

19 WELL ROAD, HAMPSTEAD, LONDON NW3 1LH

PROPOSED BASEMENT



CONSTRUCTION MANAGEMENT PLAN

NOV 2021(rev C)

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Construction Management Plan

Pro forma

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Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
May 2021	Revision A	TW
5 July 2021	Revision B	TW
9 Nov 2021	Revision C	TW

Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by
	Revision 0	
	Revision 1	
	Revision 2	

Introduction

The purpose of the Construction Management Plan (CMP) is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any cumulative impacts of other nearby construction sites, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) 6: Amenity and [CPG](#) 8: Planning Obligations).

This CMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Cyclist Safety](#) (CLOCS) scheme) and [Camden's Minimum Requirements for Building Construction](#) (CMRBC).

The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as for road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP.

Please notify that council when you intend to start work on site. Please also notify the council when works are approximately **3 months from completion**.

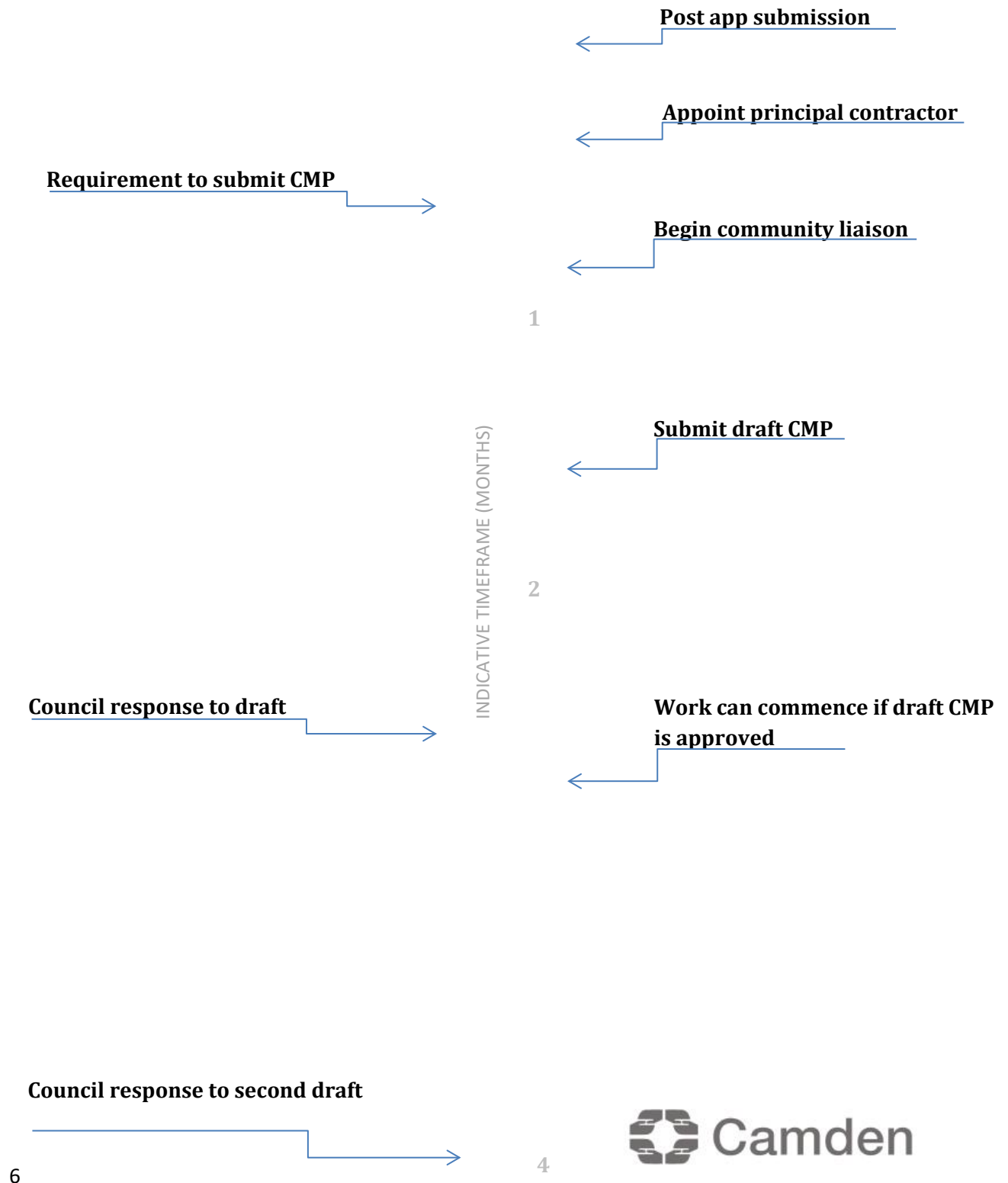
(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction, etc.)

Revisions to this document may take place periodically.

Timeframe

COUNCIL ACTIONS

DEVELOPER ACTIONS



Contact

INTRODUCTION AND BACKGROUND

The Application:

Full Planning Permission ref no 2018/6349/P has been approved and Listed Building Consent Granted ref no 2019/0421/L for an extension to the existing Basement under the front garden, including 2 x lightwells and associated internal alterations to 19 Well Road, NW3 1LH.

The Construction Management Plan

This Construction Management Plan has been prepared in support of the Planning Application in line with good practice and sets out details of the environmental protection, highway safety and community liaison measures that will be implemented in order to mitigate the impacts from the project and ensure there are no adverse effects on the Conservation Area features.

In order to minimise the likelihood of any issues arising we have put in place robust measures to control noise, pollution and dust, which may adversely impact on the health or amenity of our local neighbours.

The Plan also includes a waste management strategy for handling and disposing of construction waste.

We will update the Council on our progress and any issues/complaints throughout the construction period, and acknowledge that this CMP will be a live document throughout the construction. We are committed to having No Complaints during the build. If any complaints were to be received, the contractor would review these and any required remedial measures would be put in place,

Conservation and Listing:

It is noted that both the House and the Garden Wall and Archway are Listed Grade II, entry no 1379149 and lie within The Hampstead Conservation Area.

Well Road and Well Walk run parallel and link the village to the Heath. Well Road is identified as having two distinct characters. To the east the road is a 'quiet residential street' with 19th century development on to the north and south.

Tree Survey

Marcus Foster (Arboricultural Consultancy) has carried out a tree survey and Arboricultural Impact Assessment demonstrating that there will be no adverse impact on the trees during the construction.

Protection, Noise Storage

A key aspect of the CMP will be to ensure that the Grade II listed Garden Wall is protected throughout the construction phase. Therefore the full length of the garden wall will be protected with a hoarding to its full height throughout the construction phase

A Noise Report has been completed and is attached herein.

On site Storage

There is an area to the East end of the garden that not only gives off-street access, but also provides a suitable area to store the excavated spoil awaiting disposal and for building material storage generally

The Contractor

Morph Construction is a well-established contractor with substantial experience in delivering this type of project and in working in similar central London locations and within the Hampstead Conservation area

Morph Construction will be registering this site with the Considerate Contractors Scheme.

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

19 Well Road, Hampstead, NW3 1LH
Planning ref: 2018/6349/P, 2020/56502/P and 2019/0421/L

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Ted Walters
Address: Walters Consultancy Ltd, 109 Castlehaven Rd Camden NW1 8SJ
Email: tedwalters@gmail.com
Phone: +44 (0) 7831488 901

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Ted Walters
Address: Walters Consultancy Ltd, 109 Castlehaven Rd Camden NW1 8SJ
Email: tedwalters@gmail.com
Phone: +44 (0) 7831488901

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of Community Investment Programme (CIP), please provide contact details of the Camden officer responsible.

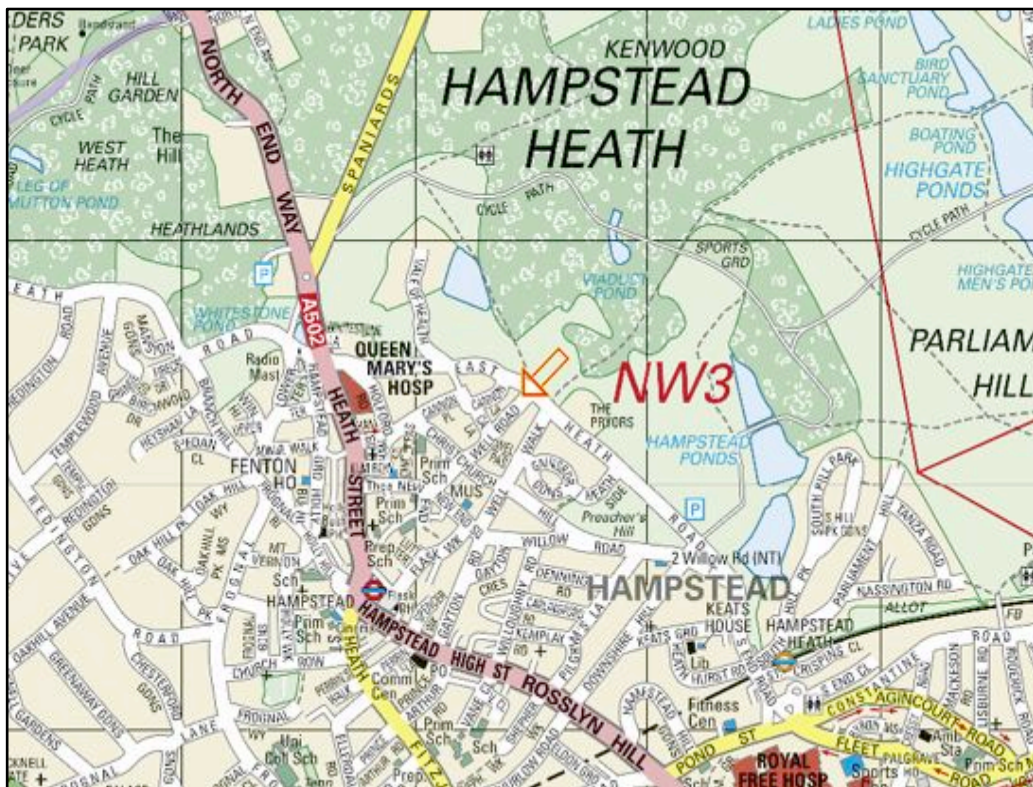
Name: Rod Pearce
Address: Morph Construction, 115 Jubilee Avenue, RM7 9LT
Email: Rodney.morphconstruction@hotmail.com
Phone: +44 (0) 7845748711

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Rod Pearce
Address: Morph Construction, 115 Jubilee Avenue, RM7 9LT
Email: Rodney.morphconstruction@hotmail.com
Phone: +44 (0) 7845748711

Site

6. Please provide a site location plan and a description of the site, surrounding area and development proposals for which the CMP applies.

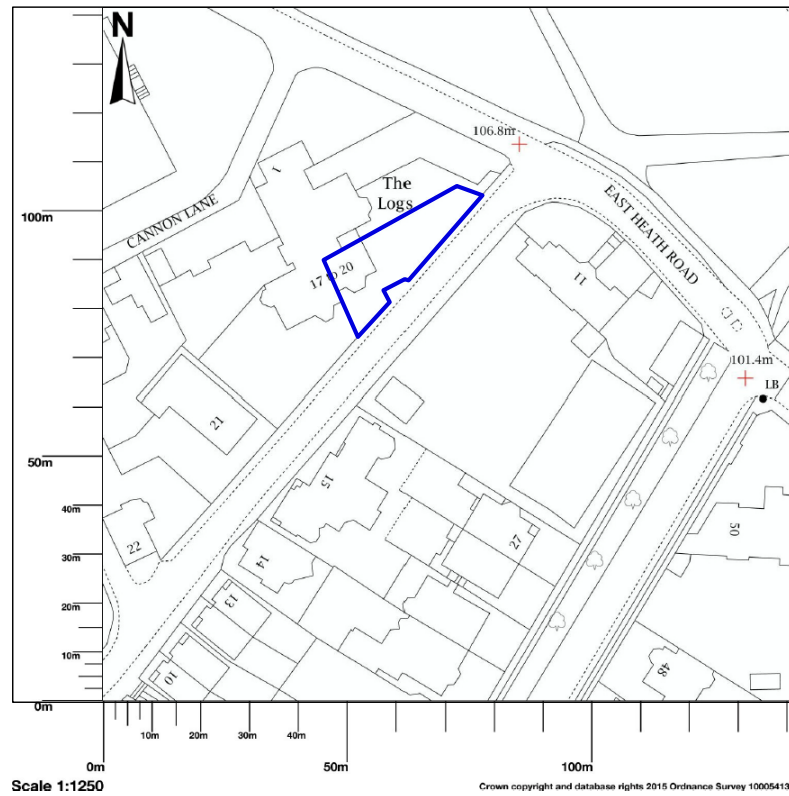


The site

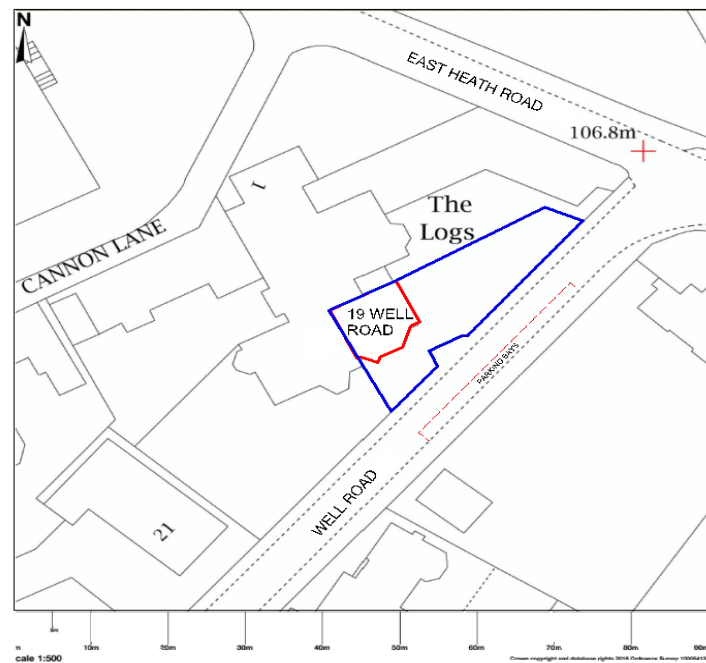
The site is situated on the North West side and at the East end of Well Road, a two-way residential street. The site is located approximately 35 meters from Hampstead Heath, and approximately 20 meters to the west of the junction with East Heath Road

The existing property at 19 Well Road is a semi-detached dwelling comprised of two stories and a basement.

LOCATION PLAN 1:1250



SITE PLAN 1:500



Parking

Well Road is a fairly narrow (approximately 5.050 metres wide) two-way road and lies within a Controlled Parking Zone (CPZ) which operates Mondays–Saturdays 9:00am– 8:00pm.

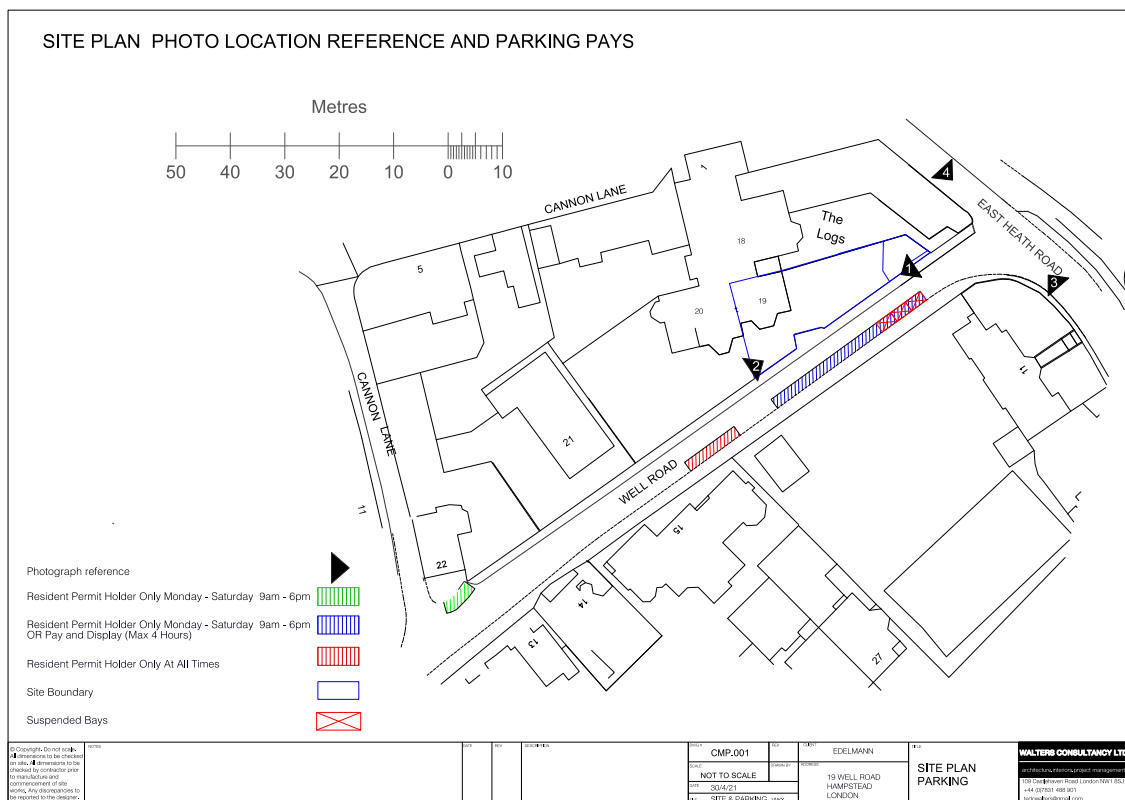
There are double yellow lines on the Northwest side of Well Road.

There are parking bays on the Southeast side of Well Road (opposite side of Well Road to the site).

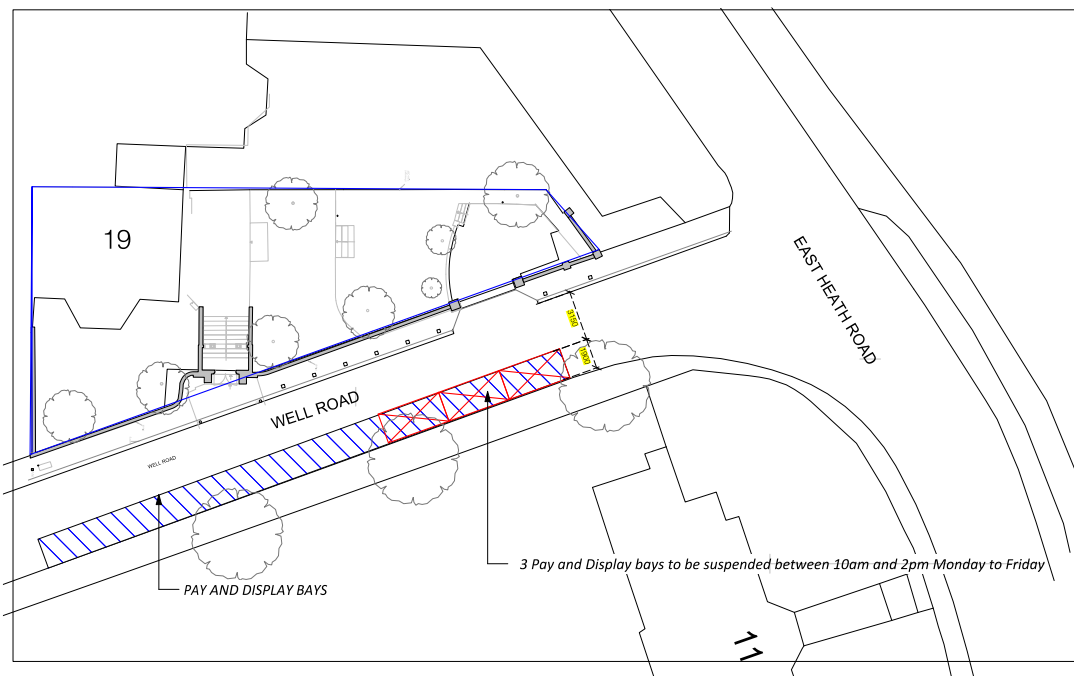
The first parking bay is set back 16 metres from the junction of East Heath Road and is 35 metres long.

These parking bays allow for parking for up to 4 hours.

The photographs on page 13 and 14 show the parking bays.



Plan showing anticipated parking bay suspensions



View of CPZ Notice Location 13806 where bays are to be suspended



PHOTO 1 – View WEST down Well Road with the parking bays on the opposite side of the road to the site entrance



PHOTO 2 – View EAST down Well Road towards East Heath Road with parking bays opposite the front entrance



PHOTO 3 – View North up East Heath Road with Well Road to the left



PHOTO 4 – View South down East Heath Road with Well Road on the right



Development Proposal

This proposal is for the construction of a small extension to the existing basement located below the front garden of the dwelling.

The new extension is separated from the footprint of the house by a short corridor, and it can be seen that the proposed works are relatively modest in size.

Morph Construction Ltd have been appointed to construct the basement and to carry out the works and associated alterations

A project at 14 Well Road was completed in 2020 and note that their CMP was approved. We have adopted part of that same vehicle strategy, recommended by the Local Council, in avoiding the need to access the site from the West along the very narrow residential streets of New End Rd and crossing Christchurch Hill.

Well Road is subject to a 20mph speed limit.

There are no street trees on the north side of Well Road in the vicinity of the site. There are 2 mature trees on the opposite side of the road though these have high canopies.

*Access and Egress will be via East Heath Road.
The junction of East Heath Road and A502 is traffic signal controlled.*

Suspension of the first 3 parking bays opposite the site entrance will be required during the 10 weeks construction phase of the project.

7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

As previously stated, proposed works are modest in size and of a routine nature, presenting no particular challenges for the Contractor.

*The proposal involves the construction of a 30 square meter extension to existing basement of the main house below the East front garden.
With the exception of a JCB micro excavator (JCB 8010) only handheld tools will be used for the excavation.*

The vehicular access and egress is proposed to be via East Heath Road, avoiding the use of the narrow roads to the west of the site.

Removing spoil and the delivery of concrete will take place on the North side of East Heath Road adjacent to the site entrance.

This will be facilitated by a Road Closure and 3no parking bay suspensions opposite no 19 to allow for the delivery of concrete lorries and the lorries removing spoil between 10.00 am and 14.00 pm Monday to Friday only, as shown on the Temporary Road Closure Drawing ref TM1224-01-D-02 attached elsewhere.

Removals and deliveries will be well supervised & permitted only between 10.00 am and 14.00 pm Monday to Fridays with a minimum of 2 qualified banksman in attendance.

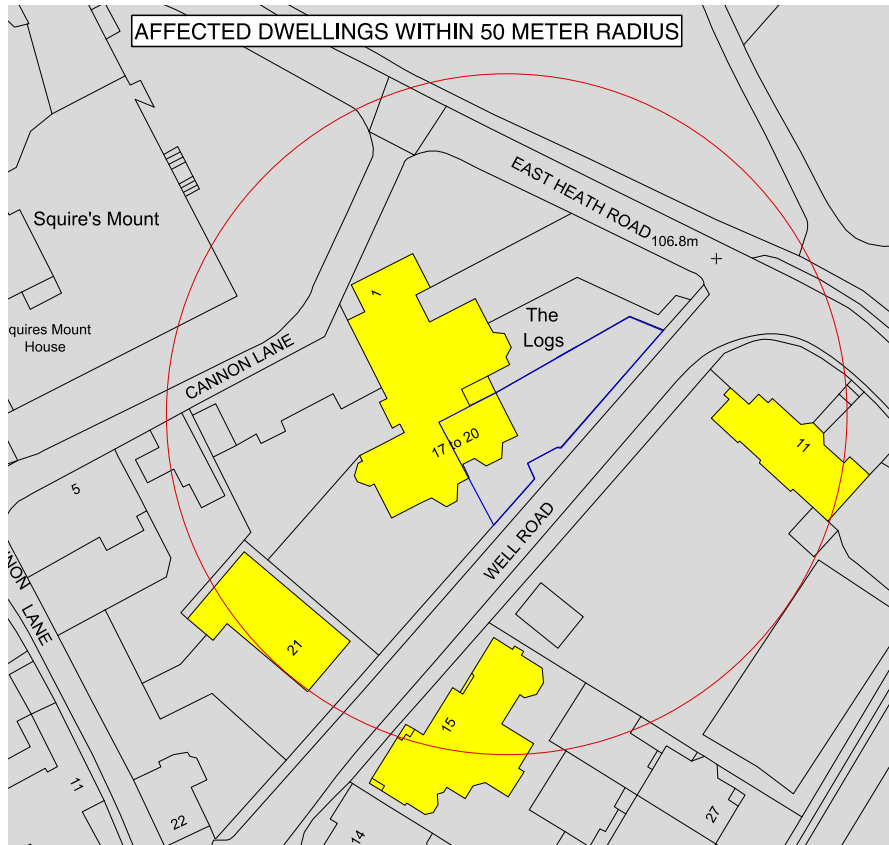
8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

The adjacent residential properties are the nearest potential receptors

Letters were initially delivered (2019) by hand to all dwellings within a 50m radius, (see drawing below) and again sent by Post in May 2021. There have been no responses from all the following recipients.

<i>Mr. G O'Dowd,</i>	<i>18 Well Road NW3</i>
<i>Mr. and Mrs. L Shiff</i>	<i>20 Well Road NW3</i>
<i>And including properties:</i>	<i>15, 17 and 21 Well Road NW3</i>
	<i>1, 2 and 3 Cannon Lane NW3</i>
	<i>11 East Heath Road</i>

The adjacent residential properties within a 50-meter radius



9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes,

PLAN

A scaled plan is provided below

The main access to the site for materials will be via on-street loading/unloading. There are parking bays on the South side of the road. There are no cycle lanes.

SAFETY

A minimum of 2 banksmen / traffic Marshals will be on site from 10:00 to 14:00 Monday to Friday, the only times during which heavy construction vehicles will be scheduled.

In order to maintain pedestrian and cyclist safety, a traffic marshal / banksman strategy will be used.

A Banksman Coordinator will log the movements of all construction vehicles to and from the site.

This strategy will include a banksman / traffic marshal stationed at the junction of East Heath Road and Well Road to meet construction vehicles, the arrivals of which will be scheduled and anticipated.

The banksman / traffic marshal will ensure safe entrance to Well Road and walk alongside the construction vehicle for the 20 meters of Well Road to the site.

When a construction vehicle is arriving or leaving the site, at least 2 banksman / traffic marshals will again walk alongside the vehicle to the junction with East Heath Road, ensuring a walking speed of 4mph along Well Road.

The traffic marshals will be HSE/Road Transport Industry Training Board recognised.

ADJACENT PROPERTY

The adjacent property at no 18 has private driveway that will not be obstructed or used by any of the construction vehicles coming to and from the site. This will be vigorously enforced by the banksman / traffic Marshall.

ACCESS AND EGRESS

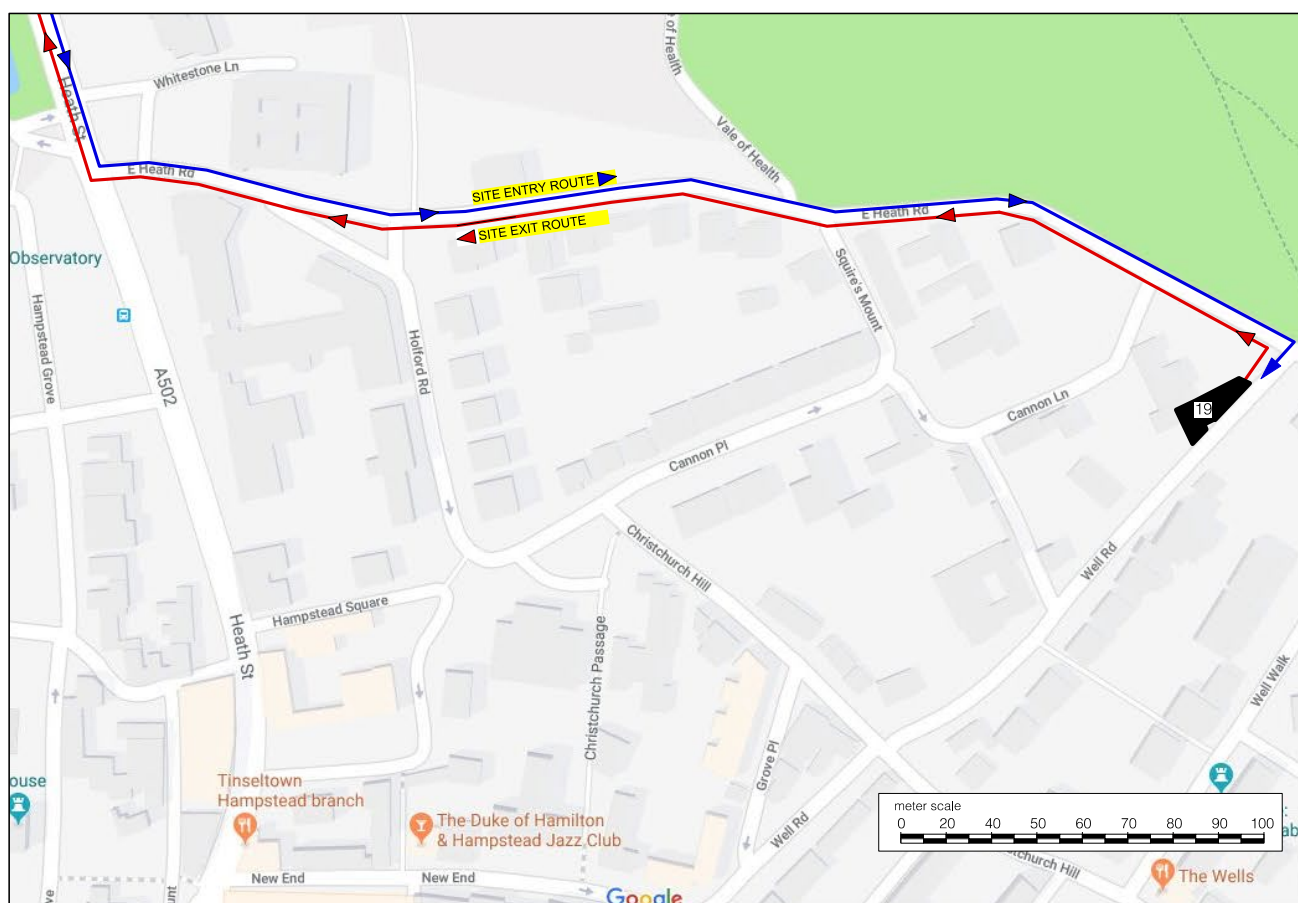
Vehicles will enter and exit via the A406 North Circular Road, which provides access towards the site via the A598 Finchley Road then via A502 /North End Road to North End Way and onto Heath Street.

Passing Whitestone Pond on the right, vehicles will turn left at the traffic lights on Heath St, onto East Heath Road. The vehicles will then turn right into Well Road travelling approximately 20 meters to access the site

This route avoids the narrow journey from Heath Street to site via roads such as New End and across Christchurch Hill

On leaving the site vehicles will reverse onto East Heath Rd and then proceed up East Heath Road and away from the area.

There will usually be 1 but a maximum of 2 heavy vehicles permitted between 10.00 am and 14.00 pm to access the site and only on weekdays.



10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal).

The total works involved are expected to last 28 weeks and will be carried out in 4 phases as follows:

- ▶ *Site Prep and set up – 4 weeks*
- ▶ *Ground Works, excavation, structural - 12 weeks;*
- ▶ *Internal fitting out works - 7 weeks*
- ▶ *Landscaping - 6 weeks (These works simultaneous with the interior fitting out)*
- ▶ *General decorations and other works*

It is intended that Site preparation works will start November 2021. These works will then be followed directly by the construction works, with an intended completion date 3 July 2022. Timescales are only provided on an indicative basis and will be confirmed once a start date has been determined

	DURATION	START DATE	COMPLETION
site prep and protection of assets	4 WEEKS	Nov-21	Dec-21
excavation and structural works	10 WEEKS	Jan-22	Mar-22
landscape and ground level drainage	6 WEEKS	Mar-22	Apr-22
Interior fitout of existing basement	6 WEEKS	Dec-21	Feb-22
Interior fitout of basement extension	8 WEEKS	Mar-22	Apr-22

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- **8.00am to 4.00pm on Monday to Friday**
- **8.00am to 1.00pm on Saturdays**
- **No working on Sundays or Public Holidays**

*Work on site will follow standard working hours for construction sites in Camden.
8.00am to 4.00pm on Monday to Friday
8.00am to 1.00pm on Saturdays (No Noisy works)
No working on Sundays or Public Holidays*

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory

undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

No new services will be required and all services connections will be within the site

Community Liaison

A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft. This consultation must relate to construction impacts, and should take place following the grant of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off. This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings. Please note that for larger sites, details of a construction-working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer

Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements should consider establishing contact with other sites in the vicinity in order to manage traffic routing and volumes. Developers in the Tottenham Court Road area have done this to great effect.

The Council can advise on this if necessary

13. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents prior to submission of the first draft CMP.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. Details of meetings including minutes, lists of attendees etc. must be included.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason should be given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors

Consultation details

An initial consultation process for the proposed works was initially undertaken with the immediately adjacent neighbours in 2019. Letters outlining the project and seeking the views, comments and concerns regarding the proposed works have recently been delivered again to all the properties, which fall wholly, or in part, within a 50-metre radius of the center of the site. These properties are:

- ▶ Nos. 15, 17, 18, 20, 21 Well Road
- ▶ No. 1, 2 and 3 Cannon Lane
- ▶ No. 11 East Heath Rd

It is considered that this represents those properties that would be most directly affected by the building works and for whom noise and nuisance arising from the construction may need to be managed.

Heath and Hampstead Society are also being consulted.

The contractor will keep the residents and others informed of any unavoidable disturbance such as noise, dust or traffic disruption in advance of these items occurring

No significant issues are anticipated from these consultations. Furthermore, as a “Neighbourly” gesture, the contractor has agreed not to undertake “noisy” works on a Saturday. We will be operating an ‘open door’ policy and will welcome our neighbours if they have any concerns etc. The Site Manager will hopefully be able to quickly answer any queries or concerns. An independent consultant will be instructed to deal with community liaison. The client is also keen to have a good and close relationship with the neighbours and is again happy to talk to them where this may be of value.

The Principal Contractor will be:

*Morph Construction
115, Jubilee Avenue
Romford, Essex RM7 9LT
Contact: Rodney Pearce
Email: rodney.morphconstruction@hotmail.com
Mobile: 07845 748711*

*The contractor is registered with the Considerate Contractors Scheme. (Reg no CO 3311)
He will be consulting and notifying our neighbours throughout the project (particularly in respect to major deliveries) and we will be operating an open door policy so that any issues that may arise can be dealt with as quickly as possible.*

In order to minimise the likelihood of any issues arising we have put in place robust measures to control noise, pollution and dust that may adversely impact on the health or amenity of our local neighbours

The CMP is an important document and therefore we have consulted our immediate neighbours and have brought this document to their attention.

The CMP will be amended where necessary in light of any feedback.

A copy of the letters are attached below

April 2021

Dear Neighbour,

COMMENCEMENT OF WORKS AT 19 WELL ROAD

*The purpose of this letter is to inform you of proposed works at 19 Well Road.
The works involve building a basement extension*

*The standard working hours for the site will be as follows:
8.00am to 4.00pm on Monday to Friday
8.00am to 1.00pm on Saturdays with no Noisy works
No working on Sundays or Public Holidays*

The Principal Contractor is:

*Morph Construction
115, Jubilee Avenue
Romford, Essex RM7 9LT
Contact: Rodney Pearce
Email: Rodney.morphconstruction@hotmail.com
Mobile: 07845 748711*

A Construction Management Plan has been prepared to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

Please do not hesitate to contact me if you have any queries or concerns or would like a copy of the Construction Management Plan.

*Kind Regards
Ted Walters
Project Manager
Email: tedcwalters@gmail.com
Mobile: 07831488901*

Copy of this letter that has been sent to
► Nos. 15, 17, 18, 20, 21 Well Road
► No. 1, 2 and 3 Cannon Lane
► No 11 East Heath Rd

November 2021
Dear Neighbour,

Re Access and Egress of Lorries at
19 WELL ROAD, LONDON NW3 1LH during Construction Works:

I am writing to inform you of the proposed three-point lorry turning at the junction of East Heath Road and Well Road.

These turnings are required in order to remove spoil and deliver Concrete and Building materials to the off road parking area at 19 Well Road.

Lorries making deliveries to the site or removing spoil from the site will approach the site traveling down East Heath Road. The vehicles will turn into Well Road to access the site
On leaving the site vehicles will reverse onto East Heath Road and then proceed up East Heath Road and away from the area.

Temporary Road Closure and Parking bay suspensions will be installed commencing 3 January 2022 until 3 July 2022

Two trained and qualified Traffic Marshalls/banksmen will be present at every three-point turn undertaken. This will be in order to provide safety to pedestrians who are in the area at the time of these manoeuvres, and to ensure that vehicles and street furniture are not damaged.

There will be a maximum 2-3 lorries per day making this turning during the 12 week construction phase with the hours restricted to between 10.00 am and 14.00 pm to avoid coinciding with school children on their way to and from school Monday to Friday only, with the expected overall loading/unloading time not to exceed 15 minutes.

The basement construction phase program is 12 weeks long, with a 4-week preparation phase and an overall 28-week project duration.

I have looked closely at Camden Council statement on lorry routes for construction sites and believe the Council will approve the solution that I have included in the required Construction Management Plan; that the Council holds as a live document to help us deliver a project that has as little negative impact as possible on the local community during construction.

Please contact me on 07831 488901 or at my email address tedcwalters@gmail.com or the postal address above if you would like to voice concerns or discuss this further.

Yours faithfully,
Ted Walters Project Manager

This letter to be delivered by hand to the following properties

- ▶ Nos. 15, 17, 18, 20, 21 Well Road
- ▶ No. 1,2 and 3 Cannon Lane
- ▶ No 11 East Heath Rd



14. Construction Working Group

Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works, the way in which the contact details of the person responsible for community liaison will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

A Construction Project Manager (CPM) will be appointed and will be responsible for implementing measures contained in the CMP and will be the point of contact for local residents. The CPM will manage a Construction Working Group, which will liaise with local residents and respond to any concerns that they may have. The CPM will be responsible for monitoring and reviewing this CMP.

Morph Construction will keep in close contact with the surrounding neighbours. The site will operate an open door policy to allow any local residents or neighbours to visit the site office and raise their concerns with the site manager. Information boards with the key staff members and head office details will be on display on the access gate.

A letter to neighbours of the properties listed above have been issued twice prior to the start of work on site, to be followed by a final letter which will include the contact details of the Construction Project Manager and the Emergency contact details. The contact details of the CPM will be displayed on the site frontage as well. A weekly newssheet will be displayed on the site hoarding that will include information on current works and progress, plus details of activities that are planned for the immediate future.

The CPM will ensure that local residents are aware of the programme of works taking place and will give advance notice of any anticipated periods of excessive noise or disruption during the works.

Residents and Representatives of the Heath and Hampstead Society, if required, will be advised of how the construction works are progressing. Local residents will be given the opportunity to be put on a mailing list for progress reports from the CPM so that they are aware of the stages of construction and any upcoming works that may directly affect them.

15. Schemes

Please provide details of any schemes such as the ‘Considerate Constructors Scheme’, such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the “Guide for Contractors Working in Camden” also referred to as “Camden’s Considerate Contractors Manual”.

Morph Construction is a member of the Considerate Contractors Scheme:

The Camden’s Considerate Contractors Manual will be adopted into the site rules for the duration of the project and all contractor and subcontractors will be required to adhere this guide.

16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

There are no existing or anticipated construction projects evident presently within the local area.

In the event of any substantial construction projects starting and taking place in proximity to the site, the CPM will liaise with these sites in order to avoid unnecessary conflicting vehicle movements.

Contact will be made with any other sites and any other consented schemes in the immediate vicinity to ensure co-ordination where required so that any disruption is minimised.

The CPM will liaise closely with the Project Managers of any other construction sites in the immediate vicinity so that consideration can be given to any measures that could be implemented to minimise the cumulative effect of construction work.

Refuse collections within the borough commence [at 7am and take place on Mondays](#). It is not expected that construction and delivery vehicles to the site will coincide with refuse collection activity; however, the CPM will liaise with the local refuse department to ensure that collections are not disrupted.

Transport

This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the CLOCS Standard.

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by the council to ensure compliance. Please refer to the CLOCS Standard when completing this section. Guidance material which details CLOCS requirements can be accessed [here](#), details of the monitoring process are available [here](#).

Please contact CLOCS@camden.gov.uk for further advice or guidance on any aspect of this section.

Please refer to the CLOCS Overview and Monitoring Overview documents referenced above which give a breakdown of requirements.

CLOCS Requirements Compliance Check

On site check

Name of checker:	Date:
Site:	Time:
Driver name:	Vehicle operator:
Employed by:	Delivering on behalf of:
Vehicle registration:	

1. Operations

FORS¹ status: Registered ☐ Bronze ☐ Silver ☐ Gold ☐ FORS ID no: Expiry date:

Pass ☐ Fail ☐ Comments

¹ Fleet Operator Recognition Scheme

2. Vehicle

	Fitted	Working
1 Class V + VI mirrors	<input type="checkbox"/>	<input type="checkbox"/>
2 Close proximity warning system, and/or camera system and/or vision-aid	<input type="checkbox"/>	<input type="checkbox"/>
3 Side guards	<input type="checkbox"/>	<input type="checkbox"/>
4 Audible left turn warning	<input type="checkbox"/>	<input type="checkbox"/>
5 Warning signage	<input type="checkbox"/>	<input type="checkbox"/>

Pass ☐ Fail ☐ Comments

3. Driver

Licence In date: ☐ Category: Invalid/no licence carried: ☐

VRU Training Safe Urban Driving: ☐ Other approved: ☐ None: ☐

Pass ☐ Fail ☐ Comments

Action taken on site

Refused access: ☐ Allowed access: ☐

Letter to driver: By (name):

Send completed form to:

CLOCS Requirements Non-Conformance Report

Follow up action

Name:

Department:

Date:

Actions taken

Action	Satisfactory Response	Notes and actions
Letter /email to supplier Addressed to:	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Meeting with supplier Present:	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Commercial action via contract Other follow up actions:	<input type="checkbox"/> Yes <input type="checkbox"/> No	

Approval and closure

Note/comment

Has root cause been identified? Yes <input type="checkbox"/> No <input type="checkbox"/>	
Have preventative measures been put in place? Yes <input type="checkbox"/> No <input type="checkbox"/>	
Closed: <input type="checkbox"/> Date:	Approved by:

Send completed form to:

CLOCS Considerations

17. Name of Principal contractor:

*Morph Construction
115, Jubilee Avenue
Romford, Essex RM7 9LT*

18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our CLOCS Overview document in the appendix and CLOCS Standard point 3.4.7).

It is a requirement on this project that all deliveries will have FORS Bronze accreditation as a minimum, and FORS Silver or Gold operators will be appointed where possible.

Where FORS Bronze operators are appointed, written assurance will be sought from contractors that all vehicles over 3.5t are equipped with additional safety equipment, and that all drivers servicing the site will have undertaken approved additional training (e.g. SUD, eLearning, Van Smart, on-cycle training etc). CLOCS Compliance will be included as a contractual requirement.

Desktop checks will be made against the FORS database of trained drivers and accredited companies as outlined in the CLOCS Standard Managing Supplier Compliance guide.

Checks of FORS ID numbers will form part of the periodic checks and will be carried out as per an appropriate risk scale. Random spot checks will be carried out by site staff on vehicles and drivers servicing the site at a frequency based on the aforementioned risk scale. Results from these checks will be logged and retained, and enforced upon accordingly. Where the contractor's own vehicles and drivers are used the above approach will be modified accordingly.

As the number of vehicles visiting the site will be low, the banksman / traffic marshal will fill in a CLOCS compliance check form for each delivery.

If there are any non-compliances the driver and the supplier will be informed and asked to ensure future deliveries occur in compliant vehicles.

If non-compliance continues the supplier will be informed that they will no longer be used. All forms will be retained for inspection.

Collision reporting data will be requested from operators and acted upon when necessary.

19. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts. Please sign-up to join the CLOCS Community to receive up to date information on the standard by expressing an interest online.

I confirm that the developer, the principal contractor and all suppliers have read and understood the Construction Logistics and Community Safety Standard documents

Please contact CLOCS@camden.gov.uk for further advice or guidance on any aspect of this section.

Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

20. Traffic routing: "Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur." (P19, 3.4.5)

Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

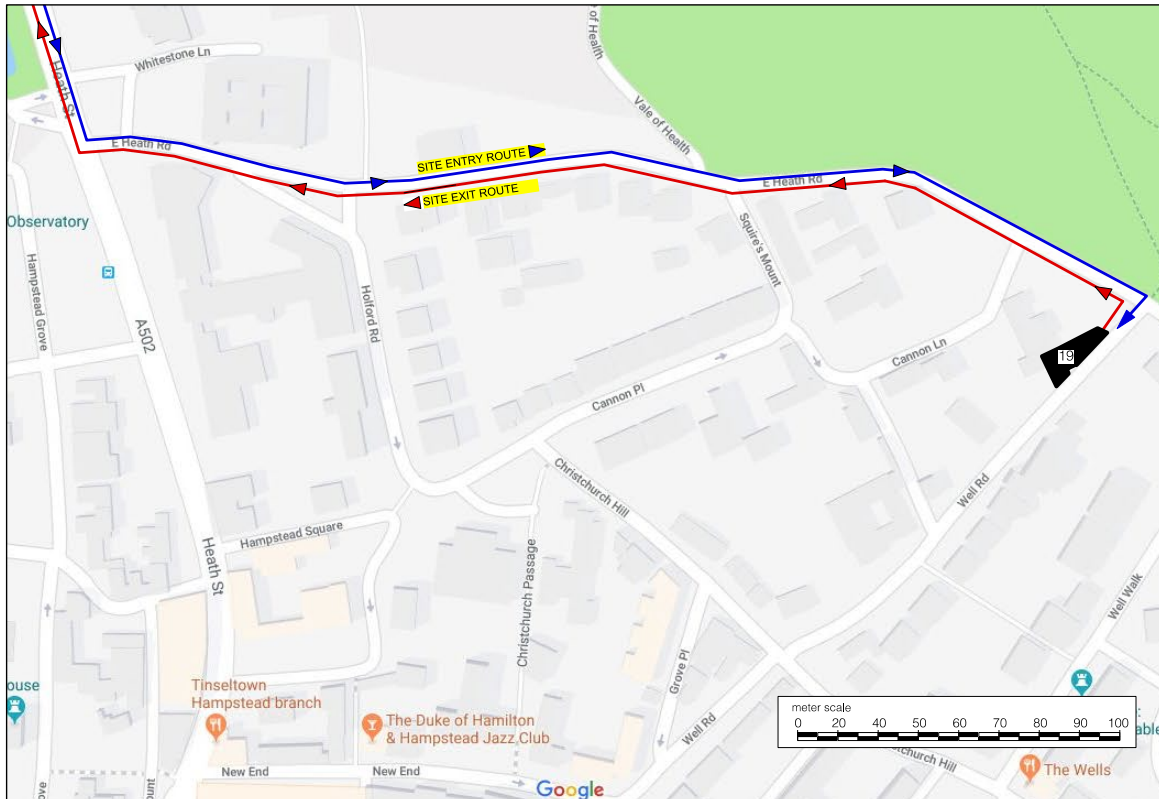
Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of links to the Transport for London Road Network (TLRN).

Network of roads in the vicinity of the site



*Heavy vehicle access and egress route from the site avoiding the narrow
Streets of New End and crossing Christchurch Hill*



b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

All contractors, delivery companies and visitors will be emailed the relevant access criteria by the principle contractor prior to their visiting the site.

The criteria will also be attached to every order placed with a sub-contractor.

Details of the access routes will be provided to all suppliers when placing orders and when arranging for visitors to the site. A strict delivery procedure will be followed for all parking and loading arrangements.

Designated site operatives will ensure that any inconvenience to other road users (drivers, cyclists and pedestrians) is kept to a minimum.

All subcontractors and suppliers will be required to give 48 hours' notice of deliveries. Deliveries will be allocated time slots to ensure good control and coordination and to minimise the chance of any disruption to other road users.

The designated site operative will be responsible for the movement of materials from delivery vehicles to the site. Where necessary site operatives will control deliveries along the footway to ensure pedestrian safety is maintained at all times.

All materials will be stored within the site boundary. No materials will be stored on the public highway.

No parking will be permitted within the site, as no spaces will be available

All sub-contractors will be informed that parking controls restricts all roads in the area and they will be encouraged to use public transport.

Hampstead underground station is a short walk from the site.

21. Control of site traffic, particularly at peak hours: “Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)

Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the *Guide for Contractors Working in Camden*).

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell t may require vehicle-holding procedures.

The following vehicles are ONLY permitted to load or unload between the hours of 10.00 am and 14.00 pm Monday to Friday and to take a maximum of 15 minutes

Concrete lorries – standard ready mixed lorry with an approximate size of 8.25 m long by 2.45m wide;

Grab Lorries (approx. size 8.1m long and 2.6m wide)

The following vehicles permitted to load or unload between the hours of 9.30 am and 15.30 pm Monday to Friday

Flat bed delivery vehicle – the flat bed vehicles will be used to deliver various materials including timber, reinforcement, brick and block work, plaster, joinery etc. The approximate size will be 7m long by 2.25m wide;

Box van (Luton) – these will be utilised for all ancillary deliveries and small components, the approximate size of this vehicle is 4m long by 2m wide; and

It is not anticipated that this scheme will require more than 2/3 collections &/or deliveries a day on average, during the 12-week construction phase.

The grab lorries will be able to load within 15 minutes.

Nearly all deliveries are also expected to last less than 15 minutes.

No vehicles will enter the site and there will be not be a tight manoeuvres required for the site access or servicing.

b. Please provide details of other developments in the local area or on the route.

There are no construction projects visible at the present time in the vicinity of the site

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

Deliveries will be allocated time slots to ensure good control and coordination and to minimise the chance of any disruption to other road use

*All subcontractors and suppliers will be required to give 48 hours' notice of deliveries.
As strict delivery procedure will be followed for all parking and loading arrangements.
All deliveries will be by appointment with telephone updates of any delays.*

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

Due to the limited number of deliveries and the booking system, no off-site holding area will be required.

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of construction material consolidation centres).

The works are of small scale and will not require the use of a Construction Material Consolidation Centre.

All deliveries will be planned and coordinated by the principle contractors site manager to ensure only one vehicle will access the site at a time.

In order to reduce the impact of associated traffic, where possible local suppliers will be used to reduce overall traffic distance by construction related vehicles.

Site Waste Management Plans (SWMP) have been developed to ensure that site waste is managed effectively through the course of demolition and construction. By virtue of managing the amount of waste produced, it will help reduce the potential harm to both the environment and to human health.

Site Waste Management Plan to be provided

Responsibility for the updating of the SWMP can transfer between individuals and organizations during these different stages.

A thorough approach to site waste management planning will produce a structured plan that can perform three different roles:

- 1. To set out the importance of effective waste management and how this is enshrined in legislation, policy and guidance at all levels from European down to local, and summarizing those that are most important and relevant to the scheme;*
- 2. Identify the baseline conditions and set out waste management issues at each of the three main stages – design, construction and operation, and establish a framework for further development of the Site Waste Management Plan which will continue to be a live document throughout the process of construction;*
- 3. To analyse these waste management issues in terms of any significant environmental effects and how the scheme has been able to avoid, minimise or mitigate against such environmental effects*

22. Site access and egress: “Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.” (P18, 3.4.3)

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and other traffic when vehicles are entering and leaving site, particularly if reversing.

a. Please detail the proposed access and egress routes to and from the site

(See DRG NO CMP 004 below)

ACCESS

Vehicles will enter and exit via the A406 North Circular Road, which provides access towards the site via the A598 Finchley Road then via A502 /North End Road/North End Way and onto Heath Street.

Passing Whitestone Pond on the right, vehicles will turn left off at the traffic lights on Heath St, onto East Heath Road. The vehicles will then turn right into Well Road travelling approximately 20 meters to access the site

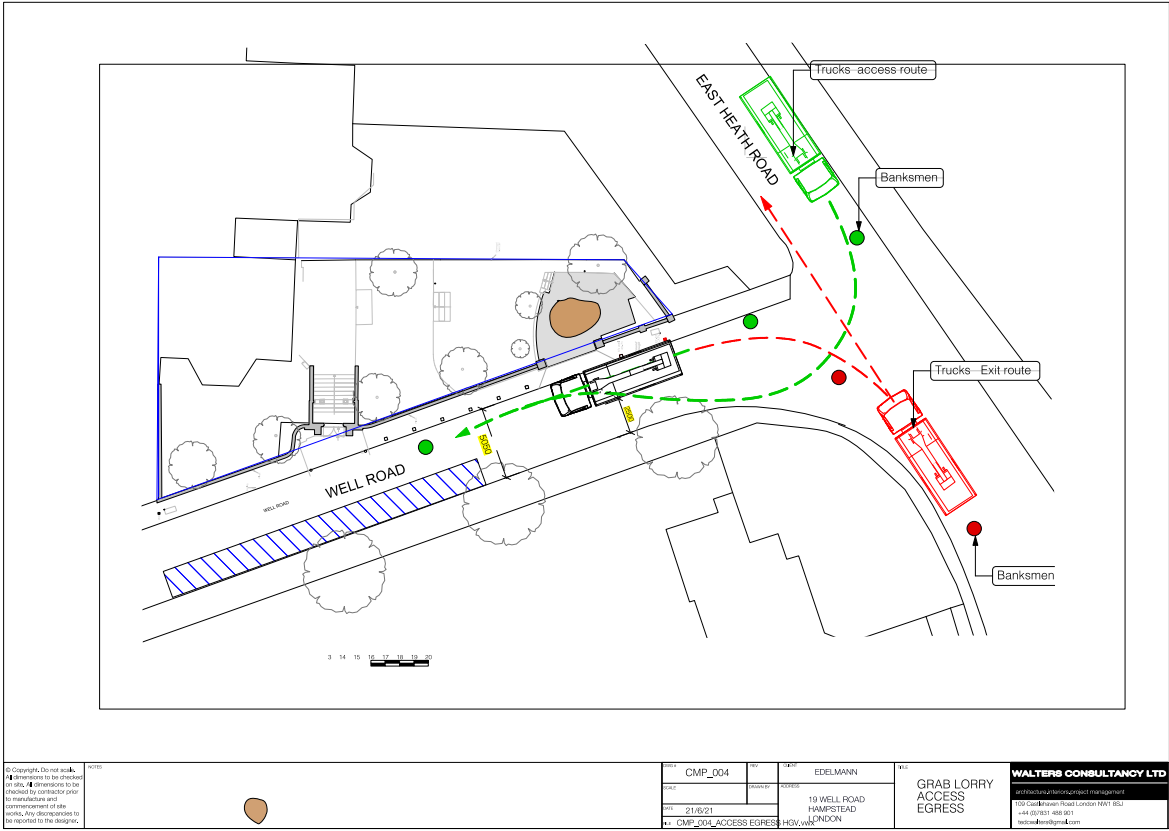
This route avoids the narrow journey from Heath Street to site via roads such as New End and across Christchurch Hill

EGRESS

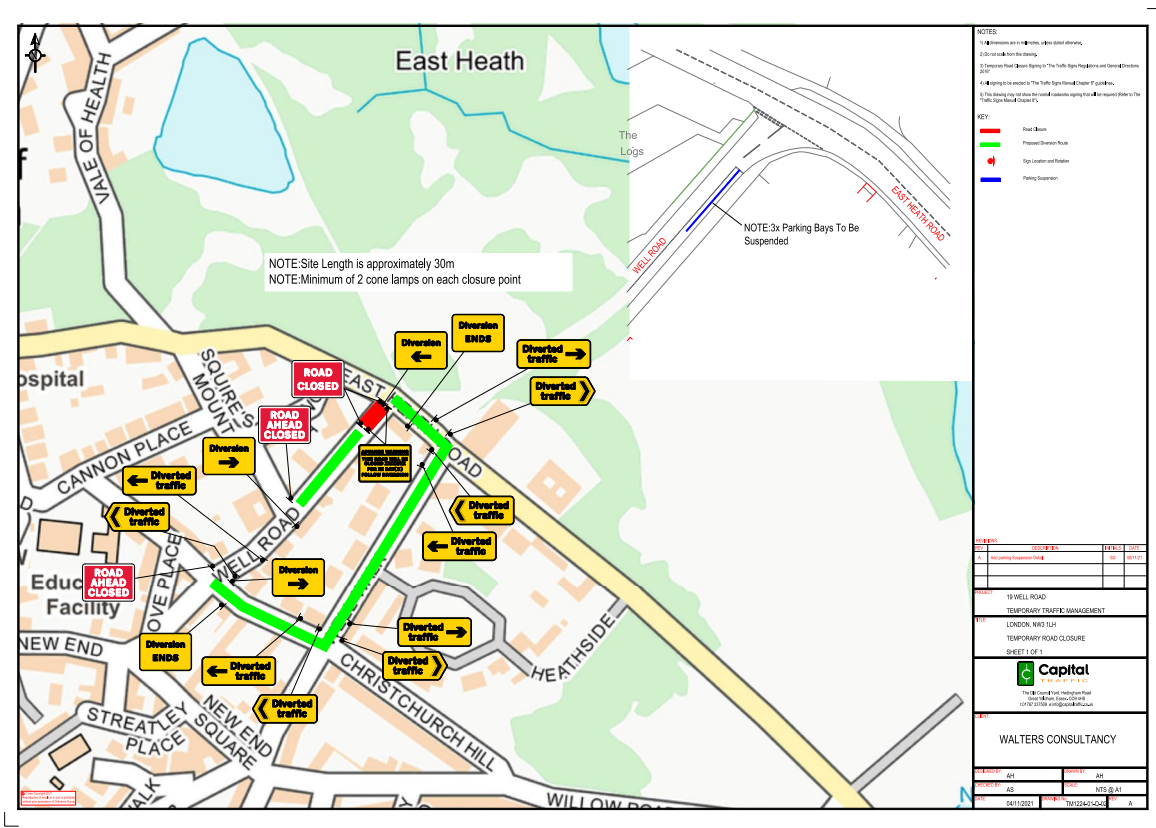
The proposal is with the assistance of 2 banksmen, for the vehicles to leave Well Road by reversing the short distance to and then down into East Heath Road and then proceed North up towards Heath Street and leave the area in the direction that they arrived.

(Tracking has shown that if vehicles turn left out of Well Road, they would be required to cross both carriageways of East Heath Road to undertake the manoeuvre.

ACCESS AND EGRESS OF HEAVY VEHICLES (Suspended parking bays not shown)



TEMPORARY ROAD CLOSURE PLAN



b. Please describe how the access and egress arrangements for construction vehicles will be managed.

GENERALLY

A strict delivery procedure will be in place for all loading and unloading arrangements

Once the vehicles are in place, designated site operatives & banksmen will undertake to install Temporary Road closure signs and barriers close, as per the Traffic management plan (attached)

Whilst loading and unloading is carried out banksmen will ensure that any inconvenience to drivers, pedestrians and cyclists is kept to a minimum. See Traffic management Plan dwg attached

All subcontractors and suppliers will be required to give 48 hours notice of their deliveries. All deliveries will be given precise time slots to minimize the chance of disruption to traffic flow. Deliveries of concrete and the loading of spoil onto the grab lorries will be undertaken at the kerbside during the construction phase

Loading and unloading on site will be permitted within the on site parking area during the 4-week set-up phase and the 7-week fit-out phase

All materials will be stored within the site boundary and none on the public highway.

HEAVY GOOD VEHICLES

All HGV arrivals and departures limited to between 10.00 and 14.00 Monday to Friday to avoid peak periods on the local road network.

ACCESS AND EGRESS

All manoeuvres will be undertaken with the assistance of a minimum of 2 traffic Marshalls to ensure the safety of other vehicles, people, trees and street furniture.

SAFETY

Additional traffic Marshalls / banksmen will be employed by the principle contractor to reduce conflict between construction vehicles and vulnerable road users such as pedestrians and cyclists.

The delivery of materials, collection of waste and delivery/collection of machinery will be scheduled and managed by banksmen.

Deliveries will be booked a minimum of 24 hours in advance and allocated a time slot. Any vehicles not booked in via this system will be turned away.

In addition all deliveries will be 'Just In Time' to minimise waiting/unloading times.

All drivers will be provided with instructions on approach routes and mobile phone contact details of the banksman who will be put on 'stand-by' to receive a vehicle.

Suppliers will be expected to call the site approximately 20 minutes prior to arrival at the site to ensure the loading area is available

c. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).

The access route has previously been checked as part of the CMP for 14 Well Road. While fairly tight, the access for construction vehicles to and from the site is acceptable. As such no further swept path drawings have been prepared.

d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled.

No wheel washing is expected to be required.

It is not anticipated that the works will result in significant quantities of dirt or debris being transferred onto the public highway.

The existing off street parking area will be utilized for storing the excavated spoil awaiting removal and kept clear of dirt by regular cleaning.

During the excavation phase (when dirt and dust are most prevalent), grab lorries will not enter the site

At the end of every day all relevant areas will be brushed and washed using an existing channel drain and a temporary silt trap, which will be regularly emptied. The footway and carriageway to the front of the site will also be cleared at the end of every working day.

23. Vehicle loading and unloading: “Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required

It is not possible for heavy vehicles to be loaded or unloaded on-site especially during the construction phase.

The available on-site parking area will be utilized for loading and unloading during the first 4 week set-up phase of the project and then again during the 7 week fit-out phase following the construction phase

As previously mentioned, both the house and the garden wall are Listed Grade II.

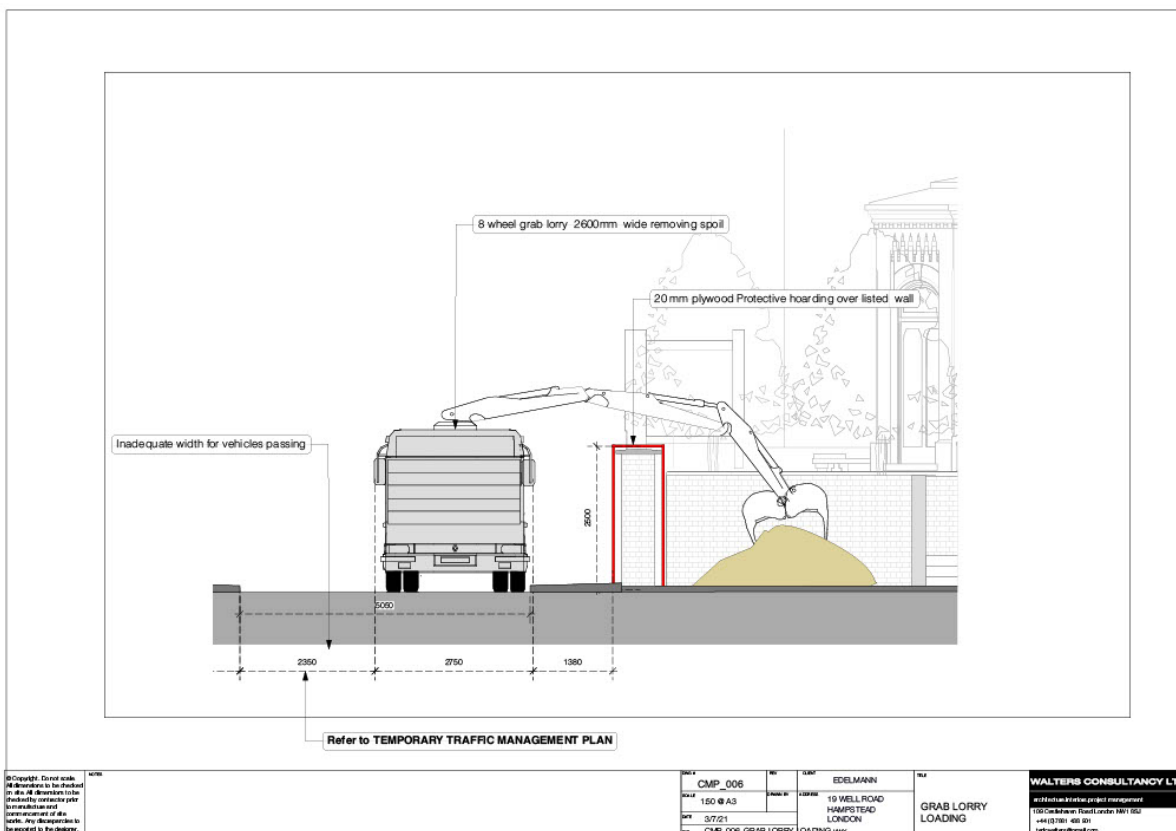
A key aspect of the CMP will be to ensure that the garden wall is protected throughout the construction phase. Therefore the full length of the garden wall will be protected with plywood cladding to its full height for protection throughout the construction phase.

The sketch below shows the on site area which will be used to store the spoil and the position of the grab lorry and method of spoil removal.

The concrete delivery lorries and grab lorries will use the same position on the North side of Well Road area directly in front of the existing off road parking area as shown in the drawings provided.

Traffic Marshall / banksmen will be employed to ensure that pedestrians and cyclists can pass deliveries safely

As already stated delivery vehicles will not enter the site during the construction phase.



© Copyright. Do not scale. All dimensions to be checked in the field. Dimensions to be checked prior to construction and compliance with all local. Any discrepancy to be reported to the Designer.

CMP_006
150 @ A3
3/7/21
CMP_006 GRAB LORRY

EDLMANN
19 WELL ROAD
HAMPSTEAD
LONDON
LOADING VIEW

GRAB LORRY
LOADING

WALTERS CONSULTANCY LTD
a Holme & Knight project management
100 Cambridge Road, London N1 1 5GJ
+44 (0)20 433 433
info@wcl.co.uk

Highway interventions

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain Temporary Traffic Order (TTO) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.

Information regarding parking suspensions can be found [here](#).

3 parking spaces are to be suspended as part of the road Closure from 3 Jan 2022 to accommodate the loading of the excavated spoil and delivery of concrete

There will be no scaffolding or cranes and therefore no overhang to the public footpath

25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

- a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).**

No highways works are required for the construction works to take place.

- b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.**

This not applicable

26. Diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

This not applicable

27. VRU and pedestrian diversions, scaffolding and hoarding

Pedestrians and/or cyclist safety must be maintained if diversions are put in place.

Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway. Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc. A secure hoarding will generally be required at the site boundary with a lockable access.

a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.

In order to ensure this is done with maximum safety for pedestrians, all deliveries will be managed and controlled by site staff (banksman) who will ensure all movement of goods/supplies is carried out safely, maintaining secure routes for pedestrians. The works will not affect cyclists. The banksman / traffic Marshall will also ensure no conflicts occur with cyclists along Well Road at the time of deliveries. Due to the delivery times being scheduled and anticipated the banksman / traffic Marshall will walk alongside the vehicle from the junction of East Heath Road to the site and after the delivery will escort the vehicle to the junction with East Heath Rd ensuring a walking speed of 4 miles per hour is maintained. The timing of deliveries should ensure that any disruption/congestion is kept to an absolute minimum. Deliveries will be programmed to only take place in the off-peak 4-hour period. In particular, it has been established that the removal of spoil and the limited concrete deliveries will be carried out outside the peak period and outside of school arrival and departure times. Whenever possible, all site deliveries will consist of full loads rather than part loads in order to reduce the overall number of trips. All workers and subcontractors will be encouraged to travel to site by public transport. As no parking will be provided for workers, it is not anticipated that these will generate vehicular movements. Pedestrian footways will be kept clear of materials at all times and the banksman / traffic Marshall will ensure that during times of deliveries, pedestrian access along the footway is maintained and will manage any potential conflicts with materials being transferred into the site. Loading will pause to allow passage of pedestrians should this be necessary.

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

No temporary structures will overhang the public highway (e.g. scaffolding, gantries, cranes etc.).

Environment

To answer these sections please refer to the relevant sections of Camden's Minimum Requirements for Building Construction (CMRBC).

28. Please list all [noisy operations](#) and the construction method used, and provide details of the times that each of these are due to be carried out.

Apart from a mini-piling rig (i.e. Klemm 702) and micro-excavator KCB8010 it is anticipated that only handheld tools will be used.

The quietest and newest vehicles/plant machinery shall be used at all times. All vehicles and mechanical plant used for the purpose of the works shall be fitted with effective exhaust silencers, shall be maintained in good and efficient working order and operated in such a manner as to minimize noise emissions.

All materials will be reduced to a suitable size for removal, loaded into barrows and moved to the collection point.

The Best Practicable Means (BPM), as defined in Section 72 of the Control of Pollution Act 1974, shall be employed at all times to reduce noise (including vibration) to a minimum, with reference to the general principles contained in British Standard BS5228: 2009 'Noise and Vibration Control on Construction and open sites.

29. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

Environmental Noise Survey

KPAcoustics have carried out a noise survey (in accordance with ISO 1996-2:2007 Acoustics): and a copy of the report is attached.

The scope of construction work is limited, but it is fully acknowledged that Noise, Dust and Vibration can cause real nuisance to neighbours and lead to complaints.

The contractor is experienced in working in sensitive locations and will have done so for many years without problems arising.

In order to minimize the likelihood of any issues arising we will put in place robust measures to control noise, pollution and dust that may adversely impact on the health or amenity of our local neighbours.

The Acoustic Consultant for the project is:

Aiden Tolkien

KP Acoustics

1 Galena Road

London W6 0LT

020 8222 8778

30. Please provide predictions for noise and vibration levels throughout the proposed works.

*KPAcoustics has undertaken a Noise Impact Assessment and Propagation Modeling with a proposal to monitor emissions from the site at 2no locations, a copy of the report is attached.
I no Noise monitoring sensor and 1 no vibration monitoring sensor
It is not anticipated that noise levels will exceed indicative 75dB action level and in line with the recommended levels in BS 5228-1: 2009 Annex E for a residential area.*

Where the measured noise levels are more than 3dB (A) above the maximum indicative 75dB action level or in the event of a complaint of noise an investigation shall be carried out to ascertain the reason for eclipsing the acceptable level, or the complaint and to check that Best Practicable Means are being used to control the noise. Noise levels shall be reduced further if it is reasonably practicable to do so.

31. Please provide details describing mitigation measures to be incorporated during the construction/demolition works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

Noise and Vibration Mitigation:

The contractor will have built up a substantial amount of experience in reducing noise and vibration by taking advice from specialist companies, and by implementing the mitigation measures and procedures as follows:

All hand operated tools and equipment shall be effectively silenced and will bear the manufacturers guaranteed maximum sound level generated.

- Any noise emitting equipment on site that is required to run continuously will be housed in a suitable acoustic enclosure.*
- Machines in intermittent use will be shut down in the intervening periods between works or throttled down to a minimum.*
- The use of and noise from, percussive tools will be limited as far as reasonably possible.*
- The hoarding (N/a) erected around site will also help to reduce noise transmission.*
- Excavators will be fitted with hydraulic pulverisers and shears whenever possible in preference to hydraulic hammers.*
- All plant and machinery will be fitted with silencers and where hydraulic hammers are used they will be fitted with bafflers as per BS 5228-1: 2009.*
- Sound reduced compressors will be used and/or fitted within acoustic enclosures where necessary.*
- The positioning of compressors will also be taken into consideration to reduce noise transfer to neighbouring properties.*
- Pneumatic tools will be fitted with silencers or mufflers*
- Electrically powered tools will be used as opposed to petrol/diesel powered, wherever possible.*
- No personal audio equipment will be allowed on site e.g. radio.*
- Acoustic blankets will be employed where necessary on the party wall.*

Visual assessments on dust levels will be taken on a daily basis by the works manager and recorded in the site diary. Should noise/vibration/dust complaints arise from the building construction/building works, these complaints must be recorded in a complaint's register and made available to the Local Authority, if requested. The complaint register shall provide information on day, time, details of complaint, details of monitoring carried out and any additional mitigation works.

As mentioned above, the proposed works are modest in size with a high brick wall around the site that will greatly reduce environmental risk and impact.

If any complaints are received, they would be reviewed by the Project Manager and also by an independent person. Remedial measures will be put in place to ensure further complaints are not received. The CMP will then be amended accordingly.

The project will be registered with the Considerate Contractors Scheme. As part of this we will be consulting and notifying our neighbours throughout the project (particularly in respect to major deliveries) and we will be operating an open door policy so that any issues that may arise can be dealt with as quickly as possible.

If any concerns are raised we will contact our specialist acoustic consultant to assess the situation and to advise the most appropriate way in which any problems can be addressed.

Due to the residential nature of the area, existing noise levels are expected to be low.

Noise levels from construction during the working day will be monitored against indicative 75dB action level and in line with the recommended levels in BS 5228-1: 2009 Annex E for a residential area. Most works will also be carried out internally or below ground that will greatly reduce environmental risk and impact.

Noise levels will be monitored during construction as follows:

- Noise and Vibration monitoring will be carried out regularly, as well as in response to requests/complaints or any new activities that have the potential to generate significant noise.
- Checks will be made on method statements to ensure that the best practice described in the standards is being applied in the method and site activities.

Noise attenuation screening will be used if deemed appropriate. Any mobile screens shall have sufficient mass so as to be able to resist the passage of sound across the barrier and to be free of significant holes or gaps between or under any acoustic panels or board materials as far as reasonably practical. Due to the nature of the works no noise attenuation screening is currently felt to be required.

Occupiers in the vicinity who may be affected by noise from these works shall be notified of the nature of the works, a contact name, telephone number (including that to be used outside normal working hours), and address to which any enquiries should be directed. Such notification shall take place, where possible within, 2 weeks but, in any event, at least a week prior to the works commencing.

32. Please provide evidence that staff have been trained on BS 5228:2009

All the recommendations in BS 5228-1 2009 "Codes of Practice for Noise and Vibration Control on Construction sites will be adopted by the Contractor and subcontractors

33. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

Best Practice Means (BPM) will be used to ensure that dust does not cause nuisance. Where dust is considered to be a risk during a specific site activity, mitigation measures will be included in the task specific method statement for the work. The controls listed in the method statement will be assessed on site to ensure they are adequately carried out and effective. The controls will be briefed to the engineers and operatives to ensure they are aware of mitigation measures and controls to be employed. During demolition the following controls will be implemented:

- *Monaflex sheeting will be placed to screen the demolition where possible.*
- *A soft strip of materials will be carried out prior to structural demolition.*
- *Materials will be removed from site as soon as possible for appropriate recycling and disposal.*
- *Drop heights will be minimized as far as possible.*
- *A water spray will be used to control dust.*

Mitigation measures to ensure dust is kept to a minimum will include:

- *Large stockpiles of materials will be avoided and are not anticipated due to the nature of the project and the restrictive available area during the initial excavation.*
- *Use dust screening where possible.*
- *Damping down the areas with water to suppress the dust whilst ensuring the application does not create excessive mud.*
- *Construction plant will be well maintained and operated to minimize emissions.*
- *Good housekeeping including the regular sweeping of floors will be maintained and debris disposed of in enclosed skips.*
- *Outer surfaces of skip will be washed before leaving site. Vehicles shall not enter or leave site via road in any but exceptional circumstances.*
- *Equipment and techniques such as dust extractors will be used to minimize dust when using cutters and saws.*
- *Portable knapsack dust suppressors will be employed on floors.*
- *The Environmental Advisor will brief operatives on good practice and will carry out regular inspections to ensure that BPM is employed across the project.*
- *Wind conditions will be taken account of when arranging activities that are likely to emit aerosols, fumes, odours and smoke.*

Materials will be pre-fabricated and pre-cut off site where possible to minimize dust from cutting and grinding activities.

If cutting and grinding cannot be mitigated off site then water suppressant systems and or local exhaust ventilation will be employed.

In terms of our Strategy we have taken due regard of the Mayor's Best Practice Guidance on Control of Dust and Emissions.

Visual assessments on dust levels will be taken on a daily basis by the works manager and recorded in the site diary

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Dirt and dust on the public highway will be greatly restricted, as no vehicles will access the site. Waste material will be transported directly from the site into grab lorries. All muck away lorries will be covered to reduce the risk of debris falling onto the highway.

If conditions require, dust will be suppressed by spraying with water. However, as there is only limited demolition and no site traffic, it is felt that this is unlikely to be required. In the unlikely event of materials being deposited on the public highway, immediate action will be taken by site staff to safely remove the material. If a large spill did occur, an approved road sweeper will be hired to remove the material and clean the public highway.

As no vehicles will enter the site, no wheel washing facilities will be provided. It is not anticipated that the works will result in significant quantities of dirt or debris being transferred onto the public highway. The loading / unloading area in the location of the off street area will be kept clear of dirt by regular cleaning.

35. Please provide details describing arrangements for monitoring of noise, vibration and dust levels.

As previously stated KPAcoustics HAVE COMPLETED the initial Noise Survey and Environmental Monitoring during the construction works and a copy is attached.

*Noise monitoring will be conducted from **the start of the excavation through** all noisy work on site. Monitoring will cease when noise levels have dropped for a significant period of time. This is anticipated to be after the new basement has been made watertight and when the windows and doors have been fitted, but in any case will be determined by monitoring data.*

Monitoring locations will be determined to reflect the potentially most noise impact from the construction/demolition.

A report containing a site map showing noise monitoring locations will be included as a PDF

Class 1 integrating logging sound level metres will be installed and calibrated (before and after) with a Class 1 acoustic calibrator. The instrumentation will be fully calibrated by the manufacturer, or other approved body, as required by the British Standard, with current calibration certificates.

Alert threshold response would be available by SMS text/e-mail facility once pre-defined trigger noise levels are exceeded.

Monitoring data will be downloaded from metres, and normal operation of the meters will be verified, on a regular basis via a remote modem link. Calibration would be checked during periodic visits to site. Weekly summary reports will be issued to relevant parties by email, as required.

All data will be remotely managed so that the involvement of non-acoustically trained staff is minimised. Raw data will be collected on a remote server and treated according to the specific customer requirements, therefore providing a highly flexible solution. All data will be accessible through a secure website, with individual access to the end-user.

Prior to the commencement of the whole monitoring exercise, we would recommend organizing a meeting with all related parties in order to agree on what type of data would be displayed on the dedicated website.

It is not anticipated that 1no Vibration monitoring sensor is installed.

Dust levels will be visually monitored and noted in daily site notes by the CPM.

36. Please confirm that a Risk Assessment has been undertaken at planning application stage in line with the GLA's Control of Dust and Emissions Supplementary Planning Guidance (SPG), and the risk level that has been identified, with evidence. Please attach the risk assessment as an appendix if not completed at the planning application stage.

This was not required at the planning stage because this development was not considered to be of sufficient scale to require this assessment.

The Risk of Dust Impacts is Low for the Earthworks, Construction and vehicle trackout phases.

As mentioned above, the work will not involve major demolition.

37. Please confirm that all of the GLA's 'highly recommended' measures from the SPG document relative to the level of risk identified in question 36 have been addressed by completing the GLA mitigation measures checklist.

Because of the scale of the development, the GLA mitigation measures checklist has not been completed.

The Contractor will:

Take into account the impact of air quality and dust on occupational exposure standards to minimize worker exposure and breaches of air quality objectives that may occur outside the site boundary, such as by visual assessment; and keep an accurate log of complaints from the public, and the measures taken to address any complaints, where they were required.

- **38. If the site is a 'High Risk Site', 4 real time dust monitors will be required. If the site is a 'Medium Risk Site', 2 real time dust monitors will be required. The risk assessment must take account of proximity to sensitive receptors (e.g. schools, care homes etc), as detailed in the SPG. Please confirm the location, number and specification of the monitors in line with the SPG and confirm that these will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.**

This is a small domestic project and as such is a low risk site.

39. Please provide details about how rodents, including rats, will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

No survey to determine rodent infestation levels has been carried out, as the home owner/occupier of 5 years has no evidence of rodents within the premises, however a Pest Management Scheme will be put in place prior to commencement of works:

A Pest control program will be initiated six weeks prior to the commencement of works; professional pest control staff will survey the property in order to identify the presence and extent of any infestations.

Rats live in sewers and move through drainage systems. They come to the surface and enter buildings through breaks and faults in the drainage system.

Capping of drainage systems will be carried out where appropriate to isolate old redundant sewers /drains,

Redundant drains and sewers will be grubbed out and the connection with the sewer effectively sealed.

Live sewer connections will be appropriately sealed and capped while construction works are in progress to prevent rat egress from the sewers.

To prevent rat egress from live drains and sewers to new systems, the live systems will be temporarily sealed off with expanding drainage stoppers until connection to new drainage is completed.

Pest monitoring and baiting programs will be instigated on site, including a proactive surface monitoring baiting program during the demolition / construction process. Exposure of construction staff to risks associated with a rodent infestation may contravene the Health and Safety at Work, Etc. Act 1974.

An all-encompassing sewer rat-monitoring program will be implemented in the sewer systems within the project area for the duration of the project.

Sewers and drains will be cleared of any remaining building debris.

While carrying out the connection of new drains to the existing system, any exposed drain shall not be left overnight without capping with a drain stopper to prevent any rodents using the drain run

Site hygiene

Contractors will ensure that construction sites are kept as clear and tidy as possible. Accumulations of surplus or damaged building materials can act as harborage for pests, and should be removed and disposed of promptly and safely.

Construction staff will not leave food debris within buildings under construction, as this will encourage pests to become established.

Staff site facilities including canteens, accommodation and sanitary provision will be constructed and maintained in a clean and hygienic manner and in accordance with relevant regulations and codes of practice. Waste must be stored safely in suitably located, pest-resistant, closable containers and removed frequently.

Compliance Monitoring

Information relating to site inspections, copies of receipts and details of any work carried out will be retained and provided to the Local Authority upon request.

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

The latest RAD survey was carried out on 26 March 2021 with no Asbestos material reported. 95% of the proposed construction works will be carried out below the existing garden

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

An outside on-site smoking area will be provided and the Principal Contractor has a policy of consideration in these respects.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

From 1st September 2015

(i) Major Development Sites – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

(ii) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

From 1st September 2020

(iii) Any development site - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

(iv) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

Construction time period:

AUG 2021 – DEC 2021

Is the development within the CAZ? (Y/N):

NO

Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N):

No NRMM with net power between 37kW and 560kW are to be used.

Please provide evidence to demonstrate that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered:

Not applicable for this development.

Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection:

Not applicable for this development.

Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required:

YES.

Light Pollution:

The works will be undertaken during the Summer and thus site lighting will not be required, However if necessary any site lighting will be positioned and directed so as not to cause a nuisance to the adjacent properties and special care will be taken to prevent light being directed towards the windows of these properties.

If nighttime lighting is deemed necessary, it will be kept to the lowest effective level and point downwards. Lighting will not be directed in such a way as to cause distraction or confusion to drivers on Well Road or on adjacent public highways.

Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Please notify that council when you intend to start work on site. Please also notify the council when works are approximately 3 months from completion.

Signed:*Ted Walters*.....

Date: *9 November 2021*

Print Name: *Ted Walters*

Position: *Lead Consultant and project Leader*

Please submit to :planningobligtions@camden.gov.uk

End of form.