

Royal College Street

Stage 1 Road Safety Audit – Designer Response

Curtins Ref: 71435

Revision: 01

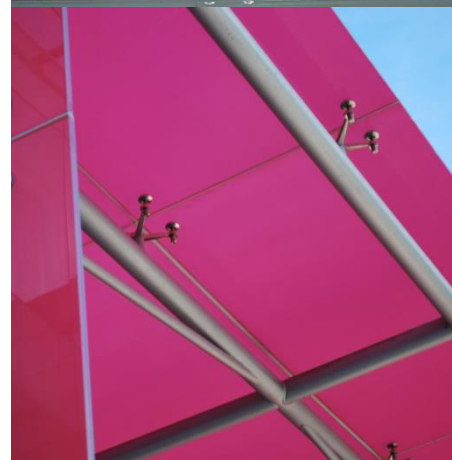
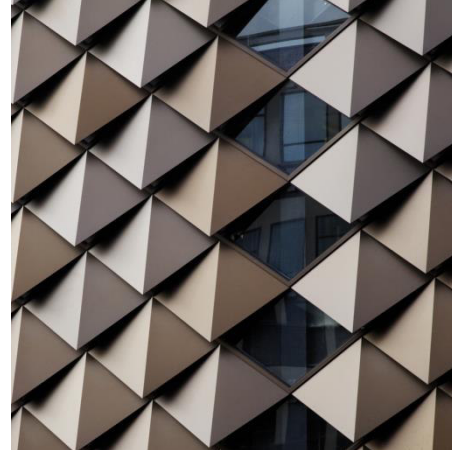
Issue Date: 17 April 2020

Client: Rocco Ventures

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Control Sheet

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1.0 Introduction

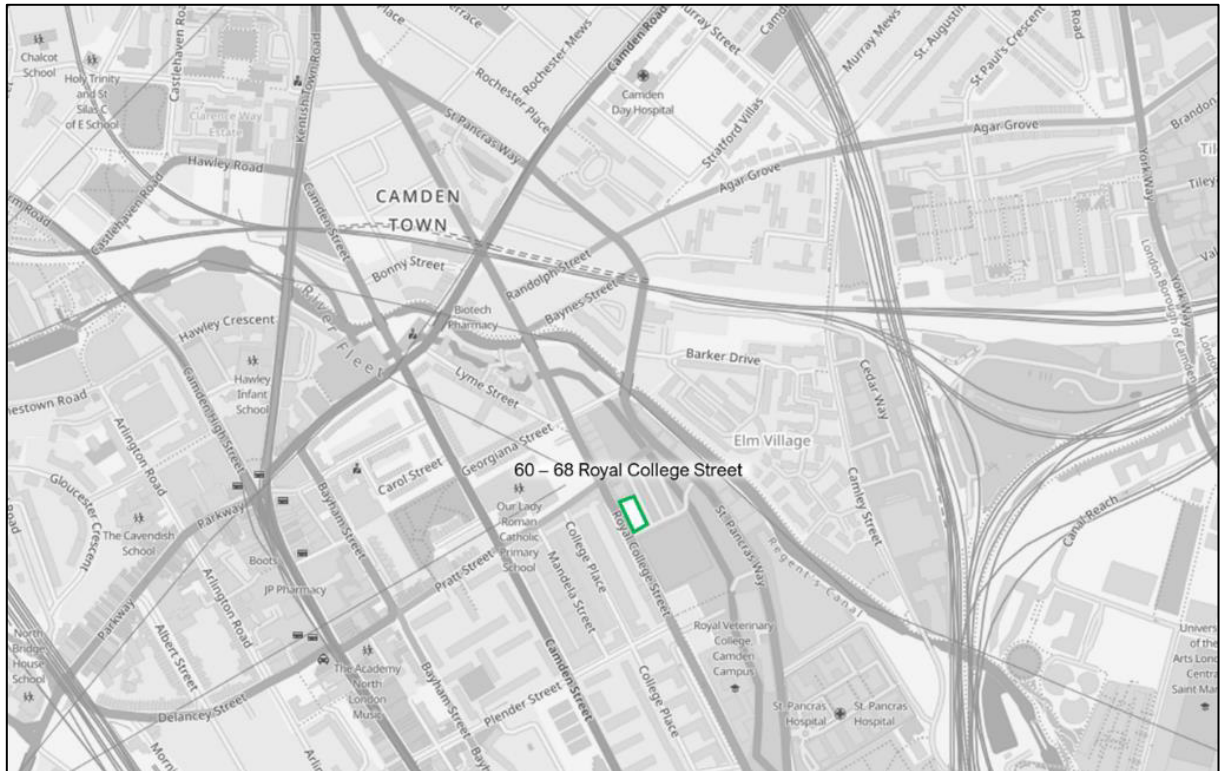
1.1 Background

- 1.1.1 Curtins has been appointed by Rocco Ventures Limited to provide transport planning advice related to the redevelopment of 70-86 Royal College Street, located in the London Borough of Camden (LBC).
- 1.1.1 The development proposals include the redevelopment of the existing site to provide a healthcare facility (Classes D1/C2) comprising Basement, Ground, plus Four storeys and Rooftop gardens, pavilions and plant enclosures. The tenant will be Central and North West London NHS Trust. The proposals will allow the NHS to relocate some existing uses from South Wing at the existing St Pancras Hospital Site, located 350m from the new site. The new building will provide intermediate care for in patients and services for outpatients.
- 1.1.2 This report has been produced in response to a Stage 1 Road Safety Audit (RSA) of the proposed site access arrangements and internal layout, which can be seen in **Appendix A**.
- 1.1.3 The Stage 1 RSA was undertaken by Highway Associates and should be read in conjunction with this response.

1.2 Site Context

- 1.2.1 The site is located at 70 – 86 Royal College Street, approximately 600m east of Camden Town. The area is mixed-use in nature, with a pub (the Golden Lion), the proposed commercial centre on Pratt Street and the Royal Mail Parcel Force depot, all adjacent to the site.
- 1.2.2 The site is bound to the southwest by Royal College Street, the southeast by a Parcelforce Centre, to the northeast by the Parcelforce car park and to the northwest by the Golden Lion pub. The existing site is a former car repair centre, which provides nine car parking spaces and a large area of hard standing for vehicles.
- 1.2.3 **Figure 1.1** illustrates the site location in the context of the surrounding area.

Figure 1.1 - Indicative Site Location

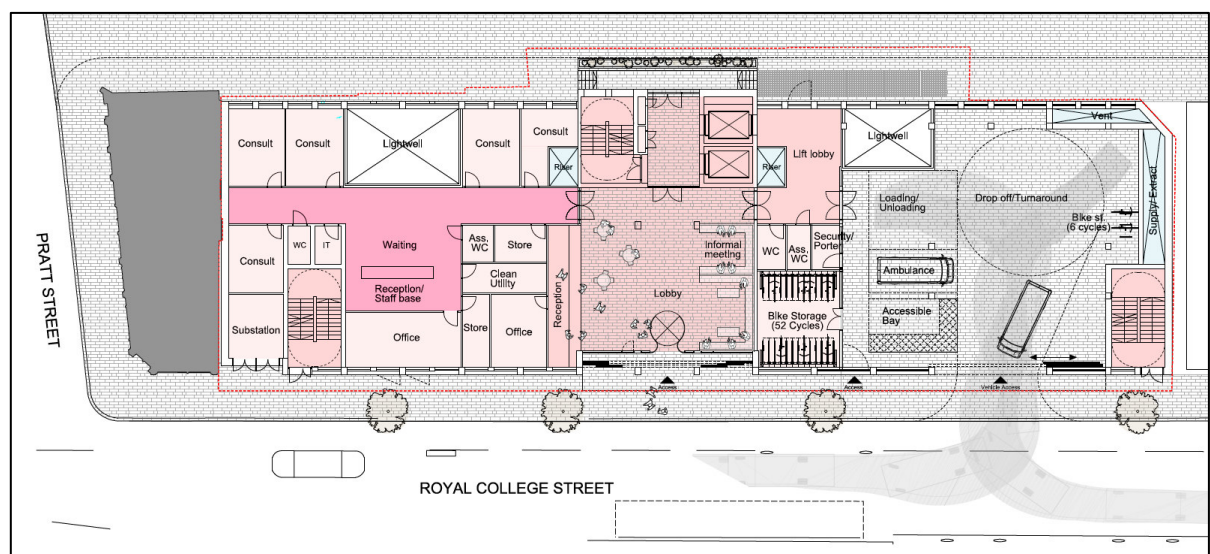


1.3 Development Proposals

- 1.3.1 The development proposals include the demolition of the existing car workshop and the construction of a new intermediate healthcare facility, which will be relocated from the South Wing at the existing St Pancras Hospital Site, located approximately 350m from the site. The new building will provide intermediate care for in patients and services for outpatients.
- 1.3.2 Consulting rooms will be provided on the ground floor, along with a lobby and reception area. 54 patient beds will be provided on the remaining four floors, alongside communal areas on each floor. A landscaped communal area will be located on the roof, alongside two covered units for staff and patients respectively. Ancillary office space will be located in the basement for the NHS Trust.
- 1.3.3 A servicing yard will be located on the ground floor, in the southern section of the site. This area will provide one disabled car parking bay and an ambulance bay.
- 1.3.4 A total of 70 cycle parking spaces will be provided, which is in line with the Intend to Publish London Plan requirements. 12 of the spaces will be located within a storage room in the basement, accessed from an appropriately sized lift.

- 1.3.5 52 cycle parking spaces will be located in a cycle store within the demise of the building (including ten short stay spaces), which will be accessed via a segregated access point to the servicing yard which has been designed to be in line with London Cycle Design Standards.
- 1.3.6 In addition, three Sheffield stands will be located within the servicing yard, accommodating up to six cycles and potentially larger, accessible and cargo cycles.

Figure 1.2- Development Proposals



- 1.1.2 The servicing yard will provide space for a 7.5t van to dwell whilst another vehicle to turn around.
- 1.1.3 The servicing yard will be controlled using sliding gates. It is anticipated that these will be open for a period of the day to allow delivery and servicing, ambulances and disabled users to enter and exit without waiting on the highway network. The gates will be closed over night to avoid antisocial behaviour and the space being abused.
- 1.1.4 It is anticipated that the site will generate up to eight delivery and servicing vehicles per day (16 two-way trips). All delivery and servicing will be undertaken using a 7.5t or smaller.

2.0 Designers Response

2.1 Introduction

- 2.1.1 The following section of this report sets out the road safety observations identified within the Stage 1 audit and provides a designer's response in light of those issues identified.

2.2 RSA Item 3.1.1

- **Item summary:** *Reduced visibility due to sight line obstructions* - Visibility could be reduced for drivers attempting to turn out of the proposed access by fixed sight line obstructions in the way of a proposed tree and existing road lighting column. This could increase the potential for failure to give-way type conflicts as drivers attempting to turn out of the proposed access fail to sufficiently appreciate approaching northbound traffic.
- **Item location:** Proposed Vehicle Access
- **RSA recommendation:** Ensure the likely visibility splay for the access junction is kept clear of any fixed obstructions.
- **Designer's response:** Drawing 71435-CUR-00-XX-DR-TP-06006, included in **Appendix B**, illustrates visibility splays for the proposed service yard vehicle access. Royal College Street is a one-way road in the northwest direction, whilst the cycle lane is contraflow north-eastbound.

Royal College Street is subject to a 20mph speed restriction, therefore, drawing 71435-CUR-00-XX-DR-TP-06006 illustrates that there is sufficient visibility to the southeast (2.4m by 25m as set out in Manual for Streets).

In general, cyclists will travel at speeds of up to 10 / 12mph. Manual for Streets recommends visibility of 11 – 14m forward visibility. Drawing 71435-CUR-00-XX-DR-TP-06006 illustrates that that there is sufficient visibility to the northwest (2.4m by 16m).

There are no proposals to introduce any fixtures within these visibility splays which will impact on the visibility for vehicles exiting the service yard. Two trees are located at each edge of the visibility splays. These are anticipated to be birch trees, which feature a slender trunk measuring between 40 – 50cm diameter. Therefore, these are not expected to hinder visibility from the service yard.

2.3 RSA Item 3.1.2

- **Item summary:** *Outward opening doors could cause conflicts with passing pedestrian traffic*- A number of outward opening emergency exit doors are to be provided directly onto the adjacent footway which could potentially be struck by passing pedestrians, causing personal injury.

- **Item location:** Royal College Street
- **RSA recommendation:** Bollards should be placed either side of the doors to direct pedestrians away from the opening door areas.
- **Designer's response:** These doors are emergency doors and therefore will not be in everyday use. Given these doors will not be in frequent use, it is not anticipated that this will effect pedestrian movements.

2.4 RSA Item 3.1.3

- **Item summary:** *On-street parking could cause conflicts* - It is unclear as to the nature of the proposed junction with Pratt Street however some on-street parking was noted within the vicinity of the proposed junction. Should this continue after the scheme is implemented, it could cause access and visibility issues for traffic turning in and out of the junction increasing the risk of junction conflict.
- **Item location:** Proposed junction access with Pratt Street
- **RSA recommendation:** Advice from the local authority should be sought and parking restrictions provided if necessary
- **Designer's response:** The road at the rear of the development is indicative of the expected access to come forward as part of the neighbouring development. Camden are currently developing designs for improving Pratt Street for cyclists and pedestrians. These had not been finalised at the time of submission. This would improve visibility and relocate some on-street car parking.

2.5 RSA Item 3.1.4

- **Item summary:** *Stairs could cause conflicts* - A set of steps has been provided to enable access to the rear of the development off a footway section which then continues on passed the development site. Concern is expressed that wheelchair users not intending to access the development will be forced into the adjacent carriageway risking conflicts with passing traffic.
- **Item location:** Un-named access road to rear of development
- **RSA recommendation:** A suitable alternative route for wheelchair users should be provided.
- **Designer's response:** The road at the rear of the development is indicative of the expected access to come forward as part of the neighbouring development. When the development proposals for the neighbouring site come forward, it is anticipated that this will be considered.

3.0 Conclusion

- 3.1.1 It is considered that the items identified as part of the Stage 1 RSA can be mitigated and addressed through further detailed highway design of the site access arrangements.

Appendices

Appendix A – RSA Document

Royal College Street, Camden - Proposed Highway Works Stage 1 Road Safety Audit

Prepared by



Distribution List

1. Caitlin Turley - Curtins
2. File Copy

REPORT NUMBER 2020/C/1138

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Issue and revision record

Revision	Date	Originator	Checker	Approver	Description
FINAL	02/04/20	N Madhavan	R Sawczyn	L Madhavan	For Issue

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Appendix A: Drawings and documents provided

Appendix B: Plan of items raised by this audit

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1 INTRODUCTION

1.1 This report results from a Stage 1 (Preliminary Design) Road Safety Audit carried out relating to highway works in the form of a new access onto Royal College Street to serve a hospital development together with rear access provision off Pratt Street. The audit was carried out during March and April 2020.

1.2 The audit team consisted of:

Team Leader: Naresh Madhavan MCIHT, MSoRSA EU RSA Cert. Comp
Director
Highway Associates

Team Member: Robin Sawczyn BEng MCIHT MSoRSA
Senior Road Safety Engineer
Highway Associates

1.3 The audit took place at the offices of Highway Associates, Chester. The Road Safety Audit was undertaken in accordance with the Road Safety Audit Brief provided by Caitlin Turley of Curtins. The Road Safety Audit comprised an examination of the documents provided and these are listed in Appendix A.

The site was visited at the following times/conditions:

Day/Date	Time	Weather Con	Traffic Flow
Sun 29 th Mar 2020	12:00 – 12:40hrs	Dry/Fine	Below Ave

1.4 The terms of reference for this audit are as given in GG119, Design Manual for Roads and Bridges. The scheme has been examined and this report compiled only with regard to the safety implications for road users of the scheme as presented. It has not been examined or verified for compliance with any other standards or criteria. However, in order to clearly explain a safety problem or the recommendation to resolve a problem, the Audit Team may on occasion have referred to a design standard for information only. Any audit comments should not be construed as implying that a technical audit has been undertaken in any respect.

- 1.5 If relevant within this report the generic term “pedestrians” can include walking pedestrians, wheelchair users, dismounted cyclists, the blind, partially sighted, mobility impaired and people with prams or pushchairs. Highway Maintenance Operatives are also included within this term. Where a problem is specific to one or more of these vulnerable groups then appropriate reference will be made in the report.
- 1.6 Any recommendations included within this report should not be regarded as being prescriptive design solutions to the problems identified. They are intended only to indicate a proportionate and viable means of eliminating or mitigating the identified problem, in accordance with GG119, and in no way imply that a formal design process has been undertaken. There may be alternative methods of addressing a problem which would be equally acceptable in achieving the desired elimination or mitigation and these should be considered when responding to this report.
- 1.7 This Road Safety Audit makes allowance for the fact that strategic decisions on matters such as route choice, junction type, standard of provision and approved Departures from Standards already reflect an appropriate balance of a number of factors including road safety.
- 1.8 The Audit Team have not been made aware of any possible geometric departures or relaxations from standards.
- 1.9 The reader is reminded that a Road Safety Audit is not limited to the new works alone where there are features that are considered by the Audit Team to affect the general safety in the area. Section 3 of this report lists ‘Problems’ identified from the drawings and as a result of the site visit relating to the proposed changes to the highway layout that are directly, or indirectly, related to the area (e.g. features/matters within the proposed works or immediate surrounds which the Audit Team feel should be brought to the attention of the Designer or other organisations even though they might not pose an immediate risk associated with the proposed works), and which are considered by the Audit Team to warrant attention under the subject proposals, generally by the maintaining authority. On occasions there are more sensitive issues or issues that fall outside the audit brief but which are of such a nature that Auditors feel should be specifically brought to the attention of the authorities - such issues, if they arise, are usually dealt with in detail in separate correspondence.

2 ITEMS RAISED BY PREVIOUS AUDITS AND REPORTS

- 2.1 It is understood that no previous road safety audits have been undertaken in relation to this scheme.

3 ITEMS RAISED BY THIS AUDIT

3.1 General

3.1.1 PROBLEM

LOCATION: Proposed vehicular access

SUMMARY: Reduced visibility due to sight line obstructions

Visibility could be reduced for drivers attempting to turn out of the proposed access by fixed sight line obstructions in the way of a proposed tree and existing road lighting column. This could increase the potential for failure to give way type conflicts as drivers attempting to turn out of the proposed access fail to sufficiently appreciate approaching northbound traffic.

RECOMMENDATION

Ensure the likely visibility splay for the access junction is kept clear of any fixed obstructions.

3.1.2 PROBLEM

LOCATION: Royal College Street

SUMMARY: Outward opening doors could cause conflicts with passing pedestrian traffic

A number of outward opening emergency exit doors are to be provided directly onto the adjacent footway which could potentially be struck by passing pedestrians, causing personal injury.

RECOMMENDATION

Bollards should be placed either side of the doors to direct pedestrians away from the opening door areas.

3.1.3 PROBLEM

LOCATION: Proposed junction access with Pratt Street

SUMMARY: On-street parking could cause conflicts

It is unclear as to the nature of the proposed junction with Pratt Street however some on-street parking was noted within the vicinity of the proposed junction. Should this continue after the scheme is implemented, it could cause access and visibility issues for traffic turning in and out of the junction increasing the risk of junction conflict.

RECOMMENDATION

Advice from the local authority should be sought and parking restrictions provided if necessary.

3.2 Non Motorised Road User Provision

3.2.1 PROBLEM

LOCATION: Un-named access road to rear of development

SUMMARY: Stairs could cause conflicts

A set of steps has been provided to enable access to the rear of the development off a footway section which then continues on passed the development site. Concern is expressed that wheelchair users not intending to access the development will be forced into the adjacent carriageway risking conflicts with passing traffic.

RECOMMENDATION

A suitable alternative route for wheelchair users should be provided.

3.3 Road Signs and Markings

No comments at this stage

3.4 Lighting and Signals

No comments at this stage

3.5 Landscaping

No comments at this stage

3.6 Protective Aids

No comments at this stage

3.7 Surface Characteristics and Drainage

No comments at this stage

4 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with the guidelines in GG119.

AUDIT TEAM LEADER

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Signed:



Date:

02/04/20

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Signed:



Date:

02/04/20

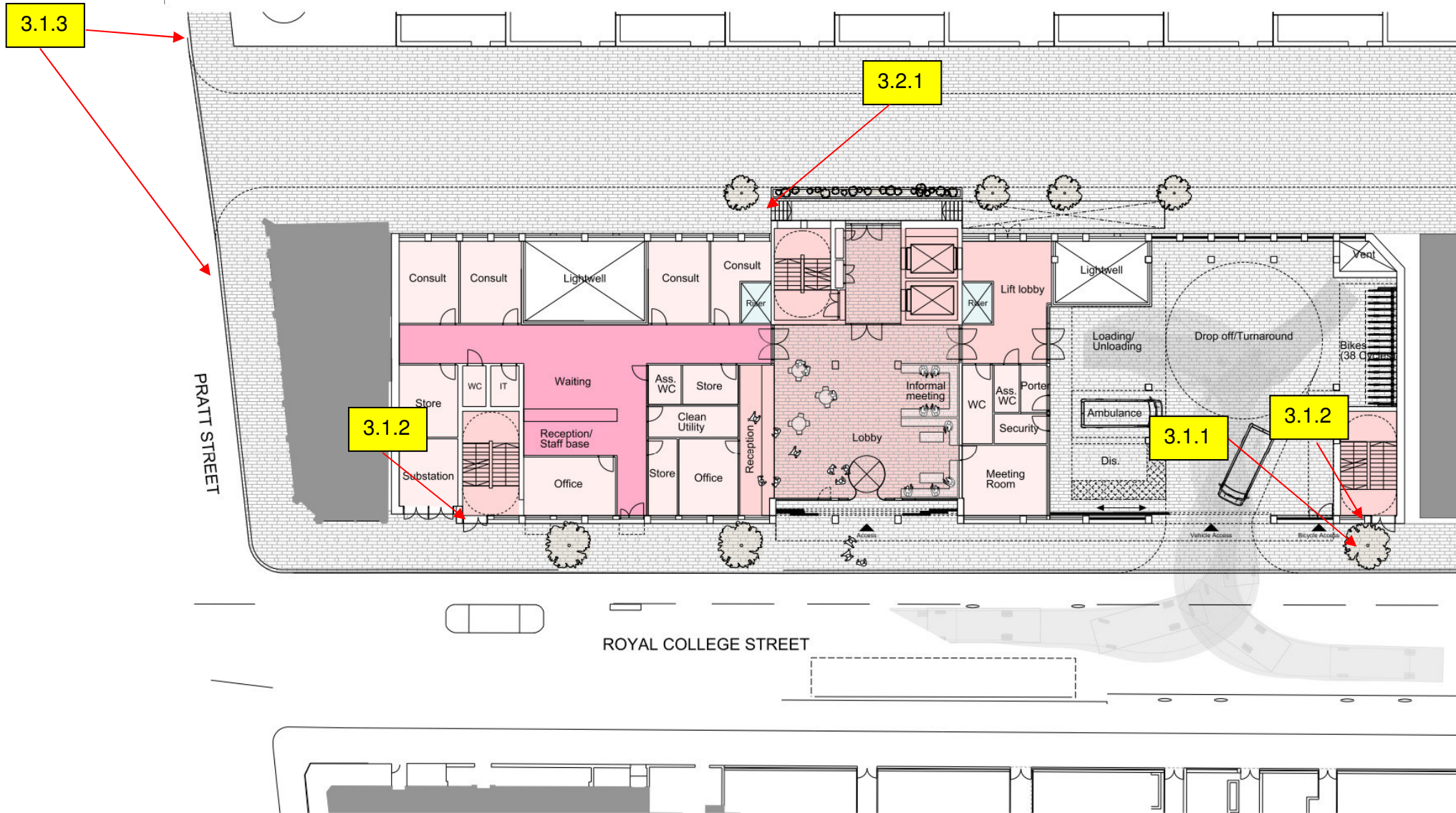
APPENDIX A

DRAWINGS AND DOCUMENTS PROVIDED

DRAWING NUMBER OR REFERENCE	TITLE	DATE
-	RSA1&2 Brief	27/03/20
14585_00(00)	Existing Drawings	-
14585_88(00)	Proposed Drawings	-
71435-CUR-00-XX-DR- TP-06003 P03	Swept Path Analysis	31/01/19

APPENDIX B

PLAN OF ITEMS RAISED BY THIS AUDIT



Scale 1:100
0 1 2 5



Revisions
01 10/12/19 First Issue for Planning

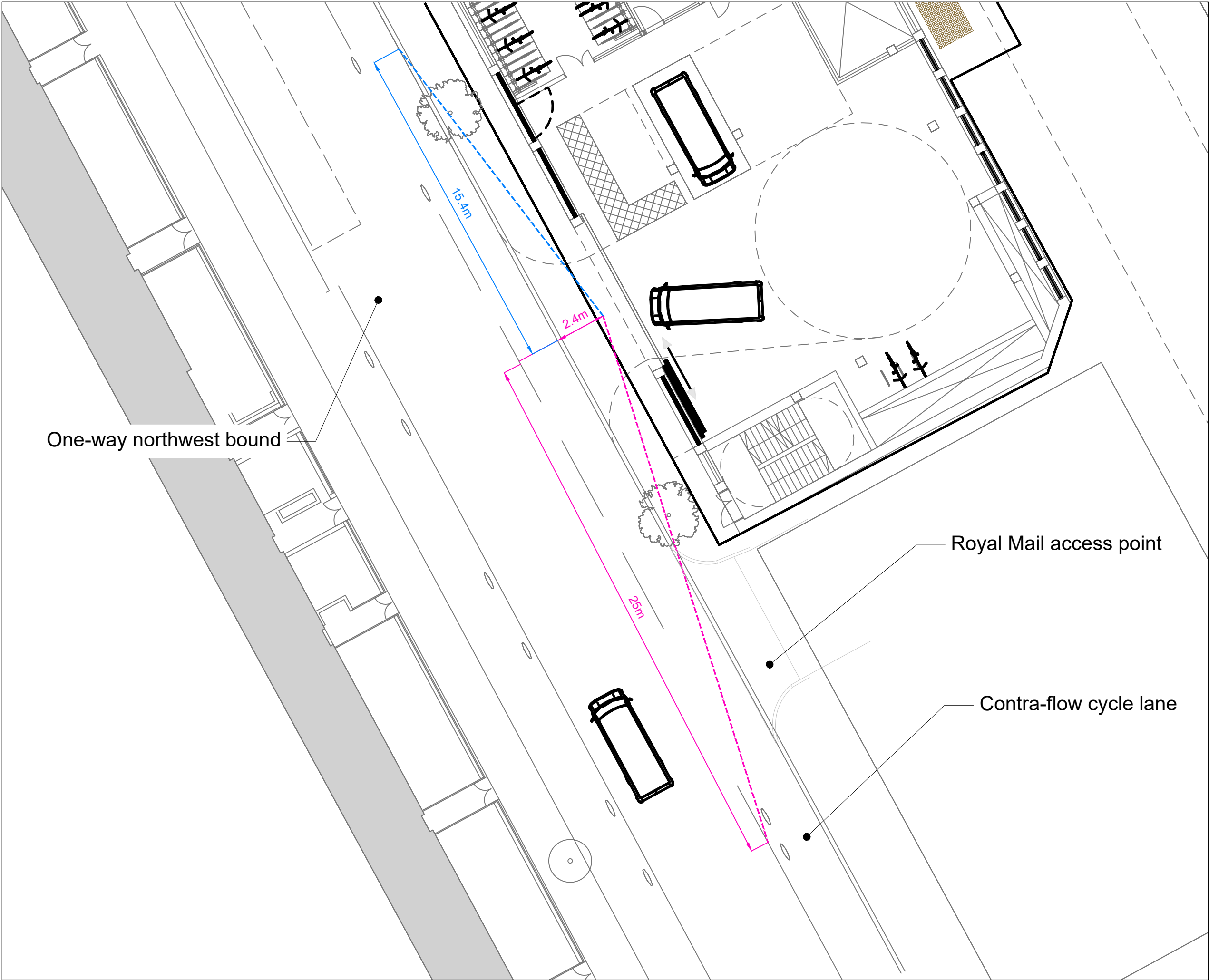
Client
Rocco Ventures Ltd.
Project
Royal College Street

Drawing Title
Proposed
Ground Floor Plan

Drawing Number
1485_00(00)_200
Scale 1:100(00)_200
REV: 01
Status: Planning
Drawn by: ew
Date: 09/11/19

ianchalkarchitects
79 Courtenay Street London EC1M 6EJ / 0203 7867355 / ianchalkarchitects.com
Do not scale. All dimensions to be confirmed on site. Information contained in the drawing is the sole copyright of Ian Chalk Architects and is not to be reproduced without permission.

Appendix B - Visibility Splays for Site Access



GENERAL NOTES

1. THE CONTENT OF THIS DRAWING IS FOR INDICATIVE INFORMATION ONLY AND NOT SUITABLE FOR CONSTRUCTION PURPOSES

2. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS AND SPECIFICATIONS.

3. DO NOT SCALE THIS DRAWING. ANY AMBIGUITIES, OMISSIONS AND ERRORS ON DRAWINGS SHALL BE BROUGHT TO THE ENGINEERS ATTENTION IMMEDIATELY. ALL DIMENSIONS MUST BE CHECKED / VERIFIED ON SITE.

4. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.

5. FOR GENERAL NOTES REFER TO DRAWING.

SOURCE: 1485_AP(00)GA_--

KEY

Junction visibility for a 20mph road

Visibility of cycle lane

N

P01

Post submission to be included in RSA response

03/04/20

CT

MW

Rev:

Description:

Date:

By:

Chkd:

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STATUS:

INFORMATION

PROJECT:

ROYAL COLLEGE STREET

DWG TITLE:

VISIBILITY SPLAYS

Scale:

1:125

Size:

A3

First Issue:

03/04/20

Drawn:

CT

Checked:

IOR

DWG No:

71435-CUR-00-XX-DR-TP-06006

Rev:

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