

CAMDEN HIGHLINE

Noise and Residential Amenity Statement

Prepared for:
Camden Highline

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FIELD
OPERATIONS**

NOISE AND AMENITY EXECUTIVE SUMMARY

This submission has been prepared to identify any overlooking and noise impacts on residential properties that are adjacent to the Highline.

OVERLOOKING

While there are no explicit guides or regulations that directly inform which properties could be impacted by newly built public space, we have taken a broad approach and are assuming that windows within 18m of a Highline pathway could be impacted by onlookers and ambient noise. At this time there is not enough information to determine the use of the spaces that face onto the Highline but the design team is assuming that each window is a living space and are proposing to use evergreen planting to block direct lines of sight into these areas wherever possible.

Phase 1 of the Highline is the widest portion of the project and also runs behind the Camden Road Overground Station. While Network Rail has strict requirements requiring that trees be planted at least 3 meters away from any railroad infrastructure, the larger dimensions of Phase 1 allow us to greatly increase the depth and types of planting. By planting evergreen trees and shrubs along stretches of parapets that flank residences, we believe that we can mitigate the majority of potential privacy concerns. On the following plans, properties that are closest to the Highline have been mapped with their respective distances from the viaduct. Page 9 shows typical sections demonstrating how planting will be used to shield residential occupants from Highline visitors.

NOISE

The Highline runs above streets with busy traffic and alongside active rail lines. Thus noises generated from park activity will inevitably not be higher than the baseline noise of the surrounding area, and therefore, no assessment of the baseline noise level has been carried out. Any noise generated from Highline activity is mitigated through strict hours of operation of the Highline, physical distance from and barriers between the closest sensitive noise receptors and the activity on the Highline.

The Highline is designed as a park in the sky that offers respite from the hectic streets below. The community has voiced great desire for a place of calm and reflection with places to sit and read and talk to friends. The event spaces on the Highline are intended to provide flexible areas where the Highline organizers can hold relatively small events that will help fund the ongoing upkeep and expansion of the Highline itself. The limited number of events means that any noise impacts will be brief and infrequent.

THE SITE

PHASE 1 DESCRIPTION OF DEVELOPMENT

Works relating to the “Camden Highline” ‘Phase 1’, a high level garden on existing viaduct, including access at Camden Gardens and Royal College Street, Commercial kiosks (within existing arches-Class E), seating area, pedestrian walkway, event spaces, woodland balcony and ancillary waste and storage facilities.’

OVERVIEW

Phase 1 of the Highline runs roughly 260m along a disused portion of a railroad viaduct structure spanning Camden Gardens at Kentish Town Road to the intersection of Royal College st and Camden Rd. (See fig.1). The park runs alongside an active Overground rail line and will be separated by a 2.5m high separation screen. The program is primarily passive strolling and seating with an event space and a small amphitheater located behind the platform of the Camden Road Overground station. The Highline will also feature two arches in Camden Gardens that will be programmed with a cafe and kiosk with future tenants providing revenue to further the operations of the Highline. Access via stairs and lift are provided from Camden Gardens and 223 Royal College St, which was the access point to the now demolished Platform 3 at Camden Road station.

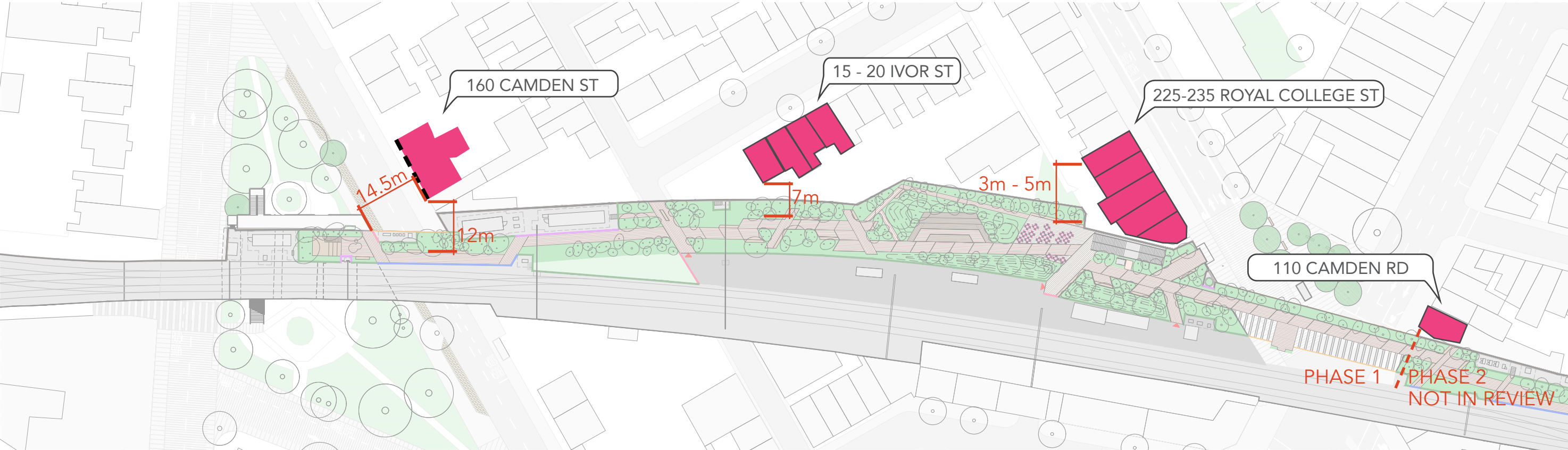
The Highline will operate from dawn to dusk which is in keeping with current Camden Town park hours. The kiosks in Camden Gardens will operate under the same hours. Both entrances are gated and will be secured at night. There are areas behind the Camden Road Station platform where events will occasionally be held with the majority occurring during normal park opening hours. There are anticipated to be up to 10 after hours events per year which will close no later than 10:30 pm.



FIGURE 1 - PHASE 1 HIGHLINE SITE PLAN

RECEPTORS

We have identified four groups of receptors along the proposed route of the Highline. We have undertaken overlooking assessments for each of the groups in the following pages. These properties are also susceptible to noise from the Highline, however, as stated, any noise created by the park will by definition not exceed the existing noise created by the Overground that shares the viaduct structure.



NOISE

ON THE LACK OF BASELINE NOISE LEVEL ASSESSMENT

The Highline runs through a part of town that is busy with cars, buses, and trains. The ambient noise level is already considerable. Noise created by the Highline will not exceed the baseline level.

The active rail line that runs alongside the Highline serves both Overground and intermittent freight trains. The trains pass frequently and contribute to a regular increase in noise as they rumble by at 25 mph. The trains run well into the night and in the early morning, outside of park operation hours.

We have not undertaken a baseline noise assessment because of these existing, and considerable, existing acoustic conditions.

NOISE ON THE HIGHLINE

The majority of the park consists of garden paths and excessive noise is not expected. Phase 1 of the Highline contains an event space. We expect this area to infrequently host mid-size events that might create some noise. We do not expect everyday use of the Highline by pedestrians to contribute to the overall noise of Camden. The Grandstand and Event Plaza will have occasional daytime programming and will host up to 10 after hours events per year, ending no later than 10:30 pm. The following pages provide more details on these uses.



CONSTANT TRAIN ACTIVITY ALONGSIDE THE HIGHLINE CREATES A
BASELINE OF NOISE THAT THE PARK WILL NOT EXCEED.

EVENT-RELATED NOISE

The Grandstand faces south, away from nearby homes and towards the platform. (See Figure 1.) The size of the grandstand is small and is intended to host small, intimate performances.

The event space is roughly 15m x 6m and can accommodate 14 round tables seating 6 each. (See Figure 1.) During normal park operations both locations are envisioned to be places for visitors to enjoy a coffee or sandwich from one of the Camden Gardens kiosks or nearby cafes. Both areas are screened from nearby homes by woodland and evergreen planting.

FIG 1. DETAIL OF EVENT SPACES

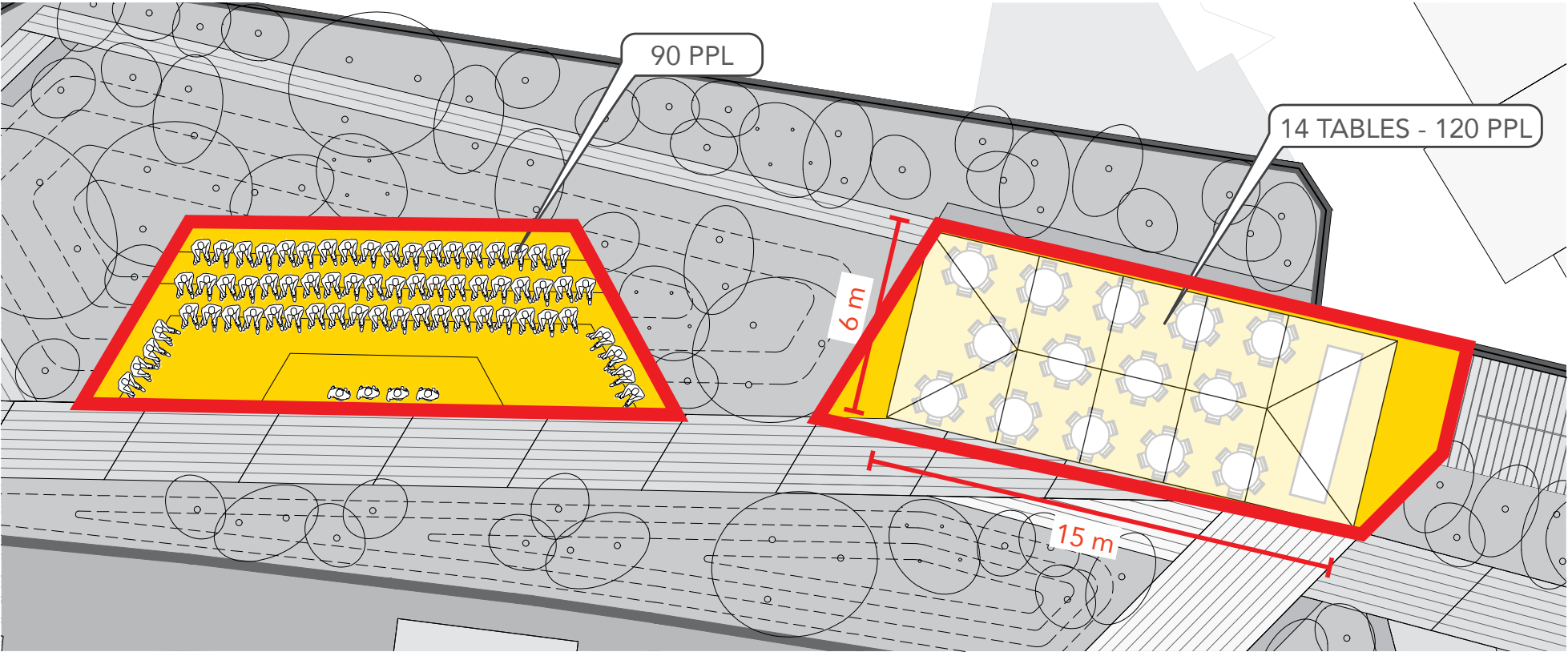
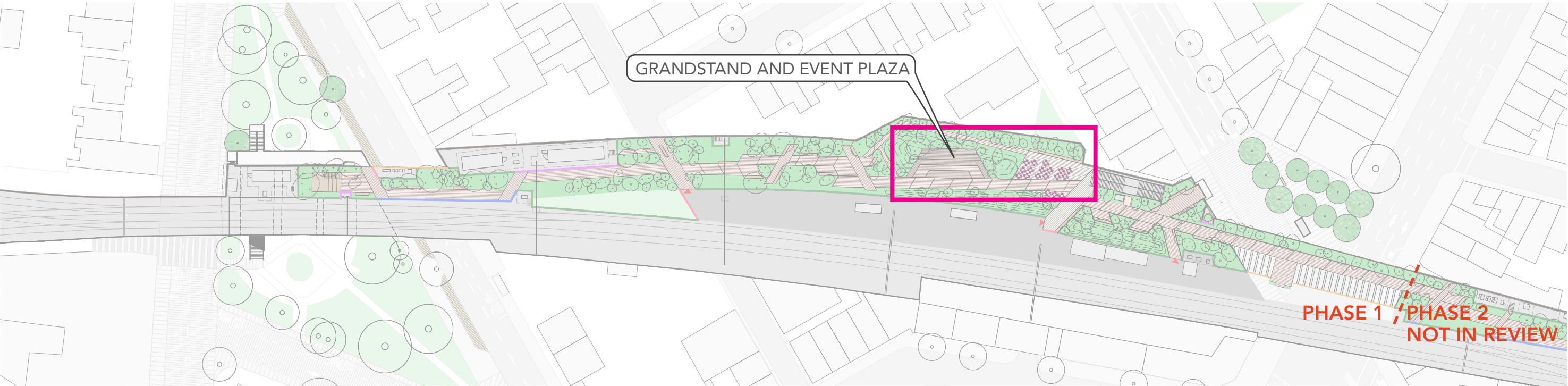


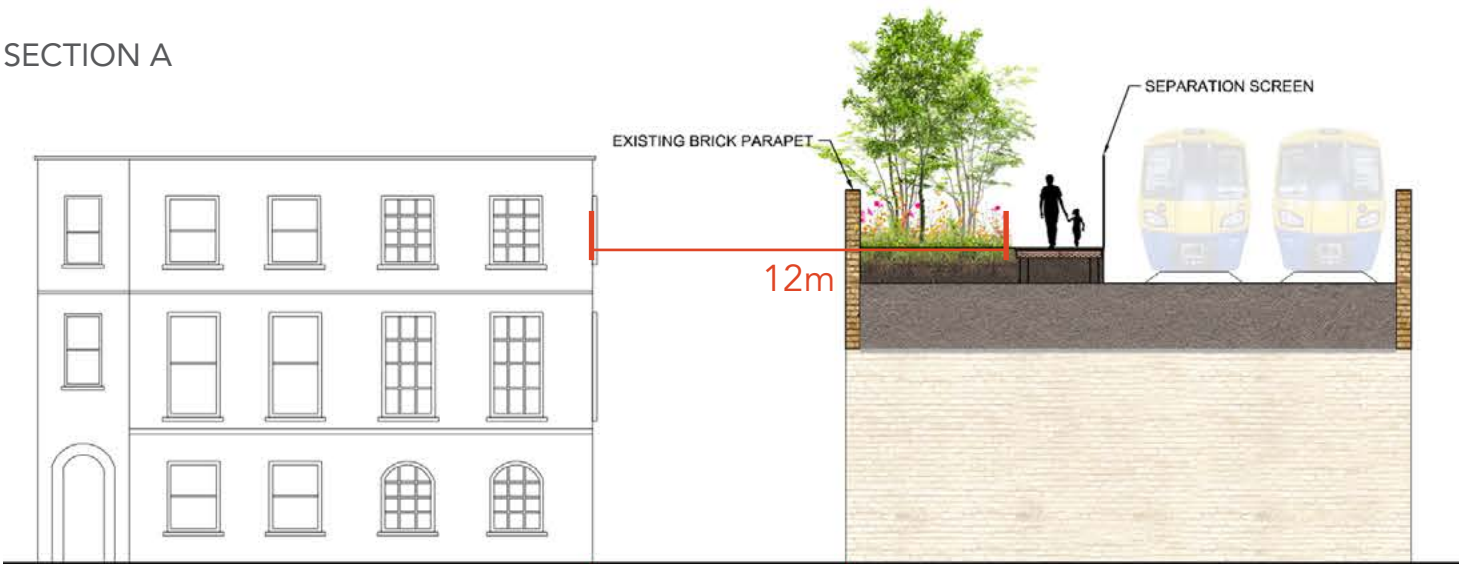
FIG 2. PLAN OF PHASE 1



RESIDENTIAL OVERLOOKING ASSESSMENT

160 Camden St is the first property that Phase 1 of the Highline approaches. The property will be separated from the pedestrian path by planting as well as number of Network Rail assets will be enclosed by a 2m high screen. The combination of screen as well as taller evergreen tree and shrub planting will prevent park visitors from causing any privacy or onlooking issues.

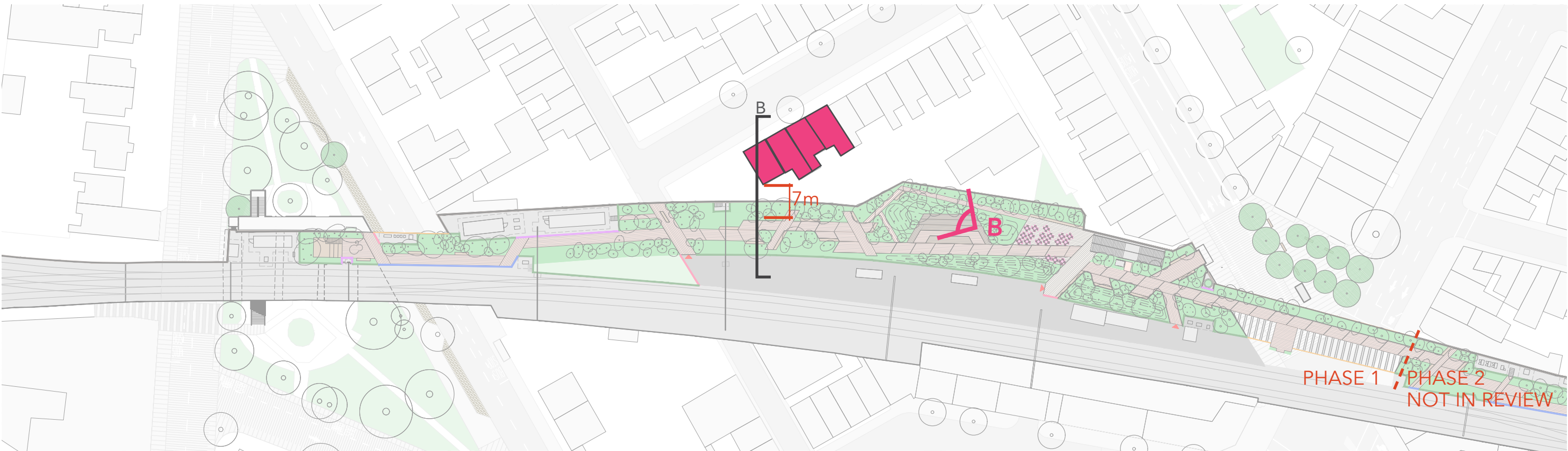
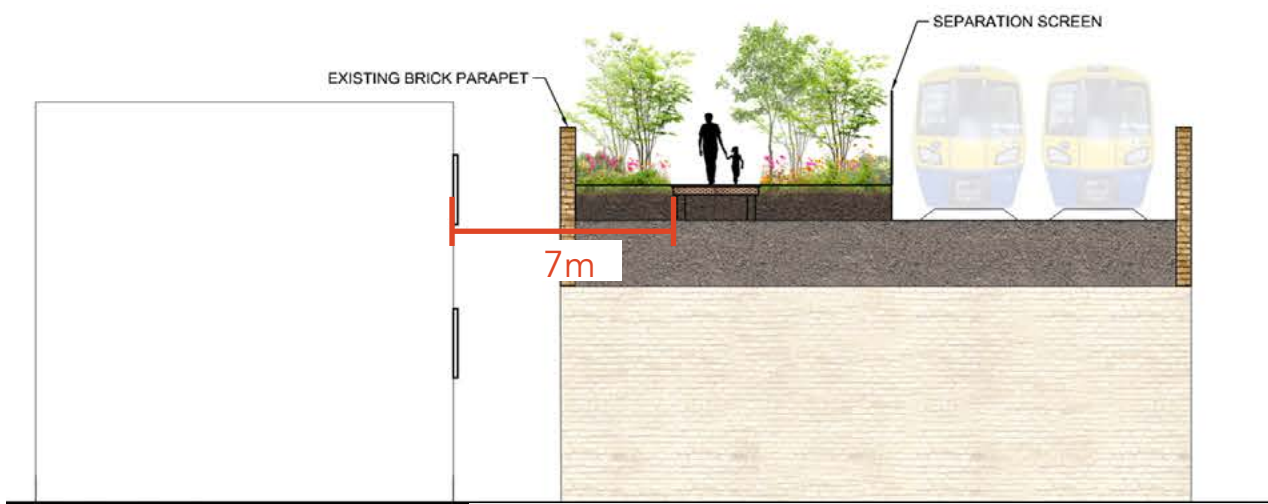
SECTION A



RESIDENTIAL OVERLOOKING ASSESSMENT

15-20 Ivor St back into a courtyard that runs behind the Highline as it passes behind the Camden Road Overground station. The properties will be separated from the pedestrian path by woodland planting as well as evergreen shrubs along the parapet wall.

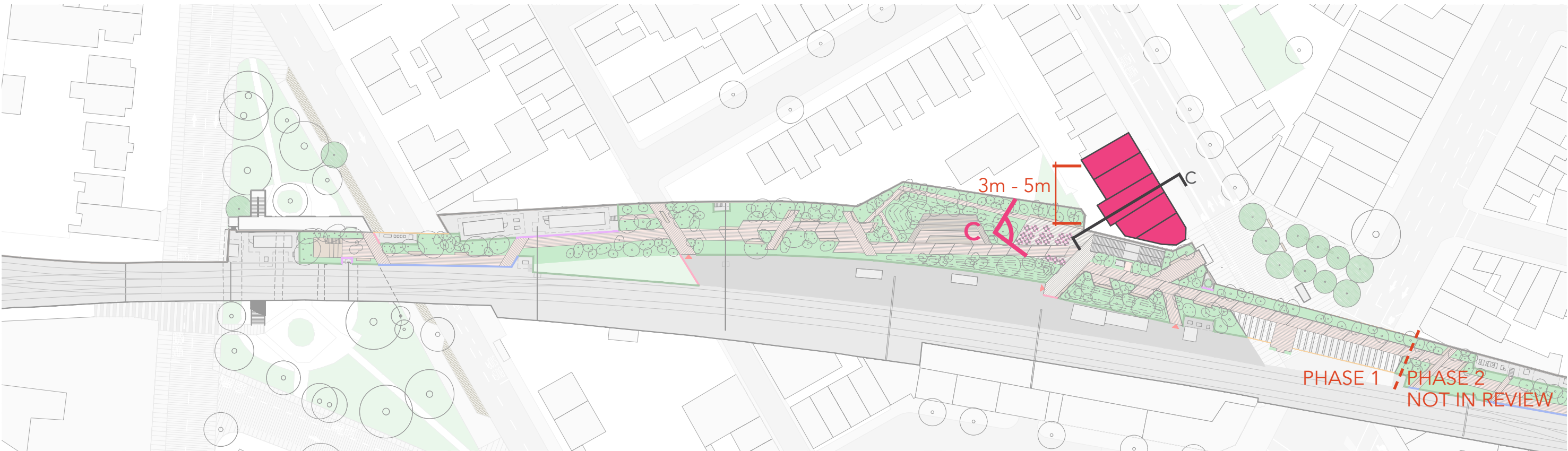
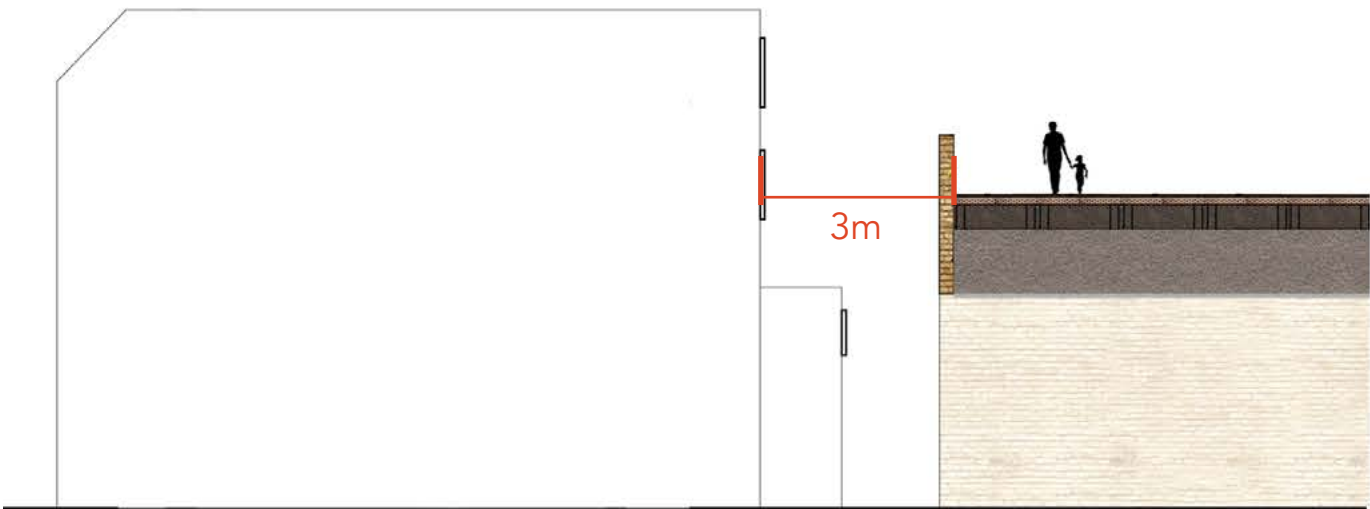
SECTION B



RESIDENTIAL OVERLOOKING ASSESSMENT

The properties of 225-235 Royal College St are adjacent to Highline entrance and are some of the closest properties to the park. Most of the buildings are set back from the Highline and the design includes planting between the pathway and parapet wall allowing for screening. 225 Royal College St is directly adjacent to the stairwell so planted screening is not possible at that location. Wherever possible the design includes planters between pathways and the parapet wall which will shield the residence from the pathways.

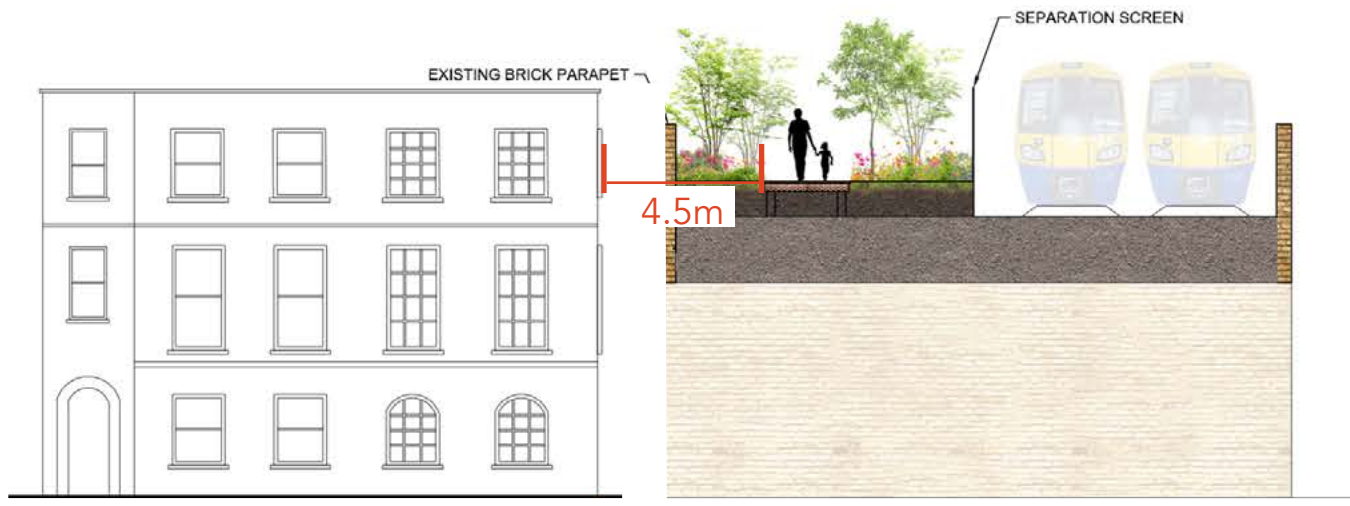
SECTION C



RESIDENTIAL OVERLOOKING ASSESSMENT

110 Camden Road is directly adjacent to the Highline where it crosses the intersection of Royal College St and Camden Rd. There are no windows facing the Highline but the front facade is visible from the bridge. The bridge has a high parapet wall and will be flanked by a raised planter with a mix of meadow and shrub planting which can be strategically arranged to block views into the residence's windows.

SECTION D



CONCLUSION

Throughout the Highline the design approach is to incorporate planting between pathways and the parapet wall. Not only does this shield park visitors from having clear lines of sight into adjacent residential properties but also prevents visitors from looking over the parapet into neighbor’s garden areas. The use of evergreen planting at key locations will ensure that these screening techniques are effective year round.

Given the Highline’s location in a busy part of town, and alongside an active rail corridor, noise generated from the Highline will not exceed the baseline datum of noise. Trains constantly rumble past on the rail line adjacent to the planned park at extended late evening and early morning hours. Any noise created by pedestrians on the park will not exceed this existing noise, and will be confined to the strict operating hours of the park. In addition,

the planting strategy and design of the park creates physical distance and barriers between the Highline and residences that mitigate ambient noise.

