



Da Vinci House
44 Saffron Hill
London EC1N 8FH
tel: +44 (0)20 3640 8508
fax: +44 (0)20 3435 4228
email: info@iceniprojects.com
web: www.iceniprojects.com

Planning Administration Team
Development Management
Regeneration and Planning
Supporting Communities
London Borough of Camden
2nd Floor, 5 Pancras Square
c/o Town Hall, Judd Street
London
WC1H 9JE

08 June 2020

BY EMAIL

Dear Sir or Madam,

DISCHARGE OF CONDITION 4 – CYCLE STANDS - CHANGE OF USE FROM 2 RESIDENTIAL UNITS (CLASS 3) TO MUSEUM (CLASS D1) INCLUDING THE ERECTION OF A SINGLE STOREY REAR CONSERVATORY, ALTERATION TO BOUNDARY TREATMENT INCLUDING ADDITION OF METAL RAILING AND ALTERATIONS TO EXISTING ENTRANCE STEPS INCLUDING THE INSTALLATION OF A DISABLED PLATFORM LIFT TO ACCESS THE UPPER GROUND FLOOR (EN18/0027)

We write with regards to the discharge of Condition 4 attached to the planning permission at 10 King Henry's Road, London, NW3 3RP for:

'Change of use from 2 residential units (Class 3) to museum (Class D1) including the erection of a single storey rear conservatory, alteration to boundary treatment including addition of metal railing and alterations to existing entrance steps including the installation of a disabled platform lift to access the upper ground floor.'

This permission was issued following an appeal (APP/X5210/C/18/3219239) against an Enforcement Notice (EN18/0027). At the appeal, the Enforcement Notice was quashed and permission issued following the recovery of the appeal by the Secretary of State.

a. Introduction

The appeal was recovered by the Secretary of State on the grounds that the development was of international significance and the appeal heard at Public Inquiry. Some relevant context in relation to the cycle stand is included within this letter.

The permission contains a number of conditions, including the following:

'Details of a secure cycle storage facility for a minimum of 2 cycles shall be submitted to the local planning authority in writing within 3 months of the date of this decision. The cycle storage facility shall be installed in accordance with the approved details within 6 months of that approval and shall thereafter be retained.'

This application seeks to discharge this condition by providing the relevant details of the location of the cycle stands and the specification of the cycle stand. This means that the first part of the condition, submitting the details within 3 months of the decision (12 June) has been discharged.

b. Cycle Stand Location - Justification

As you will note from the appended documents, the proposal involves locating the cycle stands in the front garden of the property (Appendix A1). This was discussed at the site visit of the Public Inquiry with the Inspector and he suggested that the eastern corner of the front garden was the most appropriate location. This is because it was the most accessible location for visitors. We have sought to confirm this record in writing with the Enforcement Officer at Camden Council who has noted that this is accurate and the email is attached to this letter.

The only alternative would be to locate the cycle stand at the rear of the property, but this would be problematic for visitors as they would have to take their bicycles down a flight of stairs and would cause significant accessibility issues.

At the site visit the potential for a covered cycle stand was discussed, but it was agreed that this would be detrimental to the character and appearance of the Conservation Area, given that the cycle stand had to be located at the front of the property for accessibility reasons. It was therefore agreed that an uncovered stand would be appropriate in this instance.

We would also seek to provide step free access to the cycle storage area, by creating a small concrete ramp from the entrance by the gate, down into the front garden, where there is a level change of circa 200mm. Its proposed location is highlighted in Appendix A1 and would be of a sufficient gradient to allow bikes to be comfortably wheel down it.

c. Specification

We are proposing the installation of two Sheffield type stands with a galvanised steel finish as shown in Appendix A2. The spacing dimensions would meet the Planning Portal recommendations shown in Appendix A3. For reference, the plan denotes the area within which the cycle stands will be located and the individual dimensions are contained within Appendix A2.

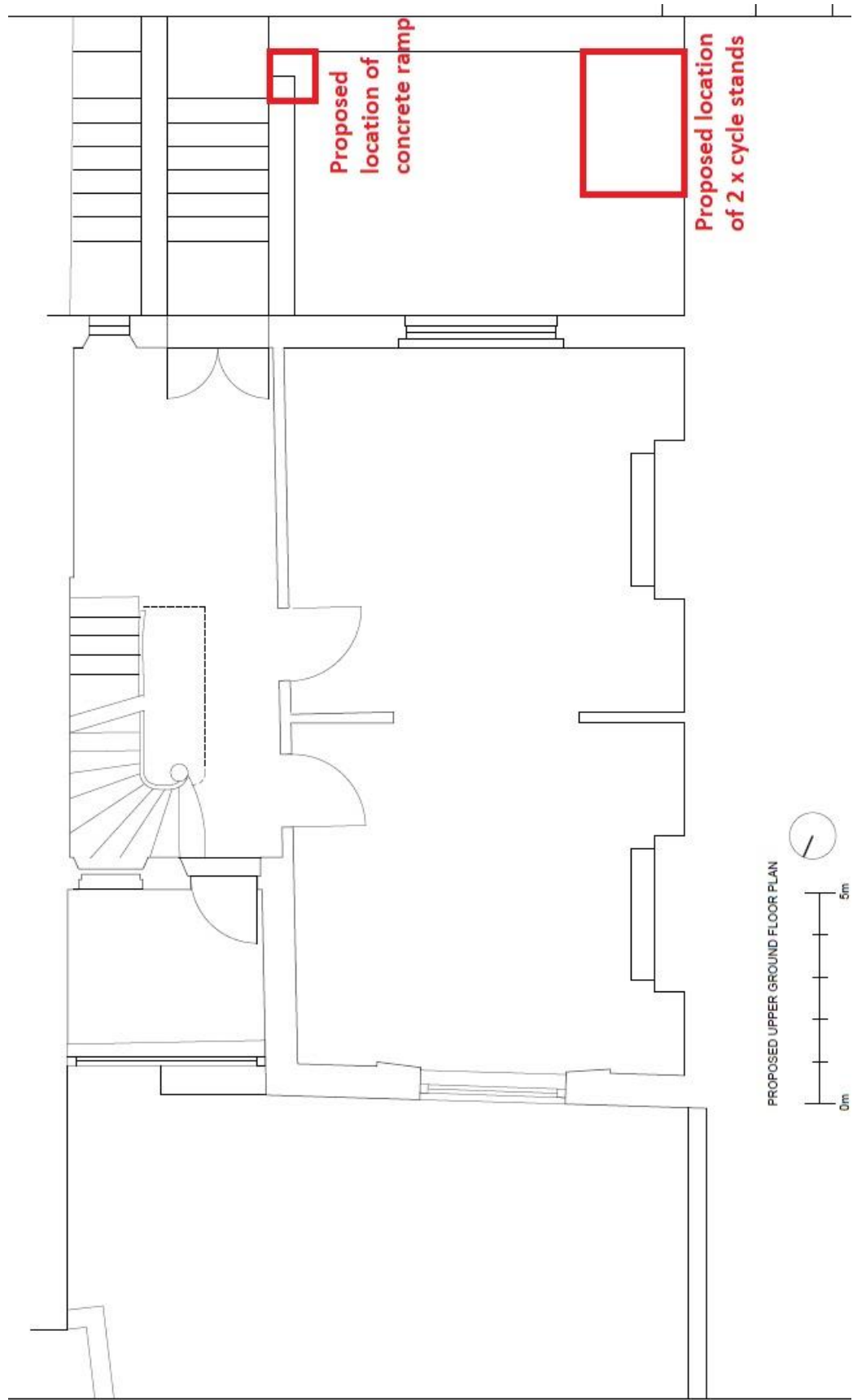
We have tested the manoeuvrability of the route through the gate to the proposed location of the cycle stands on site and there is ample spaces, even with large bulky bikes.

We trust that the above is clear, but please contact me if you require any clarifications.

Yours sincerely,

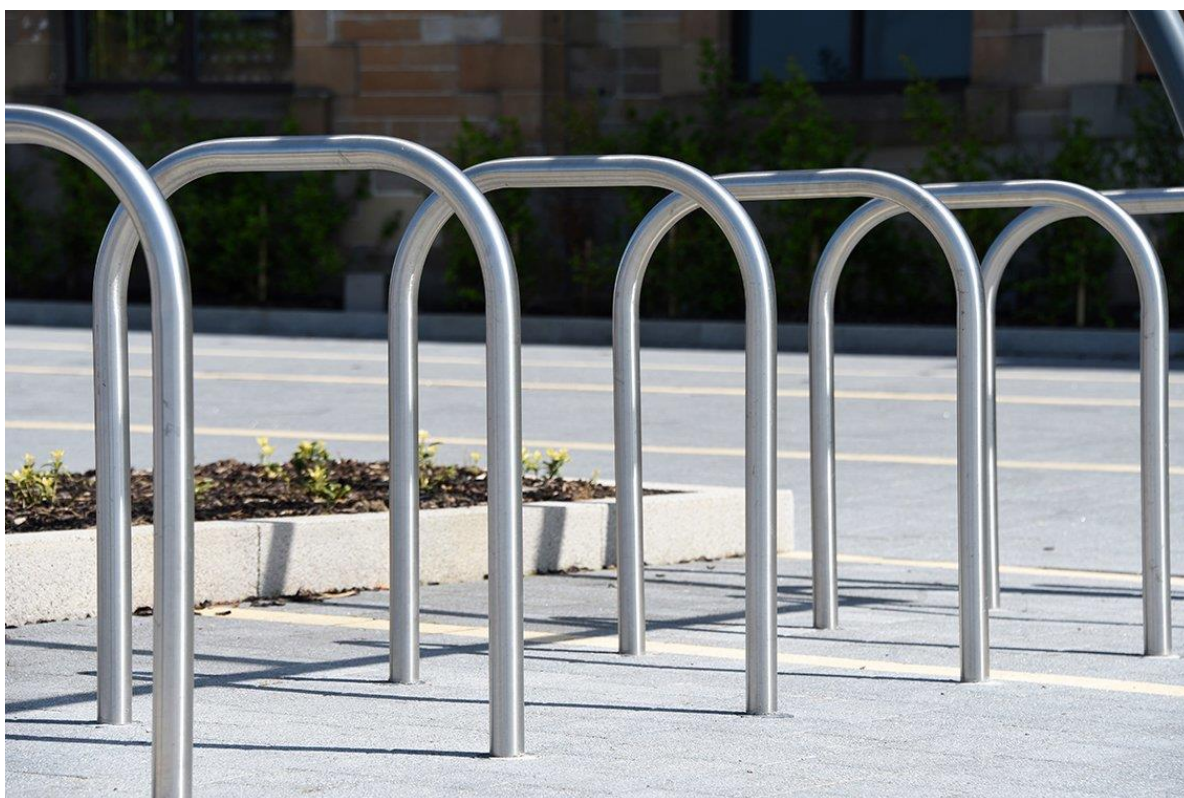
Jamie Sullivan
DIRECTOR

Appendix A1 – Cycle Stand Location

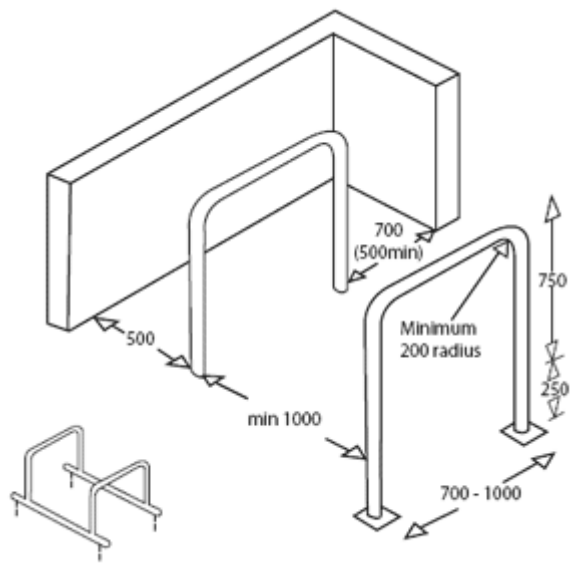


Appendix A2 – Cycle Stand Specifications

<https://www.broxap.com/sheffield-cycle-stand-4.html>



Appendix A3 – Cycle Stand Spacing Dimensions



Note: All dimensions are in millimetres