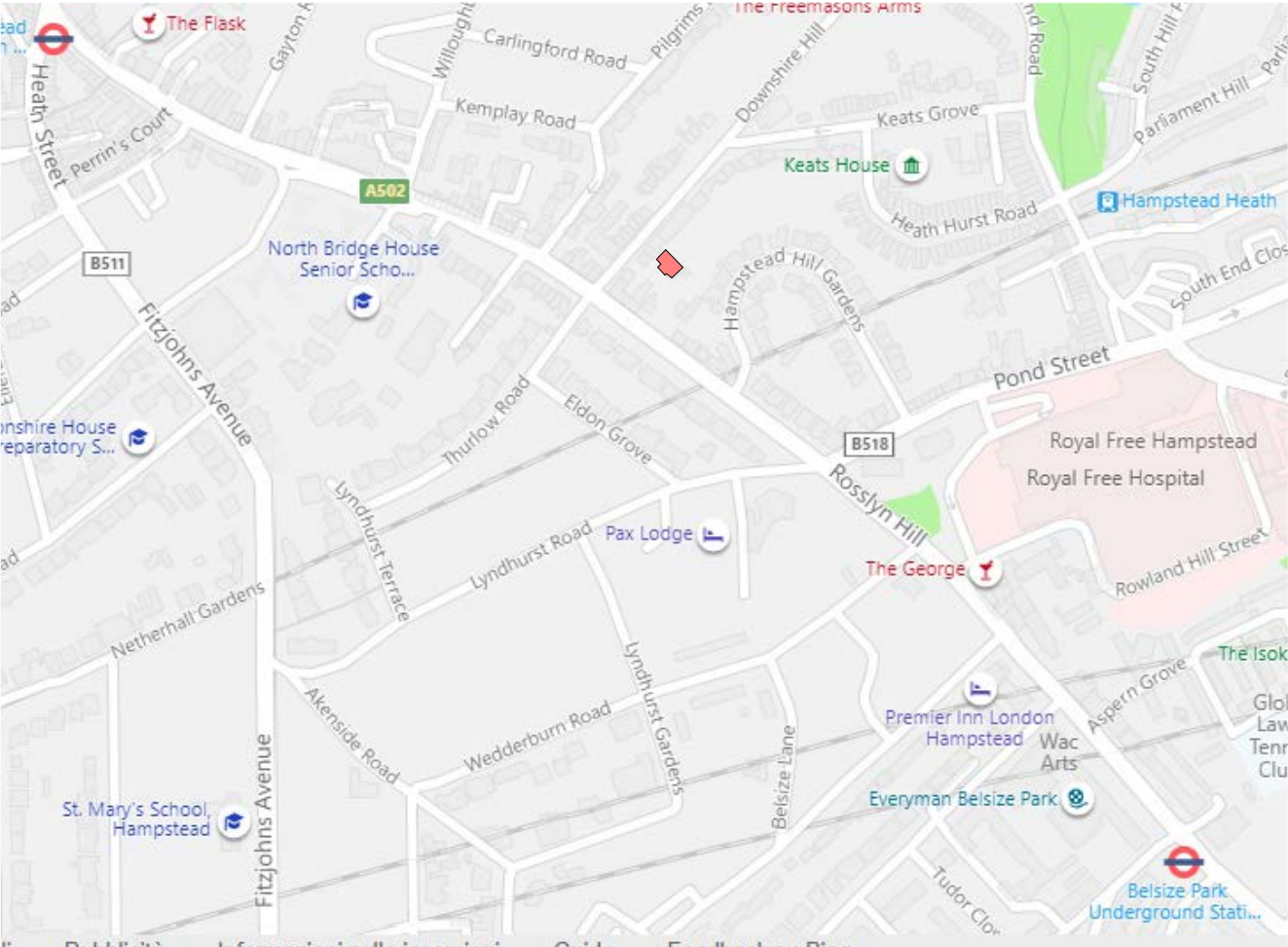


FIG.01 - STREET LOCATION MAP

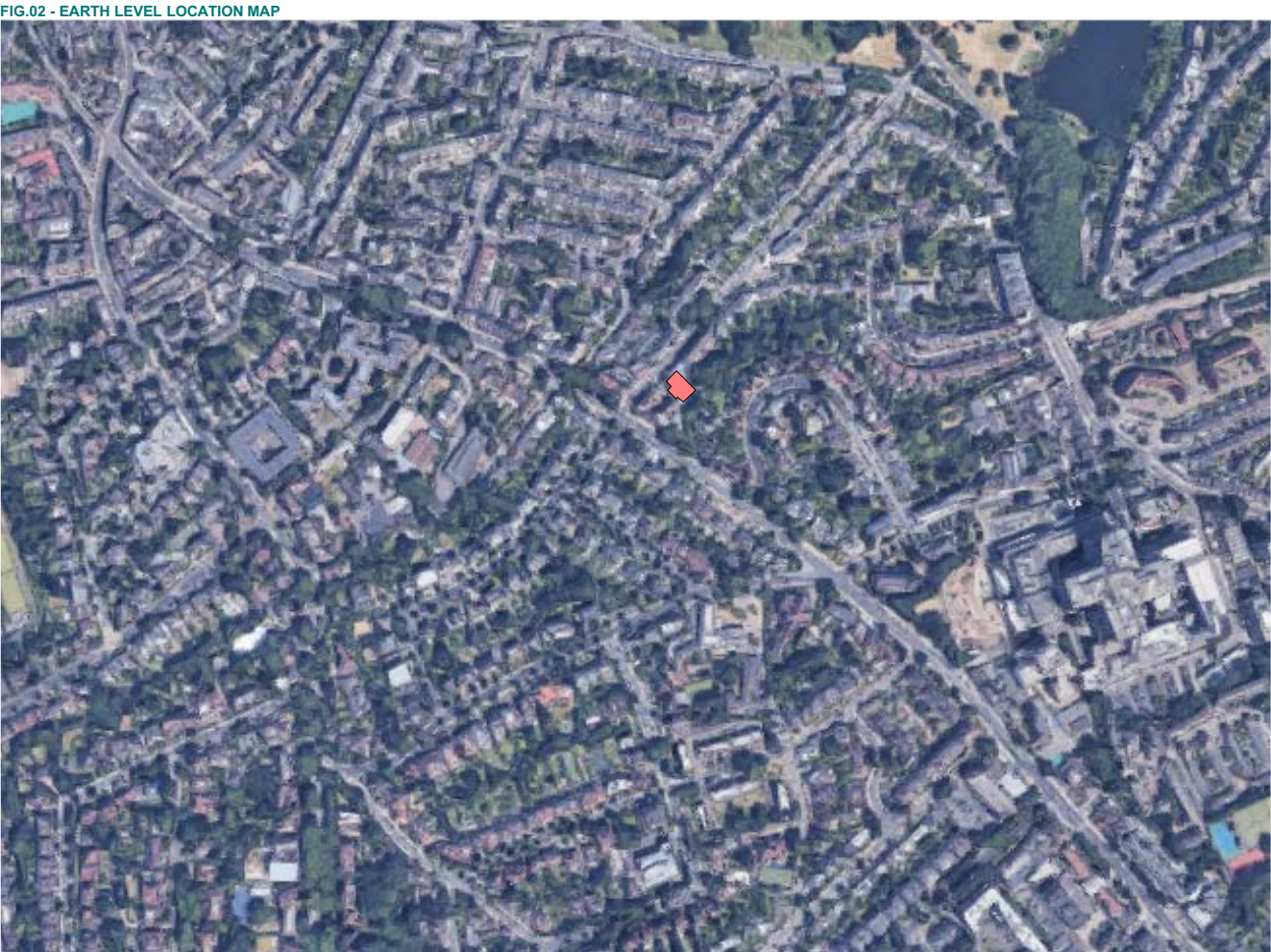


FIG.02 - EARTH LEVEL LOCATION MAP



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03_LISTED STATUS:

The Police Station was first listed on the 13th August 1998; it was purpose built as a Police Station and court house with cell block to the rear in 1910-13 by John Dixon Butler. The listing details define the stables building as SUBSIDIARY FEATURES:

" the former stable block and harness room stand at the north-east corner of the rear courtyard, facing north-west. On the ground floor are a series of doors and windows under slightly cambered, gauged-brick lintels; the left-hand side of the elevation is obscured by a late-C20 extension. Above, on the first floor, are two dormers with pairs of sash windows, and a third dormer to the left with an inserted sash, possibly replacing an opening to a hayloft. The building had been converted to offices by 1986, and is not believed to contain any features related to its original use. "

The Stables have been constructed from red brick laid in Flemish bond, with glazed brick to the ground floor and plinth, with limestone dressings, slate roofs and brick chimneystacks.



Heritage Category:	Listing
List Entry No :	1130397
Grade:	II
County:	Greater London Authority
District:	Camden
Parish:	Non Civil Parish

For all entries pre-dating 4 April 2011 maps and national grid references do not form part of the official record of a listed building. In such cases the map here and the national grid reference are generated from the list entry in the official record and added later to aid identification of the principal listed building or buildings.

For all list entries made on or after 4 April 2011 the map here and the national grid reference do form part of the official record. In such cases the map and the national grid reference are to aid identification of the principal listed building or buildings only and must be read in conjunction with other information in the record.

Any object or structure fixed to the principal building or buildings and any object or structure within the curtilage of the building, which, although not fixed to the building, forms part of the land and has done so since before 1st July, 1948 is by law to be treated as part of the listed building.

This map was delivered electronically and when printed may not be to scale and may be subject to distortions.

List Entry NGR:	TQ2686585549
Map Scale:	1:2500
Print Date:	6 January 2022



Modern Ordnance Survey mapping: © Crown Copyright and database right 2018. All rights reserved. Ordnance Survey Licence number 100024900. This is an A4 sized map and should be printed full size at A4 with no page scaling set.

Name: Former Police Station and Courthouse, including stable and harness room, railings and lamps

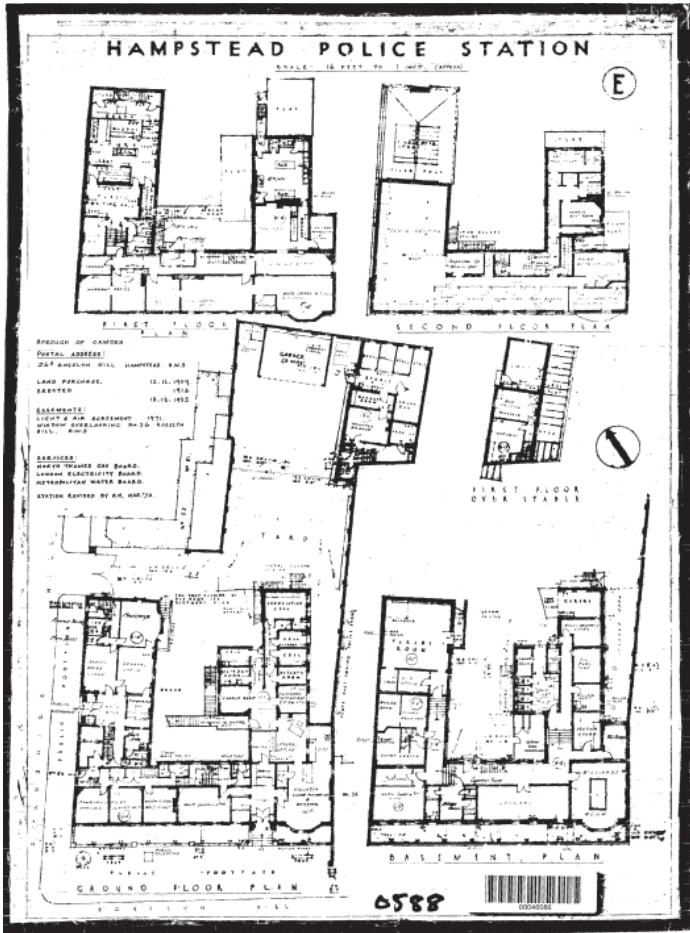
FIG.01 - ENTRY MAP HERITAGE

FIG.03 - BIRD'S VIEW PICTURE EXISTING DEVELOPMENT



FIG.02 - HISTORICAL PLANS

ROSSLYN HILL POLICE STATION ARCHITECT:
JOHN DIXON BUTLER
BUILT: 1910-1913
LISTING DATE: AUGUST 1998
LAST USE: JUNE 2013
ENGLISH HERITAGE BUILDING ID: 477876



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04_CONSERVATION STATUS:

THE ANNEX:

The Annex is a single storey addition to the Stables which was developed in the 1960's/1970's as part of several iterations of development. It has been variously used as storage, for plant and equipment, for dog kennels and for parking. The building has no architectural or heritage value and is unsafe in its current state.



FIG.01 - PICTURE EXISTING CONDITION



FIG.03 - PICTURE EXISTING CONDITION



FIG.04 - PICTURE EXISTING CONDITION

FIG.02 - PICTURE EXISTING CONDITION



FIG.05 - PICTURE EXISTING CONDITION



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04_CONSERVATION STATUS:

THE STABLES:

The Stables building is in a state of total disrepair and is currently unsafe due to structural issues. The stairs to the first floor have collapsed and there is widespread damage to the fabric of the building from water ingress and some level of vandalism. The building was originally used as stables for Police horses but subsequently converted to office space. However, the site in its current state cannot be used in any way and is only partially accessible.

The internal rooms of the Stables are modern, having been rebuilt multiple times over the years without regard to historical or architectural features. None of the rooms in the Stables have any heritage value and the plan would be to maintain the external features of the building while largely rebuilding the internal areas.



FIG.01 - PICTURE EXISTING CONDITION



FIG.03 - PICTURE EXISTING CONDITION



FIG.04 - PICTURE EXISTING CONDITION



FIG.02 - PICTURE EXISTING CONDITION



FIG.05 - PICTURE EXISTING CONDITION



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05_PROPOSAL:

The new owner is applying for planning permission in regards to the redevelopment of the site to allow it to return to use after many years of neglect.

The new owner is seeking planning permission on the following:

- **Converting the Stables from sui generis to residential use classification;**
- **Redeveloping the existing building (Former Stables) to allow for habitable accommodation consistent with NPPF, London, Camden and Neighbourhood Plan guidelines.**

The new owner has attached concept drawings which illustrate the proposed development as well as photos of the site in its current state.

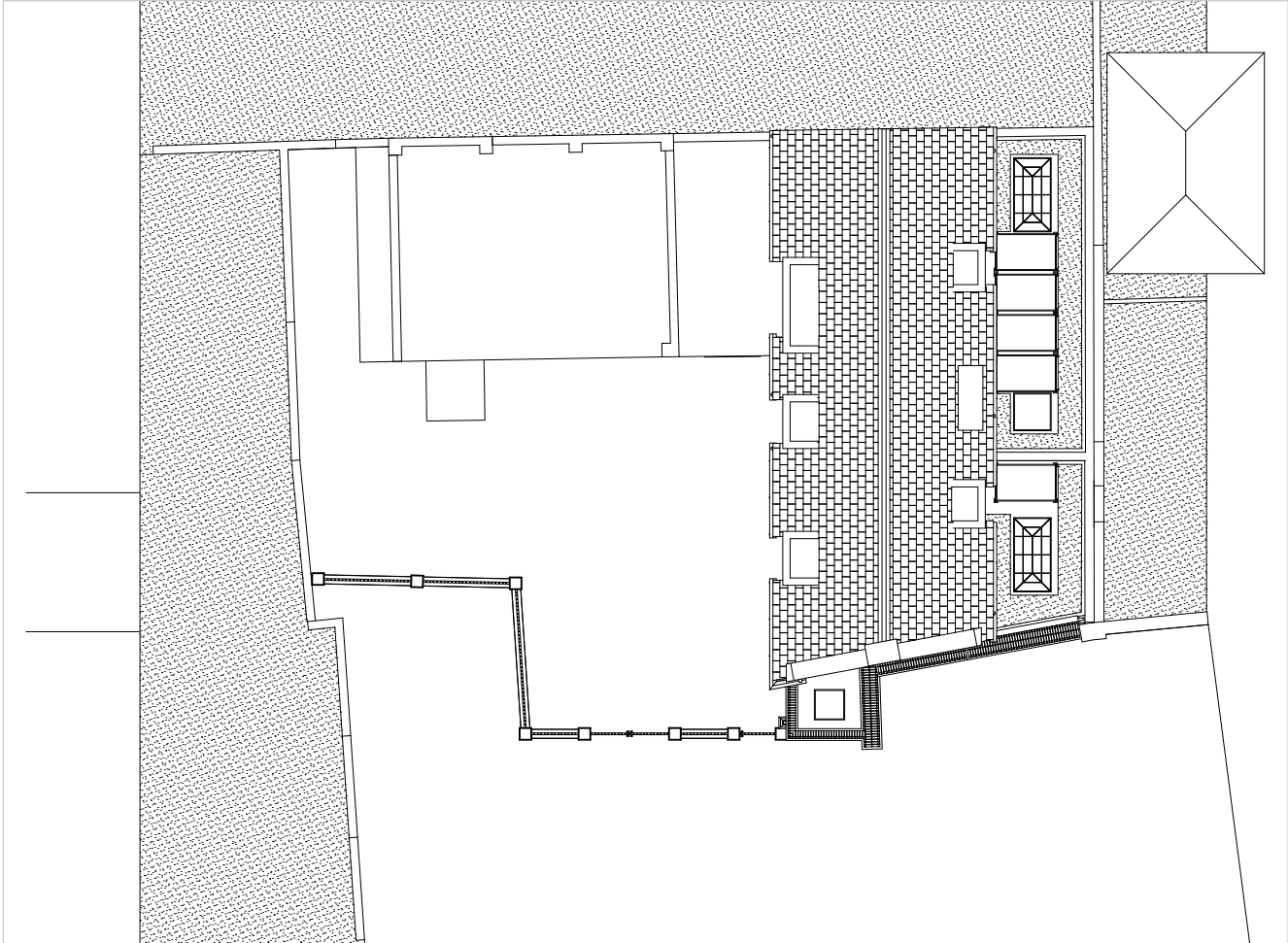
The owner is seeking to redevelop the site to maintain its historical character while simultaneously investing to create a model for sustainable regeneration. The core fabric, heritage and aesthetic characteristics of the site will be maintained but wherever possible the owner seeks to substantially upgrade the environmental credentials of the site, hopefully creating a case study for redeveloping similar buildings in an eco-friendly way.

The owner will be investing in state of the art insulation to radically improve energy efficiency; the site will be gas free, running entirely on electricity; the proposal includes discretely located solar panels as well as the latest in low noise air source heat pump technology; the flat roofs will have sedum coverings.

The proposed plans illustrate the low impact this development would have.



FIG.02 - PROPOSED DEVELOPMENT MASTERPLAN



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05_PROPOSAL:

THE STABLES:

The owner seeks to convert the Stables to residential accommodation.



FIG.01 - LOCATION MAP WITH INDICATION OF THE PROPERTY AMALGAMATION

FIG.03 - PROPOSED GROUND FLOOR PLAN

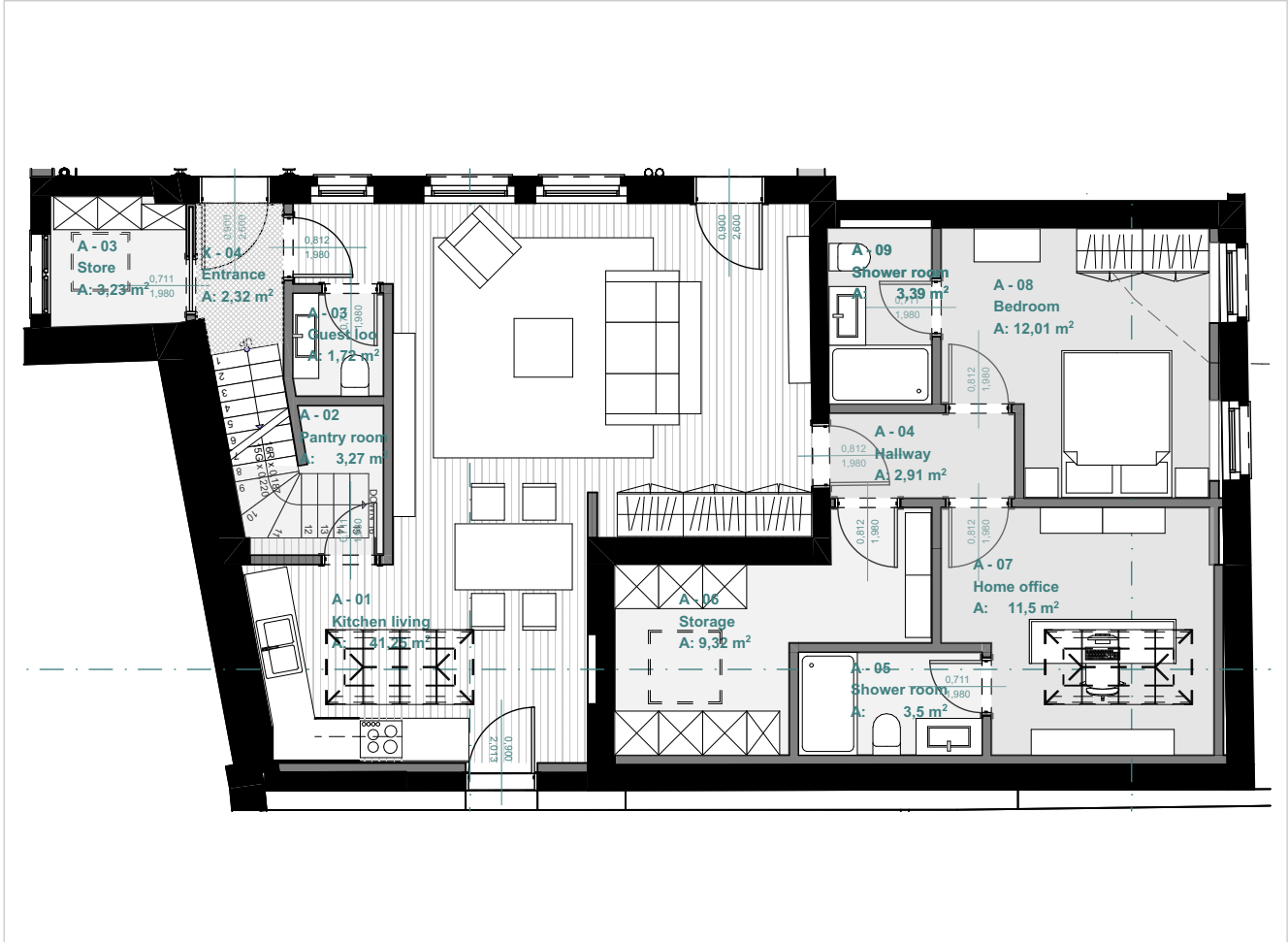
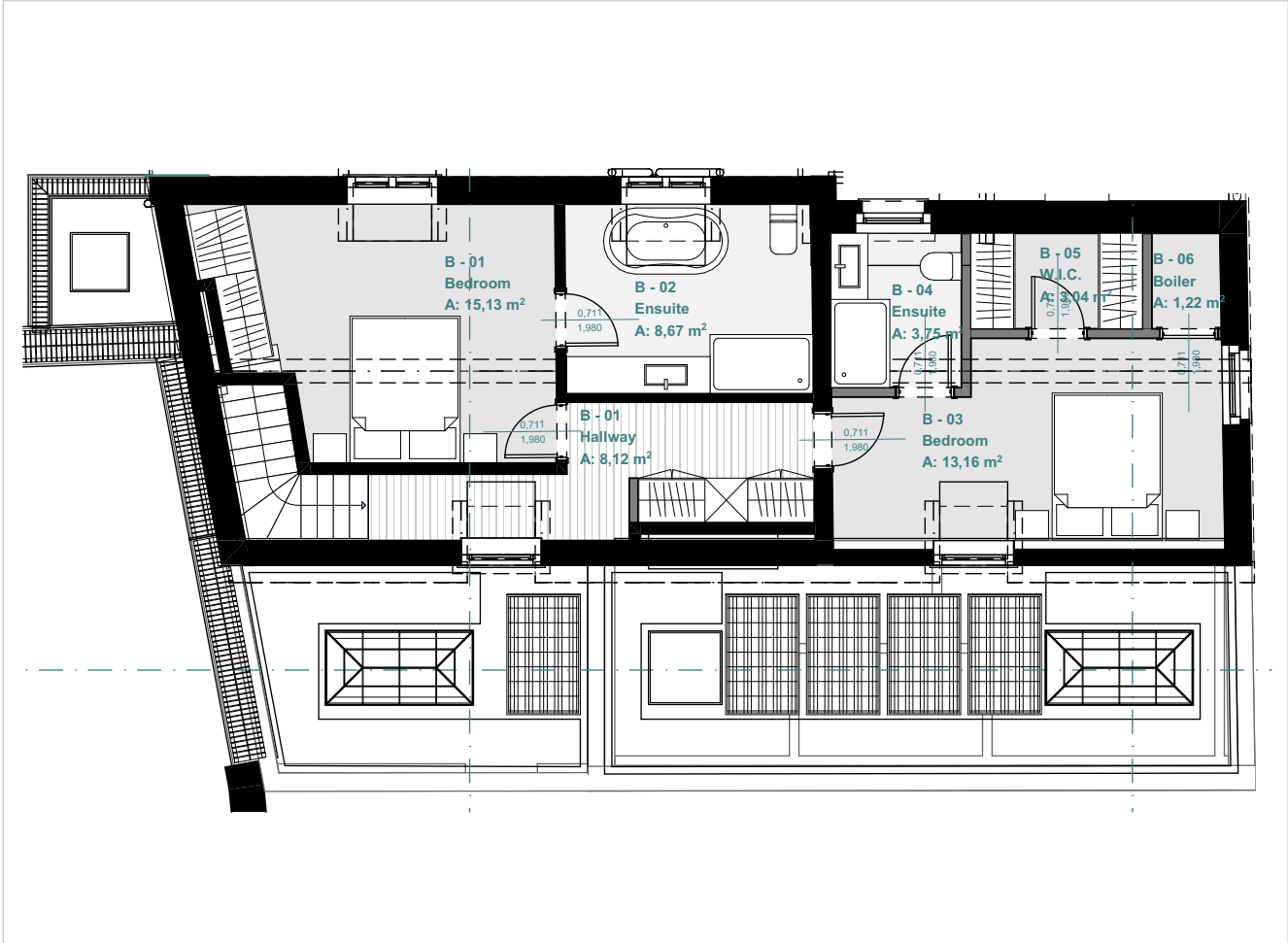


FIG.02 - PROPOSED FIRST FLOOR PLAN



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06_SIZE - GUIDELINES:

The **London Housing Design Guide** asks to provide the following combined floor areas for living/kitchen/dining space and bedrooms.

Minimum combined floor area of living, dining and kitchen spaces (sq m)

2 person	23
3 person	25
4 person	27
5 person	29
6 person	31

The minimum area of a single bedroom should be 8 sq m. The minimum area of a double or twin bedroom should be 12 sqm.

Furthermore the essential GIA shold be (sqm)

Two storey dwelling	
3b4p	87

06_ACCOMODATION - PROVISION:

THE STABLES GIA = 165.00 sqm

Living room/Dining/Kitchen TG.02 = 41.25 sqm

Bedroom A.04 = 12.00 sqm	2pp	
Bedroom B.01 = 15.13 sqm	2pp	
Bedroom B.03 = 14.90 sqm	2pp	

The above allowances exceed the guidelines of the London Plan.



FIG.02 - PROPOSED GROUND FLOOR PLAN



FIG.03 - PROPOSED FIRST FLOOR PLAN

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07_LAYOUT:

The **London Housing Design Guide and the Camden Planning guidance regarding housing (January 2021)** ask to follow the General principles for new homes as stated below:

Dual aspect

Proposals should achieve good dual aspect [London Housing SPG 2016 Standard 29]. Habitable rooms should also have suitable outlook.

The Stables building will achieve a good dual aspect configuration.

Natural light, Daylight/sunlight

All the habitable rooms must have direct natural light, particularly the main living room. The applicant must ensure that the levels of daylight and sunlight that enter habitable rooms comply with BRE standards and that the report for 'Daylight and Sunlight' is submitted with the proposal [London Housing SPG 2016 Standard 32; CPG for Amenity].

All the habitable rooms will have direct natural light.

Privacy

The habitable rooms of a home should provide adequate levels of privacy for the new occupier. This is set out in the CPG for Amenity. The applicant must ensure all the habitable rooms have a suitable outlook and have suitable privacy. [Local Plan Policy A1; London Housing SPG 2016 standard 28].

The habitable rooms have been configured to offer the best level of privity possible.

Circulation space

Rooms must be laid out around and accessed via sensible circulation spaces to ensure there is no excessive corridor length or wasted space.

The circulation spaces have been deigned to ensure access to all the rooms.

Ceiling heights

A minimum 2.3m headroom for at least 75% of the floor area is required as set out in the Nationally Described Space Standard technical requirements 10(i).

A minimum of 2.5m headroom has been maintained throughout.

Storage

Adequate storage space should be provided in accordance with Table 1 in the Nationally Described Space Standard. Provision of a utility room for a washing machine and drying area is particularly welcome for family homes. Space must also be provided for storing separated and sorted waste for recycling.

The Stables have been provided with utility rooms and waste storage facilities.

08_AMENITY:

Amenity of neighbours

The proposal should not have a significant detrimental impact to neighbouring amenity in terms of neighbouring outlook, privacy, sunlight, daylight, noise or vibration. Additionally, the proposal should not result in any overlooking into neighbouring habitable rooms. [Local Plan Policy A1; CPG for Design and for Amenity].

The development will not result in any overlooking into neighbouring habitable rooms.

Outdoor space

All new homes should have access to some form of private outdoor amenity space, e.g. balconies, roof terraces or communal gardens. Existing gardens and green space should be retained. New homes should meet the open space standard of 9sqm per resident.

The development will be provided with ample outdoor amenity space.

09_PARKING:

Cars

The new home must comply with Camden's Car-free policy. The Council will limit the availability of parking and require all new developments to be car-free [Local Plan Policy T2]

The development will have limited parking space within the boundary of the property for mainteinace and drop off/delivery purposes.

Cycle

A suitable number of secure covered cycle spaces or storage space for them must be provided [London Housing SPG 2016 standards 20 & 2

The development has been provided with ample cycle space storage.

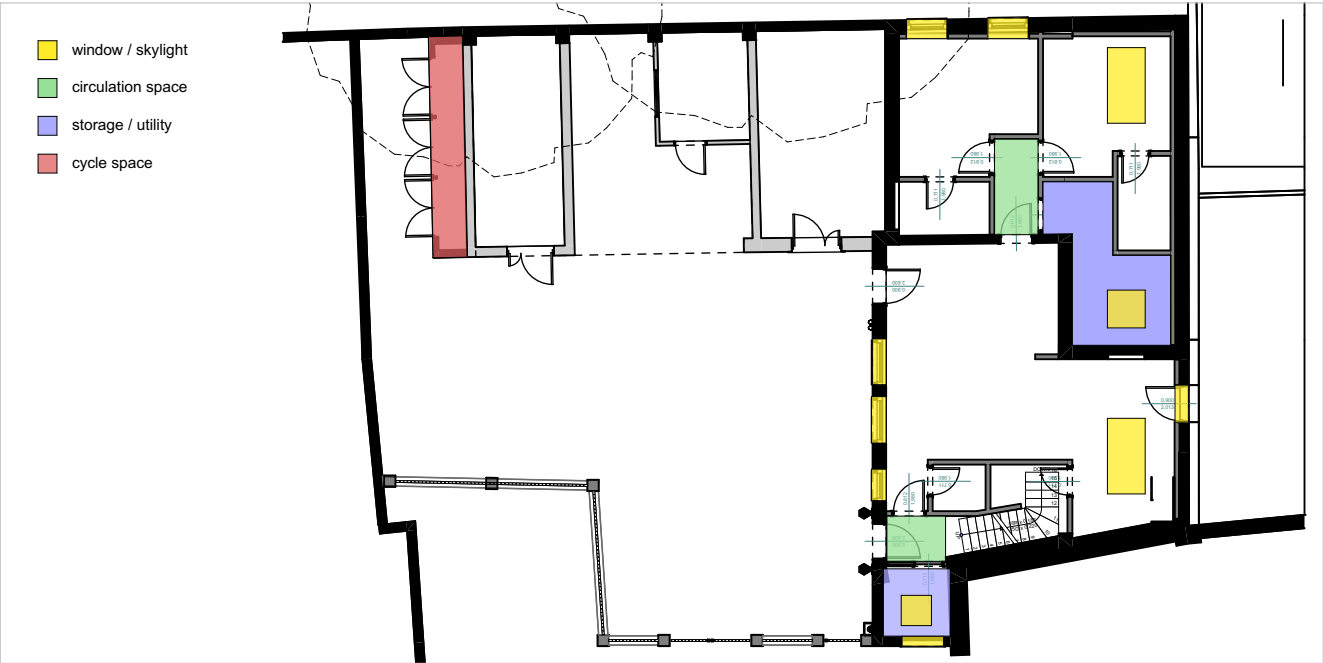


FIG.01 - PROPOSED GROUND FLOOR PLAN



FIG.02 - PROPOSED FIRST FLOOR PLAN

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10_SUSTAINABILITY:

A range of energy efficiency measures will be incorporated into the homes to reduce energy demand.

As requested in the National Planning Policy Framework (NPPF), to support the move to a low carbon future, local planning authorities should:

- plan for new development in locations and ways which reduce greenhouse gas emissions;
- actively support energy efficiency improvements to existing buildings; and
- when setting any local requirement for a building's sustainability, do so in a way consistent with the Government's zero carbon buildings policy and adopt nationally described standards.

Minimising carbon dioxide emissions

The development proposal will make the fullest contribution to minimising carbon dioxide emissions in accordance with the following energy hierarchy:

- 1 Be lean: use less energy
- 2 Be clean: supply energy efficiently
- 3 Be green: use renewable energy

The client is committed to the fabric first approach and has proposed target u-values well below the limiting values of the current Building Regulations.

Technology Options (Be Lean)

Having looked at the fabric and the energy reduction required to heat the home, the client has assessed the most appropriate low carbon technologies available. These are listed below:

Mechanical Ventilation with Heat Recovery (MVHR)

Mechanical air supply in airtight buildings is extremely important to ensure healthy indoor air quality and occupant comfort. Mechanical ventilation with heat recovery takes heat from warm moist air extracted from wet rooms such as kitchens and bathrooms, and pre-warms fresh outside air supplied to living areas. An efficient system can recycle around 90% of the extracted heat

Wastewater heat recovery

Wastewater heat recovery works by pre-heating the cold-water supply to the domestic hot water system using the heat from the water as it drains from the shower. This pre-heated water can then be recirculated back into the shower feed or back into the water heater or a mix of the two. The only requirement is that you have room within the floor void below the shower to enable a unit to fit although new designs mean there are now a variety of installation options.

London Heat Network (Be Clean)

It can be seen from the London Heat Map below that the development is outside of any current or proposed district heating scheme. The small size of the scheme makes it impractical to commission a district heating scheme on its own. It is therefore proposed that onsite efficiency is maximised to reduce the scheme's energy demand and carbon emissions.

Technology Options (Be Green)

Air Source Heat Pump (ASHP)

Air source heat pumps work in an almost identical way to ground source pumps but require significantly less capital investment in ground works and collector pipes. Instead the low-grade energy for the system is collected and processed using an outdoor fan unit.

Photovoltaic Panels (PV)

Solar photovoltaic panels use the sun's energy to produce electricity that can be used within the home and reduce the amount of energy used by the household from the main electricity network. This in turn reduces the overall electricity bill for the home and the home's environmental impact.

Solar Hot Water (SHW)

Solar thermal uses flat panels or vacuum tubes orientated towards the sun that absorb energy. The water in the panels or tubes is heated and then pumped down to a hot water cylinder or thermal store to exchange heat via an indirect secondary coil in the water cylinder. Solar thermal systems are predominantly used to supplement domestic hot water but can also be used for swimming pool heating and space heating using thermal stores.

- PV /SHW
- ASHP
- Green roofs



FIG.01 - PROPOSED DEVELOPMENT MASTERPLAN

Water Efficiency

The client has agreed to specify water efficient sanitaryware and white goods to achieve 105 litres per person per day (Code for Sustainable Standards)

The Roofs

The Green roofs will absorb rainwater and provide insulation, creating a habitat for wildlife, increasing benevolence and decreasing stress of the people around the roof by providing a more aesthetically pleasing landscape, and helping to lower urban air temperatures and mitigate the heat island effect.

They effectively use the natural functions of plants to filter water and treat air.

The green roof option considered is extensive roofs, which are shallow, ranging in depth from 2 cm to 12.7 cm (5 in), lighter than intensive green roofs, and require minimal maintenance.



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11_CONCLUSIONS:

The owner has sought to develop the site in a way which maximises habitable space while limiting visual impact, all while committing to regenerate the site in a way which substantially improves the efficiency of usage while minimising its environmental footprint. The owner has worked to meet or exceed the recommendations and requirements of the NPPF, the London, the Camden and the Hampstead Neighbourhood Plans and seeks the advice of Camden Planning to ensure that the proposed development meets the highest standards of design and development for the area.



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