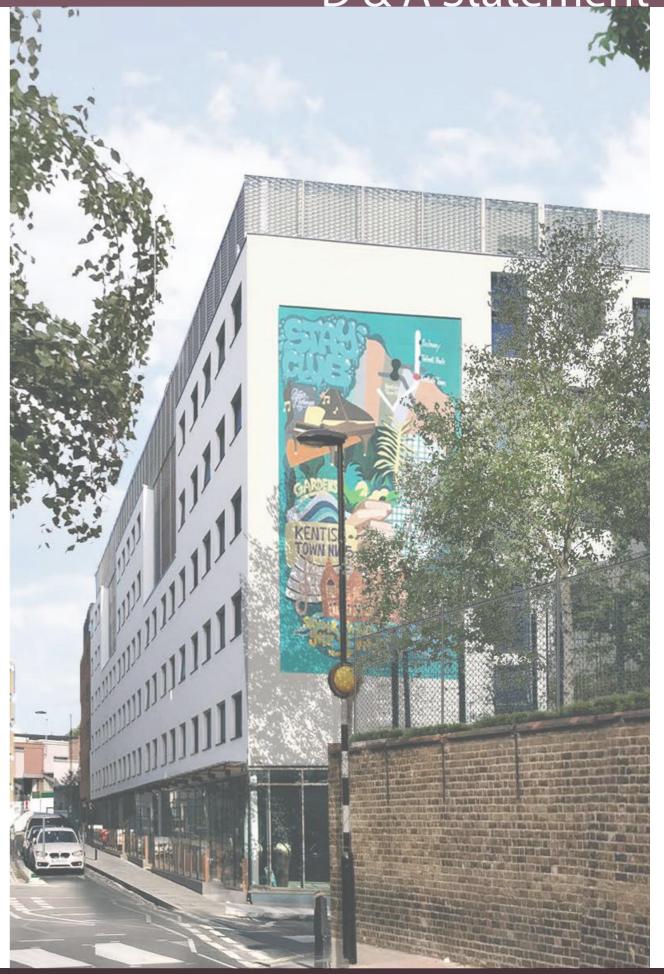
D & A Statement

FULL PLANNING APPLICATION

Proposed 7th floor extension to existing ground plus 6 storey student accommodation with B8 warehouse + showroom to ground and basement levels. (as approved under application reference 2017/6786/P)

65 Holmes Road London NW5 3AN



65 Holmes Road - September 2021 Contemporary Design Solutions

1.1 Introduction

This D&A Statement update has been prepared by Contemporary Design Solutions LLP for 65 Holmes Road Ltd in support of a full planning application to form a 7th floor extension to the approved scheme 2017/6786/P.

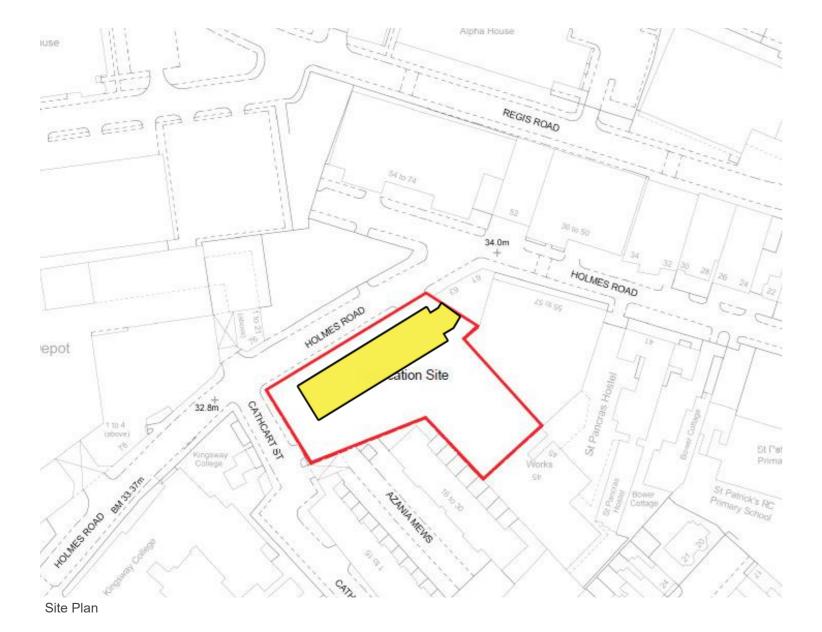
This update is focused on documenting the proposed seventh floor extension to provide an additional 25 student accommodation units.



Aerial Photograph Showing Site Location

2.1 Site Location

- Building Type: Student Accommodation (Sui Generis) + B8 Warehouse Use
- The Application Site: The extent of the application site is identified by the red line. The yellow area is the extent of the proposed development area.



2.1 Existing Site Photographs / Information

View 1 - View from north end of Holmes Road looking to the South west.

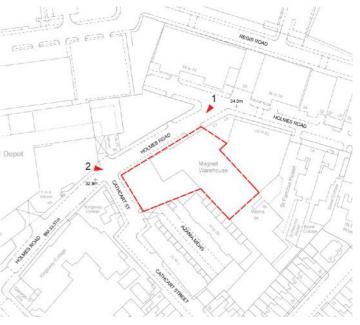
View 2 - View from Holmes Road looking east towards Holmes Road and Cathcart Street.

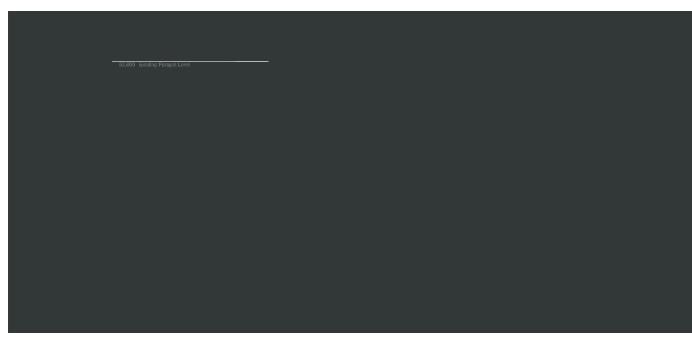


Existing View 1

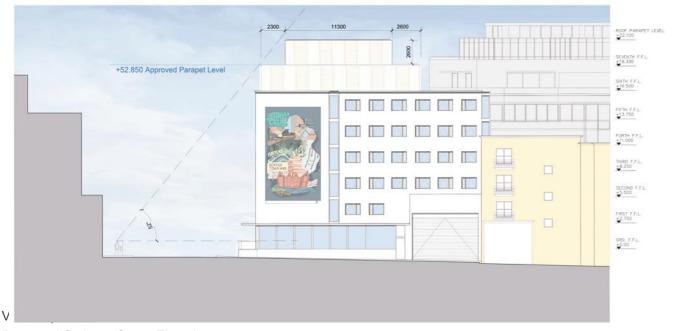


Existing View 2





Existing Cathcart Street Elevation



Proposed Cathcart Street Elevation

3.1 Use

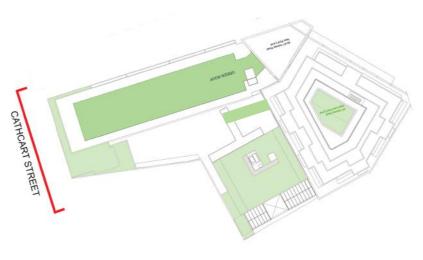
The use remains as per the approved scheme with an additional 25 student rooms formed by the proposed 7th floor, resulting in a total of 298 student units.

There is no change to the B8 warehouse space which remains as per the approved scheme.

3.2 Cathcart Street Elevation - Scale and Massing

The existing Cathcart Street elevation contains a varied palette of simple geometric shapes and colours. It is primarily a rendered elevation but broken up by a strip curtain wall glazing at the ground floor, a large metal entrance roller shutter to the warehouse loading bay and a regular grid of aluminium windows that form the student accommodation levels. A vertical line of windows represent the end of the accommodation level corridor. A large feature public wall art has been created to give the building corner a distinctive identity.

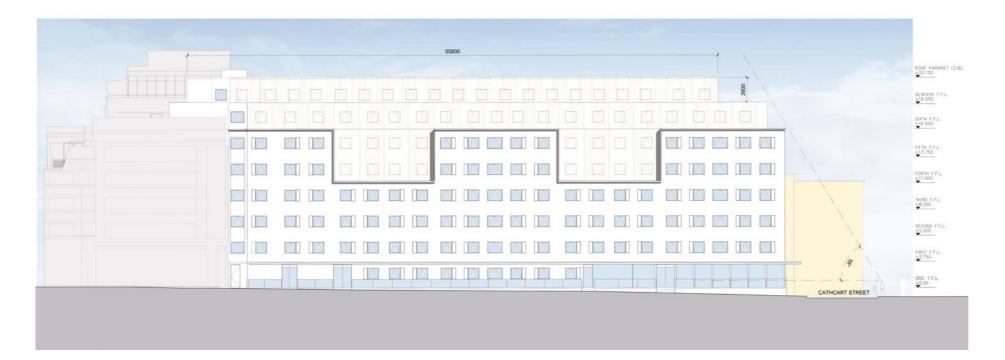
The existing sixth floor set back is overclad with distinctive aluminium mesh panels in a simple alternating colour pattern. The proposed 7th floor is clad with the same mesh cladding of the 6th Floor to match patterns on the overall facade while also generating a homogenous form with simple detailing to integrate with the existing architectural language.



Key Plan



Existing Holmes Road Elevation



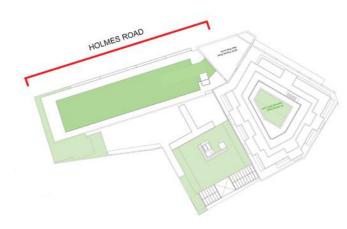
Proposed Holmes Road Elevation

3.3 Holmes Road Elevation - Scale and Massing

The existing Holmes Road elevation is mainly comprised of a white rendered facade punctuated by a regular grid of windows. At the upper levels, a castellated parapet is formed with the contrasting auminium mesh cladding filling in the gaps and finishes with the set back 6th floor level. A horizontal strip of curtain wall glazing defines the ground floor show room facade.

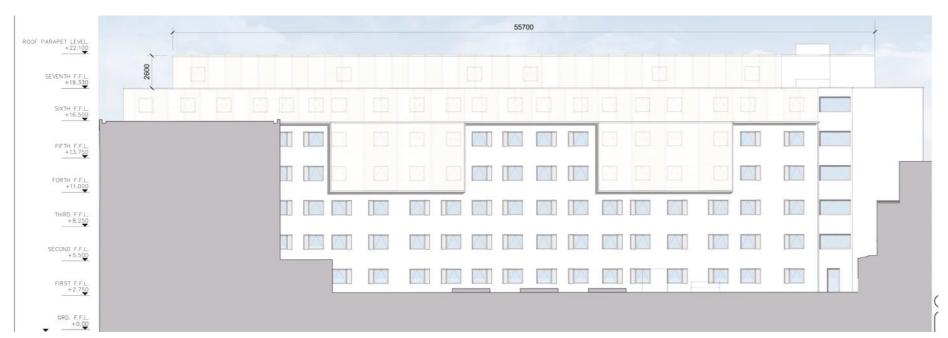
The proposed 7th floor is defined by a simple box that is set back from the 6th floor and covered with matching aluminium mesh to create a unitised homogenous form .

By using the same facade treatment as on the floor below, the aim is to not undermine the existing architectural style. This results in a minimal aesthetic that is complementary in relation to the existing building form. The set back of the new floor aims to reduce the visual impact from street lever as well as to neighbouring properties.



Key Plan

Existing South facing courtyard elevation



Proposed South facing courtyard elevation

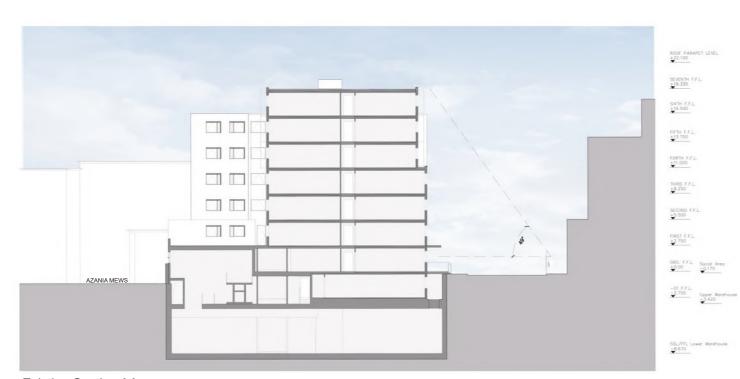
3.3 Courtyard Elevation Facing Azania Mews - Scale and Massing

Similarly, on the rear courtyard elevation, the proposed 7th floor is defined by aluminium mesh pannels to match the existing 6th-floor cladding and keep the facility integrated.

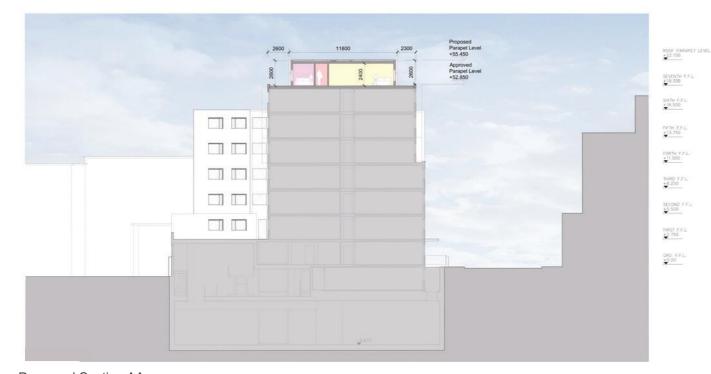
Replicating the method of using the metal mesh panels to unitise the existing 6th-floor elevation allows the creation of a harmonised 7th-floor aesthetic which sits comfortably on top of the existing building form.

The semi transparency of the mesh panels will also create a visual lightness to the additional massing and reduce the visual impact from street level and neighbouring buildings.





Existing Section AA



Proposed Section AA

Section AA - Scale and Massing

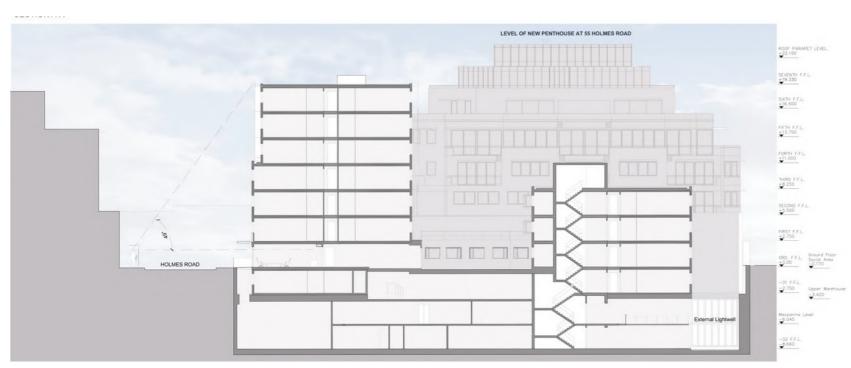
The internal floor to ceiling height is 2.4m across the whole proposed 7th floor.

The proposed section shows the setback of the new external walls to be 2.3m from the existing Holmes Road 6th floor elevation and 2.6m from the existing courtyard elevation.



Proposed Detail Section AA





Existing Section BB

Proposed Passet 1800 2000 11000 2000 11000 2000 11000 2000 11000 2000 11

Proposed Section BB

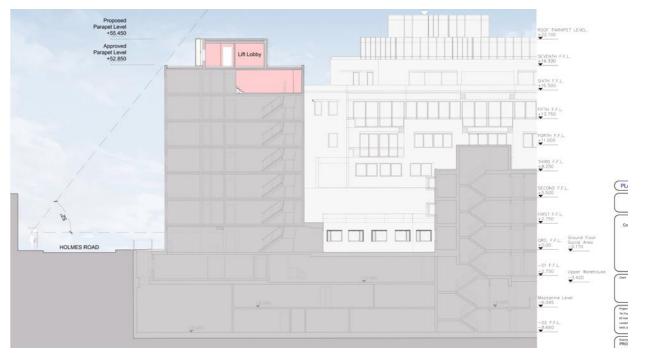
3.3 Section BB - Scale and Massing

The internal floor to ceiling height is 2.4m across the whole proposed 7th floor.

The proposed section shows the setback of the new external walls to be 2.3m from the existing Holmes Road 6th floor elevation and 2.6m from the existing courtyard elevation.

The purpose of the increased setback to the rear courtyard elevation is to minimise the massing impact on the neghbouring buildings at 55 Holmes Road and Azania Mews.

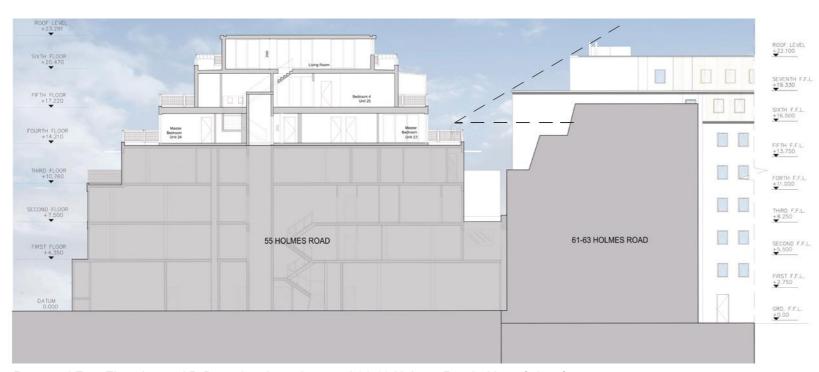
The below section CC detail through the lift lobby shows a greater set back relative to the adjacent 55 Holmes Road building, to further minimise visual and overshadowing impacts.



Proposed Section CC



Key Plan



Proposed East Elevation and D-D section throught 55and 61-63 Holmes Road - Line of view from terrace.

3.4 Eaast Eleavtion and Section EE - Impact on neighbouring properties

The purpose of the increased setback to the rear courtyard elevation is to minimise the visual and overshadowing impacts on the neghbouring buildings at 55 Holmes Road and Azania Mews.

The proposed section and elevation shows the setback of the new external walls to have no impact on neighbouring properties. As shown, the view from the next door property is not blocked and the extension does not impact sunlight what is indicated and proved in the Daylight and Sunlight Report.

The below, long section EE through the lift lobby, new staircase and rooms, shows a greater set back relative to the carthart st, by recessing the extension of 5 metres back from the current 6th-floor parapet, the proposed extension is almost unnoticeable from the street level of Holmes rd, what is shown on the page 22.

There is a minimal visual impact on the neighbouring terrace and bedroom at 57-55 Holemes Road. Section D-D shows the existing condition where the adjacent 61-63 Holmes Road already impacts the view from the neighbouring terrace.



Key Plan

Proposed Long Section E-E

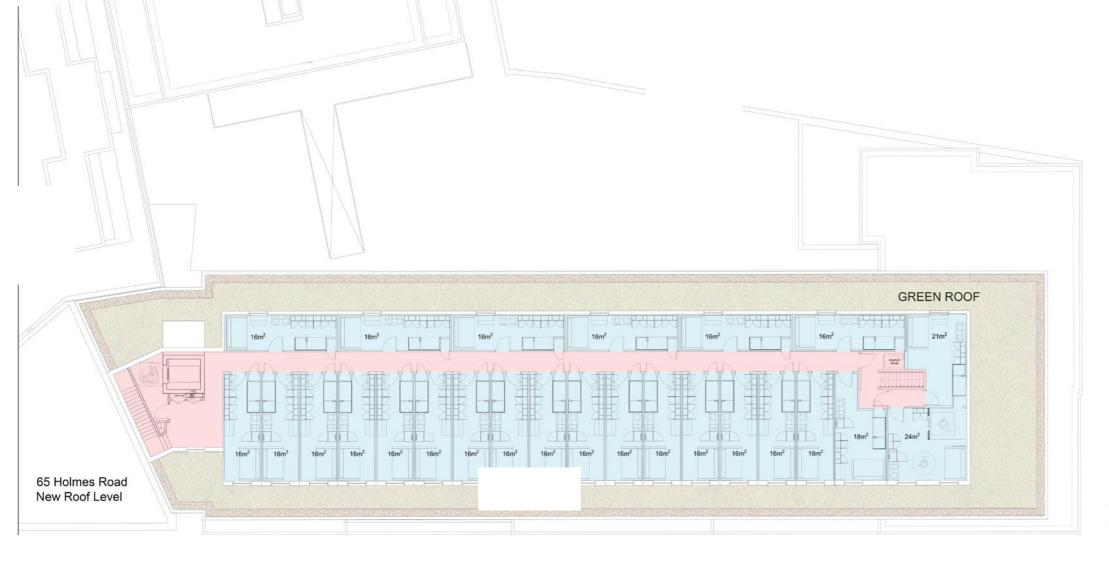
3.4 Proposed 7th Floor Plans

On plan an outer L shaped building (block 1), central courtyard and lower inner building (block 2) reflect the current as-built design.

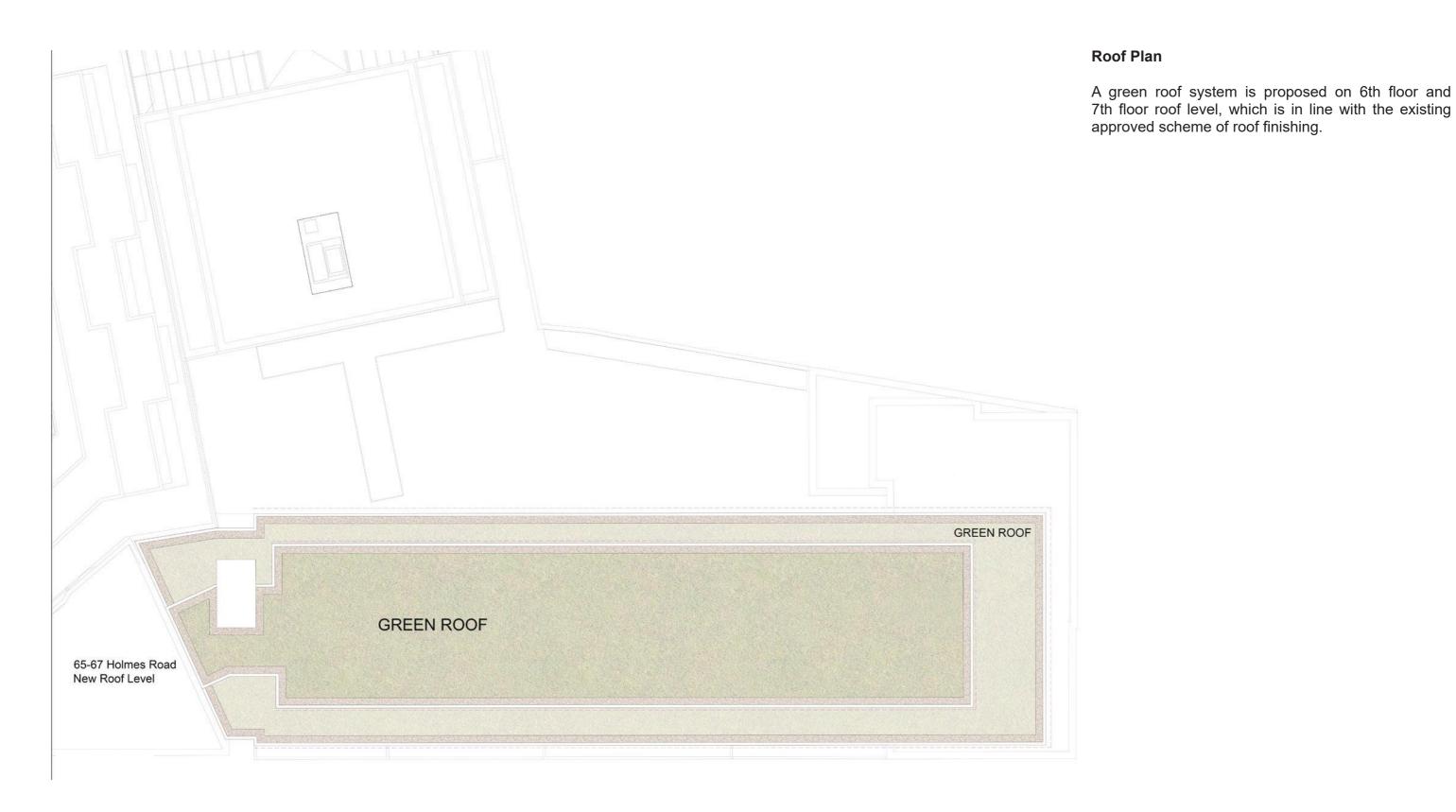
The proposed 7th floor layout will accommodate 25 studio units of similar size as the existing student accommodation units.

Access to the proposed floor will be via the existing lift and stair core which will extend up to the proposed floor but with reduced external massing.

The 20% disabled room requirement has been met on previous floors therefore all the rooms on seventh floor extension are to be standard single rooms.



GIA - 552 sqm GEA - 588 sqm



Proposed Roof Plan



3.5 Appearance and Visual Impact

The overall height has increased with the addition of the seventh floor extension but the use of the mesh cladding minimises the visual impact from long views.



Proposed

View from Holmes Road 3.0 Design



3.5 Appearance and Visual Impact

The setback of the proposed seventh floor aids in minimising the visual impact.

The mesh cladding would blend in the current design of the sixth floor and create a uniform image of the building.



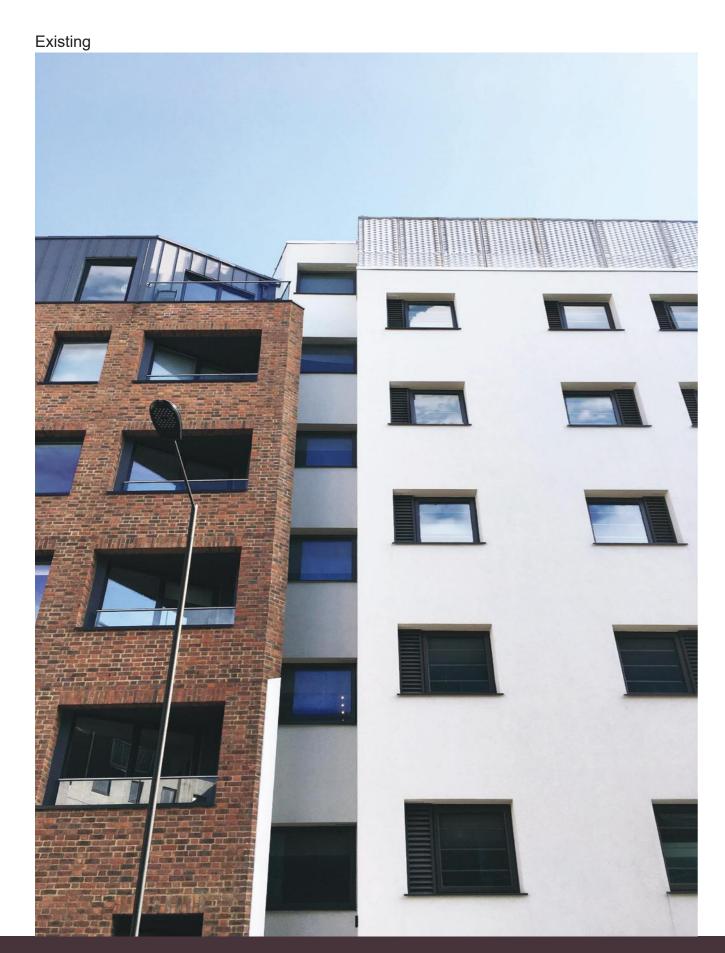


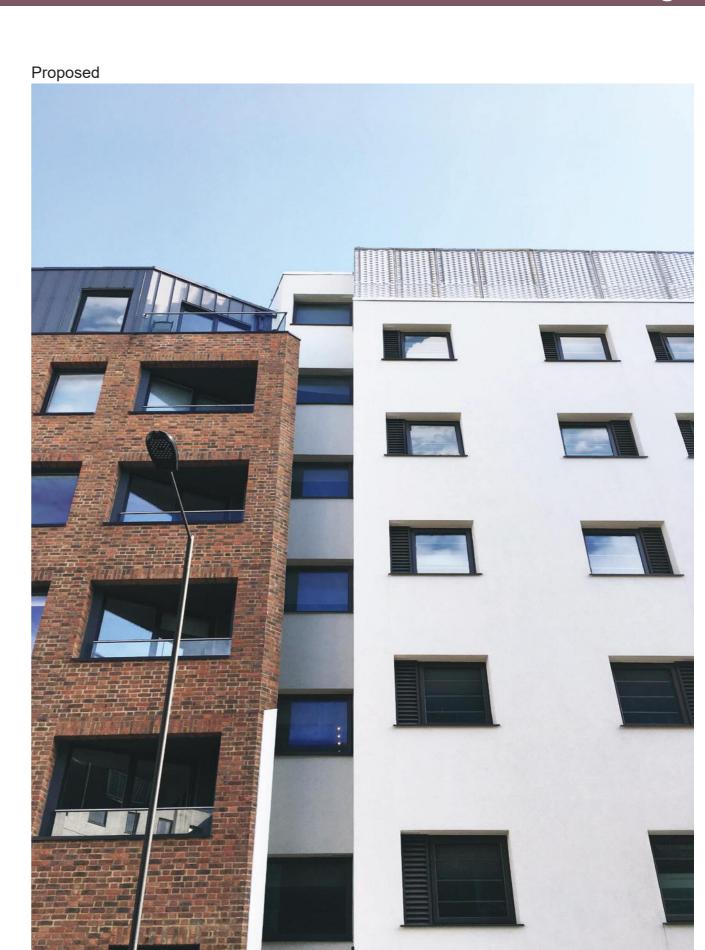
Existing



Proposed







Existing



Proposed



Approved view













Existing



Proposed



Existing



Proposed



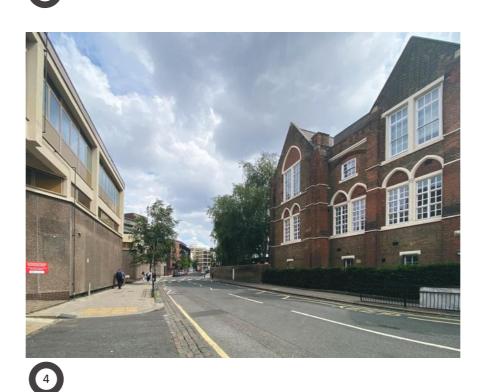




The adjacent images show a progression of views moving away from the site towards Willes Road.

- The five metre set back of the proposed 7th floor extension has minimal visual impact at street level when viewed at 20m from the building on Holmes Road.
- The proposed extension becomes visible between 30-40m away from the site.
- At 60m distance, the existing trees on the boundary of the French school CFBL obscures the view of the building and proposed extension.
- At 80-100m distance, the site development is not visible, being completely obscured by the trees of CFBL.











2 40 metres

3 60 metres 4 80 metres

5 100 metres

4.1 Accommodation Schedule

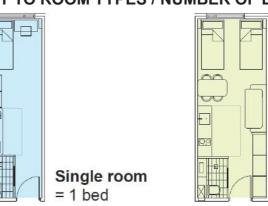
- The number of student rooms has increased to 366, total bed space: 464, total units: 298
- There are no changes to the warehouse use in this application.

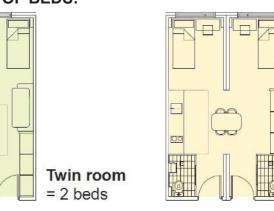
Proposed accommodation schedule

FLOORS	SINGLE ROOMS 14.5 - 26.8 sqm	TWIN ROOMS 17.7 - 30.3 sqm	DOUBLE ROOMS 28.7 - 47.7 sqm	DISABLE ROOMS 16.3 - 17.3 sqm	CLUSTER ROOMS 14 - 20 sqm	ROOM TOTAL	UNIT TOTAL	BED SPACES
LOWER BASEMENT	0	0	0	0	0	0	0	0
MIDDLE BASEMENT	0	0	0	0	0	0	0	0
UPPER BASEMENT	11	8	12	0 0		31	25	39
GROUND	10	3	6	0	0	19	16	22
FIRST	23	18	14	1	0	56	49	74
SECOND	21	13	10	6	8 (inc. 1x twin room - 20sqm)	58	46	72
THIRD	12	11	8	7	8 (inc. 1x twin room - 20sqm)	46	35	58
FOURTH	12	13	8	5	8 (inc. 1x twin room - 20sqm)	46	35	60
FIFTH	10	13	10	5	8 (inc. 1x twin room - 20sqm)	46	34	60
SIXTH	8	15	12	4	0	39	33	54
SEVENTH	25	0	0	0	0	25	25	25
TOTAL	132	94	80	28	32	366	298	464

Total Bedspaces = 464	Total Rooms = 366	Total Units =	298		
Warehouse B8 space gross inte	ernal area:		Student accommodation GIA (sqm)		
Lower Basement warehouse space		1080 sqm	Upper basement	942	
Upper Basement mezzanine warehou	se space	130 sqm	Ground Floor	628	
Upper Basement warehouse space		723 sqm	First floor	1350	
Ground Floor goods yard / showroom	space	384 sqm	Second floor	1385	
Total area		2317 sqm	Third floor	1100	
			Fourth floor	1079	
Internal communal area:			Fifth floor	1079	
			Sixth floor	915	
Study/meeting room/reading room/soc	ial area	1185 sqm	Seventh floor	552	
			Total gross internal area	9030 sqm	
Amenity area:					
Central courtyard		549 sqm			
Lower basement lightwell		31 sqm			

KEY TO ROOM TYPES / NUMBER OF BEDS:





Double room = 2 beds

Existing/ approved accommodation schedule February 2018

FLOORS	SINGLE ROOMS 14.5 - 26.8 sqm	TWIN ROOMS 17.7 - 30.3 sqm	DOUBLE ROOMS 28.7 - 47.7 sqm	DISABLE ROOMS 16.3 - 17.3 sqm	CLUSTER ROOMS 14 - 20 sqm	ROOM TOTAL	UNIT TOTAL	BED SPACES
LOWER BASEMENT	0	0	0	0	0	0	0	0
MIDDLE BASEMENT	0	0	0	0 0		0	0	0
UPPER BASEMENT	11	8	12	0 0		31	25	39
GROUND	10	3	6	0 0		19	16	22
FIRST	23	18	14	1 0		56	49	74
SECOND	21	13	10	6 8 (inc. 1x twin room - 20sqm)		58	46	72
THIRD	12	11	8	7 8 (inc. 1x twin room - 20sqm)		46	35	58
FOURTH	12	13	8	5 8 (inc. 1x twin room - 20sqm)		46	35	60
FIFTH	10	13	10	5 8 (inc. 1x twin room - 20sqm)		46	34	60
SIXTH	8	15	12	4	0	39	33	54
TOTAL	107	94	80	28	32	341	273	439

Total Bedspaces = 439	Total Rooms =	341	Total Units =	273	
Warehouse B8 space gross	internal area:			Student accommodation Gl	A (sqm)
Lower Basement warehouse space	9		1080 sqm	Upper basement	942
Upper Basement mezzanine wareł	nouse space		130 sqm	Ground Floor	628
Upper Basement warehouse space	Upper Basement warehouse space			First floor	1350
Ground Floor goods yard / showroo	Ground Floor goods yard / showroom space			Second floor	1385
Total area			2317 sqm	Third floor	1100
				Fourth floor	1079
Internal communal area:				Fifth floor	1079
				Sixth floor	915
Study/meeting room/reading room/	social area		1185 sqm		
				Total gross internal area	8478 sqm
Amenity area:					
Central courtyard			549 sqm		
Lower basement lightwell			31 sqm		
ŭ			•		

5.1 Accessibility Statement

The proposed 7th floor extension will include the extending of the passenger lift to provide wheelchair access to the new floor level. The existing stairs will continue up to the new floor level.

Building Entry is as per the existing with the main entrance directly from street level off Holmes Road. A disabled chair lift provides wheelchair access to reception level.

Communal Facilities: The communal areas of the building, including corridors, principle doors, lifts and stairwells, have been designed to provide sufficient width and ease of circulation throughout. The main corridors are all minimum clear 1200mm width.

There are pre-existing 28 disabled rooms in the current student accommodation. There will be one additional disabled room proposed on the 7th floor extension.