

## 7.0 **Access and Inclusivity Statement**

# **Access and Inclusivity Statement**

## **Google Ground Floor**

**Access and Inclusivity  
Statement (AIS)**

**March 18<sup>th</sup> 2022**  
**Google Ground Floor, King's Cross, London**

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# 1 Introduction

This AIS Appendix (the AIS) has been prepared to supplement the Access and Inclusivity Statement of May 2017, which supported the revised Reserved Matters submission for a significant new commercial building on Development Zone A of the King's Cross Central ('KXC') development. The original report and this appendix responds to and seeks discharge of Condition 19 (Access Statement) of the KXC outline planning permission (ref. 2004/2307/P) granted in December 2006 for this building.

**Project Name:** Google Ground Floor, new retail façade, King's Cross,

**Client:** Google

**Project Managers:** Turner and Townsend

**Architects:** Heatherwick Studios  
Contact Person: Martin Greenhalf  
<martin.greenhalf@rewsprojects.com> and  
Anna Palmieri  
<anna.palmieri@turntown.co.uk>

**London Borough:** Camden

**Address:** Zone A King's Cross Central

**Type of scheme:** Retail only.

**Site Description:** The retail façade runs along the east side of King's Boulevard from

near King's Cross Station to the top by Goods Way.

## AIS Scope

The new retail façade runs for approximately 220m total in length and is made up of 21 different retail unit facades. The scope of this AIS excludes the South Anchor and the interiors of the retail units in general, however there is a note included on the design intentions of the mezzanine levels.

The retail facade intersects with the paved landscape, and whilst there is coordination where the two intersect, the landscape design is also excluded from this scope.

This AIS deals with the design of the facade: the effective management of the development is key to providing the highest levels of inclusion and user satisfaction. In addition, it is therefore always advisable to address some management matters in a suitable Management Plan (such as an Inclusive Access Management Plan or IAMP). This may consider, for example, Disability equality and awareness training for staff working in the development and area (tenants / ground staff).

## Tenant Fit-out

The retail units will be subject to fit-out later in the design development. The Tenant Fit-out Guidelines include a section on inclusion. This is a separate document to the AIS and includes advisory measures for tenants on complying with the

relevant inclusive design and access legislation; this should also be referred to.

## Fire Safety Report Scope:

This report does not cover fire safety in any detail, its status is advisory in this respect; it mentions the escape need(s) of disabled people but please refer to reports by others for formal details on fire safety / means of escape.

## General scheme description:

The site is 220m long and tapers in width from approximately 20m at the south end to approximately 60m at the north.

The 21 retail units include the larger North Anchor and a Market Hall which is also larger than other units, but each are generally individual although vary in size / depth.

The ground plane along King's Boulevard is approximately 7m higher in level than that of the southern portion of the site.

## Terms used

For the purpose of this AIS, the use of the term 'pedestrian' includes wheelchair users and mobility scooter users, and those with other types of wheeled mobility aids, those with push chairs / prams etc but not those with cycles nor cyclists nor e-scooter users.

Also, 'visual contrast' refers to where there is at least 30% difference between light reflectance values (of adjacent services).

## 2 Inclusive design standards and references

### Applicable standards, policies & legislation:

- Building Regulations
- The London Plan 2021
- Camden Planning Guidance 'Access for all' March 2019
- British Standards BS8300-1 and BS 8300-2.
- The Equality Act 2010

### Building Regulations

The inclusive design and access standards that apply to the scheme the Building Regulations Part M (Non-Dwellings) 2016 and Part K 2013 and unless stated the design refers to regulation compliance through / with the Approved Document(s) i.e. 'AD M' and 'AD K'.

### Local Plan Policy

The new London Plan 2017 (D5 Inclusive Design) requires the highest standard of inclusive design to be applied; it stipulates various requirements specifically and refers more generally in other areas to BS 8300 2018 Volumes 1 and 2, which has been used in the designing of the proposal. This is referred to simply as BS8300.

### Conservation area

The development lies within the Conservation Area of King's Cross Central. Where relevant, the proposals will seek appropriate consent from the Heritage Officer / Conservation Officer.

### Fire officer or fire safety risk assessor

Proposals have been made that may have an effect on fire safety or the evacuation strategy, and this has been co-ordinated with the appropriate fire safety consultant.

### Consultation

The aspirational brief that has guided the design development of the new ground plane of the Google KGX1 building is based on the conclusions of the detailed analysis of Argent St George's community consultation in Kings Cross for the site's regeneration. This has been used in addition to the issues raised at the King's Cross Design and Access Forum when the original design was presented in 2017. Therefore, the key design driver has been ensuring that the built environment is accessible and inclusive to all:

*"Welcoming diversity and creating a sense of belonging."*

*"A place truly for everyone that will serve Google and the wider community"*

### The Equality Act 2010

The AIS addresses and recognises the disability and access related requirements of the Equality Act 2010 (the Act).

The Equality Act 2010 ('the Act') combines and supersedes previous separate discrimination legislation (including the Disability Discrimination Act 1995 as amended ('the DDA') for England, Wales and Scotland. People are protected from discrimination and harassment based on 'protected characteristics'; victimising anyone as a result of action taken in connection with the Act is also unlawful.

There are nine different protected characteristics under the Act which have different levels of protection depending on the context (such as employment, provision of goods and services or the provision of education). This IDAS focusses on the protected characteristic of disability.

The types of discrimination that can arise in relation to disability are:

- direct disability discrimination
- indirect disability discrimination
- treating disabled people unfavourably because of something arising in consequence of their disability without justification
- a failure to make Reasonable Adjustments for disabled people ('the RA duty').

The Act also provides protection for people who are treated less favourably because of their relationship with a disabled person (such as a carer) or for people treated less favourably because they are mistakenly believed to be disabled. A disabled person can always be treated more favourably than a non-disabled person.

The RA duty works in different ways depending on who requires the reasonable adjustment to be made, for example whether for a visitor, or an employee.

#### Duties towards the public

As service providers, the public realm, approach and retail and must not have any design or part of it that could discriminate against disabled people.

The Reasonable Adjustment duty is an anticipatory and ongoing duty for service providers which requires them to take reasonable steps to:

- avoid substantial disadvantage (defined as more than minor or trivial) where a provision, criterion or

practice puts disabled people at a substantial disadvantage compared to non-disabled people;

- remove or alter a physical feature or provide a reasonable means of avoiding such a feature where it puts disabled students at a substantial disadvantage compared to non-disabled students;
- provide auxiliary aids and services (such as induction loops, infrared broadcast systems, or audio-visual fire alarms) for disabled students where to fail to do so would put them at a substantial disadvantage compared to non-disabled students.

If a policy or auxiliary aid relates to the provision of information, then a reasonable step will include ensuring the information is in an accessible format. Disabled customers, visitors or staff cannot be charged for the cost of the reasonable adjustment.

#### Further References

Further guidance and references used in this AIS are listed below

- The Sign Design Guide – A Guide to Inclusive Signage (2004), by P. Barker, J. Fraser; JMU Access Partnership & Sign Design Society
- DETR Guidance on the Use of Tactile Paving Surfaces, Department for the Environment, Transport and the Regions, 1998.
- Inclusive mobility: DfT, 2005 (superseded 2022)  
<https://www.gov.uk/government/publications/inclusive-mobility>
- Camden Streetscape Design Manual: Street Furniture  
<https://www.camden.gov.uk/ccm/content/transport-and-streets/transport-strategies/streetscape-design-manual/streetscape-design-manual-street-furniture/>

## 3 Arrival

### 3.1 Public Transport

King's Cross public transport systems include the adjacent railway stations King's Cross and St Pancras, King's Cross International station and the London Underground. There are also multiple bus stops nearby, including within 250m of the south end of King's Boulevard on Euston Road, and further bus stops within the same distance from the north end of King's Boulevard on York Way (via Goods Way).

All parts to the tube station / railway stations and all London buses are wheelchair accessible.

There are Blue Badge bays (on the private land) outside Waitrose 200m from the north end of King's Boulevard, and approximately 140m from the south end on Pancras Road there are further Blue Badge bays (outside You Tube offices).

There is a large taxi rank and various set-down / pick-up points including on Pancras Road, near to Battle Bridge Place which is at the southern end (or foot) of King's Boulevard which slopes up travelling northwards.

Informal set-down can take place at either end of King's Boulevard on Pancras Road or on Goods Way.

### 3.2 Site Access

#### General Accessibility

King's Boulevard provides a key retail pedestrian 'high street' area that is also a link between Granary Square / the King's Cross Estate to the north, down to St Pancras Square, Battle Bridge Place and King's Cross public transport termini in the south. It is open to cyclists and e-scooter users.

The street has removable bollards at both ends. It comprises kerbed pavements with a central roadway in the middle (with controlled access for service vehicles, no other vehicle access is allowed). Dropped kerbs are provided. However, it is understood that the road will be one surface in the future, with no kerbs and no vehicle access (except services and emergency vehicles). An informal cycle lane lies centrally in the King's Boulevard, running North-South, delineated with 800mm widths of tactile corduroy on both sides. This is outside of the development area however of relevance to anticipating visitors with various access needs that will be visiting the retail units.

## 4 Landscape

### 4.1 Introduction

King's Boulevard falls from the north to the south by around 7m along the development boundary as stated.

On the west side (opposite the development) the gradient along King's Boulevard towards the retail is slightly steeper (in parts) than guidance taken from regulations, for example 1:15 to 1:18 (approximately) outside the London Underground entry point. This is outside of the development boundary but of significance in gaining and understanding of the challenges.

For the Google Ground Floor (which, as stated, is on the east side of King's Boulevard), if it were possible, a regular fall gradient of around 1:32 would be provided; however, since the retail façade has level approaches and landings to entrances (as well as level fire escape routes / level landings outside), all of which are required by regulation, near the building the gradients will naturally have to exceed (be steeper than) this average.

The retail façade design for the development along the unit frontage addresses the rise in the landscape along King's Boulevard as well as the slight rise towards the buildings.

There are no steps or ramps across this part of site around the King's Boulevard (other than leading down to the canal north of Good's Way and on the west side of St Pancras Square) and this is something that is welcome in terms of inclusive design and does not change in this proposal.

- For the record, some people use the lifts within the Underground Station to avoid the slope on King's Boulevard, by entering the lift within the station to the south, accessing the step-free route internally to another lift, which brings them to the surface higher up along King's Boulevard.
- There is also a public lift within a restaurant building off Good's Way by Granary Square which is used by the public to provide access to the canal.

This is relevant because with knowledge spreading and as more and more people visit the area, these features will contribute to the incentive for disabled people to visit.

There is limited access to any dog spending area nearby and this was considered but it has not been implemented as there is very limited space and it would not be appropriate for the King's Cross retail façade, being bounded at a close proximity to the building where it would not be appropriate as a location. There are some

(limited) green areas with trees / planting around St Pancras Square and Granary Square both of which are within 100m of the north end of the King's Boulevard. More extensive green and grassy areas are Camley Street Natural Park, Gasholder Park and Lewis Cubitt Park which are 250m – 350m from the north end of the Boulevard.

### 4.2 External circulation

The development area pavement / path does not include the South Anchor but starts along the line of front-facing retail units north of this. It continues north along King's Boulevard and wraps around the North Anchor eastward to a small extent and provides two entrances along Good's Way.

The west boundary for the scheme is for most purposes a straight line parallel to the King's Boulevard and façade; it is mostly 1m to 2m from the façade / retail entrance doors, but this varies for some units which are set back at different distances, the extreme being at the Market Hall where the entrance doors are some 6-7m from the boundary line.

The retail frontages provide a varied arrangement of entrances and window displays which will be subject to visual designs (including retail displays and lighting) by tenants.





In terms of surface materials, beginning at a long, straight north-south edge there are sandstone paving setts, which give a slightly different tactile texture. This is intentionally an unremarkable change in comparison to changes experienced underfoot when encountering one of the standard tactile paving surfaces used (see DETR Guidance on the Use of Tactile Paving) so that it does not get mistaken for any of these surfaces. It is located as you near the retail facade (where they are 100mm x 100mm rather than 600 - 900mm x 600mm further out). This design mirrors with the west side of King's Boulevard giving consistency and generally assisting with the 'legibility' of the space including in particular for those with vision impairment / other disability but to also increase legibility for everyone.

The cross-fall / gradients have been worked on extensively to achieve an optimal design for inclusion within the constraints imposed by the existing conditions.

Suitable paths from the North-South route leading to the entrances in an East-West direction have also been developed, in consideration of the balance necessary to achieve a suitable level, appropriate cross-falls and path widths as well as a level entrances / landings to each entrance / escape route.

Inclusive design and accessibility matters have been addressed in the design to ensure that levels / steepness, surface types (non-slip, good evenness, small gaps / joints for example) and drainage are suitably accessible. See 5.3 Approaches.

#### Lighting

Lighting will be provided by a neutral - warm white, 3000K, which will ensure the effectiveness of the visual contrasting provided for all the key features is maximised without harsh lighting that is particularly unhelpful to some people with neurodivergent differences.

A discrete downlighting to the entrance doors highlights features to facilitate locating each doorway; this also boosts lux levels for facial recognition should this be used in the future.

There is to be some uplighting used for the Market Hall. The specification for these uplights is glare free with a half moon glare shield and extremely compact with a narrow beam (10 degrees). The fixtures do not

allow views into the light source from any angle and they avoid producing glare.

## 5 Retail units

### 5.1 General design

The 21 units of the facade comprise a variety of sizes and a variety of activities is anticipated to be associated with them. Two Google lobbies break the sequence of frames, namely the South lobby between units 16 and 17, and the Events Lobby between units 2 and 3.

- The individual unit widths vary from 6 to 11m:
- Clear heights vary from 4.8m to 8m. The unit depths also vary from 3.5m to 27m

Each unit has its own entrance or entrances facing the King's Boulevard.

Every unit has a consistent design of frame surrounding it; this defines each unit, thus providing a prominent feature clearly visible from near or far along the King's Boulevard. See also 5.2 Frames.

The unit façade / glazing / entrances are set back to various depths from an imaginary straight façade line drawn parallel to King's Boulevard; also, the depth of inset from the front of the frames to the façade glazing / entrance varies; together these features give some unique identity to each unit; this will assist with orientation and way finding.

In general, the designs are otherwise consistent in materials / appearances and

hence in providing good legibility across all units, with some small and minor exceptions (that do not adversely affect legibility) indicated later in this AIS.

The key design features of all the unit facades are consistent overall and described in sections 5.2 and 5.3. Following this, in 5.4, some unique areas and anomalies are discussed.

### 5.2 Frames

The defining frames around the retail units are 400mm thick in elevation and constructed in light-coloured wood which will assist people to identify the different shopfronts; there is visual contrast provided by the light colour frame(s) against the darker anodised aluminium caps providing the surroundings. Tenants will not be able to adjust the colour of these at will, but this will be subject to a review by the tenant; this way, the visual contrast can be ensured to remain as has been included within the Tenant Guidelines for inclusive design. This is important because:

- The frames add clarity to the extent or size / width of each individual retail unit within the wider facade.
- The frames will assist people to identify the retail unit they wish to visit, as well as bring them to within a few metres of the entrance, which in turn has its own consistent and identifiable features.

The frames next to lobbies that provide access to the building above / behind the retail unit façade (which is not within the scope of this AIS) have a tilted leg, whereas others do not; this will add to the legibility, indicating where a line of units stops / starts.

Legibility and wayfinding are important design qualities useful to everyone, in particular those with visual impairment and those with neurodivergent differences.

At the foot of the frames on both sides of a unit there is a precast pigmented concrete skirting on the frame. This is in a darker foot / band than the frame itself. It varies in height in accordance with the external levels; this will give an indication of the level change and help some people, including those with balance issues, to navigate more comfortably to and within the unit.

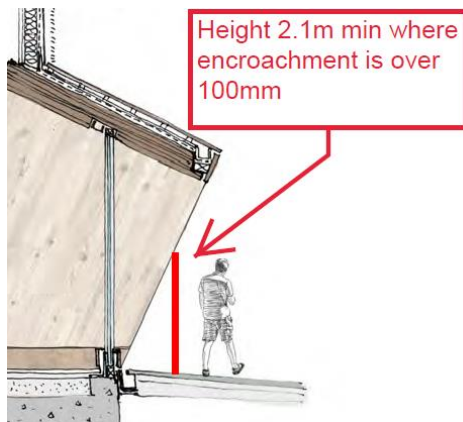
While the frames themselves are consistent in design in terms of thickness, materials used and hence appearances, they slope at varied angles with the top of the frame leaning towards the King's Boulevard.

The tilts are limited to 3 angles; 60, 70 and 80 degrees.

The differing angles of the sloping frames of the separate retail units will add to the overall legibility; no two adjacent frames have the same angle of tilt. However, as there would be a risk that some angled

frames leaning over the ground outside could be a headroom collision hazard to pedestrians if left untreated, certain treatments and mitigations have been made.

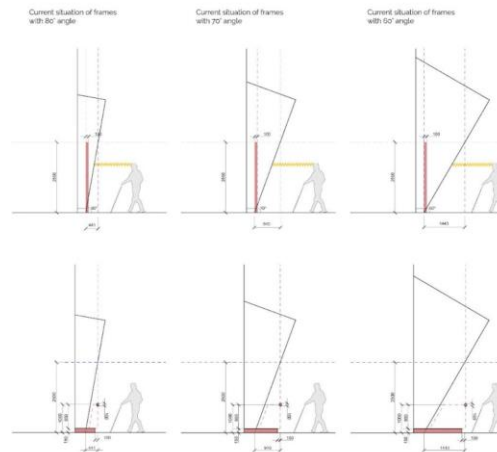
- The overhanging parts of the frames will not lean over any space where it may be a headroom issue; this could otherwise be a hazard, in particular to vision impaired people.
- A horizontal encroachment into the space of over 100mm up to ahead height of 2.1m is in alignment with the maximum tolerance expressed for similar hazards described in the AD M; this is the reference standard adopted for the strategy.



A strategy using 1m minimum height visually contrasting planting will protect any areas of low headroom. The planter with

planting will achieve the 1m min requirement.

These will be placed directly underneath where the frame is lower than 2.1m in such a way as to prevent access to any area of low headroom through providing a physical barrier. Tenants will not be able to adjust the colour of these planters.



*A comprehensive study has been undertaken and a site-wide strategy proposed to locate planters under leaning frames to protect any areas of low (below 2.1m) in terms of headroom*



*Planters (green circles) protect areas under frames where headroom is low*

### 5.3 Unit Approaches

Approaches and levels / path widths have been a challenge in terms of providing for inclusion: as previously mentioned there are existing gradients in both N-S and E-W directions.

The design approach has been to achieve consistent and compliant gradients, prioritising a gentle slope on the approach with a level landing and entrance (no cross-fall as well as a level threshold at the door), but also aiming to achieve slopes less steep than 1:20 (ie gentle slopes) universally as far as can be achieved.

Geometrically it was not possible to achieve gentle slopes everywhere N-S while providing suitable gradients / cross-fall to the approach paths to every entrance.

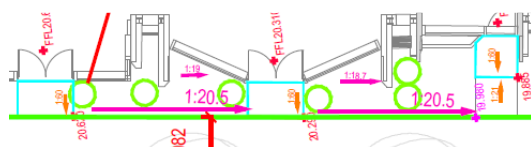
This was almost fully achieved in the design, with the following provisions made successfully.

- Although the existing topology compels steeper gradients in the

south in an E-W direction, gradients are 1:21 at most.

- All approach paths are at least 1200mm wide.
- In the spaces to the sides of the approach paths in front of the retail units the gradients have been adjusted to less steep than 1:20 in all but 3 places.

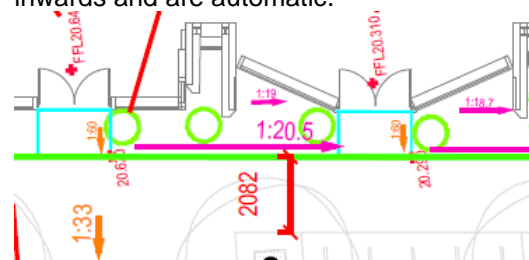
In these three instances however, the areas 1:20 or steeper are limited to the space up against the façade; these areas are also protected by large visually contrasting planters at least 1m tall designed to 'book-end' them. Tenants will not be able to adjust the colour of these planters. The planters will hence help to ensure that no-one could inadvertently encounter a steeper slope by walking along the façade N-S, but would be diverted away from the façade to avoid the planter (and hence would avoid the area).



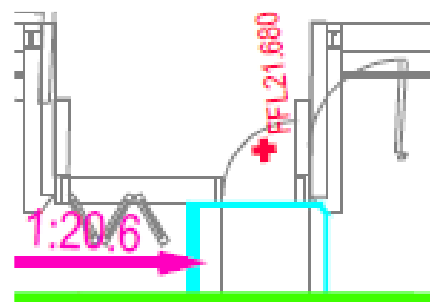
*Planters protecting the area immediately in front of the façade where the gradient is 1:20 or steeper; having to walk around the planter(s) will push the route further out from the façade and into a gentle sloped area.*

Landings to the retail entrances are at least 1500x1500mm in size, apart from in three locations; where the entrance is located in a slightly narrower portion of the street, nearing trees or street furniture, the lengths of the landings are at least 1200mm and the width is 2000mm or more to ensure there is more than adequate space on the landing for a wheelchair user to be positioned when opening the door.

This deemed as acceptable because of the width provided, but also as 1200mm is the minimum length for a landing on a ramp in Part M, and because the doors open inwards and are automatic.



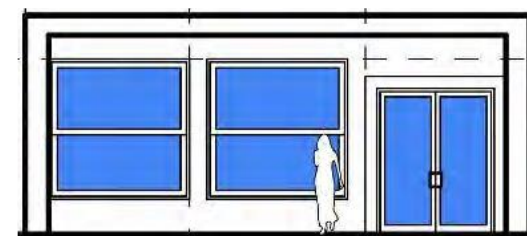
*Two images showing the areas with 1200mm long x 2000mm wide landings*



## 5.4 Entrances

Entrances are all automatic and inward opening as stated.

In the event of an emergency, they will default to the open position.



*Indicative image showing how the design achieves enhanced visibility of doors against the rest of the façade*

The entrances are identifiable in the façade by the design of the shop frontage, where the design of the door frames have a different appearance to the remainder of the facade, reaching the ground without horizontal elements / solid infills at the lower height.

All entrances will provide 1000mm ecw and will be power assisted.

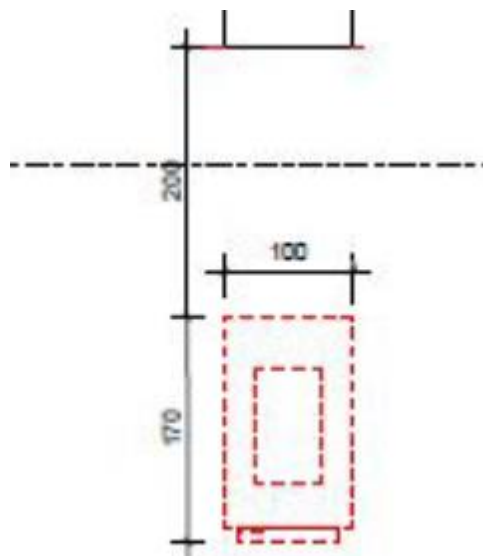
In addition, each door has a push plate which is highlighted visually by size / in area and being provided with visual contrast against the anthracite grey RAL 7016 door edge so they give legibility to the door location and its opening edge(s).

There will be post-mounted automatic door controls: these will be located on 'totems' very near to the façade (a 200mm gap is allowed for maintenance / servicing) rather than being a free-standing post in isolation, and so this is not deemed to be a hazard.

Where permanent planters are integral to the façade to the sides of entrances, the totem is located 200mm in front of the planter, so that it is set well out from any internal corner to facilitate reaching.

For others, the totem gap to the façade combined with the thickness of the totem will mean the control is always at least 370mm and so also could not be in the corner of a space.

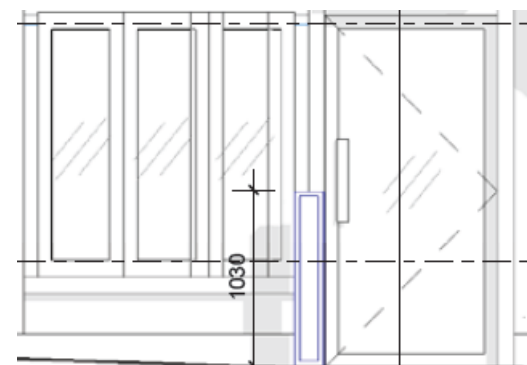
The designs of the totems are consistent and facilitate use with a hand or foot to operate them.



*Totem positions against the façade are consistent and allow ease of access*

The totem and automatic door control plates fitted on them are visually contrasting and bear a large universal symbol of access (a wheelchair).

The controls pad extends from 140mm to 1030mm above ground level and is approximately 170mm in width as shown; it is operable with a closed fist as required in regulations.



The push plate controls are designed to be suitable for use at foot / footrest height which will facilitate independent use by a wider group of wheelchair users, including some electric wheelchair users, as well as anyone with limited upper body strength / reach; this will also be of assistance to others with upper limb / manual dexterity conditions; it will help prevent the spread of infections (eg Covid).

Using push / kick plates will avoid unnecessary opening / inconvenience from sensor-operated doors or from manual doors on closers; sensor-operation was not considered suitable because of the expected number of people near to the unit not wanting to enter (e.g. those window shopping, collecting in groups around seating, or in a queue to buy a drink through a servery window).

Tenants will need to provide automatic door signage as detailed in Part K / Part M, as well as manifestations on any unmarked glazing as required by Part M / Part K and



as detailed in the relevant tenant fit-out guidelines.

The entrance / façade is not mirrored glazing; this can be confusing to people, especially those with vision impairment and to those with autism / others with neurodivergent differences.

Each entrance to every unit provides an effective clear width (ECW) of at least 1000mm, which meets building regulations.

Level thresholds are provided.

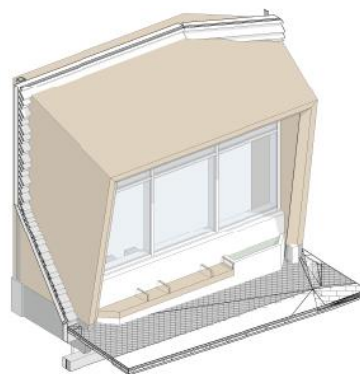
Level landings on both sides of all entrance doors are in general at least 1.5m x 1.5m clear of door swings.

Finger trap guards will be provided – this will make the proposal safer and promote more independent use including for children.

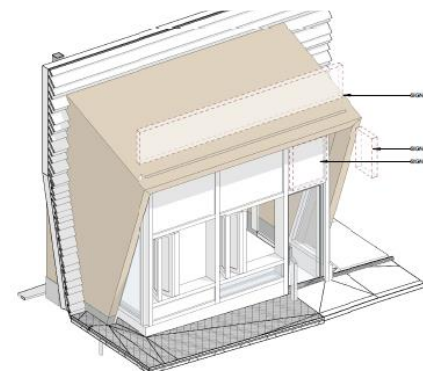
Drainage (where provided) by the entrance threshold is always achieved with grille / slot / gap spacings no greater than 13mm (6-10mm is provided in general at most); this reduces the risk of narrow heels being caught, allows small wheelchair castors to traverse smoothly and avoids walking sticks / canes getting snagged.

Absorbent matting is required in regulation to assist keeping internal surfaces free from wet which can cause slips. Coir or similarly vertically compressed fibres should not be used as these are difficult for wheelchair users. Guidance on this has been provided in the tenant fit-out guidelines.

Incidental seating is provided as part of some facades. This offers a variety of heights in different locations (eg 350mm to 554mm in the Market Hall entrance) and some throughout the different unit facades have armrests.



Where facades open to provide a bar / counter arrangement, the most part of the counter provides an accessible height, between 760mm and 850mm above the external ground level. This will facilitate a reach for people of short stature as well as for wheelchair users; it also assists with communication between staff serving and the customer outside, allowing eg better proximity for hearing and easier views for recognising faces / lipreading.



## 5.5 Anomalies

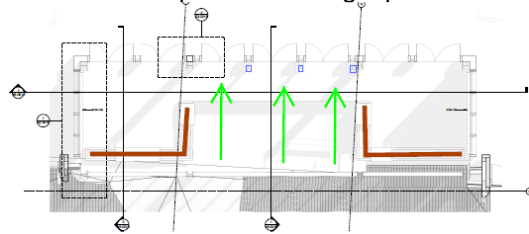
Some parts of the design are unique and have differing inclusive design requirements; this section introduces these parts and explains how the relevant standards and best practice measures are achieved.

The larger (combined) units include the North Anchor and the Market Hall respectively and have multiple entrances / entrance doors; the North Anchor has entrances on both King's Boulevard and facing Goods Way; the main entrance to the unit is located on the corner of the building, taking advantage of sightlines from Granary Square, Goods Way and King's Boulevard. The entrances are not treated any differently in principle to the entrances of the other units and for example have a minimum effective clear width (ecw) of 1000mm.

Market Hall;

The entrance to the Market Hall has a single step to each side: this is made safe from circulation by planters added.

The main through route is central and three totems with automatic door openers are provided, each to the right of separate pairs of doors that open with a single push.

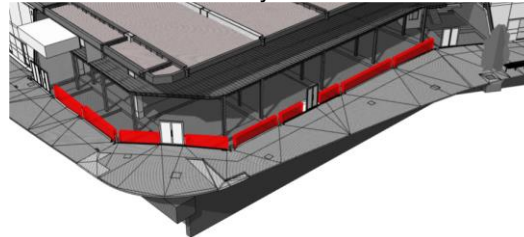


*The Market Hall with planters, access route and totems in front across the width of the access to facilitate movement*

The Market Hall entrance is particularly wide and in order to accommodate the slopes outside, there is a central level access route beyond the garage doors leading to the inner entrance doors; there are two areas (one each side) of this route that are approximately one step height different to the level access route; these two areas are accessed for everyone from the side of the Market Hall, via level access; the step encountered on all other sides is closed off and made safe with planters 1m tall and visually contrasting. Tenants will not be able to adjust the colour of these planters.

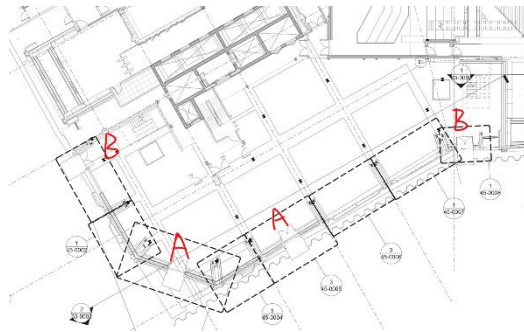
The North Anchor:

The North Anchor has Hostile Vehicle Mitigations made. These designs are outside of the scope of this report; for information they do not impact on the inclusive design in general, importantly requiring maximum widths / separation for security rather than minima, and these dimensions are in excess of the minimum widths for accessibility.



*North Anchor Entrances and HVMs*

In the North Anchor there are 4 doors: 2 doors at level A (by the corner with Goods Way) and 2 doors at level B (at opposite ends and adjacent sides of the North Anchor).



Internal level changes are to be designed by the retail unit fit-out team, but it is recommended that a gentle slope (preferred) or a ramp should be provided between levels to guarantee the most inclusive design experience. See Tenant fit-out guidelines for design guidance.

Up-and-over 'garage' type doors are provided in the Market Hall gates (these are extra doors external to the main entrance doors). These will only open once in the morning and will then be closed at night, moved at any time only under supervised operation; they also have an emergency brake mechanism.

## 5.6 Internal level changes

Most retail units are set over a single ground floor level and this presents the simplest arrangement for access and egress. For the two adjoined units to the south of the Market Hall entrance, there is a level change of 210mm where the units meet inside that will be ramped; this was considered the best option in terms of accessibility and space; otherwise long doubling-back ramps would have been required to reach the back of house corridor, or platform lifts would have been needed, or one unit would need to not have an entrance / access to the BoH corridor, and in summary all such and similar options were not considered as suitable in terms of inclusive design. Instead, the levels are to be reconciled with a Part M compliant ramp including handrails

and a visually contrasting surface in accordance with regulations. See Tenant fit-out guidelines.

## 5.7 Signage

Illuminated signage is being allowed for in the design, with power being installed for the purposes of potentially lighting it.

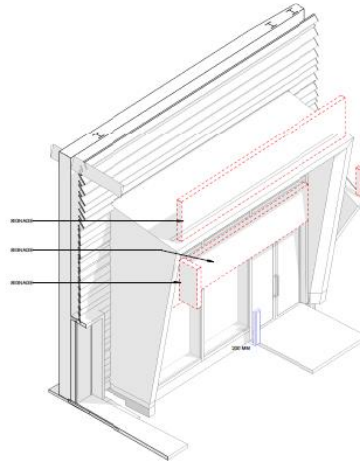
The blade signage part of the strategy will facilitate way finding / orientation along the façade from further afield than front-facing signage; all types will facilitate way finding and navigation through being adequate in size and (hence) visibility.

Units each have a combination of sign types, facilitating a clear view from any direction.

No signage projecting over 100mm from a wall has headroom of less than 2.1m below it.

It is assumed that tenants will have their own branding, logo or signage and it is expected that the architects will design / provide a palette of options that can be picked up by tenants.

Signage allows tenants to use adequate heights for symbols / letters to be clearly legible from distances expected along King's Boulevard. See Tenant Fit-Out Guidelines.





# Access Statement

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## Zone A Building

King's Cross Central General Partner Ltd  
and Google UK Limited

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May 2017

**King's Cross**





all clear designs  
3 devonshire mews  
london w4 2ha

t: 020 8400 5093  
f: 020 8400 5094

mail@allclear.co.uk  
www.allclear.co.uk

**King's Cross Central:**

**Zone A Building**

**Access & Inclusivity Statement**

**Prepared by All Clear Designs Ltd**

**May 2017**

# Introduction

This Statement has been prepared to support the revised Reserved Matters submission for a significant new commercial building on Development Zone A of the King's Cross Central ('KXC') development. Specifically, the document responds to, and seeks discharge of in relation to the proposed building, Condition 19 (Access Statement) of the KXC outline planning permission (ref. 2004/2307/P) granted in December 2006. It also addresses the requirements of Section V of the associated Section 106 Agreement on Access and Inclusivity.

The Zone A Building occupies a prominent location in the southern half of the KXC development site. It is wedged between King's Cross Station and its railway track, to the east, and the King's Boulevard, to the west. The German Gym, the Stanley Building, and Buildings B2, B4 and B6, all of which have been approved and are now occupied, line the western side of the King's Boulevard. The Eastern Goods Yard ('EGY'), which includes the Granary Building and Granary Square, lies to the north of the Regent's Canal and Goods Way.

The proposed Zone A Building comprises a single, predominantly office building of 7 to 11 storeys, above retail uses at ground floor, and two levels of basement. The office use will include ancillary uses for 4,500 members of staff and their visitors, including cafes, gym and pool facilities, a covered multi-use games area (MUGA), an Events Centre and staff training facilities. At roof level, landscaped terraces and a walking 'Trim Track' will provide outdoor amenity and recreation space for office workers and their visitors. The existing access ramp will provide service access to the basement areas, as well as to the existing Shared Service Yard.

The Zone A Building will primarily provide accommodation for a UK Headquarters for Google UK Limited ('Google'), with a mix of different retailers at ground floor. The project team has worked closely with the King's Cross Central General Partner Limited ('KCCGPL') and the prospective occupier, Google, to create a highly accessible and inclusive environment. The submission site comprises the building and some landscaping to the north, west and south of the building, which covers parts of Goods Way, the King's Boulevard and Battle Bridge Place, respectively.

## 1.1 Executive Summary of Inclusivity

- The design principles are extremely coherent and aim to produce a building that has a high degree of architectural legibility and therefore ease of use.
- The organisation of major floor plates and minor floor plates provides a wide range of features that will assist users in their navigation of what could otherwise be a complex building.
- The ground plane provides a great opportunity for the public to interact with the building, creating permeability and a transition between the private spaces and the public realm.

## 1.2 Context

Documents which relate to access and inclusivity within KXC are developed under a document hierarchy as follows:

1. King's Cross Central Access and Inclusivity Strategy (Sept 2005)	A scene setting document establishing the principles and containing the master plan philosophy and over arching strategies, as referred to in the S106 Agreement.
2. Access Statement (this document)	A detailed document containing expanded descriptions explaining how the strategy has been implemented in the individual schemes.
3. Building Regulations Access Statement	A document accompanying the building regulations application for each of the areas being applied for individually. This document will contain a further level of detailed description to accompany the increased level of detail of the Building Regulations submission.

## 1.3 Scope

This Access Statement contains an explanation of measures that will be incorporated within the proposals for Zone A to facilitate access and use by all people including disabled people, and indicates how the design meets the required design standards, good practice guidance and Building Regulations access requirements.

The statement takes into account the needs of people with mobility impairments including wheelchair users and those with sensory and cognitive impairments. However, it is recognised that the issues considered in this report will affect the convenience of access for all occupants, not just disabled people.

This Access Statement is based on the strategies set out in the King's Cross Central Access and Inclusivity Strategy (September 2005) and addresses the items set out in Appendix D of that document, including:

- Explanation of policy and approach to access;
- Sources of advice and guidance on accessibility;
- Details of consultations undertaken or planned;
- Details of access consultant involvement;
- Explanation of specific issues affecting accessibility and details of access solutions adopted; and
- Details of potential management policies and procedures to be adopted to enhance and maintain accessibility.

Areas where technical or other constraints have prevented or constrained the application of the principles set out in the above strategy are highlighted as appropriate.

The areas covered in the building include entrances, horizontal and vertical circulation, facilities and sanitary accommodation. At this stage, the statement does not cover operational aspects in detail, but it identifies and comments on areas where management procedures are likely to be required to ensure good accessibility.

Public realm and landscaping is considered in so far as it relates to the proposed building and related works, for example, at interfaces between the street and entrances/thresholds, and the design of the landscaped terraces and swimming pool at roof level.

This Access Statement is based on, and should be read in conjunction with, the submitted scheme drawings and information provided by BDP and Gillespies Architects.

## **1.4 Role of Access Consultant**

The access consultant has been actively involved in the preparation of the submitted proposals. The role of the access consultant is to advise the design team and appraise elements of the design at the relevant stages of the design process to ensure that the best possible level of access is achieved and that the proposals meet relevant legislation and S106 Agreement requirements and apply recognised good practice guidance. The consultant has provided recommendations about measures that can be incorporated within the scheme to facilitate access and use by disabled people.

The access consultancy services have ensured the integration of accessibility measures into the building whilst also maintaining the overall concept of the design.

## **1.5 Criteria for Assessment and Design Guidance References**

The following documents and guidance are used for assessment:

- Argent (King's Cross) Limited, King's Cross Central Access and Inclusivity Strategy, September 2005
- GLA, Accessible London: Achieving an Inclusive Environment, April 2004;
- Building Regulations Part K, Approved Document K, 2013 edition
- Building Regulations Part M, Approved Document M, 2015 edition
- British Standard BS8300:2010A Design of buildings and their approaches to meet the needs of disabled people – Code of Practice;
- British Standard BS9999:2008 Code of practice for fire safety in the design, management and use of buildings
- DETR, Parking for Disabled People, Traffic Advisory Leaflet 5/95, 1995
- Other currently recognised good practice design guidance including *Sign Design Guide*, (SDS, 2000); *Guidance on the use of Tactile Paving* (UK, DETR), *Inclusive Mobility* (DoT); *Designing for Accessibility* (CAE, 2004), *The Access Manual*, (Blackwell, 2006) and *Manual for Streets* (DfT and DCLG 2007).

In using these documents and this guidance, the design team and access consultant have observed and had regard to reasonable functional and financial practicalities; and taken into account the nature of the use of building. Wherever possible, the design team have gone beyond the minimum requirements of Part M (Building Regulations) and the guidance

provided in the Approved Document M. This will assist the occupier(s) in meeting its/their duties under the Equality Act 2010.

## **1.6 Factors Contributing to Accessibility**

This Access Statement considers accessibility at a relatively early stage in the design (RIBA Stage 3). Detailed design issues such as fixtures, fittings, lighting, communication systems, management and other issues which contribute to the accessibility of the services and facilities provided will need to be considered in the future (RIBA Stage 4).

The individual needs of visitors cannot always be known in advance, thus it is acknowledged that further adjustments to building management policy or procedure or to physical features may become necessary. However, it is the intention of the design team to ensure that the need for further physical alterations and the inevitable cost implication of this is reduced to a minimum.

## **1.7 Consultation**

During the design development process the overall scheme and specific elements of the scheme have been discussed with London Borough of Camden Building Control Department, including their Access Officer.

The proposals were also presented to and discussed with the King's Cross Design and Access Forum on the 5<sup>th</sup> April 2017. These meetings have been developed as one method of ensuring that there is user input into the design development process.

No specific access issues were raised at the Forum but the following elements were confirmed:

- There are 4 accessible parking bays in the basement (there is no other car parking for the site);
- There will be sufficient accessible WCs for the office workers and for the users of the Events Centre, but these are not generally available to the public, as public WCs will be provided elsewhere in the KXC site (as part of the Coal Drops Yard, which is currently under construction, and Pavilion H, which will be brought forward at a later date);
- Where the roof has ramps to allow disabled people (office workers and visitors – generally not publicly accessible) to access the main levels they will have a maximum gradient of 1:15. As they are located in a landscaped element, and the distances are large (but the space on the roof finite) they will have flights that rise 1000mm between resting platforms. In this location and usage this is regarded as acceptable;
- A query was raised about making the glazed elements and entrances of the shopfronts visible to people with a visual disability. The response was that glazing manifestation and tactile paving would be used to indicate these areas, similar to that used elsewhere in the KXC site.

## **2.0 The Zone A Building and associated landscaping**

### **2.1 Overview**

Zone A is situated in the southern part of the KXC development site. It is a long, wedge-shaped site which runs broadly parallel to the railway lines leading into King's Cross Station to the east and King's Boulevard to the west. The northern and southern ends of the proposed Zone A Building are bounded by Goods Way/the Regent's Canal and Battle Bridge Place respectively.

The proposed building is conceived as a continuous 'terrace' running the full length of Zone A. The building will be principally office use, providing Google's new UK headquarters, with retail uses at ground floor, interspersed by entrances to the offices and Events Centre. The office floors will include workspace and ancillary uses for 4,500 members of staff and their visitors, including cafes, gym and pool facilities, a covered multi-use games area (MUGA), an Events Centre and staff training facilities. At roof level, landscaped terraces and a walking 'Trim Track' will provide outdoor amenity and recreation space for office workers and their visitors.

The proposal includes an Events Centre, which comprises an auditorium located on the first and second floors, and four smaller event spaces, known as 'town halls', located on Level 00. These spaces, along with foyers, pre-function and support spaces, are all connected by stairs and a lift so that they can operate independently, or together to host a single event.

The Zone A Building will include two levels of basement. The basement areas, which include disabled car parking (4 bays) for the office functions (no on-site parking is provided for retail use) and loading areas for servicing the office and retail space, will be accessed via an existing vehicular access ramp from Goods Way, which connects up to the existing Shared Service Yard, which services King's Cross Station.

The submission does not include any new principal areas of public realm, though it does extend approved/existing public realm along King's Boulevard, Battle Bridge Place and Goods Way to "fill in" gaps created where the proposed building envelope steps back from the development zone boundary, for example to the south of the building and outside the retail unit on the north-west corner. These areas are designed to be consistent with the approved (and in some cases completed) public realm in terms of site levels and materiality and therefore are only considered in this section to the extent they interact with the building entrances. Further details are provided in the submitted Urban Design Report.

### **2.2 Parking**

An overall site-wide strategy for parking has been approved under the KXC outline planning permission, with maximum numbers of parking spaces agreed, and appropriate percentages of parking provision for disabled people.



### 2.2.1 Car Parking

The Zone A Building will have a total of 4 accessible spaces in the upper basement which will serve disabled employees and authorised visitors. The spaces, shown on Figure 1, are located next to lift Core 3.

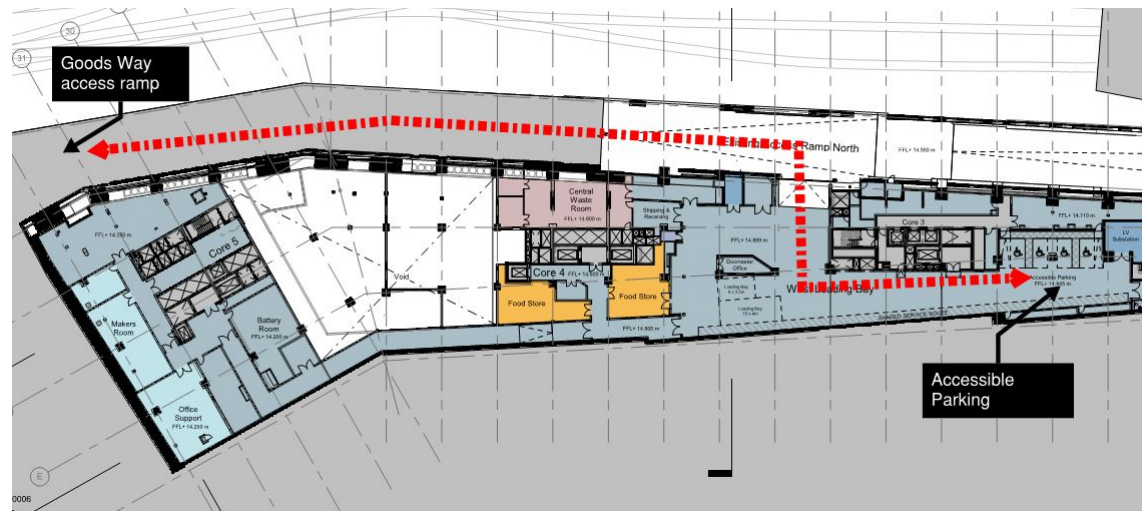


Figure 1: Upper basement plan showing location of 4 accessible parking spaces

Vehicles will access the spaces via the existing Access Ramp, entered/exited from Goods Way. Users will then access the relevant reception area (if a visitor) or office area (if staff) via the lifts in the adjacent Core 3, as illustrated in Figure 2 below.

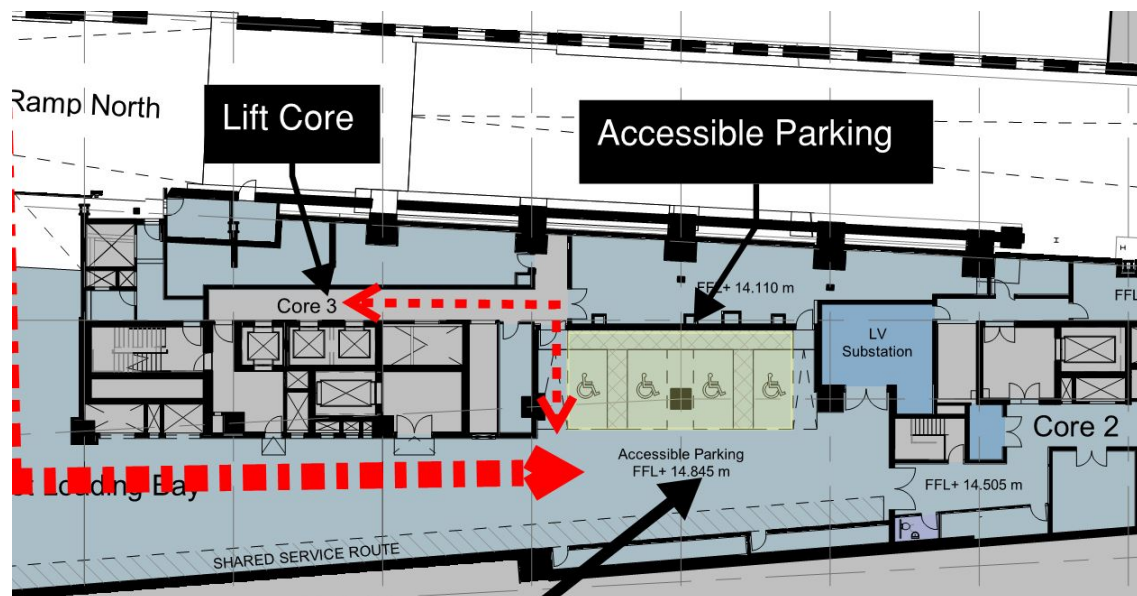


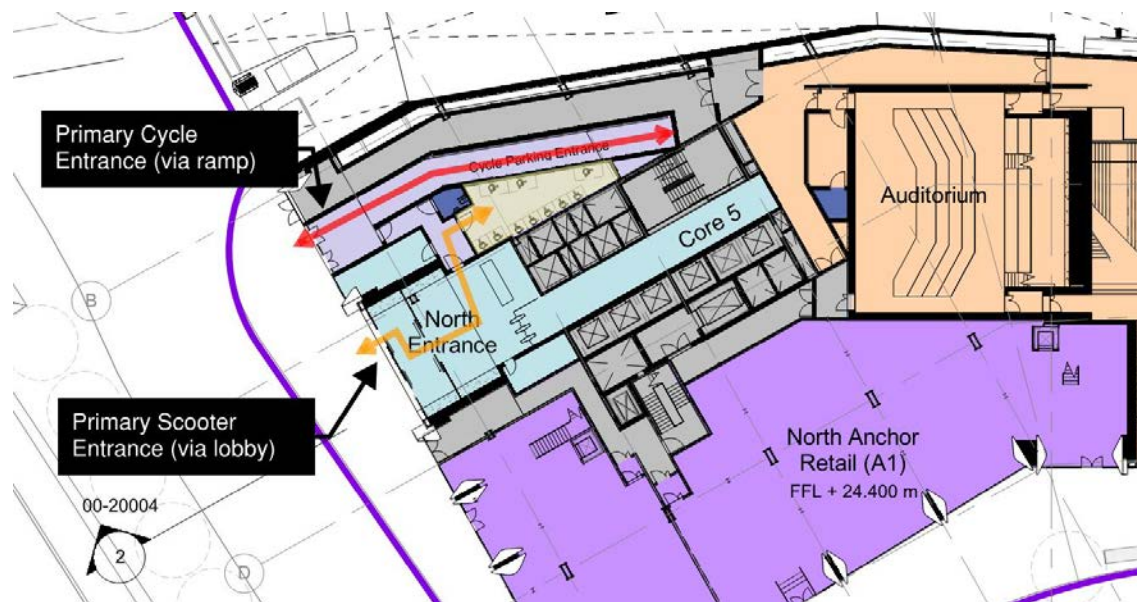
Figure 2: Upper basement plan extract showing access from accessible parking bays into the building via Core 3

The use of these spaces by visitors will require coordination by the office staff being visited to gain access and egress via Core 3.

## 2.2.2 Cycle/Scooter parking

An access-controlled Cycle Entrance is located next to the North Entrance (shown in Figure 3 below). Cyclists will travel down a slope with a gradient of approximately 1:10, which is considered acceptable for a cycle ramp, into the cycle storage area on the mezzanine floor.

Cycle and scooter users that cannot traverse a 1:10 gradient can enter via the North Entrance and make use of the lift (Core 5) to enter the cycle storage area, as shown in Figure 3. Shower and changing facilities (at Level 00) are similarly accessed via Core 5.



*Figure 3: Plan showing access arrangements from Goods Way to the accessible cycle and scooter parking facilities on the mezzanine level below*

Scooters and hand propelled tricycles for disabled people will be accommodated in a store located in the lobby of the North Entrance.

This storage area contains parking and charge points for electric scooters and indoor wheelchairs to allow users to transfer from one to another and charge the one that remains behind.

## 2.3 Entrances

The Zone A Building is located along the existing King's Boulevard which slopes upwards from King's Cross Underground station in the south to Goods Way at the north. The change in level along the Boulevard is approximately 7 metres from the lowest to the highest point (south to north).

Figure 4 (below) indicates the principal entrances to the southern half of the building and Figure 5 (below that) indicates entrances to the northern half. Since the street level changes, rising to the north, these entrances are on different levels of the building.

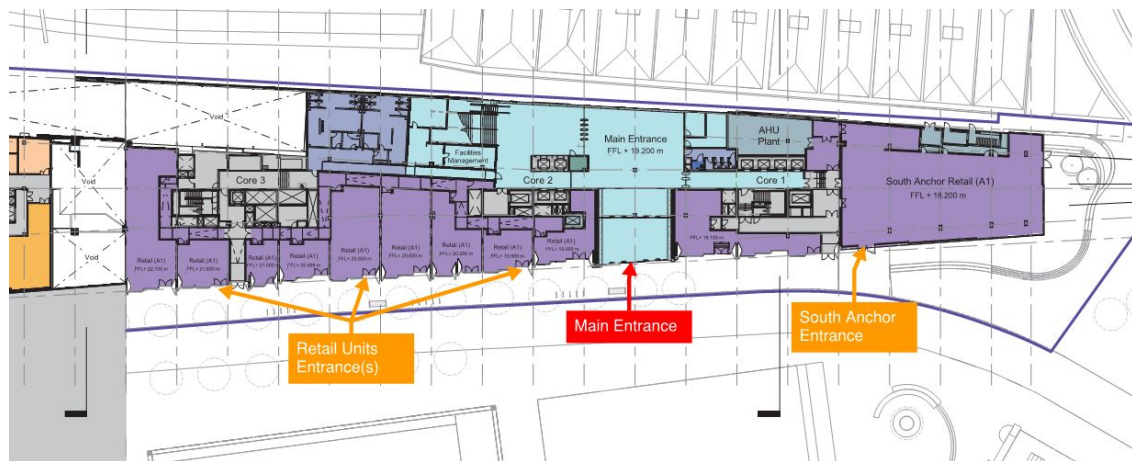


Figure 4: Level 00 southern plan indicating the location of Office entrances (in red) and retail entrances (in orange)

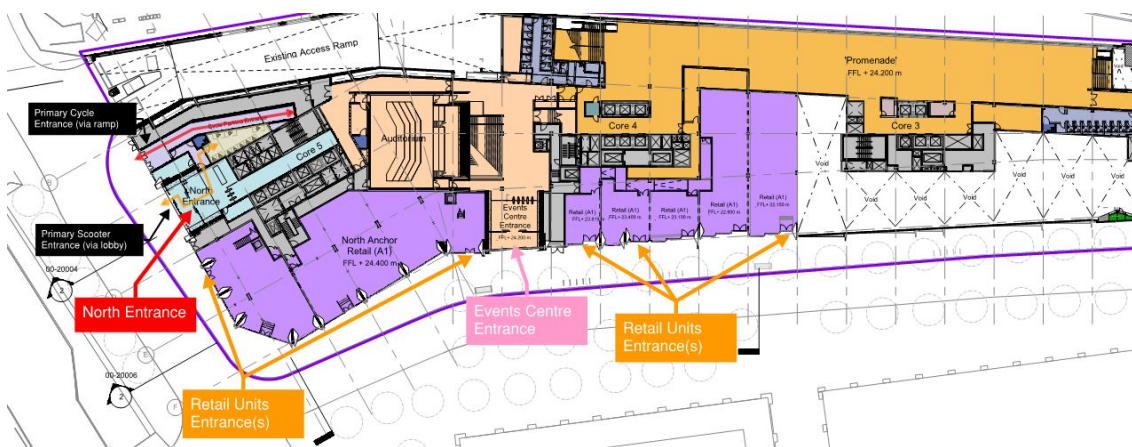


Figure 5: First floor northern plan indicating the location of Office entrances (in red), the Events Centre Entrance (in pink) and retail entrances (in orange)

There are two office entrances which respond to the anticipated desire lines of staff and visitors and provide convenient connections to the different functions and spaces within the building. The Main Entrance is located towards the southern end of King's Boulevard, near one of the entrances to King's Cross Underground Station (beneath Building B2) at 19.20m Above Ordinance Datum (AOD), while the North Entrance is located on Goods Way at 24.20m AOD. Between these two office entrances, towards the northern end of the building, is the ancillary Events Centre Entrance, at 24.20m AOD. The Cycle Entrance, as discussed in the previous section, is located next to the North Entrance, at 24.4m AOD.

The entrances to the retail units along King's Boulevard follow the levels and contours of the existing Boulevard which were set within the Outline Planning Permission to deliver an accessible landscaped slope. The retail frontages will provide a varied arrangement of entrances and window displays which will be subject to sufficient visual signals (retail displays and lighting), along with sandstone paving setts, which give a different tactile texture close to the glazed elements of the shop front, to avoid potential collision by people with visual disabilities.



Some of the retail units may span across a sufficient width to require a level change within. Where this occurs, one entrance may be stepped with another offering level access, but both entrances would carry equal importance within the hierarchy of entrances. Retail units in this kind of exposed location with the possibility of an increased wind pressure holding the doors closed may require automated entrance doors. The retail street is bookended by a North Anchor retail unit, which addresses Goods Way and Granary Square, and a South Anchor retail unit, which addresses Battle Bridge Place and King's Cross Station.

### 2.3.1 Main Entrance

The Main Entrance (anticipated to receive up to 80% of office workers and visitors) is located towards the southern end of the Boulevard at 19.20m AOD. Sliding entrance door-sets will allow large numbers of people to access the reception area without hindrance.

From the reception area, staff and visitors will primarily travel up (via stairs and escalators) to the 'Promenade' area located along the eastern edge of the building, on the First Floor at 24.20m AOD (see Figures 6 and 7 below). This is anticipated to be the main horizontal setting off point as users then select the appropriate vertical core to take them closest to their desired destination within the building.

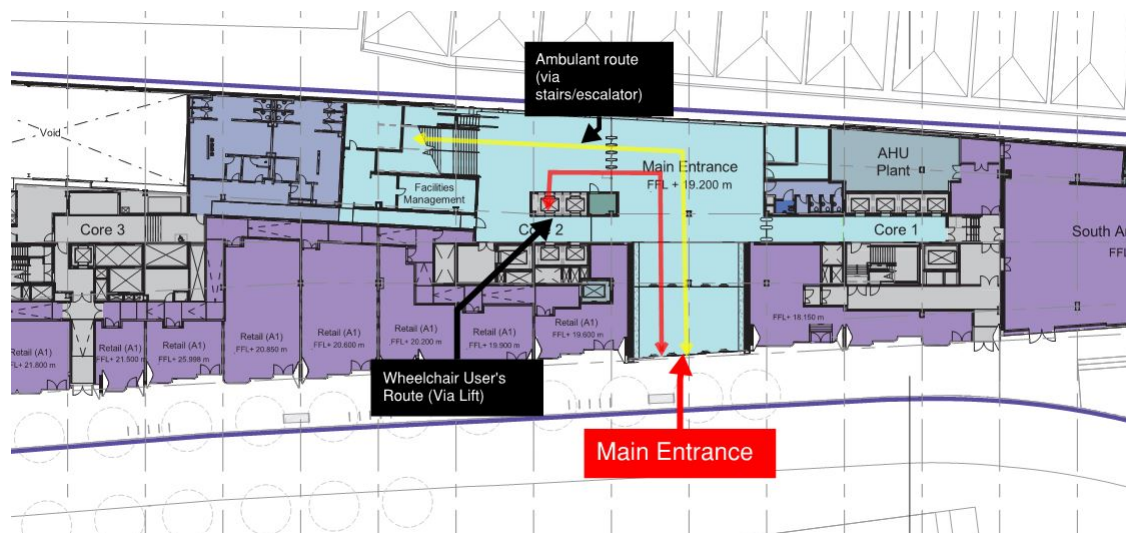


Figure 6: Plan showing access from the Main Entrance to the 'Promenade' on the level above

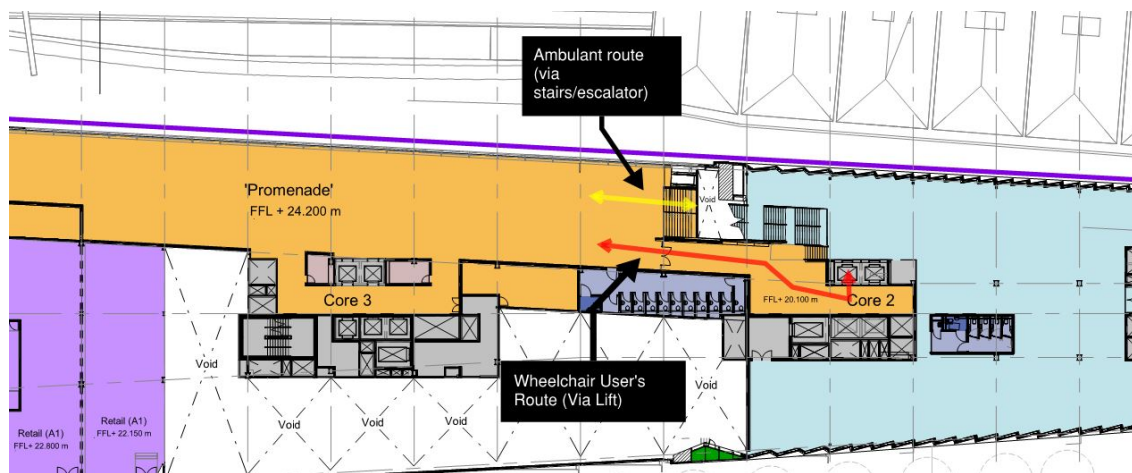


Figure 7: Arrival at the 'Promenade' level for wheelchair users and ambulant users

### 2.3.2 South Anchor Retail Unit Entrance

The South Anchor is located approximately 1 metre above the level of Battle Bridge Place. It is intended to provide a plinth level outside the South Anchor and provide stepped access onto the plinth, and to use the natural rising slope of the landscape to allow non-ambulant users to make the transition from Battle Bridge Place to the South Anchor entrance. The height of the Shared Service Yard in the basement below the South Anchor constrains the ability to provide level or more direct access. Many options for this transition were considered and the solution presented provides the most legible pathway and best gradient for wheelchair users (1:21+).

The South Anchor can also be accessed via the Main Entrance. In this event, ambulant users will make use of the stairs between the two levels, and wheelchair users will make use of a platform lift which appears from beneath the steps.

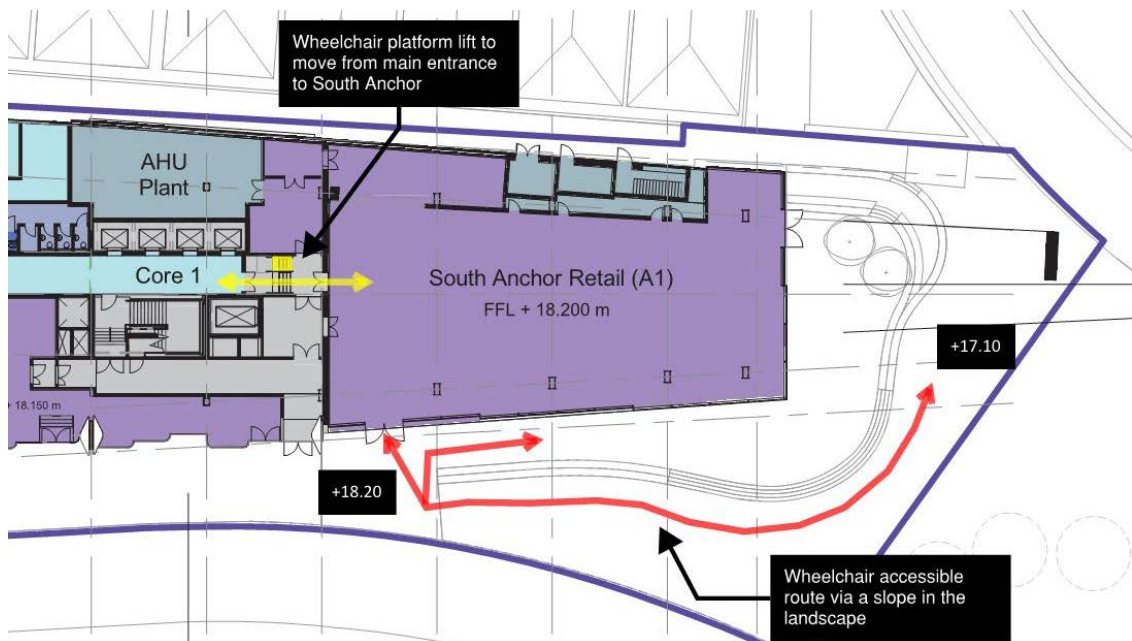


Figure 8: South Anchor Entrance arrival sequence and platform lift location

### 2.3.3 Events Centre Entrance

The Events Centre entrance is located towards the north of the building and directly accessed from King's Boulevard. The entrance from King's Boulevard is level and will be a lobbied sliding door arrangement with security barriers before the user gains access to the foyer area (see Figure 9 below).

### 2.3.4 North Anchor Retail Unit Entrance

The North Anchor has the potential to be divided into two units, and has four entrances. The corner of the street is 25.150 m AOD whereas the units have finished floor levels (FFLs) of 24.4m AOD. Level entrances are located where the pavement drops down to the FFL (24.4m AOD), either side of the corner. Two further entrances are located closer to the corner, with steps down from the higher level (see Figure 9 below).

### 2.3.5 North Entrance

The North Entrance is a pair of sliding doors which leads to the entrance lobby. This entrance can accept both office workers and visitors (anticipated to be approximately 20% of those), and it is the primary entrance for disabled cyclists and deliveries (see Figure 9 below).

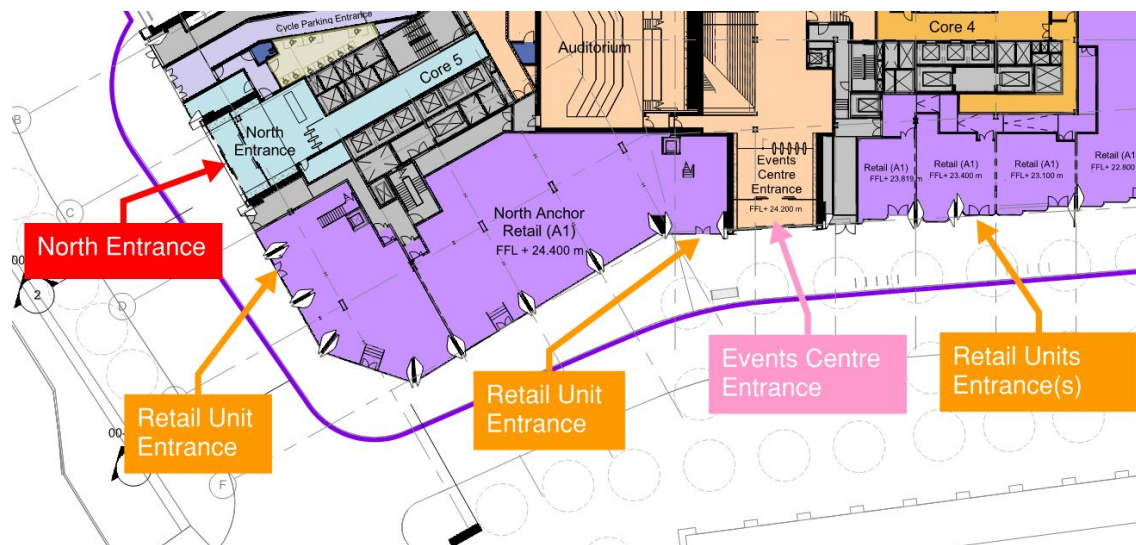


Figure 9: North end of the building showing the Events Centre, North Entrance and the North Anchor retail units

### 2.3.6 Entrance Doors

The main entrances will take the form of automatic sliding doors.

All entrances will provide level thresholds with the adjacent public realm, solid entrance matting and will be clearly articulated within the building facade.

Glazed doors and screens will have manifestation in line with guidance in Part M.

## **2.4 Vertical Circulation**

### **2.4.1 Passenger Lifts**

The Zone A Building is served by five principle cores, referred to as Cores 1 to 5, distributed along a central spine from south to north. Cores 1, 3 and 5 have associated staircases and fire escape routes.

From the reception area of the Main Entrance, staff and visitors will primarily travel up (via stairs and escalators) to the 'Promenade' area located along the eastern edge of the building, on the First Floor at 24.20 m AOD (as shown in Figure 7 above). This is anticipated to be the main horizontal setting off point as users then select the appropriate vertical core to take them closest to their desired destination within the building.

The cores are well-located with relatively short travel distances horizontally.

Generally, Core 2 will not be available to those entering the Main Entrance as it would be overwhelmed by the volume of people. Staff and visitors will generally move to the Promenade area described above, using either the stairs or the escalator, and from there select the appropriate vertical core.

However, when required by a disabled office user or visitor, access will be provided to the reception side of the double-sided glass lift in Core 2, which will deliver them to the 'Promenade' area on the First Floor (see Figure 7 above). The lift will be a through-lift which means that it will generally be easier for a wheelchair user (straight in and out) and can accommodate longer mobility vehicles that can otherwise be difficult to negotiate through lifts.

### **2.4.2 'Promenade' Café Platform Lift and Ramp**

The 'Promenade' Café on Level 2 has a change in level from the main floor level up to the level of the café of approximately 1050 mm, as it is located over the existing Access Ramp to the Shared Service Yard. This level change is navigated via a series of switchback ramps with a gradient of less than 1:20, and a platform lift located alongside a staircase (see Figure 10 below).

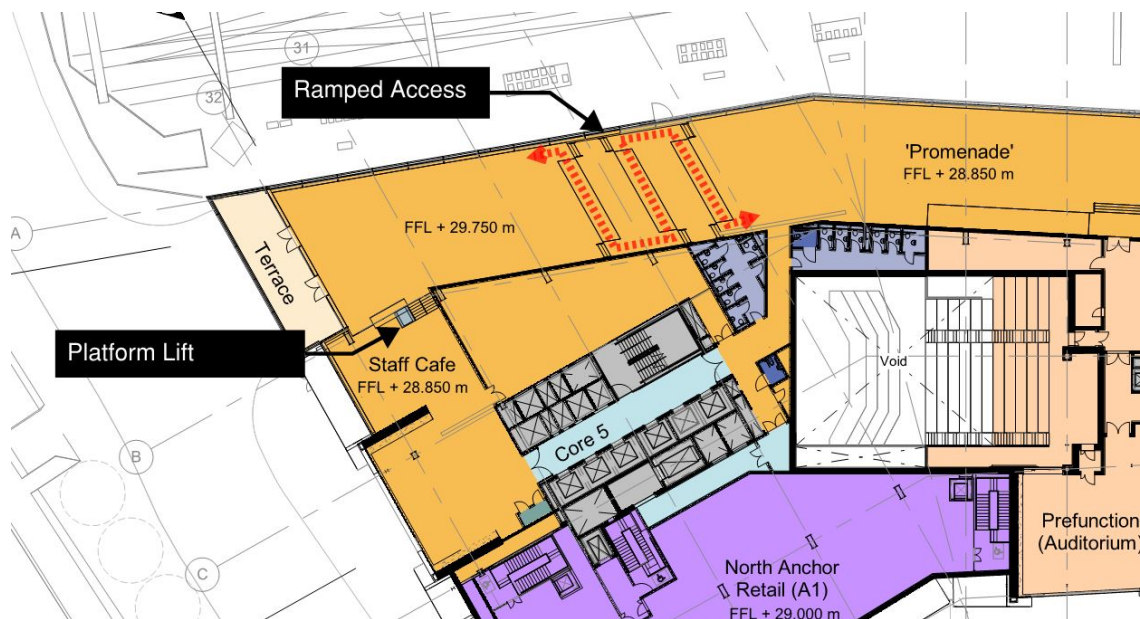


Figure 10: Second Floor Plan showing 'Promenade' Café Ramp and Platform Lift

### 2.4.3 Stairs

Staircases are located in Cores 1, 3 and 5. These extremely generous (2.4 metres wide) staircases are intended as fire escape and accommodation staircases (General Access Stair). In order to encourage their use, the stairs have been given a clear connection to the main office cores with varying degrees of glazing in the elevation walls inside the main cores.

Circulation and accommodation stairs within the cores will meet Part K and Part M of the Building Regulations and BS8300:2010A guidelines for use by people with ambulant and visual disabilities.

### 2.4.4 Diagonal Staircase

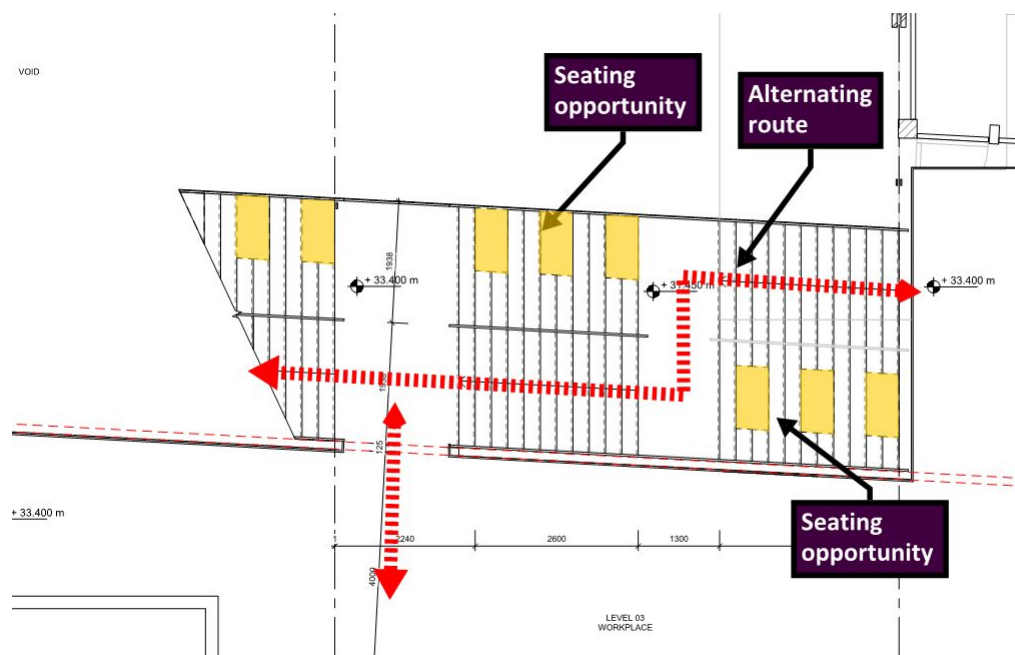
Central to the design of the building is a 'Diagonal Staircase', which runs diagonally up through the whole building from first floor to the roof (accessed from ground floor via the 'Promenade'). This provides continuity and a visual connection vertically through the building.

This staircase will provide a main thoroughfare for the building users, but also seating and resting opportunities to facilitate informal meetings and discussions.

The staircase is extremely wide – up to 4 metres in most points - and this will be divided with a handrail to define two halves. One half is intended to be for circulation, and the other would accommodate seating and meeting opportunities.

Because there are potential dangers from long flights of continuous stairs, the allocation of seating/stair use alternates approximately every three flights as the Diagonal Staircase rises through the building, as illustrated in Figure 11 below.





*Figure 11: Plan Illustrating the Alternating Seating and Circulation Arrangement along the Diagonal Staircase*

Particular care will be taken with this staircase to clearly signal the change in direction, the nosings, the handrails and the seating opportunities.

During design development, the following features of the Diagonal Staircase were agreed with Building Control in the form of an exchange of emails:

- **Landing Depths:** Intermediate landings are not the 'width of the stair' but less than this. It was agreed that this is satisfactory as building regulations require a landing of 1200mm in length as a minimum and these stairs meet this requirement.
- **Handrail Division:** It was agreed because this is an office building that the stair could be 2000mm between the inner face of handrails before an additional central handrail is required. This meets Part K of the building regulations. The first flight from Level 1 to 2, and the last flight from level 11 to the roof have a different rise/going to the main body of the stair, which is otherwise consistent. This was felt to be acceptable because this is an office building and the flights are sufficiently distant from each other and users are unlikely to use the staircase from top to bottom of the building.
- **Tread/Stair/Handrail Orientation:** The stairs are set at an angle of 87 degrees to match the grid of the building. I.e. there is not a perpendicular relationship between the direction of travel and the steps. This did not attract any adverse comments.

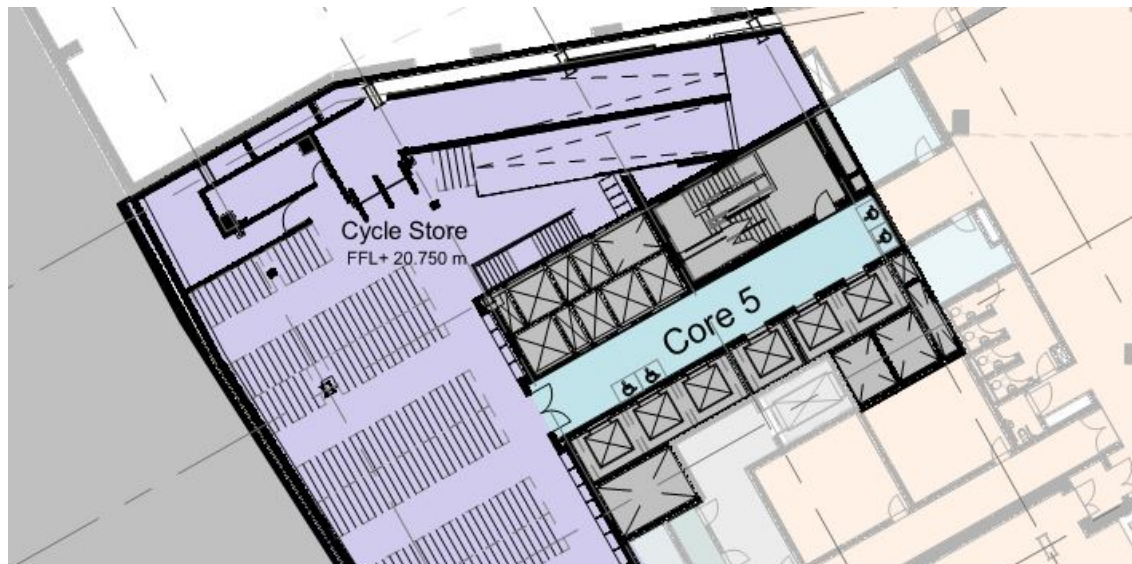
More detailed proposals for materials and visual treatment of the stairs will be brought forward during the Building Control process.



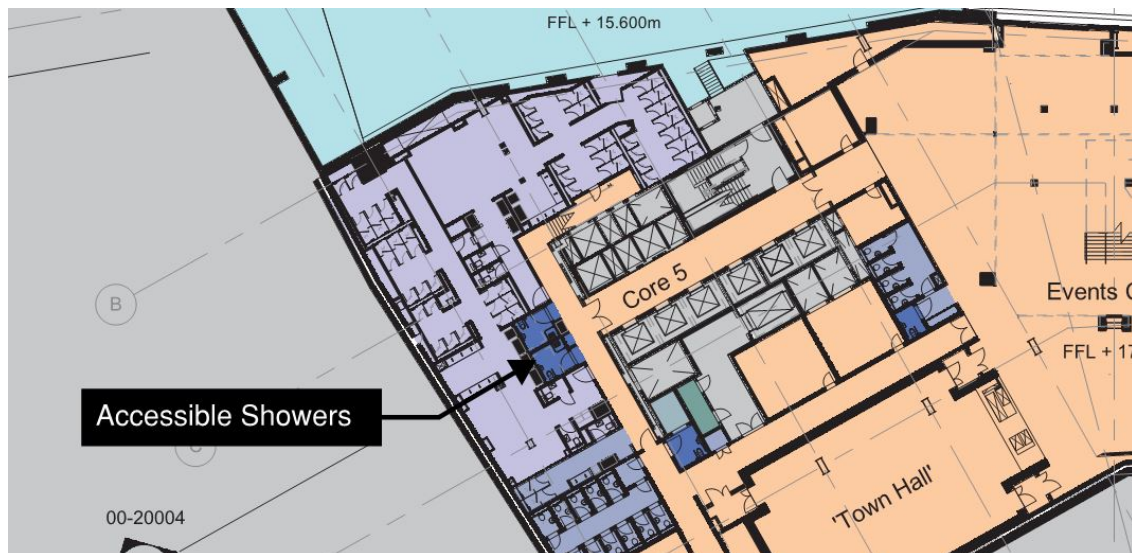
road which provides access to the cycle parking area only. This approach has previously been discussed and agreed with Camden Building Control.

Direct connection between the cycle store and Core 5 is provided for convenient access to other parts of the building.

Among the facilities provided for cyclists (and indeed other staff) are changing and shower rooms and toilets, including an accessible shower room and WC of 2400mm by 2500mm (see Figure 14 below), located at Level 00 (below the cycle storage area at Level 00M).



*Figure 13: Plan showing Cycle Storage Area at Level 00M*



*Figure 14: Plan showing Cycle Shower and Change Facilities at Level 00*

## 2.5 Internal Doors

All doors within the office areas will provide a minimum clear opening width of 800mm and will be provided with visual manifestations where glazed, and/or vision panels where solid and on an access route.

There may be a number of back-of-house/plant/service doors which necessarily have individual opening widths of less than 800mm per leaf but which are suited to their primary function.

## 2.6 Floor Finishes

Floor finishes will be further defined as a part of the Building Control process.

## 2.7 Sanitary Accommodation

### 2.7.1 Accessible WCs

Accessible toilets are distributed across each level, with each level offering a choice of right and left hand transfer (as opposed to alternating on each floor).

Typical WC arrangements are illustrated in Figure 15 below.

On the northern section of the upper floors there are areas where the total travel distance to an accessible WC may exceed 40 metres. However, the floor plate is open in its nature with few impediments to reaching the WC and subject to this remaining the case, it is generally agreed with Camden's Access Officer.



Figure 15: Typical AWC Distance and Core Layout

In all other areas an accessible WC is always included as a part of the main WC accommodation.

Both the Main Entrance and North Entrance reception areas have an accessible WC as close by as possible.

All toilet accommodation will comply with Part M of the Building Regulations and BS8300:2010A in terms of layout requirements, materials and finishes.

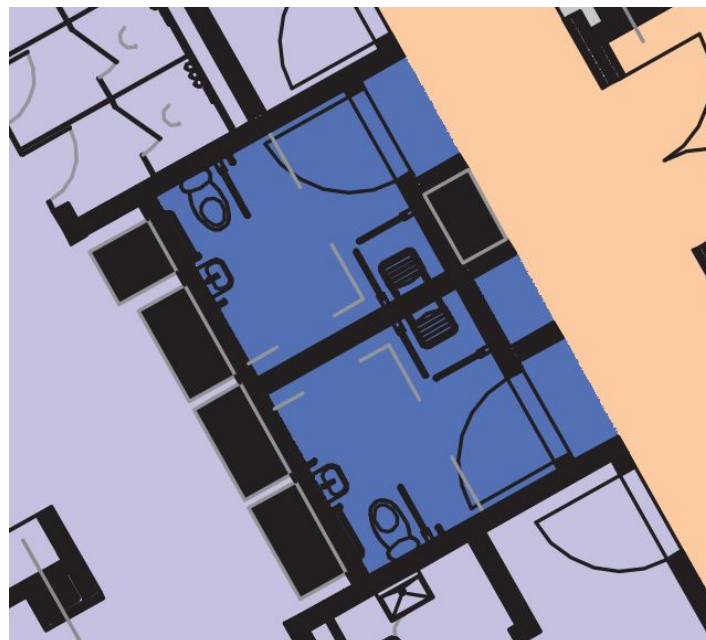
To improve circulation through the core areas, the majority of accessible WCs have been provided with inward opening doors. These cubicles have been enlarged in accordance with the guidance in BS8300:2010 to accommodate the door swing. Emergency escape hardware has been specified for the doors to be used in the event of someone falling against the inside of the cubicle.

### 2.7.2 Showers

Accessible toilet/showers are increasingly in demand by office staff. Six such facilities are therefore provided throughout the Zone A Building in association with other shower facilities, specifically within:

- the cycle shower and change facilities at Level 00;
- the gym changing area at Level 10; and
- the Swimming Pool Changing area at Level 9.

The facilities have been designed to meet Part M accessible shower/WC layouts and will be available for use at all times. A typical layout (taken from the swimming pool facilities) is shown in Figure 16 below.



*Figure 16: Typical Layout of Accessible WC and Shower Facility*

## **2.8 Escape Arrangements**

Areas of refuge have been provided at all levels within the core designs associated with escape evacuation lifts. Management procedures will be put in place by the occupier and building management team to ensure that refuges are checked in the event of an emergency and/or for staff to respond to a disabled person in the refuge.

In all instances where platform lifts exist there are alternative level access routes or the fire fighting lift.

Staff will be suitably trained to assist disabled people and to assist with use of evacuation chairs where provided.

Operator policy, procedures and practices will be developed together with a means of escape strategy for disabled people, whether staff or visitors. Personal Emergency Egress Plans (PEEP) for individual disabled users will be developed as required.

## **2.9 Special Facilities**

The nature of Google UK's operation is such that it brings together a number of ancillary uses that support the office functions, ensure the health and wellbeing of staff and foster the innovation and creativity that defines the organisation. These uses include the Events Centre and recreational facilities such as gyms, swimming pool and roof terraces. Specific requirements and features relating to the accessibility and inclusivity of these spaces are detailed below.

### **2.9.1 Events Centre/Auditorium**

Access to the Events Centre will be from a dedicated Events Centre Entrance on King's Boulevard. A dedicated lift moves between the entrance level and the 'Town Halls' below and auditorium above (see figures 17 and 18 below).



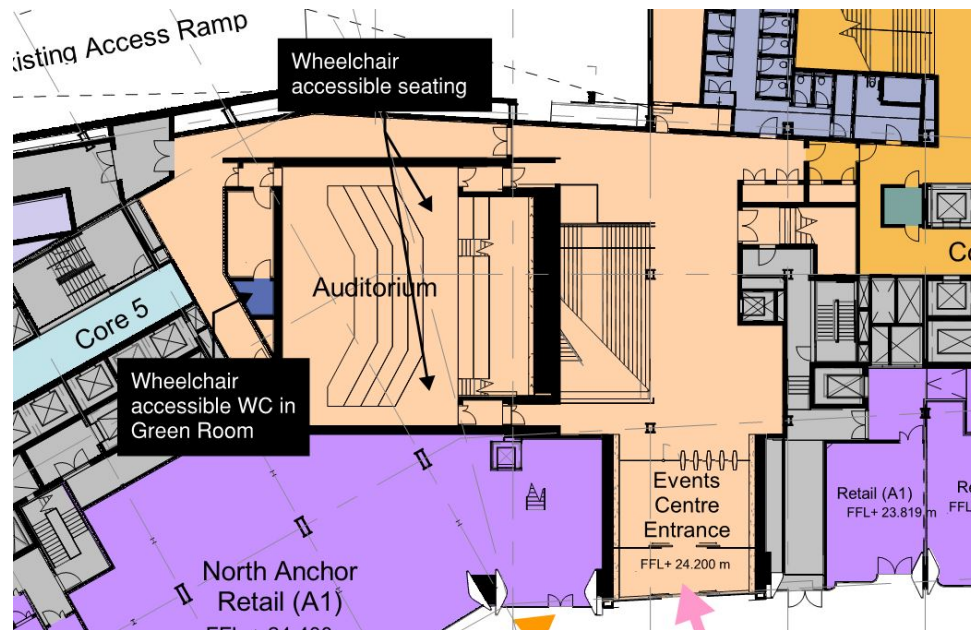


Figure 17: First Floor Plan showing the lower level of the Events Centre Auditorium

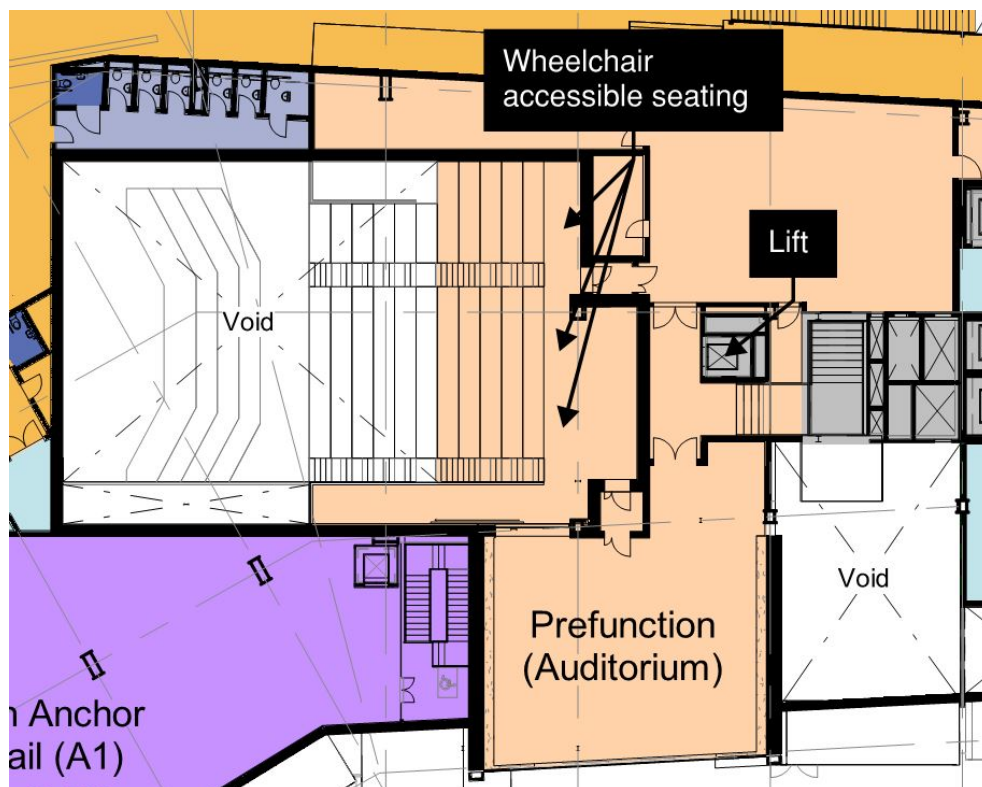


Figure 18: Second Floor Plan showing the upper level of the Events Centre Auditorium

As shown in Figures 17 and 18 above, the auditorium is arranged over two levels with raked seating and internal stairs run alongside the seating. The auditorium will be mainly accessed

from the Events Centre foyer (level access from the King's Boulevard). There is level access from the rear of the auditorium direct to the stage level.

The green room of the auditorium has accessible WC facilities, and there is a level stage format, which means that the usage of the auditorium will be extremely flexible for a wide range of disabled people. The Events Centre auditorium will have flexible seating arrangements to accommodate wheelchair users throughout the space. The specification of such seating will form part of the fit-out of the building. However, it is envisaged that it will include flexible seating designs that will allow for individual or group removal to accommodate a minimum of 6 integrated wheelchair users or larger groups of wheelchair users sitting together.

The auditorium will be fitted with communications devices to assist people with hearing disabilities. The specification of this system will be determined as part of the fit-out of the building.

## 2.9.2 Roof

The roof follows the contours of the building and steps down from the highest point at the north (Level 12) to the lowest point at the south (Level 7).

As shown in Figure 19 below, the roof will be extensively landscaped, to provide a green amenity space for building users. The design comprises four main character areas, known as the 'Plateau' (at the top/north), the 'Fields', the 'Gardens' and the 'Headland' (bottom/south).

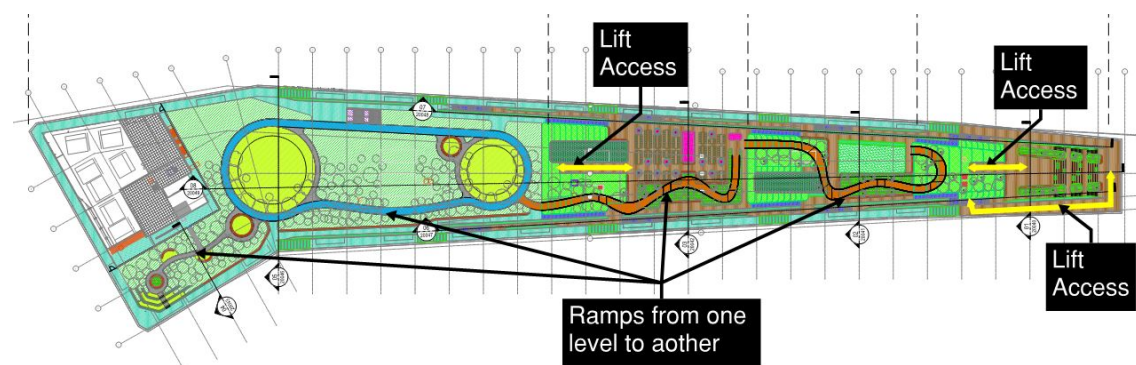


Figure 19: Plan Showing Accessibility Details of the Roof

### 2.9.2.1. Stairs

As stated above, the roof comprises a series of terraces which reflect the changes in level illustrated below. These are connected by, lifts, ramps and stairs.

The stairs are external and will meet the requirements of Part M.

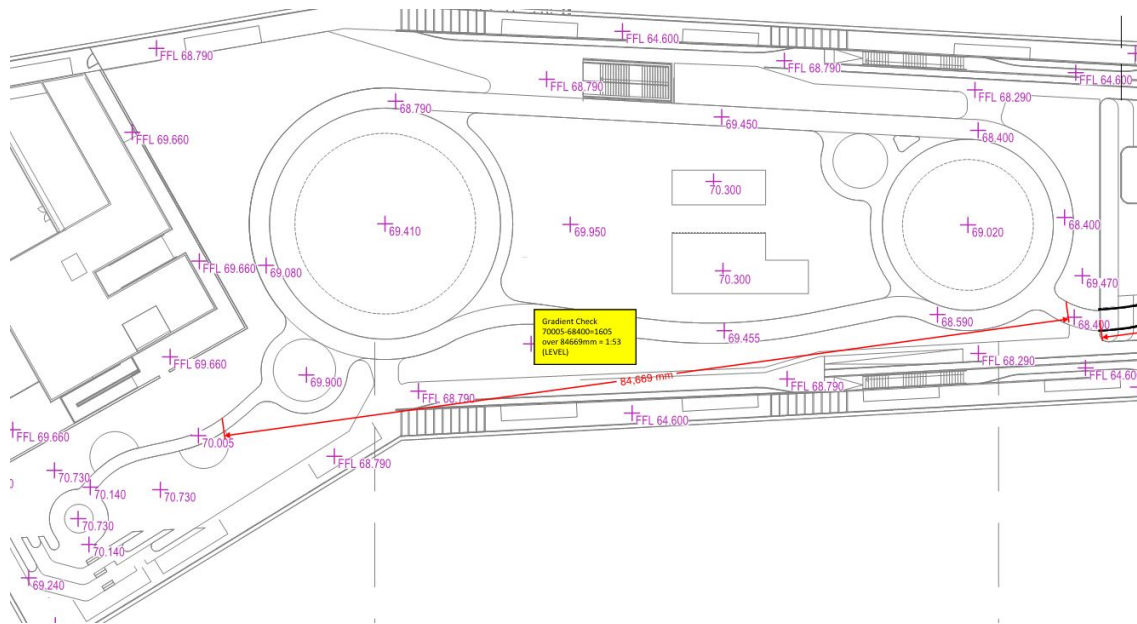


The roof provides several sections of ramped access over large areas of the landscaped area. Detailed development of the ramps is taking place, but Figures 20 to 22 below illustrate a gradient check to ensure that a gradient no worse than 1:15 (and sometimes better) can be achieved. The gradients will often be better than those shown in this check as the centreline of the ramp can be taken. The ramp will be a minimum of 1800mm wide (and probably wider than 2000mm in most areas) to allow passing on the slope, and for those who have stopped on the platforms to enjoy views.

This technical drawing shows a cross-section of a building with multiple levels and rooms. Key features include:

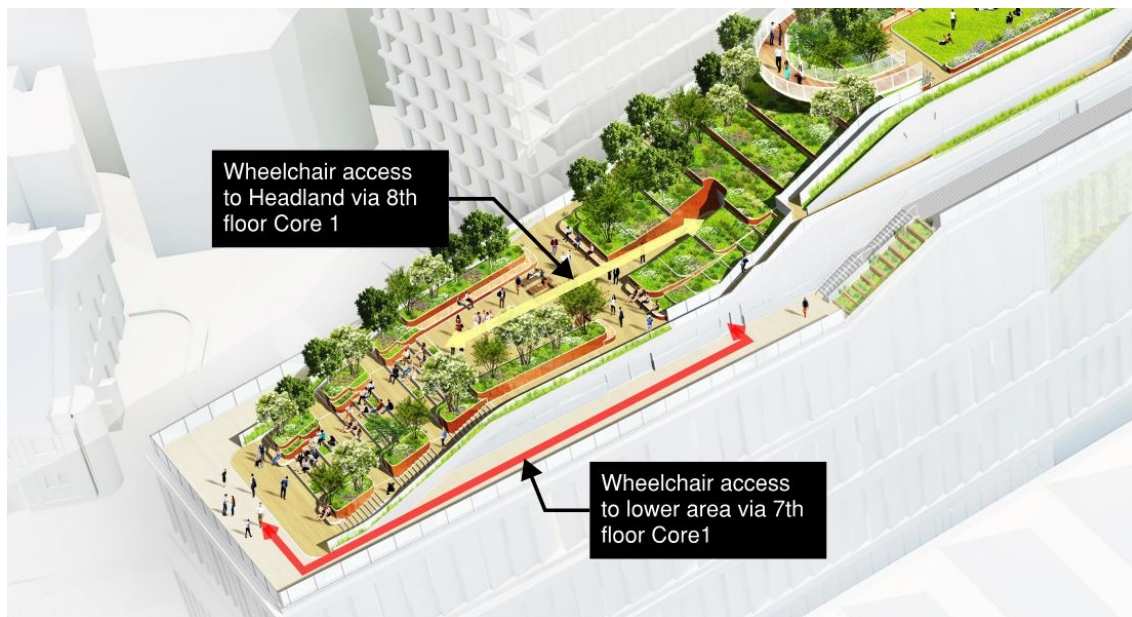
- Floor Levels (FFL):** Various levels are indicated with purple crosses and labels such as FFL 61.125, FFL 56.800, FFL 49.000, FFL 53.325, FFL 52.900, FFL 60.700, and FFL 65.125.
- Gradient Checks:** Three yellow boxes provide calculations for specific sections:
  - Box 1: Gradient Check 6575-64075=1590 over 15000mm = 1:15
  - Box 2: Gradient Check 6400-61125=315 over 9575mm = 1:17.7
  - Box 3: Gradient Check 64075-62610=1465 over 22518mm = 1:15.4
- Dimensions:** Red arrows indicate horizontal distances like 15,031 mm, 12,275 mm, and 12,045 mm, as well as vertical offsets like 15,675 mm and 10,806 mm.
- Structural Elements:** The drawing depicts walls, columns, and roof structures with hatching used to differentiate materials or structural types.

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*Figure 22: Plan Showing Ramp Gradients for the Northern Section of the Roof*

Figure 23 below shows the access to both levels of the 'Headlands', via the Core 1 lifts on Level 7 and Level 8.



*Figure 23: Render showing access to Headlands*

Figure 24 below shows the access to the 'Fields' and the 'Plateau' via the 'Gardens' level, which is accessed via the Staff Café and Core 4.

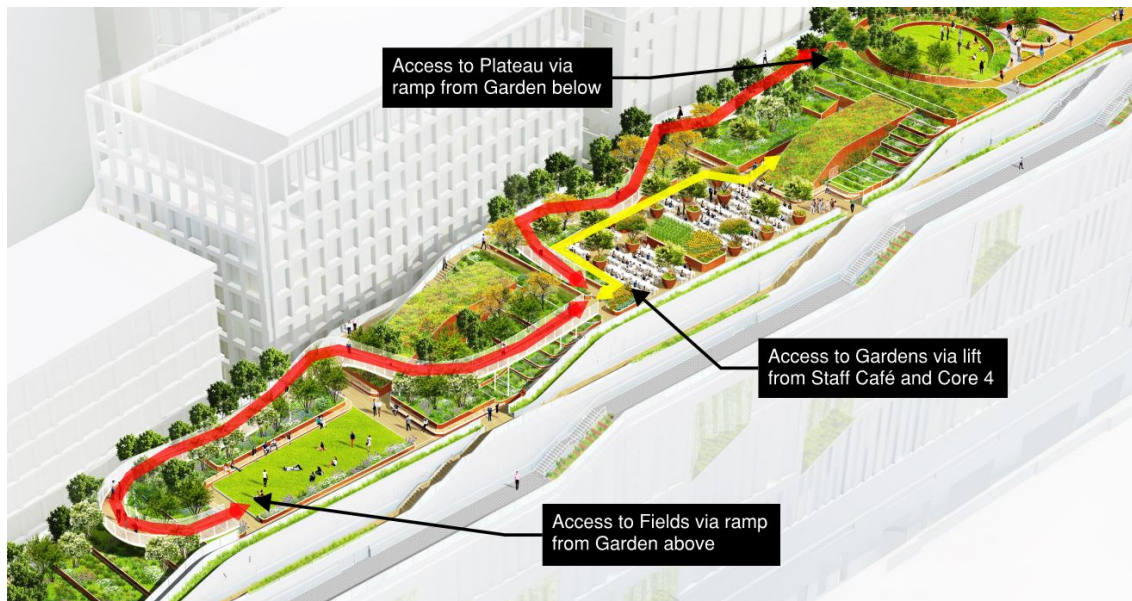


Figure 24: Render showing access to Fields, Gardens and the Plateau

### 2.9.3 Swimming Pool

A swimming pool is located on the 9th floor. Accessible changing /shower facilities have been provided at poolside, illustrated in Figure 20 below.

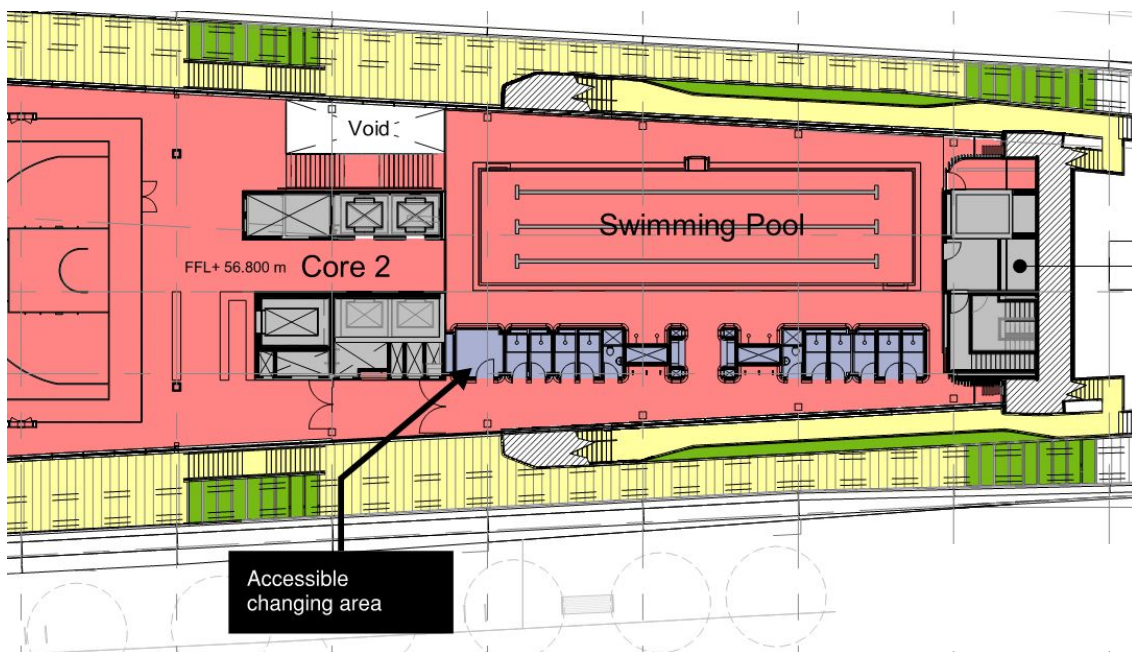
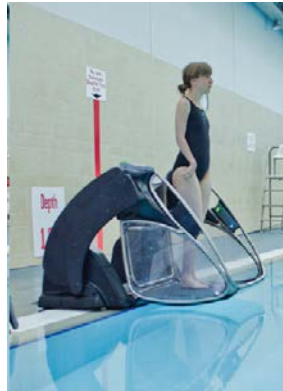


Figure 20: Level 9 Plan Showing the Swimming Pool and Accessible Changing Area



A hoist system for disabled users will be provided to access the pool. At this stage it is envisaged to be the 'pool pod' which has been successfully utilised at the London Aquatics Centre, illustrated in Figure 21 below.



*Figure 21: 'Pool Pod' Disabled Entry System Proposed for the Swimming Pool*

#### **2.9.4 Gym Facilities & MUGA**

While no specialist facilities are required for the MUGA, the Gym(s) will have equipment suitable for use by disabled employees and any treatment/sauna/steam facilities will be accessible for those with mobility disabilities.

### **2.10 General Details**

Details of the following areas and how they will be made accessible shall continue to be developed in Stage 4:

- decoration
- lighting (high frequency tubes where used)
- service counters – food offering details
- sanitaryware selection and layouts
- fire alarm details
- lift details
- toilet layout details
- signage
- furniture selection
- kitchen layout

In addition, Appendix A of this statement sets out the management issues which the occupier/building management team should be aware of to ensure access is achieved and maintained.

## Appendix A

### Management issues

The following management issues will be brought to the attention of relevant parties to ensure that access is achieved and maintained:

external routes – keep in good repair and free of obstructions and leaves, ice, snow and surface water;

doors – adjustment of door closers; ironmongery to be kept in good working order;

horizontal circulation – keep routes free from obstructions and furniture layouts/seating arrangements accessible;

vertical circulation – regular checking of lifts to ensure floor of car aligns with finished floor level;

WCs – checks to ensure that manoeuvring space in accessible compartments is not obstructed by bins, sanitary disposal equipment etc; replenishment of toilet paper and paper towels in accessible WCs as well as other WCs;

communication – new signs to integrate with existing sign system, no ad hoc homemade signs; all information to be kept up-to-date; signers and translation services to be provided as necessary; appropriate provision of accurate access information and other literature;

hearing enhancement systems – advertising; regular checking and maintenance of systems;

alarm systems – checking of systems; staff training in procedures;

surfaces – ensuring cleaning does not cause slippery surfaces; maintaining junctions to avoid worn surfaces becoming tripping hazards; replacing surfaces like with like; maintaining colour contrast in redecoration;

lighting – prompt replacement of bulbs; keeping windows and light fittings clean;

means of escape – specific evacuation strategies to be devised for people who need assistance, including staff and visitors; staff training; regular practice drills; maintenance of fittings and equipment; reviewing evacuation procedures;

security – ensuring security procedures do not conflict with accessibility good practice;

training – staff training is critical to maintain access and to provide accessible services and employment opportunities. Training can cover areas such as disability awareness and equality, use of equipment such as platform lifts and induction loops, British Sign Language, hearing awareness, clear lip speaking, guiding people with visual impairments and general access awareness.

health and safety policies – implementation of policies on access, risk assessment;

responsibilities for access – identification of responsible people to approve improvements, set priorities, ensure access is included in maintenance and refurbishment programmes, provide auxiliary aids, review numbers of disabled people using a service and establish and run user groups;

funding for access improvements – identification of specific access funds or grants; funds for specific employees such as 'Access to work'; use of the maintenance budget;

policy review – regular reviews of all policies, practices and procedures affecting access.



# King's Cross

4 Stable Street  
London  
N1C 4AB

T +44 (0)20 3664 0200  
[www.kingscross.co.uk](http://www.kingscross.co.uk)