

06

Waste storage



# Waste storage

## 06.01 - Waste / Refuse strategy

A refuse store is located at basement level and accessed through the parking area which have access from the street.

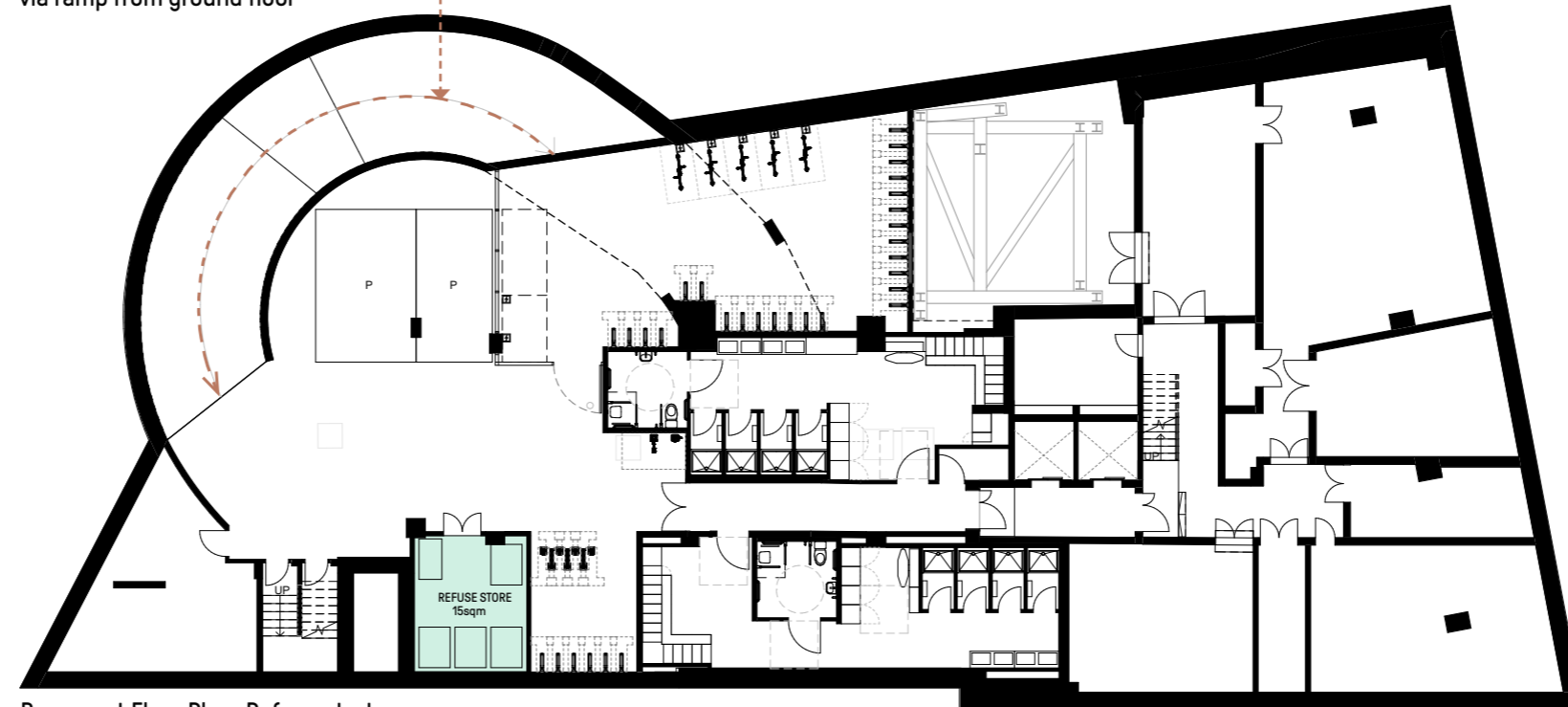
Both general and recycling containers will be accommodated within the store which accommodates a total of No.5 of 1100l Eurobins.

Of this total, 70% of this capacity must be retained for the storage of separate material for recycling (50% paper, 10% other dry recyclables, 10% food waste). We are providing signage or line painting to denote the area dedicated for recyclable & general waste.

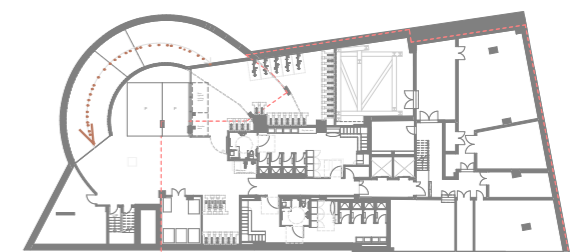
The scheme aims to meet BREEAM Non-Domestic Refurbishment & Fit-Out 2014 credit WST 03 for stage 04. This item requests the provision of a central dedicated storage space for the recycling of materials in addition to the general waste area provision, which require the below:

- Sized at least 2m<sup>2</sup> per 1000m<sup>2</sup> of net floor area for buildings < 5000m<sup>2</sup>
- Located accessible to building occupants or facilities operators for the deposit of materials and collection by waste management contractors
- Provided with signage or line painting to denote the area dedicated for recyclable & general waste

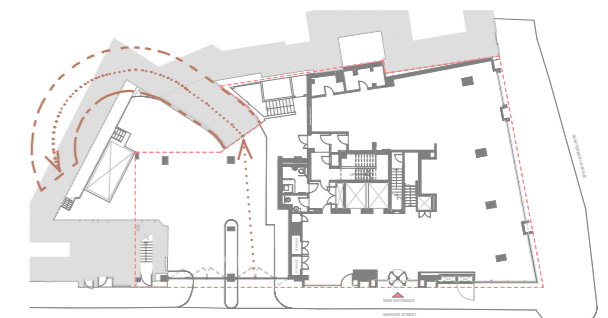
Access to refuse store is done via ramp from ground floor



Basement Floor Plan\_Refuse strategy



Basement Floor Plan\_Refuse access route



Ground Floor Plan\_Refuse access route



07

Parking and Cycle strategy



# Parking and cycle strategy

## 07.01 - Parking and Cycle

	1. London Plan	2. BREEAM	3. WELL	4. BCO	Proposed
Cycle Parking Long Stay	26	30	15	30	36
Cycle Parking Short Stay	2	0	7	0	10

The scheme supports low-carbon travel encouraged by the London Plan offering an excellent amount of cycle parking and associated facilities that exceeds the requirements of London Plan, BREEAM and BCO.

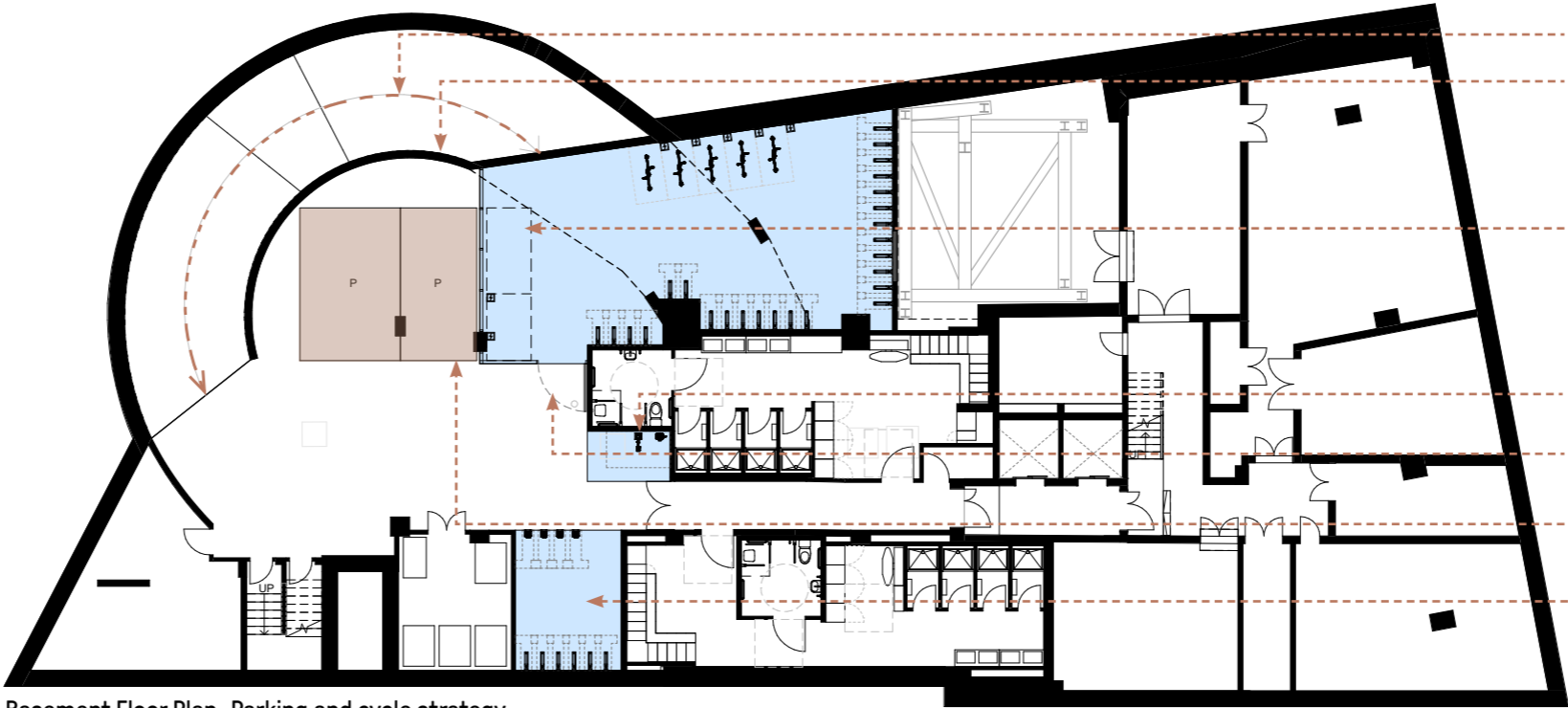
Greater quantum of cycle parking and more secure facilities, exceeding the number required for London Plan and BREEAM standards will be provided.

Access will be provided via the vehicular access ramp or a lift from ground floor which will be suitable sized to allow for cycle access.

A total of 46 cycle parking spaces are proposed at basement level, separated into two cycle stores (36 long stay & 10 short stay). The long stay cycle parking, next to car parking space will be secured and will accommodate 25 wall mounted racks and 5 Sheffield stands with electric bike chargers, an area for scooters, and 1 unit of non-typical cycle parking space, as required by London Cycling Design Standards. Cycle parking will take into account all user needs, so as not to exclude or disadvantage riders of certain types of cycle. This includes people who use handcycles, tricycles, tandems and models adapted to suit the rider's specific needs as well as cargo cycles. The short stay cycle parking spaces will contain 6 wall mounted racks and 4 free standing bike racks.

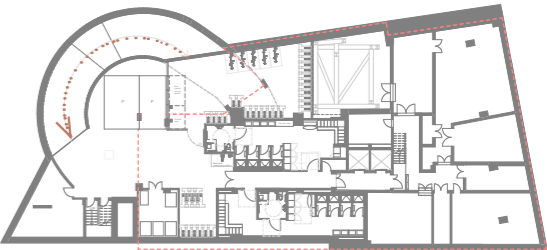
As part of the new cycling facilities complying with BREEAM standards, the design includes repair and maintenance bike station, dedicated female and male shower rooms with accessible shower room on each one, changing facilities, appropriate lockers and dedicated drying space.

The current proposal is looking to maximise to number of bike storage spaces using wall mounted racks, sheffield stands and free standing racks. Providing a larger number of horizontal bike racks will reduce the total number of bike storage.

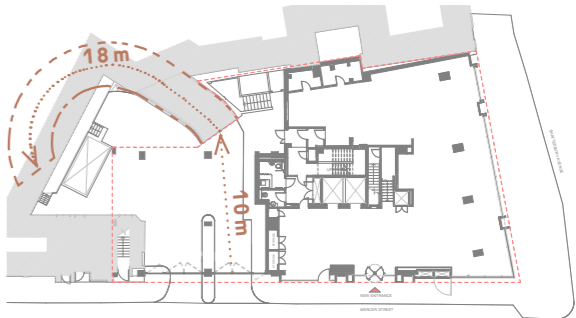


Basement Floor Plan\_Parking and cycle strategy

- Access via ramp from ground floor
- Feature ramp with supergraphics
- Non-typical cycle parking space, as required by London Cycling Design Standards. Space to be used by people who use handcycles, tricycles, tandems and models adapted to suit the rider's specific needs as well as cargo cycles.
- Bike maintenance/ repair station.
- Secured long stay cycle parking area
- Two parking spaces in total including an Electric vehicle charging point and wheelchair parking space
- Visitors cycle parking area



Basement Floor Plan\_Parking access route



Ground Floor Plan\_Parking access route



08

Inclusive design



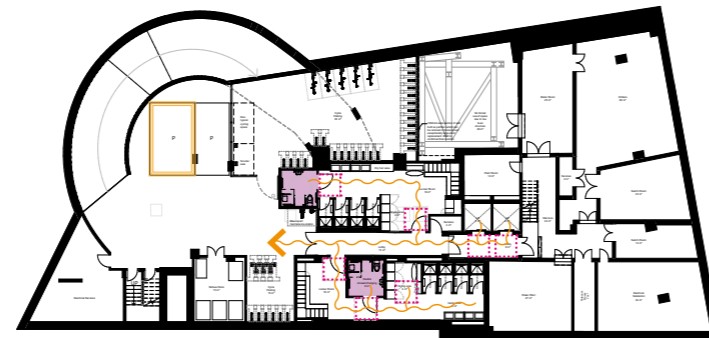
# Inclusive design

## 08.01 - Inclusive design and accessibility

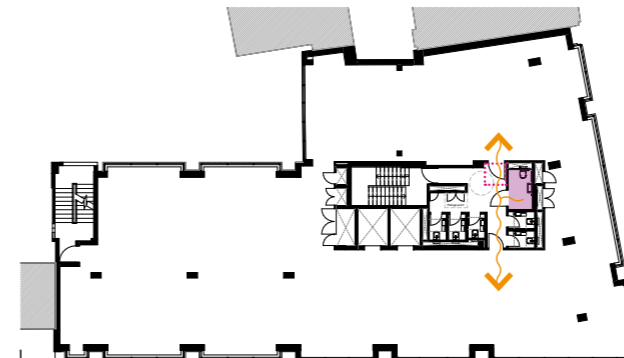
Office design is centred around agile working to deliver solutions that suit everyone. It should offer flexibility, fluidity and create appropriate work environments for all members of staff. Inclusive office design is about accommodating choice, flexibility and practicality – a work environment that benefits everyone, regardless of their abilities. User experience has been considered to ensure ultimate user comfort and control.

CGL will endeavour to incorporate the following principles of inclusivity:

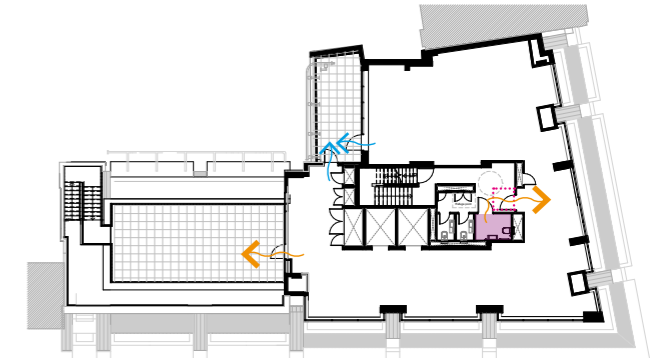
- Provision of step free access and automatic doors to comply with part M requirements. Providing suitable door handles and openings for people with limited manual dexterity such as levers instead of knobs.
- Wheelchair bathrooms on each floor - accessible showers available at lower floors.
- Clear wayfinding. In addition to written information, tactile wayfinding (braille lettering) and graphical cues will be incorporated into all wayfinding design so that the information is accessible for everyone.
- Multi-sensory safety alarms (auditory; visual), and print instructions for emergency and safety equipment
- Green walls and natural elements such as timber wood, stone will reduce stress levels and enhance aesthetics.



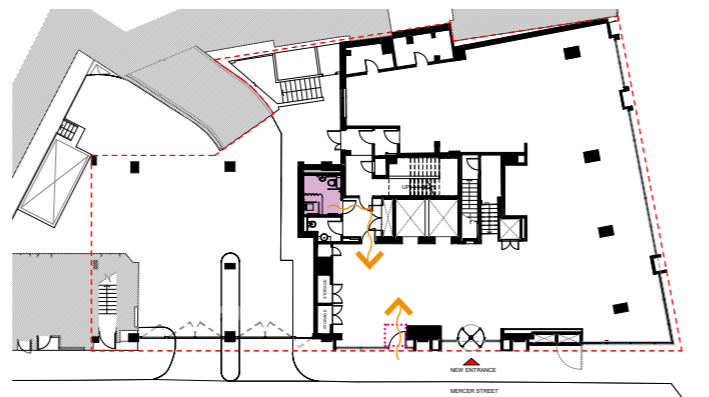
Basement Floor Plan



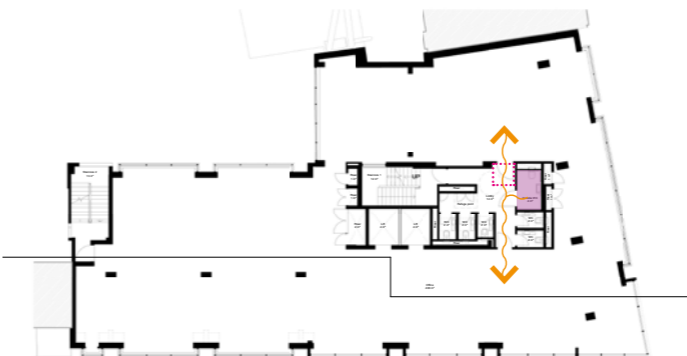
Second Floor Plan



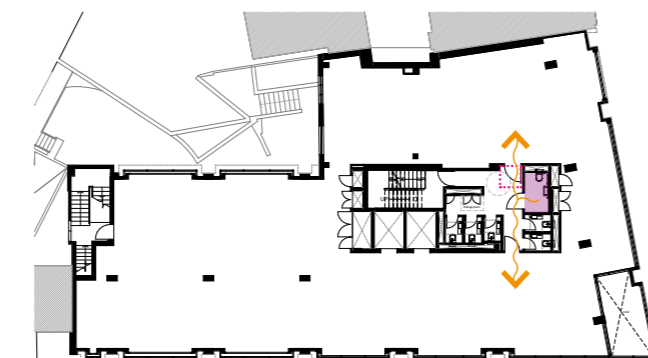
Sixth Floor Plan



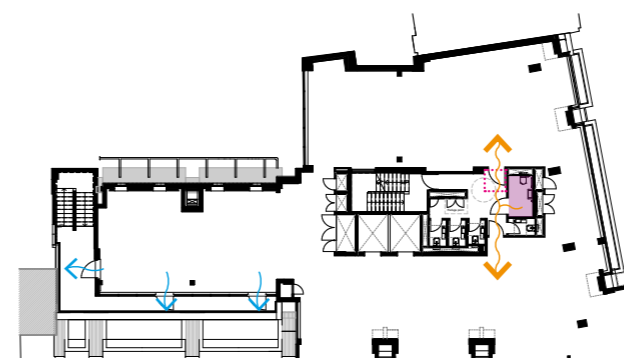
Ground Floor Plan



Third, Fourth Floor Plan



First Floor Plan



Fifth Floor Plan

Wheelchair WC / Shower

Step free access

1500mm turning circle

Threshold access

Due to existing building constraints and the need for achieving the appropriate U-value for the building envelope, the threshold will need to be retained. Access strategy to be agreed with building management.

Disabled parking

09

Maintenance Strategy



# Maintenance strategy

## 09.01 - Maintenance strategy

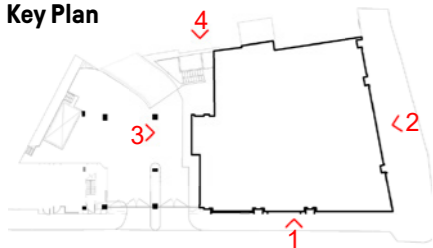
A cleaning and maintenance strategy has been developed to enable safe access to both the existing and new facades of the building for the purpose of regular cleaning and maintenance as required.

The maintenance strategy has been developed to ensure this can be achieved in the safest way possible, given the constraints of the building curtilage.

- Low level areas of the façade (ground floor and first floor) are to be cleaned via a rigid long pole to allow a regularly high quality clean in the visible façade at street level
- The existing window cleaning cradle to clean the south-west and south-east elevations will be retained, with the cradle being stored at ground level when not in use
- The north-west and north-east elevations are to have the existing abseil cleaning adapted to suit the internal refurbishment and the new curtain walling should consider a safe and efficient means of access for periodic cleaning and long-term maintenance developed by a vertical access consultant
- The glazing located on the respected terraces are to be used the reach and wash method.

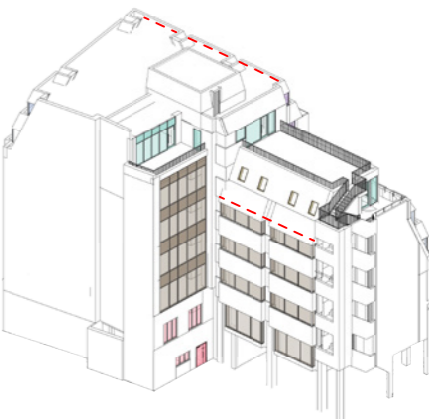
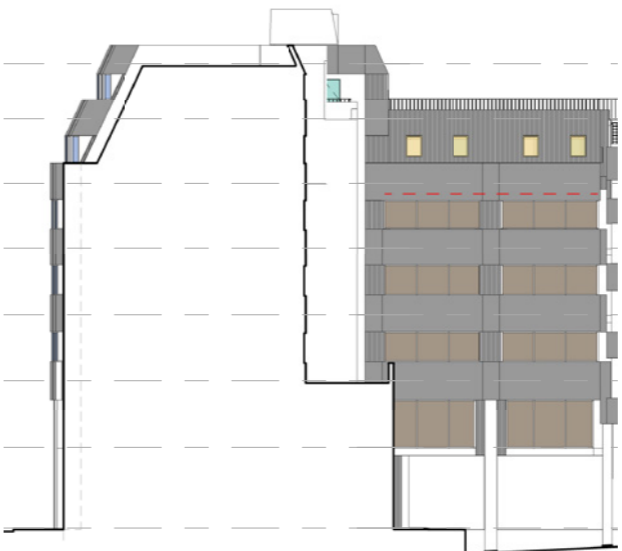
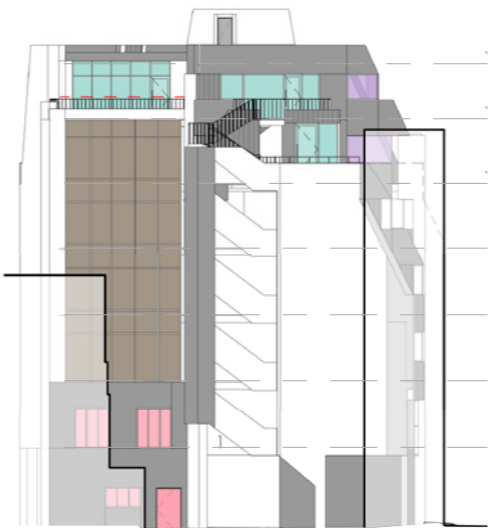
A robust cleaning strategy to be considered for safe and efficient means of access for periodic cleaning and long term maintenance.

### Key Plan



### Maintenance Strategy Legend

- Windows cleaned from exterior, accessible from terrace.
- Existing Abseil window cleaning:
  - Zones inaccessible from office area, higher than 18m above ground level / terrace.
- Abseil window cleaning:
  - Zones inaccessible from office area, higher than 18m above ground level / terrace.
- Water-fed Pole system:
  - Windows unable to be cleaned from the inside.
  - Glazing systems / Glazed doors on Ground Floor to and First Floor be cleaned with manual cleaning methods. It is suggested that these are cleaned by a specialist cleaning contractor.
- Existing window cleaning cradle:
  - cradle being stored at ground level when not in use
- Abseil access anchor points location.



# 10

## Appendix

- Acoustic report by Clarke Saunders
- Sustainability report by JLL Net Zero Design Consulting
- Heritage report by KM Heritage

