

HS2

High Speed Rail (London – West Midlands) Act 2017

HS2 Ltd

London Borough of Camden

Granby Terrace Bridge Western Wing Wall

Schedule 17 Plans and Specifications Written Statement for Information

LBC.PS.10008

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1 Introduction

1.1 Background Information

Site	Details
Scheme	High Speed Two
Applicant	High Speed Two (HS2) Limited
Applicant Address	<i>c/o Agent:</i> SCS Railways Joint Venture (SCS) Black Arrow House 2 Chandos Road London NW10 6NF
Site Address	Granby Terrace, London Borough of Camden. NW1 3SA The works are located at: Easting: 529053, Northing: 183063
Description	Submission under Schedule 17 of the High Speed Rail (London-West Midlands) Act for approval of Granby Terrace Bridge Western Wing Wall, comprising a parapet wall (approval for location only) and earthworks including a retaining wall.

Table 1: Granby Terrace Bridge West Wing Wall Schedule 17 - Address Details and Description of Works

1.2 Terms of Reference

- 1.2.1 This Written Statement is compiled in accordance with the High Speed Two (HS2) Phase 1 Planning Memorandum and Planning Forum Notes (PFNs) as required by the planning regime established under Schedule 17 of the High-Speed Rail (London – West Midlands) Act 2017.
- 1.2.2 This statement provides the London Borough of Camden with information to assist with the determination of the Plans and Specifications submission under Schedule 17, in relation to the above description of works.
- 1.2.3 The information in this Written Statement is provided for information to assist in determining the request for approval. It is not for approval.

1.3 Introduction to High Speed 2

- 1.3.1 HS2 is a new high-speed railway network that will connect major cities in Britain. It will bring significant benefits for inter-urban rail travellers through increased capacity and improved connectivity between London, the Midlands and the North. It will release capacity on the existing rail network and so provide opportunities to improve existing commuter, regional passenger and freight services.
- 1.3.2 Phase One of HS2 will provide a dedicated high-speed rail service between London, Birmingham and the West Midlands. It will extend for approximately 230km (143 miles). Just north of Lichfield, high speed trains will join the West Coast Main Line for journeys to and from Manchester, the North West and Scotland.
- 1.3.3 For further information on HS2 and the route through the London Borough of Camden please refer to the Planning Context Report for the London Borough of Camden, deposited with the Council by HS2 Ltd.

1.4 High Speed Rail (London – West Midlands) Act 2017

- 1.4.1 The High-Speed Rail (London – West Midlands) Act 2017 ('the Act') provides powers for the construction and operation of Phase 1 of High Speed Two. HS2 Ltd is the nominated undertaker in relation to the works subject to this Plans and Specifications submission.
- 1.4.2 Section 20 to the Act grants deemed planning permission for the works authorised by it, subject to the conditions set out in Schedule 17. Schedule 17 includes conditions requiring the following matters to be approved or agreed by the relevant LPA.
- Construction arrangements (including large goods vehicle routes);
 - Plans and specifications;
 - Bringing into use requests; and
 - Site restoration schemes.
- 1.4.3 This is therefore a different planning regime to that which usually applies in England (i.e. the Town and Country Planning Act) and is different in terms of the nature of submissions and the issues that the LPAs can have regard to, in determining requests for approval.

- 1.4.4 Schedule 17 of the Act sets out the grounds on which the LPA may impose conditions on approvals or refuse requests for approval.
- 1.4.5 This Written Statement includes information supporting the Plans and Specifications submission in relation to the matters outlined in **Table 2** below.

Site	Details
Plans and Specifications (permanent works)	<ul style="list-style-type: none">• Paragraph 3: Earthworks - Earthworks to facilitate construction of retaining wall• Paragraph 3: Fences or walls - Parapet wall

Table 2: Schedule 17 Plans and Specifications Submission Details

- 1.4.6 The works to which this application relates, and the cumulative impact of the works in conjunction with other HS2 development, have been assessed and are compliant with paragraph 1.1.3 (bullet point 2) of the HS2 Phase 1 Environmental Minimum Requirements General Principles¹.

1.5 High Speed Two: Code of Construction Practice

- 1.5.1 HS2 Ltd as the nominated undertaker is contractually bound to comply with the controls set out in the Environmental Minimum Requirements. The Environmental Minimum Requirements include the High Speed Two Code of Construction Practice (CoCP).

1.6 Structure of Written Statement

- 1.6.1 This Written Statement is structured as follows:
- A description of the location and main characteristics of the works area is provided in **Section 2**;
 - **Section 3** describes the main works being undertaken in the area, as set out in Schedule 1 of the Act, and those that are the subject of this Schedule 17 Plans and Specifications submission;
 - The design criteria and rationale for the works which are the subject of this Schedule 17 Plans and Specifications submission are described in **Section 4**;

¹
https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/618074/General_principles.pdf

- **Section 5** summarises the pre-submission consultations that were undertaken, including a list of the consultees, dates, attendees at meetings and a brief summary of the outcome of these discussions;
- A high-level programme for the works and how they fit into the wider programme for other works in the area, as set out in Schedule 1 of the Act, is provided in **Section 6**; and
- **Section 7** identifies any other main consents, or known forthcoming consents associated with the works.

2 Site Location and Characteristics

2.1 Site Location

- 2.1.1 The application site is located in the London Borough of Camden, an inner city London Borough to the north of the River Thames. It is situated to the north-west of Euston Station (**Figure 1**).
- 2.1.2 The site is located to the north side of the approved extension of Granby Terrace Bridge (GTB) (LPA ref: 2021/0356/HS2). GTB traverses the West Coast Main Line (WCML) cutting, linking the residential areas of Park Village East (PVE) and Regents Park to the west, and Hampstead Road (a main thoroughfare between Tottenham Court Road to the south and Camden Town in the north) to the east.

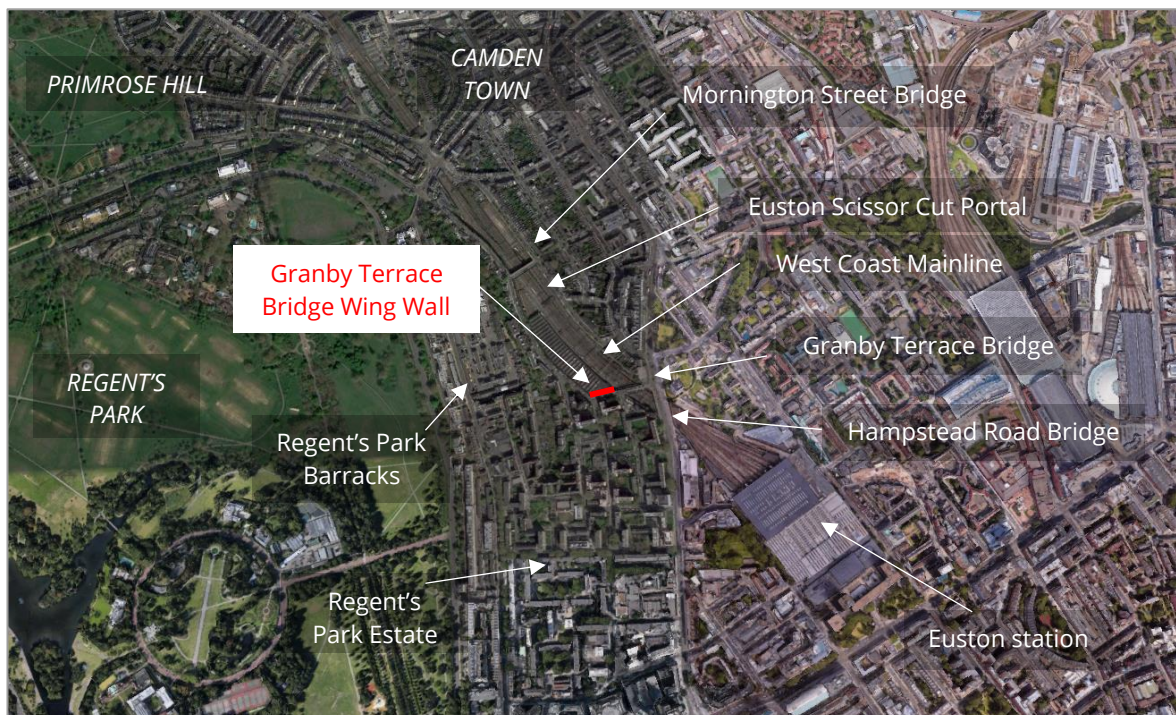


Figure 1 Aerial view of Euston and surrounding area. Approximate site location demarcated in red (Map data Google 2020)

- 2.1.3 The application site has a length of approx. 21.1 metres and a depth of approx. 6.13 metres. The lower extent of the site interfaces with made ground, situated adjacent to the high speed rail track level. The site extends upwards to a height of approximately 3.04 metres above highway level on Granby Terrace Bridge.

- 2.1.4 The nearest public transport links include London Euston Station, Mornington Street Underground Station, and key bus routes with a stop to the east on Hampstead Road (**Figure 2**). Further details of the surrounding highway network are summarised in **Section 2.4****Error! Reference source not found..**

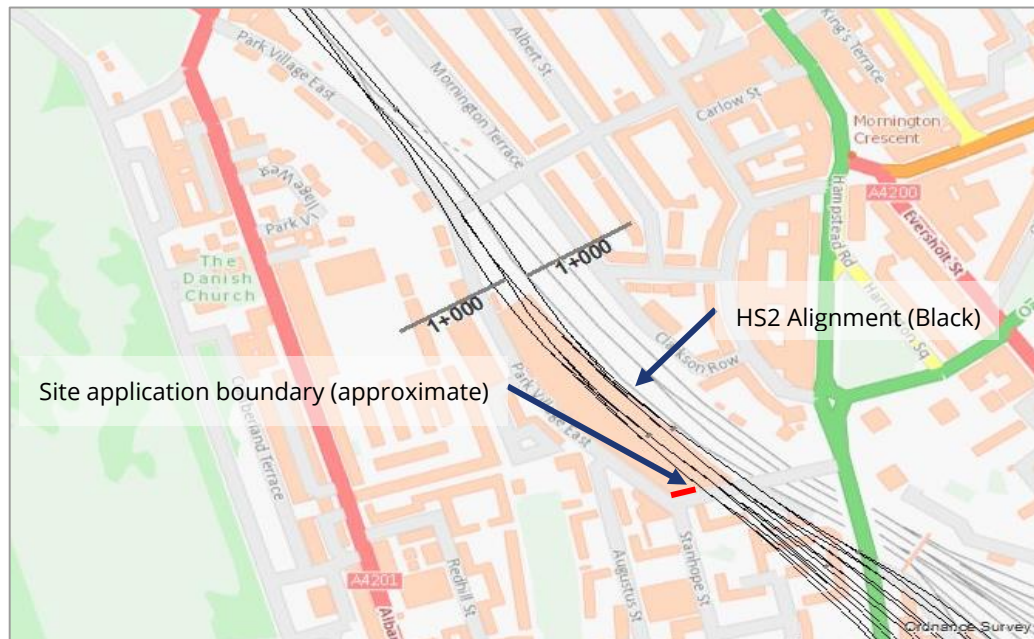


Figure 2 HS2 Track Alignment and Schedule 17 application boundary (demarcated in black and approximate site application boundary in red)

2.2 Adjacent Land Uses

- 2.2.1 The immediate setting to the north of the application site comprises rail infrastructure within a deep cutting, substantially below the level of Granby Terrace Bridge. This includes high speed rail (for which enabling works are currently underway) and the existing West Coast Mail Line (WCML).
- 2.2.2 The immediate setting to the south and west of the application site comprises the highways and associated pedestrian footways of Granby Terrace Bridge and Park Village East (see **Figure 3** & **Figure 4**).
- 2.2.3 The wider setting to the south and east is residential in character and predominantly comprises post-war housing of varying styles. The Regent's Park Estate (to the south) and housing between Eversholt Street and Hampstead Road both consist of medium to high rise social housing blocks.

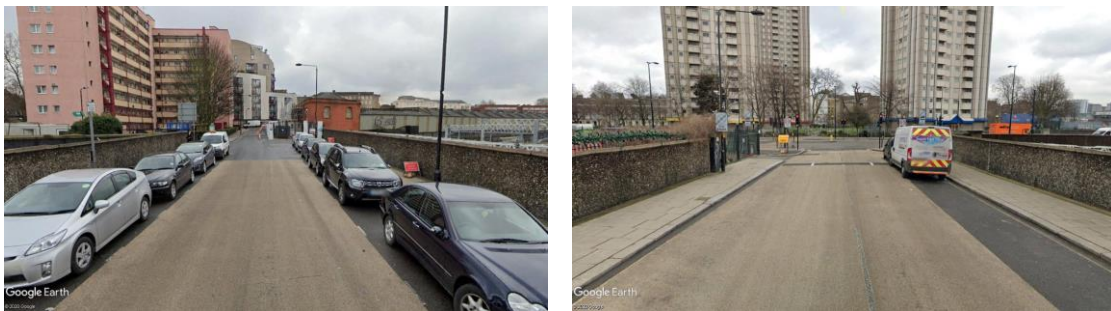


Figure 3 Street level views of existing Granby Terrace Bridge. Photos were taken from Google Maps in 2017. Construction works have since commenced on approved Bridge extension works (Source: Google Earth)

- 2.2.4 To the north and west, the area between Park Village East and Regents Park comprises predominantly Georgian housing stock, including the Grade II* listed Nash Villas. The existing West Coast Main Line railway corridor forms a physical barrier which separates Park Village East from more Georgian housing located along Mornington Terrace to the north east.
- 2.2.5 Euston Station, one of Britain's busiest main line rail stations with connections to major UK cities north of London (e.g. Birmingham, Liverpool, Manchester, Edinburgh and Glasgow), is located to the south east. Railway tracks and associated operational land are located to the north and east.

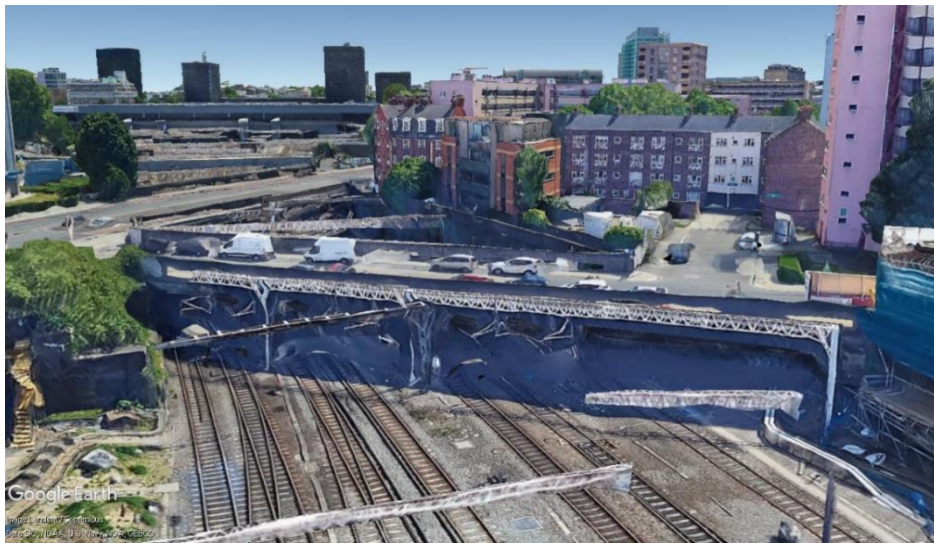


Figure 4 Pre-construction views of Granby Terrace Bridge (Source: Google Earth)

- 2.2.6 Regents Park lies approximately 350m to the west of the site and is one of London's largest and most significant areas of open space. Closer to the site, there are other smaller open spaces and play areas throughout the residential areas immediately to the west. Regents Park Barracks is approximately 150m to the north-west of the site.

2.3 Environmental Characteristics

- 2.3.1 Regent's Park Site of Metropolitan Importance (SMI) lies approximately 580m to the south-west of the application site. The park comprises mature parkland trees, a small enclosed woodland, an ornamental lake and a grassland area managed specifically for wildlife.
- 2.3.2 There are several designated heritage assets in the area, summarised in **Table 3** and **Figure 5**.

No.	Listed Asset	Type	Distance to application site
1	Nash Villas along Park Village East	Grade II*	250m to the north-west
2	Regent's Park Barracks	Grade II	150m west
3	Mornington Crescent No's 1, 2-35 and 261/263	Grade II	70 – 150m north / east
4	Mornington Street Bridge stone piers, i.e. pillars and associated lamp posts, west and east ends of bridge	Grade II	365m north
5	Listed buildings towards Regents Park	Grade II*, II and I	Approximately 200m southwest
6	Regents Park Conservation Area	-	Adjacent to west
7	Camden Town Conservation Area	-	Adjacent to north/east

Table 3 List of designated heritage assets in proximity to application site

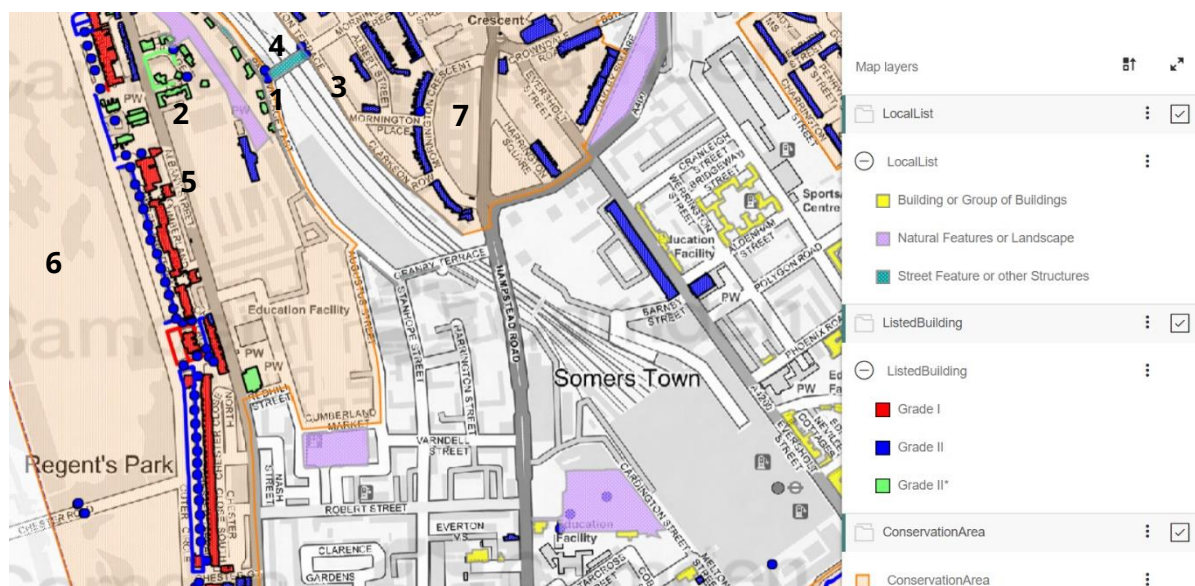


Figure 5 Heritage assets in area surrounding application site (approximately marked on in red)

Source: Camden Local List

2.3.3 There are also a number of non-designated heritage assets, as included on Camden's Local List (adopted on 21 January 2015) and shown in **Figure 5**:

- Portions of the Park Village East western wall (parapet only) to the western side of the WCML cutting (running along Park Village East to Granby Terrace); and

- The wall to eastern side of the railway cutting (along the extent of Mornington Terrace and Clarkson Row).

2.4 Surrounding Highway Network

- 2.4.1 Granby Terrace joins the A400 Hampstead Road at a signalled junction at the eastern end of the street. The A400 Hampstead Road is a key connecting route across this part of inner London. It links with the A501 Euston Road to the south at an extremely busy, split level, signalled interchange (adjacent to Warren Street London Underground Station), and to the north, joins with the southern end of Camden High Street at a signalled junction with Crowndale Road, adjacent to Mornington Crescent London Underground Station. Further south from this, at the junction adjacent to Harrington Square Gardens, Lidlington Place becomes the A400, which links to Crowndale Road, a short distance to the north east.
- 2.4.2 Approximately 0.7km south of the junction with Granby Terrace, the A400 Hampstead Road has a junction with A501 Euston Road. The A501 extends westwards to join the A40 Westway, which in turn links to the M25 and M40 motorways. Extending eastwards, the A501 connects to the A1, and to the A10 and the A11 via Commercial Street. To the west of the site, the A5205 links to the A41, which continues north to join the North Circular and M1 motorway. To the north of the site is Parkway road which joins with the A5205.
- 2.4.3 The western end of Granby Terrace connects with Park Village East, with onward northern connections to the A4201 Parkway through Camden Town. To the south, Park Village East connects with a series of local roads, with onward connections to the east with the A400 Hampstead Road.



Figure 6 Highway network in area surrounding application site (source: SCSJV Maps)

3 Description of the Works

3.1 Introduction

- 3.1.1 This Written Statement supports the Schedule 17 submission for the approval of plans and specifications for Granby Terrace Bridge Western Wing Wall located in the London Borough of Camden.
- 3.1.2 The Plans and Specifications submitted for approval are listed in the proforma accompanying the application. A summary of the proposed works for approval is provided in sections 3.2, 3.4 and 3.5.
- 3.1.3 Information on adjoining HS2 works is supplied in section 3.3. This information is not for approval under Schedule 17.
- 3.1.4 Sections 3.6 to 3.10 provide information on other aspects of the works to assist in understanding the context of the works being submitted for approval. The text contained within these sections is not for approval under Schedule 17.

3.2 Works for Approval

- 3.2.1 The relevant scheduled works as set out under Schedule 1 of the Act to which this Schedule 17 submission relates are:
- **Work No. 1/1** - *A railway (23.48 kilometres in length) partly in tunnel, commencing at a point 235 metres east of the junction of North Gower Street with Drummond Street passing north-westwards and terminating beneath a point 80 metres north-west of the bridge carrying Ickenham Road over the Marylebone to Aylesbury Railway. Work No. 1/1 includes shafts at Cobourg Street, Mornington Street, Granby Terrace, Parkway, Adelaide Road, Alexandra Place, Canterbury Works and Greenpark Way, a station at Old Oak Common and a Crossover Box at Victoria Road;*
 - **Work No. 1/13** - *A diversion of Granby Terrace commencing by a junction with Work No. 1/11 at a point 30 metres north of the junction of that road with Hampstead Road and terminating at a point 4 metres south-west of the junction of Granby Terrace with Stanhope Street. Work No. 1/13 includes a bridge over Works Nos. 1/1, 1/2 and the West Coast Main Line Railway.*
- 3.2.2 The works submitted for approval under this Schedule 17 application comprise:

- The location of the parapet wall and pilasters at street level on the north side of the approved GTB extension, adjoining the GTB extension parapet wall and Park Village East wall.
- A retaining wall below street level on the northern elevation of the Granby Terrace Bridge extension, adjoining the Granby Terrace Bridge retaining wall and Park Village East Berm and Upstand.

3.2.3 The grounds for approval of the proposed works are summarised in **Table 4**. Details of each work are included below at sections 3.4 and 3.5.

Proposed works	HS2 Act Grounds for Approval
Parapet wall and pilasters	As 'Fences and walls' under Sch.17, Part 1, Paragraph 3: That the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.
Retaining wall	As 'Earthworks' under Sch.17, Part 1, Paragraph 3: That the design or external appearance of the works ought to, and could reasonably, be modified – a) To preserve the local environment or local amenity, b) To prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area or, c) To preserve a site of archaeological or historic interest or nature conservation value.

Table 4 The works submitted for approval and their grounds for approval under the HS2 Act

3.3 Other works

- 3.3.1 There are a range of HS2 works in the area surrounding the application boundary for this Schedule 17 application. **Figure 7** provides an overview of immediately adjacent works.
- 3.3.2 In relation to item (1) in **Figure 7**, the proposal will adjoin the approved GTB extension to the east. The GTB extension was granted approval, under Schedule 17, on 24 March 2021 (LPA ref: 2021/0356/HS2) by the London Borough of Camden.
- 3.3.3 The GTB extension comprises:
- A bridge superstructure over new HS2 route: A reinforced concrete deck slab that carries a 6.6m wide highway (with a 5m wide footpath to the north side) and

encloses two utility troughs. It will traverse the new HS2 route from the existing Granby Terrace Bridge to the junction of Stanhope Street and Park Village East. Alongside the footways, the bridge will have 1.8m high precast concrete parapets with H4a containment capacity² on either side and will be shaped to deter climbing.

- Retaining walls on either side of bridge superstructure: Required as HS2 track alignment will be at lower level than the existing adjacent WCML. These will comprise reinforced concrete contiguous bored pile walls, with external reinforced concrete lining walls (in a fair faced concrete finish).

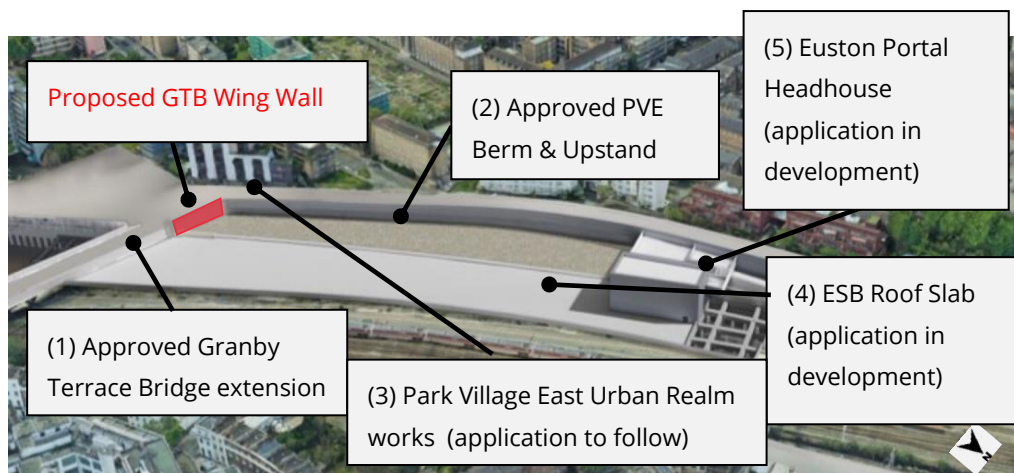


Figure 7 Indicative visualisation of the proposed extension to Granby Terrace Bridge

- 3.3.4 In relation to item (2) in **Figure 7**, the north-east side of the proposed retaining wall will adjoin the approved Park Village East Berm & Upstand. The Park Village East Berm & Upstand was granted approval, under Schedule 17, on 17 March 2021 (LPA ref: 2021/0126/HS2) by the London Borough of Camden.
- 3.3.5 The Berm & Upstand will comprise a support structure that will structurally support the existing Park Village East retaining wall, to the west side of the WCML railway cutting, between the Euston Scissor Box and Parkway Tunnel.
- 3.3.6 In relation to item (3) in **Figure 7**, the north-east side of the proposed parapet wall will adjoin a reinstated parapet and planter wall along Park Village East, as part of wider urban realm works in this locality. The design development of

² H4a refers to the parapet containment level corresponding to the load the parapet must be designed to withstand. This is correlated to testing the parapet based on a test vehicle and impact speed. H4a is the highest level, or "very high containment level" and is measured by a 30t rigid HGV test vehicle and impact speed of 65kh/h. This type of containment is required in high risk areas, especially in proximity to railways.

these works will be initiated in the short to medium term and will form a separate future Schedule 17 application.

- 3.3.7 In relation to item (4) in **Figure 7**, the proposal will adjoin the Euston Scissor Box (ESB) Roof Slab to the north, a concrete slab positioned over the HS2 tracks with potential for future Over-Site Development. The slab design is in development and will form a separate future Schedule 17 application.
- 3.3.8 In relation to item (5) in **Figure 7**, the proposal will lie to the south of the Euston Portal Headhouse – the design for this element is still in development. The Headhouse is situated on Park Village East and houses mechanical and electrical plant, a separate auto-transformer station that provides power to the railway, and space for maintenance vehicle parking.
- 3.3.9 The design development and rationale of the interface with these assets is dealt with in Section 4 of this Statement.

3.4 Works for Approval: Paragraph 3 – Fences and Walls

- 3.4.1 A brick parapet wall with pilasters will be situated to the north side of the approved GTB extension. It will have a width of approx. 21.1 metres and a maximum height of approx. 3.04 metres above highway level on GTB.
- 3.4.2 The wall will form the gap between the approved GTB extension parapet to the east and the reinstated parapet and planter wall on Park Village East to the west. The design of the Park Village East parapet and planter wall is yet to be developed.
- 3.4.3 There are three proposed pilasters along the proposal wall: one at the interface with the GTB extension parapet wall, one at the interface with the Park Village East parapet wall and one at the mid-point of the proposed wall. The pilaster which interfaces with the GTB extension will be of a lower height, in comparison to the other two pilasters.

3.5 Works for Approval: Paragraph 3 – Earthworks

- 3.5.1 The retaining wall will act as a retaining structure to facilitate the level difference between the road level on the Granby Terrace Bridge and the ground below, and so comprises elements both above and below ground.

- 3.5.2 In relation to the above ground element, at its upper extent, it will adjoin the proposed parapet wall and, at its lower extent, it will interface with made ground (above the proposed high speed track level). The above ground element will have a span of approx. 21.1 metres and a height of approx. 4.09 metres. The exposed face of the wall will have a brickwork finish.
- 3.5.3 The below ground element of the retaining wall will comprise underground supporting structures. This element will not require approval, by virtue of Paragraph 30, Schedule 17 of the Act.

3.6 Operational Noise

- 3.6.1 The design of Granby Terrace Bridge Western Wing Wall will not result in new or altered adverse airborne and ground borne vibration impacts at receptors from those assessed within the Environmental Statement.

3.7 Indicative Mitigation

- 3.7.1 No specific indicative ecological or landscape mitigation is provided as part of this application. In line with paragraph 11 of HS2 Planning Forum Note 10, it is not considered reasonably necessary to provide mitigation for the proper consideration of the design proposed.

3.8 Construction Method

- 3.8.1 The works subject to this request for approval of Plans and Specifications will be undertaken in accordance with the HS2 Code of Construction Practice and the Class Approval issued by the Secretary of State (March 2017).
- 3.8.2 The general construction methodology for this proposal is still under development and so details are not yet available.

3.9 Historic Environment

Background

- 3.9.1 The HS2 Heritage Memorandum (part of the HS2 Environmental Minimum Requirements) explains that a route-wide generic written scheme of investigation (Historic Environment Research and Delivery Strategy (GWSI: HERDS)) has been prepared in consultation with Historic England (HE) and the

LPAs. It sets out the research framework and general principles for design, evaluation, investigation, recording, analysis, reporting and archive deposition to be adopted for the design development and construction.

- 3.9.2 The HS2 Heritage Memorandum also sets out how the historic environment (including heritage assets and their setting) will be addressed during design. The HS2 Environmental Memorandum sets out the approach to landscape and visual mitigation which takes account of the historic environment.
- 3.9.3 The arrangements for the management of archaeology during construction are not a matter for approval under Schedule 17.

Archaeological assets neighbouring the site

- 3.9.4 No assets pre-dating the 19th century have been identified within a 100m study area of the application site, with Rocque's map of 1756 indicating that the early post-medieval landscape had seen little urban development. However, by the early 19th century, the site and surrounding area underwent significant development, with the loss of the rural landscape and the construction of Euston Station, opened in 1837, to the south of the site, which truncated the surrounding landscape and subsequently dictated the development of the immediate townscape.
- 3.9.5 With the exception of Stalbridge House and Granby House, all Victorian and Edwardian development within the site was lost during the mid 20th century, in part due to WWII bomb damage, but primarily due to an extensive programme of post-war era slum clearance, and the subsequent championing of high-rise social housing. Subsurface archaeological remains of these structures and garden may still survive, outside of the cutting area, and if encountered, may have the potential to contribute towards an understanding of 19th century urban expansion.
- 3.9.6 Truncating the landscape, Euston Station to Parkway cutting (EUS036) retains much of its early 20th century rail character, having been extensively broadened during the latter half of the 19th century.³ The Decision Record – DB Cargo (DRN: 1EW02-CSJ-CO-NOT-S001) notes that the Euston Downside Carriage Shed, which formerly occupied much of the area was recorded and demolished in 2017-2018. The expansion and excavation of the cutting and subsequent railway engineering works removed all previous Georgian structures and associated

³ HS2 Phase One Environmental Statement Volume 1 Technical Appendices CFA1 Euston –Station and Approach (CH-001-001)

garden soils which may have contained any in situ archaeological features. The document further notes that there is low value to carrying out archaeological mitigation activities within the Euston Throat. The document indicates that further archaeological work is not required in this area. Nonetheless, some built heritage recording is ongoing.

Heritage

- 3.9.7 The HS2 Heritage Memorandum also sets out how the historic environment (including heritage assets and their setting) will be addressed during design. The HS2 Environmental Memorandum sets out the approach to landscape and visual mitigation which takes account of the historic environment.
- 3.9.8 The proposed works are confined to an area to the east of Regent's Park Conservation Area. The proposal does not physically impact any heritage asset identified in Section 2 apart from the railway cutting retaining wall between Euston station to Parkway (part of group asset EUS036). The works will impact the setting of the Regents Park Conservation Area – however, these impacts are not considered to be greater than those reported in the ES (as amended). Careful consideration of the historic environment in this area has informed the design development of the Wing Wall. In particular, it is designed to reflect the current setting of Park Village East (EUS012) to reduce impacts on the adjacent assets. The detailed design is carefully considering the historic environment to propose a final design without new or more adverse effects as those reported in the ES (as amended).

3.10 Environmental Management during Construction

- 3.10.1 The Environmental Memorandum, which forms part of the High-Speed Rail (London -West Midlands) Environmental Minimum Requirements, sets out the arrangements for the management of environmental issues during construction and the Code of Construction Practice (CoCP) sets out specific details and working practices that apply. The CoCP is supported by Local Environmental

Management Plans (LEMPs) which include specific measures by topic, relevant to each relevant local authority area.⁴

- 3.10.2 Environmental management arrangements during construction do not form part of this request for approval of Plans and Specifications under Schedule 17.

⁴ The LEMP relevant to the works subject to this Schedule 17 submission is P1S Local Environmental Management Plan - London Borough of Camden and can be found here: <https://www.gov.uk/government/publications/local-environmental-management-plans-for-hs2-phase-one>

4 Design Criteria and Rationale

This section provides an overview of the proposal's design principles, requirements and options. It also accounts for the site constraints.

4.1 Design Principles & Requirements

Design Principles

- 4.1.1 The design approach for the proposal has been developed through an iterative process of refinement, working with key stakeholders including the London Borough of Camden. Details of engagement are provided in Section 5.
- 4.1.2 The design team has also sought to explore the relationship with the site environs. In particular, consideration has been given to the visual context of both the railway heritage within the cutting and parapet walls within the locality including to Park Village East and Mornington Street Bridge.
- 4.1.3 This has resulted in the overall vision of “Duality”, which aims to demonstrate the long-established mixed-use typology present in the area. This concept guided the design approach, including materiality choice.

Design Requirements

- 4.1.4 The key requirements of the proposed parapet wall are as follows:
 - To visually integrate with the interfacing parapet walls to the west and east. This includes integration with the profile of the new parapet wall, its physical connections (via pilasters) to the interfacing parapet walls, and its relationship with the materiality of the surrounding context.
 - To meet relevant highways, railway and other relevant technical standards. This includes standards for containment as a rigid parapet loading (as opposed to high containment or H4a⁵) in accordance with BS EN 1991-2:2003. Several other details must be considered, such as an anti-climbing profile at the top as well as the interface with the existing parapet.
- 4.1.5 The key requirements of the proposed retaining wall are as follows:

⁵ See Footnote 2 for H4a definition. The proposed parapet is not required to be to H4a standard, since it is not above the railway.

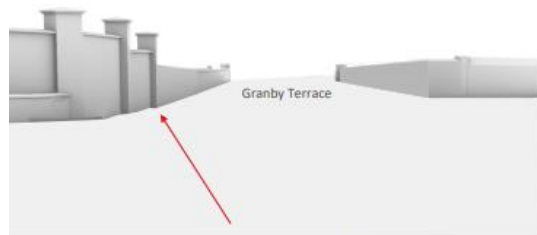
- To visually integrate with the WCML railway cutting wall and north face of above proposed parapet wall
- To act as a retaining structure to facilitate the level difference between the road level on the Granby Terrace Bridge and the level below

4.2 Design Options Considered

Geometry of pilasters

- 4.2.1 Both cylindrical and rectilinear geometry were considered for the pilasters. With agreement from the London Borough of Camden⁶, a rectilinear design will be taken forward as the preferred option as it appears more visually consistent with the form of pilasters to the existing Granby Terrace Bridge (see **Figure 8**).

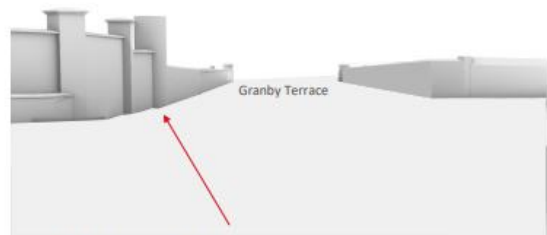
Option 1 – Rectilinear (preferred)



Rectilinear pilaster consistent with Granby Terrace Bridge context.



Option 2 – Cylindrical



Cylindrical pilaster inconsistent with Granby Terrace Bridge context.



Figure 8 Options considered for pilaster geometry

Materials palette

- 4.2.2 Considering the overall vision of “Duality” and the long-established mixed-use typology present in the area, it was noted that parapet walls within the locality broadly include historical blue engineering brick within the railway cutting and red brick on the streetside.
- 4.2.3 In line with this established palette, to achieve visual continuity, the parapet wall will be comprised of a blue brick to the north side (facing towards the cutting) and a red brick to the Granby Terrace streetside. In particular, this approach

⁶ Agreement reached at pre-application meeting with the London Borough of Camden on 7 October 2021

serves to integrate the parapet with the assets along Park Village East, including the Park Village East wall and the parapets to Mornington Street Bridge (see **Figure 9**).

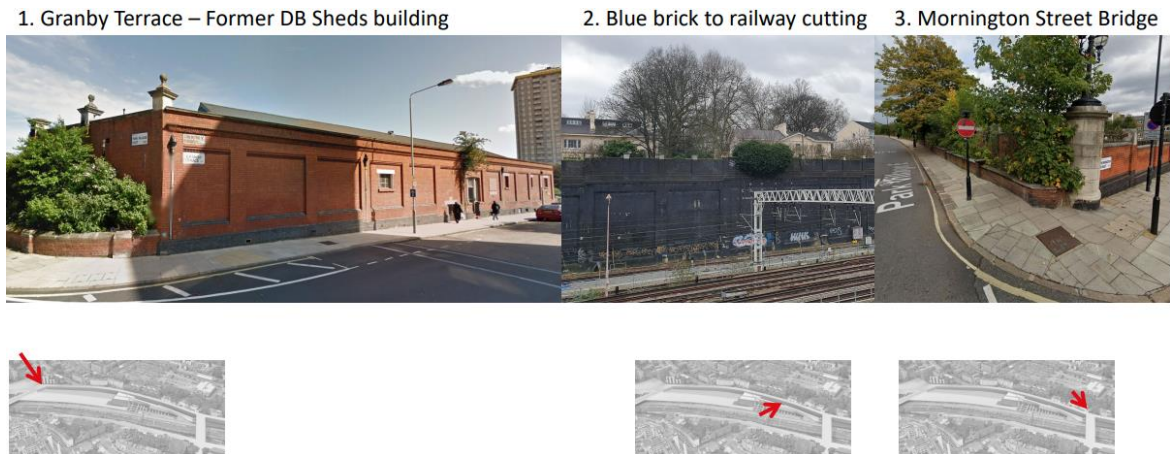


Figure 9 Visual context for development of parapet options

Materiality of pilaster

- 4.2.4 For the pilaster that would adjoin the Granby Terrace Bridge extension parapet to the east, three brick finish colour options have been considered (see **Figure 10**). These include:
- 1) Single colour pilaster, of either blue or red brick. The entire pilaster would be finished in brick.
 - 2) Colour transition pilaster, with red brick to the streetside and blue brick to the cutting side.
 - 3) Neutral colour pilaster of either exposed aggregate or rough stone.

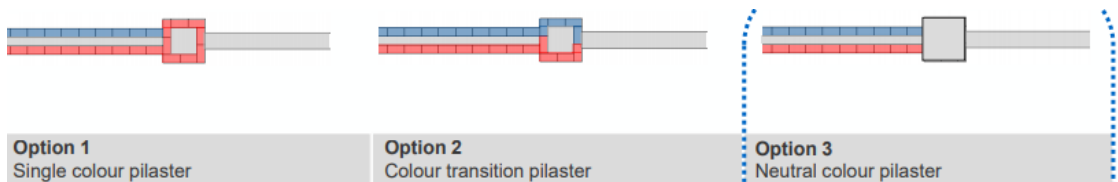


Figure 10 Options considered for materiality of pilaster

- 4.2.5 In reviewing these options, while Option 1 would create a visual mismatch on the cutting side, it is considered that the red pilaster would predominantly be viewed by pedestrians on the streetside. As such, the pilaster would be 'read' as part of the proposed wall, with red brick to the streetside, by footpath users. It would also visually complement the parapet walling along Park Village East.
- 4.2.6 On Option 2, the colour transition on the pilaster would create a complex brick coursing pattern on its eastern façade that would unduly attract attention.
- 4.2.7 Option 3 could provide a clear transition from the exposed aggregate parapet at the Granby Terrace Bridge extension into the proposed red brick parapet. However, it would visually contrast with the other pilasters along the proposed wall, and so would not provide a sense of continuity.
- 4.2.8 Through discussion with the London Borough of Camden, Option 1 will be taken forward with a red brick pilaster⁷, as illustrated in **Figure 11** and **Figure 12**.

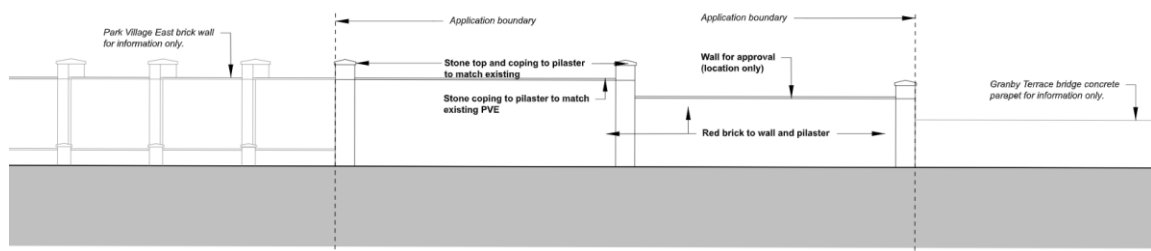


Figure 11 South Elevation (Drawing Reference: 1MC03-SCJ_SDH-AR-DGA-SS01_SL03-040013)

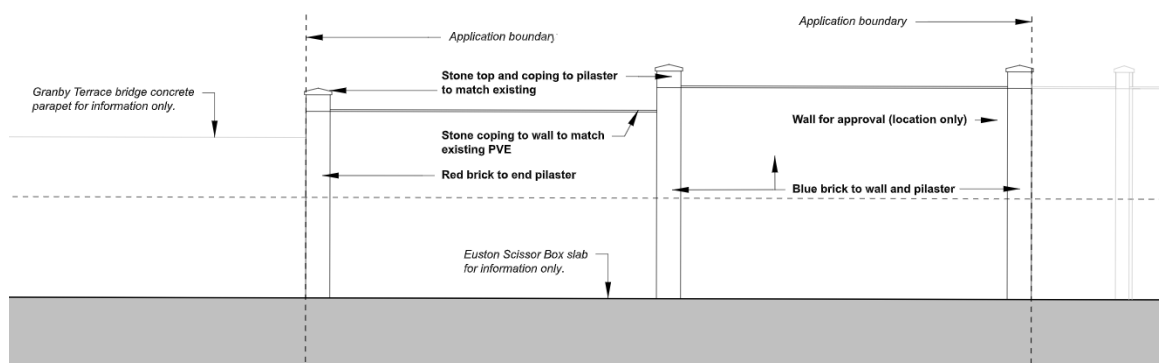


Figure 12 North Elevation (Drawing Reference: 1MC03-SCJ_SDH-AR-DGA-SS01_SL03-040013)

⁷ Agreement reached at pre-application meeting with the London Borough of Camden on 11 January 2022

Parapet and pilaster materiality and form

- 4.2.9 As requested by the London Borough of Camden⁸, the design team have considered how to create visual interest and depth to the proposed wall, given that the neighbouring historic parapet walls are distinct and architecturally interesting.
- 4.2.10 The parapet and pilaster materiality and form have been informed by the local vernacular, as shown in **Figure 13** below.



Figure 13 Parapet and pilaster materiality and form to street side (upper image) and rail side (lower image).

⁸ Discussion initiated at pre-application meeting with London Borough of Camden on 7 October 2021

Retaining wall

- 4.2.11 The above ground element of the retaining wall will have a brick finish. It will align with the blue brick colour of the north face of the parapet wall, in order to maintain the same design approach, materiality and palette.

4.3 Proposed Design

- 4.3.1 This section will provide a summary of the proposed design against the relevant HS2 Act grounds for approval. In accordance with Paragraph 3 of Schedule 17, the Local Planning Authority may only consider Schedule 17 applications against limited grounds. These grounds are listed in **Table 4**, by work for approval.

That the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits

- 4.3.2 This ground for approval applies to both the proposed retaining wall and the proposed parapet wall.
- 4.3.3 The proposal has been positioned on the application site, in order to serve the functions detailed at sections 4.1.4 and 4.1.5. It therefore cannot be positioned in an alternative location within HS2's permitted limits.

To preserve the local environment or amenity

- 4.3.4 This ground for approval applies to the proposed retaining wall only.
- 4.3.5 As detailed at section 4.2.11, the form and materiality of the proposed retaining wall will achieve a visual continuity with the railway cutting wall. It will therefore continue the narrative of "duality" within the Euston approaches.
- 4.3.6 In terms of preserving amenity, the position of the proposed retaining wall below street level means that it will be sufficiently distanced from neighbouring residential properties so as not to impact their access to light.

To prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area

- 4.3.7 This ground for approval applies to the proposed retaining wall only.
- 4.3.8 Given the nature of the proposed retaining wall, it will not impact road safety or the free flow of traffic in the local area.

To preserve a site of archaeological or historic interest or nature conservation value

- 4.3.9 This ground for approval applies to the proposed retaining wall only.
- 4.3.10 The only nearby archaeological asset has now been demolished and appropriately recorded, as highlighted at section 3.9.6. As such, there is no archaeological interest to consider on site or in the locality, and so the proposal will not impact any archaeological assets.
- 4.3.11 In terms of assets of historic interest, section 3.9.8 demonstrates how the design will successfully preserve the historic interest of neighbouring designated assets.
- 4.3.12 The application site is not known to impact any site of nature conservation value.

5 Pre-submission Consultation

5.1.1 Pre-submission consultation is summarised in **Table 5** below.

Consultee Name	Consultation Date	Method of Consultation / Attended by	Summary of Consultation Outcome
London Borough of Camden (LBC)	24/08/2021	Schedule 17 pre-application meeting between HS2 Ltd, SCS and LBC	<ul style="list-style-type: none">• Introductory meeting for the design of the proposed GTB parapet wall and retaining wall.
	07/10/2021	Schedule 17 pre-application meeting between HS2 Ltd, SCS and LBC	<ul style="list-style-type: none">• Reached agreement with LBC on the geometry of the pilaster interface with GTB extension parapet.• Reviewed the materiality options for the proposed parapet and pilaster interface.
	11/02/2022	Schedule 17 pre-application meeting between HS2 Ltd, SCS and LBC	<ul style="list-style-type: none">• Reached agreement with LBC on the materiality of the pilaster interface with GTB extension parapet.

Table 5: Pre-submission Consultation with LPA and Statutory Consultees

6 Construction Programme

- 6.1.1 The general construction methodology for this proposal is still under development and so details are not yet available.

7 Other Consents

- 7.1.1 There are currently no other known consents required for the GTB Wing Wall. Nonetheless, consent requirements may alter during design development and so further consents may be required.
- 7.1.2 Consents within the vicinity of the GTB Wingwall are summarised in **Table 6** and section 3.3.

Table 6: Other Consent Requirements

Consent	Works Requiring Consent	Status
Any other relevant Schedule 17 Plans and Specifications submissions for adjacent or associated works	Plans and Specifications – Granby Terrace Bridge Lighting	To be submitted
	Plans and Specifications – Euston Throat Retained Cut	Application 2019/6302/HS2 was approved by the inspector via the recent appeal decision (PINS ref: APP/ HS2/ 6).
	Plans and Specifications – Park Village East Berm Wall and Euston Scissor Cut	Application 2021/0126/HS2 was approved on 17 March 2021
	Plans and Specifications – Granby Terrace Bridge Superstructure	Application 2021/0356/HS2 was approved on 24 March 2021.
	Plans and Specifications – Euston Scissor Box Roof Slab	To be submitted
	Plans and Specifications – Euston Portal Headhouse	To be submitted
Any other relevant Schedule 17 Site Restoration submissions for adjacent or associated works	Site Restoration – Park Village East	To be submitted
Any other relevant Schedule 17 Bringing into Use submissions for adjacent or associated works	Bringing into use of the railway (Work Nos. 1/1 and 1/16)	To be submitted

Consent	Works Requiring Consent	Status
Schedule 4	Approval for Realignment of Highway – Granby Terrace Bridge	To be submitted
Schedule 33(1)	Temporary Ground Anchors to Park Village East Wall	Approved by LBC Highways on 14 April 2021 (unique ref ID: TA011-THW-LBC-SCS-5032)
	Approval for Bridge Construction Extension – Granby Terrace Bridge	To be submitted
Schedule 33(5)	Retaining Walls to Euston Scissor Cut	Confirmed exemption on 21 Sept 2020 by Environment Agency
Schedule 2	Drainage Scheme – Granby Terrace Bridge	To be submitted