

Construction/ Demolition Management Plan

pro forma

Contents

Revisions	3
Introduction	4
Timeframe	6
Contact	7
Site	9
Community liaison	12
Transport	14
Environment	26
Agreement	31

Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
17/04/2020	1	Ian Jones

Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by

Introduction

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to all construction activity both on and off site that impacts on the wider environment.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any cumulative impacts of other nearby construction sites, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and nature of development. Further policy guidance is set out in Camden Planning Guidance **(CPG) 6: Amenity** and **(CPG) 8: Planning Obligations**.

This CMP follows the best practice guidelines as described in the [Construction Logistics and Community Safety \(CLOCS\)](#) Standard and the [Guide for Contractors Working in Camden](#).

Camden charges a [fee](#) for the review and ongoing monitoring of CMPs. This is calculated on an individual basis according to the predicted officer time required to manage this process for a given site.

The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise during construction. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

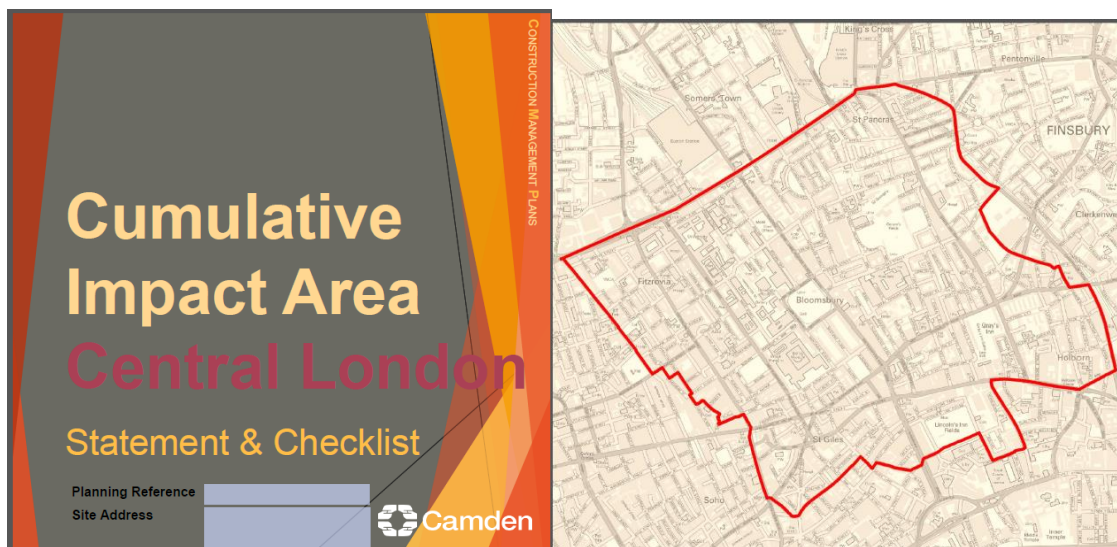
Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP. Please only provide the information requested that is relevant to a particular section.

(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction etc.)

Revisions to this document may take place periodically.

IMPORTANT NOTICE: If your site falls within a Cumulative Impact Area (as of 03/02/2020 to 03/08/2020 there is only one established CIA for the Central London area) you are required to complete the CIA Checklist and circulate as an appendix to the CMP and included as part of any public consultation – a CMP submission will not be accepted until evidence of this has been supplied.

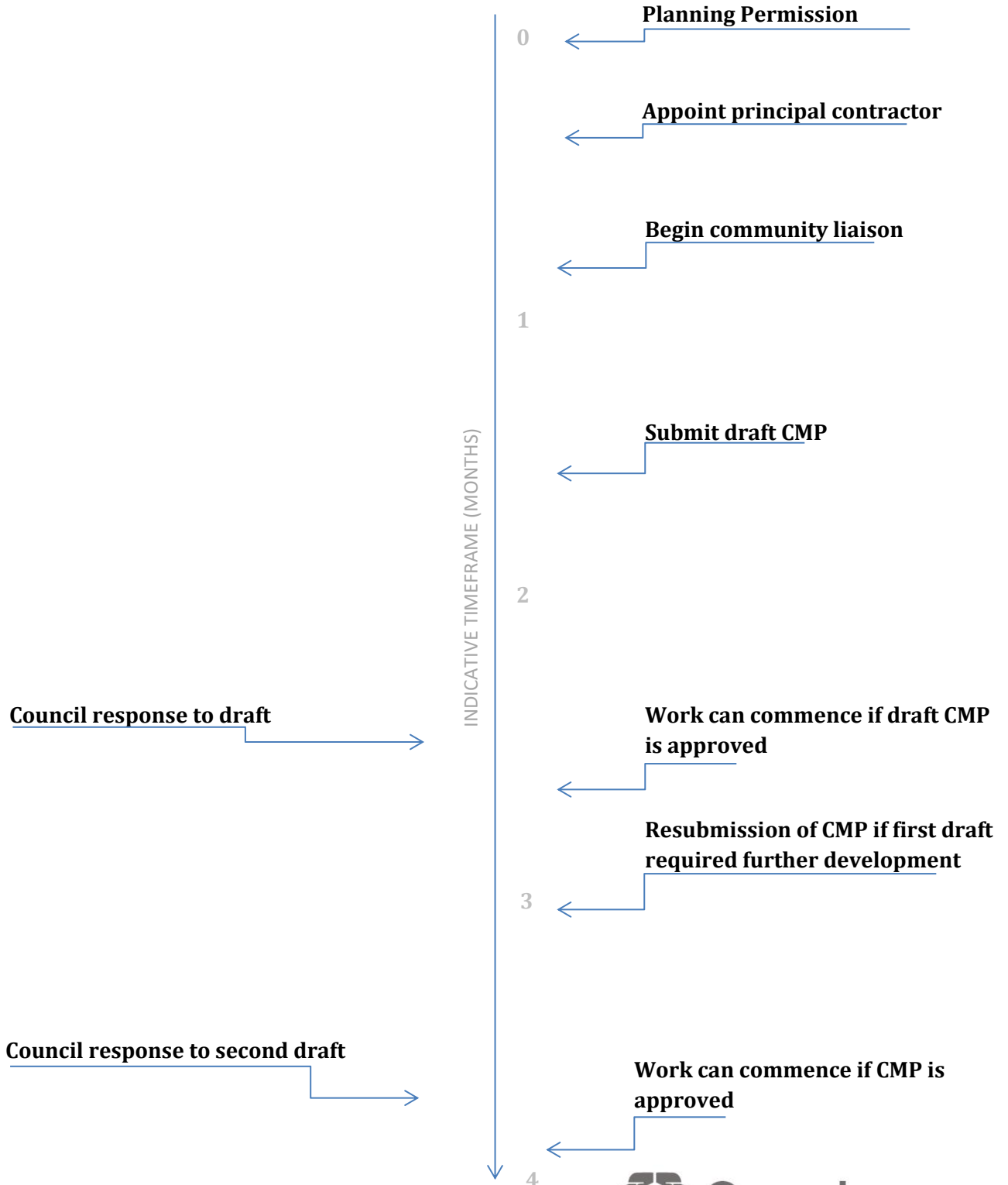
The CIA Checklist can be found at <https://www.camden.gov.uk/about-construction-management-plans#sumf>



Timeframe

COUNCIL ACTIONS

DEVELOPER ACTIONS



Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: 12-42 Southampton Row & 1-4 Red Lion Square

Planning reference number to which the CMP applies: TBC

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Ian Jones

Address: Sheet St, 16-19 Eastcastle Street, Fitzrovia, London, W1W 8DY

Email: ian.jones@sheetst.com

Phone: 07557 191 867

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: TBC

Address: TBC

Email: TBC

Phone: TBC

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of Community Investment Programme (CIP), please provide contact details of the Camden officer responsible.

Name: TBC

Address: TBC

Email: TBC

Phone: TBC

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: TBC

Address: TBC

Email: TBC

Phone: TBC

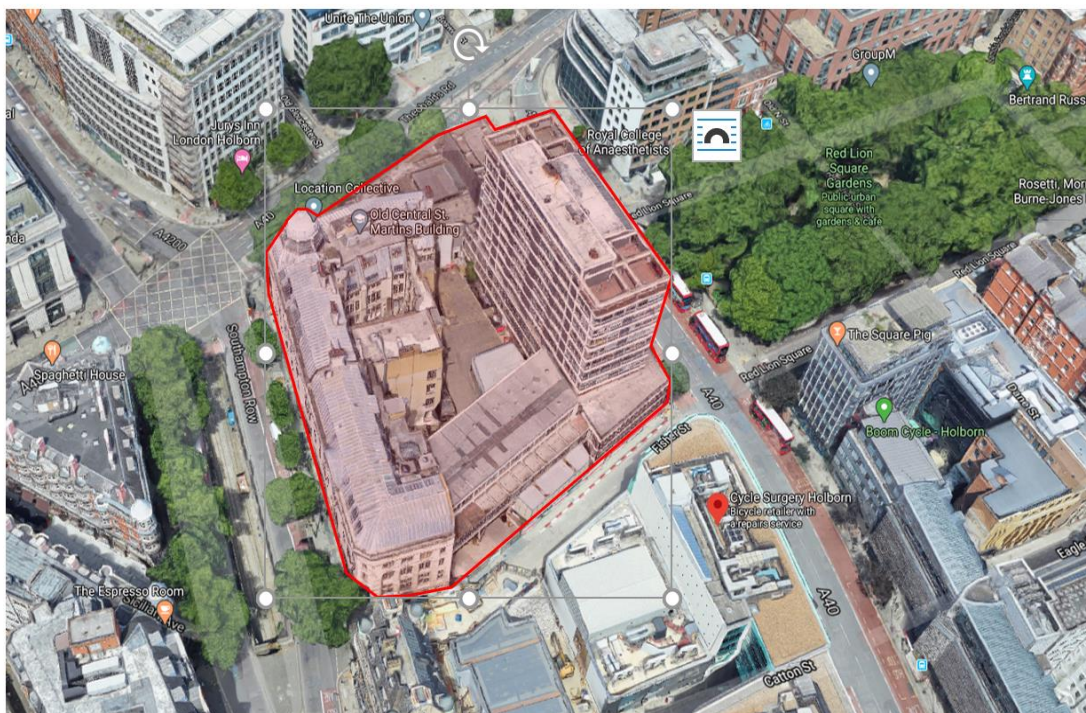
Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

The former Central St Martins College (University of the Arts) is a key site in the Holborn area and is allocated in the Council's adopted Site Allocations Document, 2013 and emerging Draft Camden Site Allocations Local Plan, 2020 and draft Holborn Vision, 2019 for mixed-use development including hotel, cultural, office and retail use, as well as residential. It lies within the Holborn Growth Area.

The former Cochrane Theatre would be demolished, the structure of the eleven storey 1960s slab and podium block (which fronts Red Lion Square and Fisher Street) would be retained and the building extended and significantly refurbished to create a ground plus 14 storey building. A stand-alone building is proposed with a cultural use at lower level and 34 affordable housing units above (mix of London Affordable Rent and Intermediate Rent). The conference and event facilities would largely be located within a new three storey basement. The listed Lethaby building would be altered internally with some careful and considered internal and external alterations in association with its conversion to a hotel.

The property is an island site surrounded by Southampton Row to the west, Procter Street/Drake Street/Red Lion Square to the east, Theobald's Road to the north and Fisher Street to the south.



7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

The proposal is for the partial redevelopment of the site, to provide a hotel and conference facility (including events space) with ancillary uses (café, bar, restaurant), a cultural use, maker space and affordable housing.

Construction comprises of demolition works, basement excavation and piling, installation of the main structure, cladding and fit out.

Careful consideration will need to be taken with the local residents and access/egress through the construction process.

8. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal).

Project Start Date	Q3 2021
Project Completion Date	Q1 2025
Demolition, enabling works and temporary works	56 weeks
Shell & Core	120 weeks
Fit out all areas	104 weeks

9. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

Working Times - Monday to Friday	08.00 to 18.00*
Working Times - Saturday	08.00 to 13.00*

Community Liaison

A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft.

This consultation must relate to construction impacts, and should take place following the granting of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. **The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off.** This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements and/or generate significant sustained noise levels should consider establishing contact with other sites in the vicinity in order to manage these impacts.

The Council can advise on this if necessary.

10. Sensitive/affected receptors

Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

Jury's Inn

Royal College of Anaesthetists (Red Lion Square)

Red Lion Square residents

11. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents **prior to submission of the first draft CMP**.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation should be included. Details of meetings including minutes, lists of attendees etc. should be appended.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

Statement of community involvement has been prepared and issued by Quatro Public Relations

12. Construction Working Group

For particularly sensitive/contentious sites, or sites located in areas where there are high levels of construction activity, it may be necessary to set up a construction working group.

If so, please provide details of the group that will be set up, the contact details of the person responsible for community liaison and how this will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

Community liaison meetings will be arranged by the Main Contractor in addition to newsletters circulated to the local residents.

13. Schemes

Please provide details of your Considerate Constructors Scheme (CCS) registration. Please note that Camden requires [enhanced CCS registration](#) that includes CLOCS monitoring. Please provide a CCS registration number that is specific to the above site.

Contractors will also be required to follow the [Guide for Contractors Working in Camden](#). Please confirm that you have read and understood this, and that you agree to abide by it.

A CCS registration number will be available and provided closer to when the works are due to commence onsite.

The Guide for Contractors working in Camden will be abided to.

14. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

There is a hotel scheme on Fisher Street, immediately south of the site. This has a resolution to grant planning permission. The contractors will liaise with one another.

Transport

This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the CLOCS Standard.

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by CCS monitors as part of your enhanced CCS site registration, and possibly council officers, to ensure compliance. Please refer to the CLOCS Standard when completing this section.

Please contact CLOCS@camden.gov.uk for further advice or guidance on any aspect of this section.

CLOCS Contractual Considerations

15. Name of Principal contractor:

TBC

16. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our [CLOCS Overview document](#) and [Q18 example response](#)).

All vehicles coming to site will be inspected at the security point next to the site gates before entering to ensure that the vehicle is in a safe condition (CLOCS compliance check). A further inspection at the gate security access point will be carried out when vehicles exit the site so that a check can be made to make sure any necessary documentation has been provided / completed and that any loads removed from site are secured / covered as appropriate.

17. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers:

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers:

Please contact CLOCS@camden.gov.uk for further advice or guidance on any aspect of this section.

Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

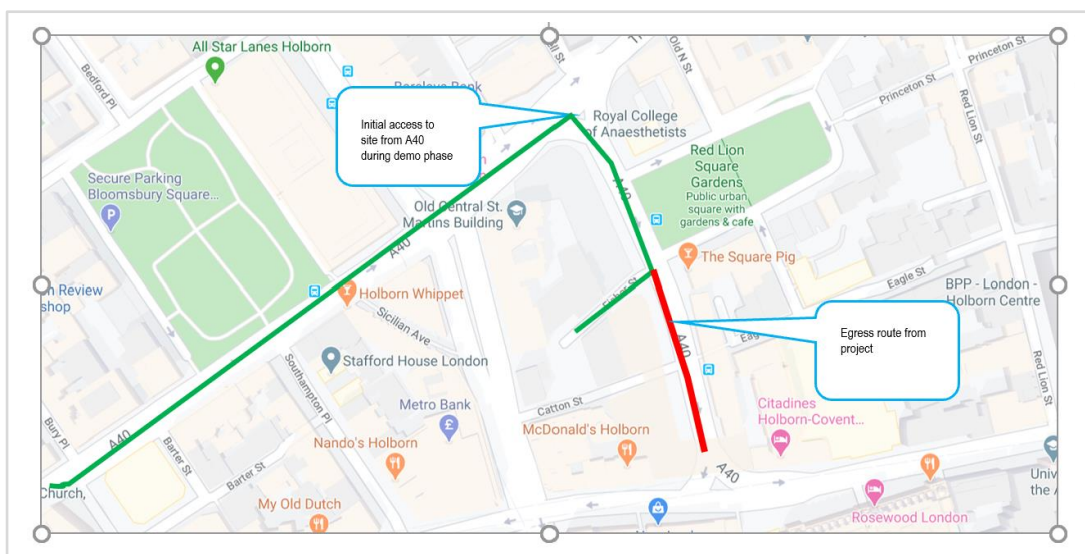
18. Traffic routing: *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, stations, public buildings, museums etc.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

Please show vehicle approach and departure routes between the site and the Transport for London Road Network (TLRN). Please note that routes may differ for articulated and rigid HGVs.

Routes should be shown clearly on a map, with approach and departure routes clearly marked. If this is attached, use the following space to reference its location in the appendices.



b. Please confirm how contractors and delivery companies will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

The nominated route to and from site will be advised in writing to all interested parties particularly the waste lorry companies and concrete suppliers as these will make the majority of site vehicle movements. Defined separation of vehicles and pedestrians by identified routes into and onsite confirmed by a site developed Site Traffic Management plan.

19. Control of site traffic, particularly at peak hours: *“Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)*

Construction vehicle movements should be restricted to the hours of 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays. If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to the hours of 9.30am and 3pm on weekdays during term time.

Vehicles may be permitted to arrive at site at 8.00am if they can be accommodated on site. Where this is the case they must then wait with their engines switched off.

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors.

Please provide details of the types of vehicles required to service the site and the approximate number of deliveries per day for each vehicle type during the various phases of the project.

For Example:

32t Tipper: 10 deliveries/day during first 4 weeks

Skip loader: 2 deliveries/week during first 10 weeks

Artic: plant and tower crane delivery at start of project, 1 delivery/day during main construction phase project

18t flatbed: 2 deliveries/week for duration of project

3.5t van: 2 deliveries/day for duration of project

Description		Loads per day	
		Average	Peak
Demolition	Demolition Works	15-25	30-40
Substructure Excavation & Construction	Underpinning	2 to 5	10 to 15
	Piling	10 to 15	20 -25
	Excavation	50 -70	75-85
	Construction	20-30	35-50

b. Cumulative affects of construction traffic servicing multiple sites should be minimised where possible. Please provide details of other developments in the local area or on the route that might require deliveries coordination between two or more sites. This is particularly relevant for sites in very constrained locations.

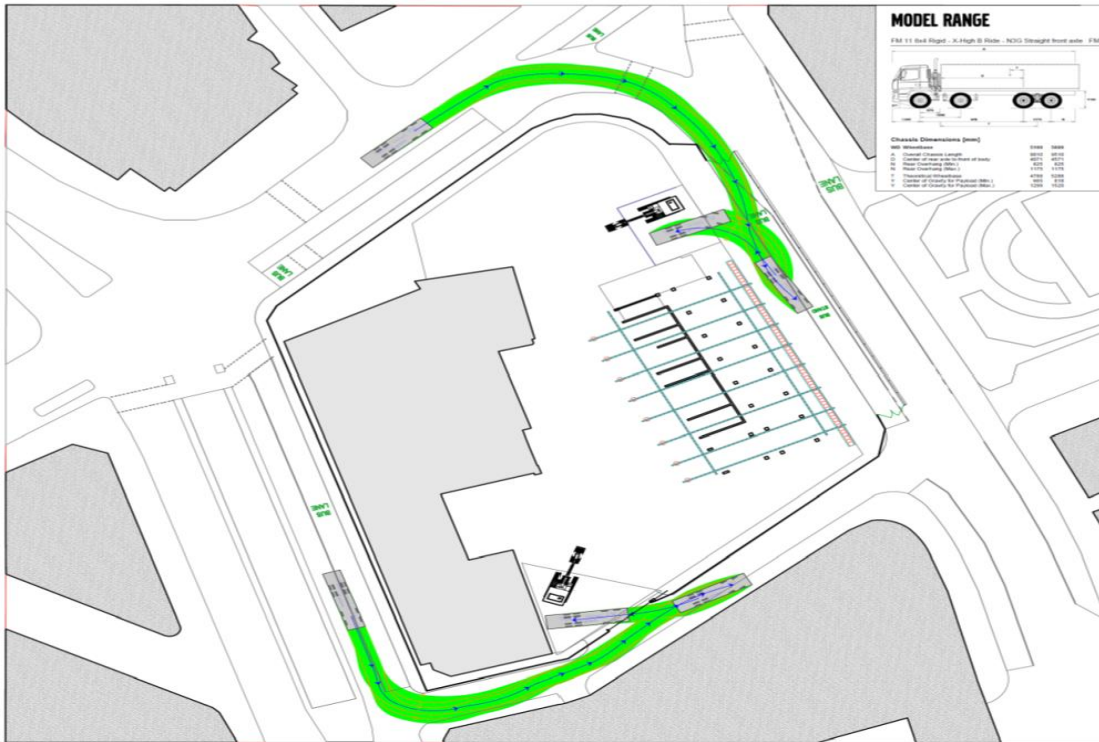
Careful consideration will be required for deliveries taking place between Drake Street and Fisher Street.

c. Please provide swept path analyses for constrained manoeuvres along the proposed route.

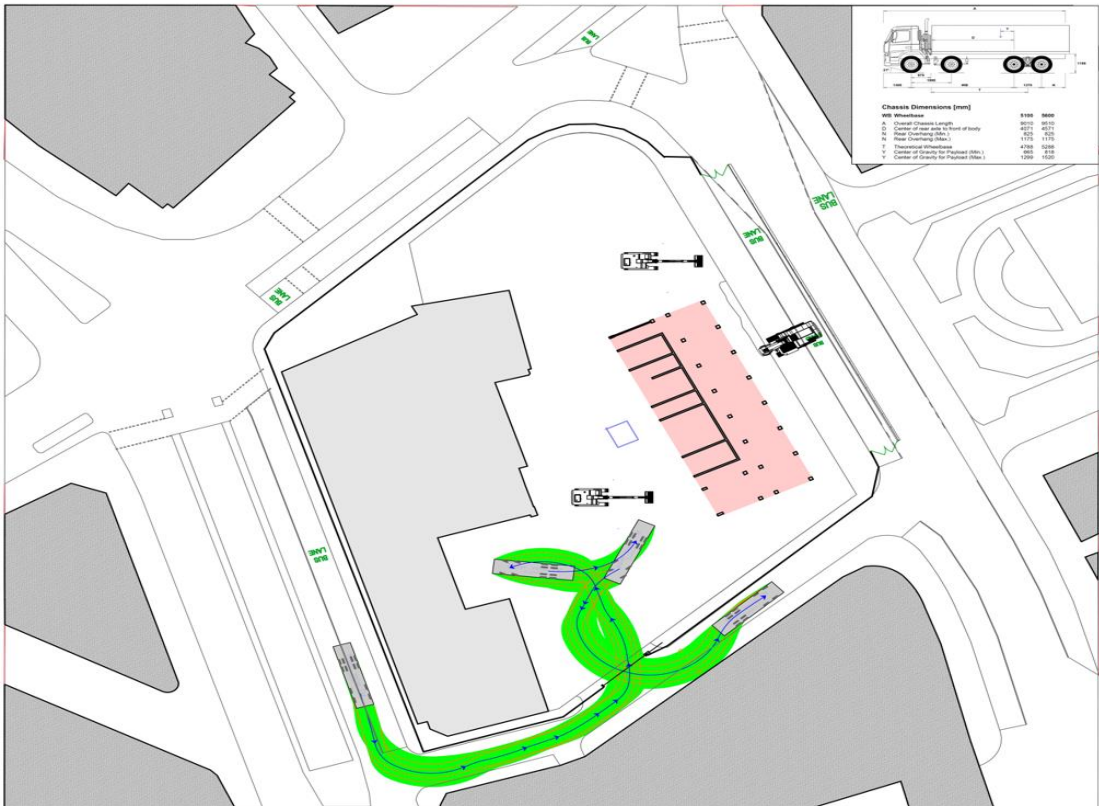
Arctic



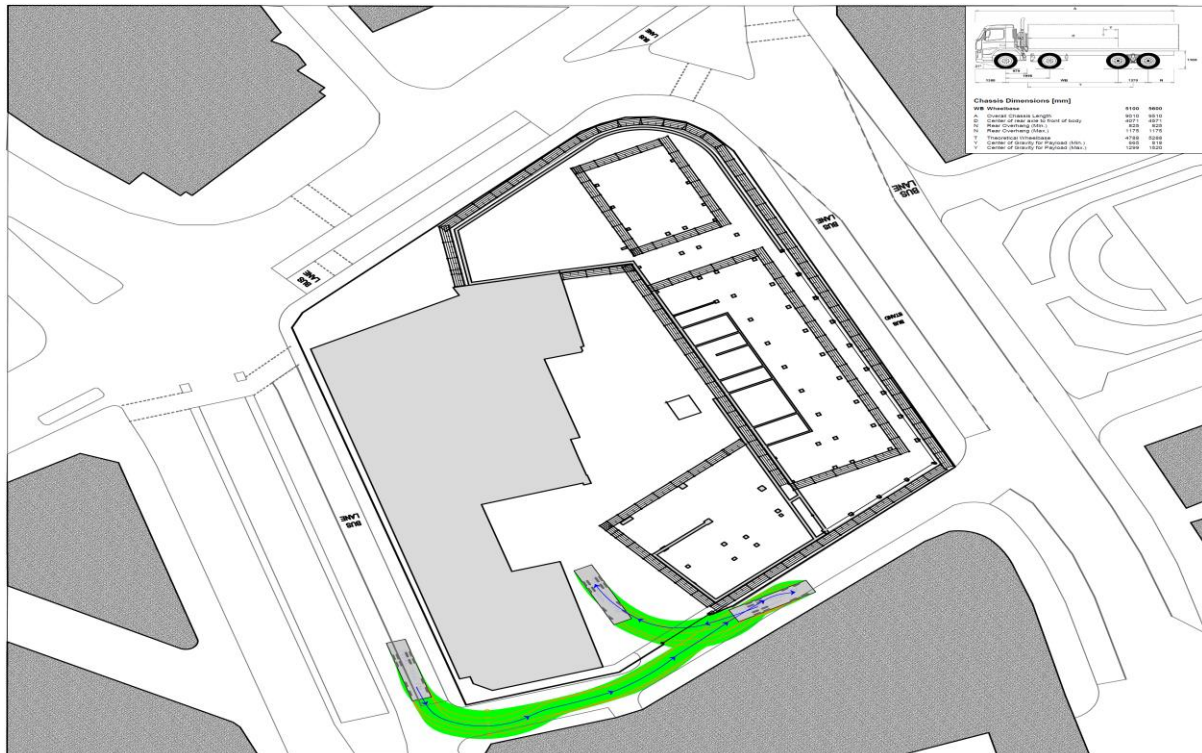
Tipper



Tipper



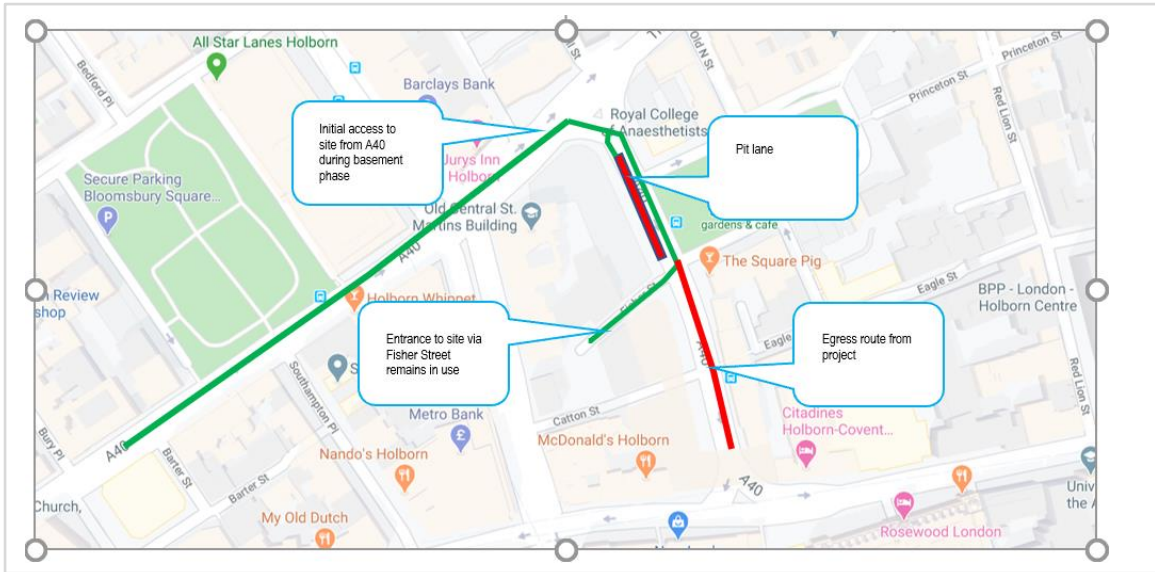
Demolition Phase



d. Consideration should be given to the location of any necessary holding areas/waiting points for sites that can only accommodate one vehicle at a time/sites that are expected to receive large numbers of deliveries. Vehicles must not queue or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

Please identify the locations of any off-site holding areas or waiting points. This can be a section of single yellow line that will allow the vehicle to wait to phone the site to check that the delivery can be accommodated.

Please refer to question 24 if any parking bay suspensions will be required to provide a holding area.



e. Delivery numbers should be minimised where possible. Please investigate the use of construction material consolidation centres, and/or delivery by water/rail if appropriate.

Agreed.

f. Emissions from engine idling should be minimised where possible. Please provide details of measures that will be taken to reduce delivery vehicle engine idling, both on and off site (this does not apply to concrete mixers).

The site management team will check the use of the designated routes by vehicles leaving site. No vehicles will be allowed to park in any of the adjacent roads particularly with engines left ticking over; and there will be no parking within the constraints of the site.

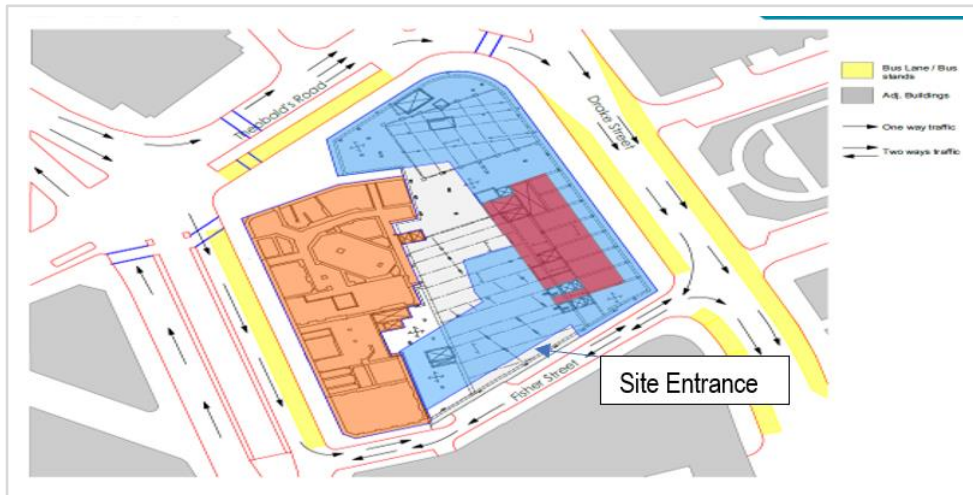
20. Site access and egress: *“Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.”* (P18, 3.4.3)

This section is only relevant where vehicles will be entering the site. Where vehicles are to load from the highway, please skip this section and refer to Q23.

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic marshals must ensure the safe passage of all traffic on the public highway, in particular pedestrians and cyclists, when vehicles are entering and leaving site, particularly if reversing.

Traffic marshals, or site staff acting as traffic marshals, should hold the relevant qualifications required for directing large vehicles when reversing. Marshals should be equipped with 'STOP – WORKS' signs (not STOP/GO signs) if control of traffic on the public highway is required. Marshals should have radio contact with one another where necessary.

a. Please detail the proposed site access and egress points on a map or diagram. If this is attached, use the following space to reference its location in the appendices.



b. Please describe how the access and egress arrangements for construction vehicles in and out of the site will be managed, including the number and location of traffic marshals where applicable. If this is shown in an attached drawing, use the following space to reference its location in the appendices.

The delivery of all materials, plant and equipment will be pre-planned and to designated times of the day to minimise the impact on the local area from the continuous accessing and egressing of the site by vehicles. Any deliveries and movements of large vehicles will be restricted to outside of peak times to avoid any significant impact on the local traffic. Peak times are 8:30 – 9am and 15-15:30 (Monday to Friday) which will be avoided where possible throughout the works. All goods deliveries will be to a designated area on site where all vehicles will be unloaded, and the materials taken to the appropriate storage area immediately.

To ensure an efficient management of and minimising the number of vehicles coming to and from site, where required in liaison with site management requirements a designated management representative will be appointed to act as the Site Transport Co-ordinator. Who will manage all the waste removal collections and documentation, and for the efficient ordering and delivery of site consumables, all of which will be at agreed appointed times.

All such vehicle movements will be under the strict control of appointed Banksmen and this system will be subject to a site speed limit of 5mph

c. Please provide swept path drawings for vehicles accessing/egressing the site if necessary. If these are attached, use the following space to reference their location in the appendices.

Included in 19.c

d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled. Please note that wheel washing should only be used where strictly necessary, and that a clean, stable surface for loading should be used where possible.

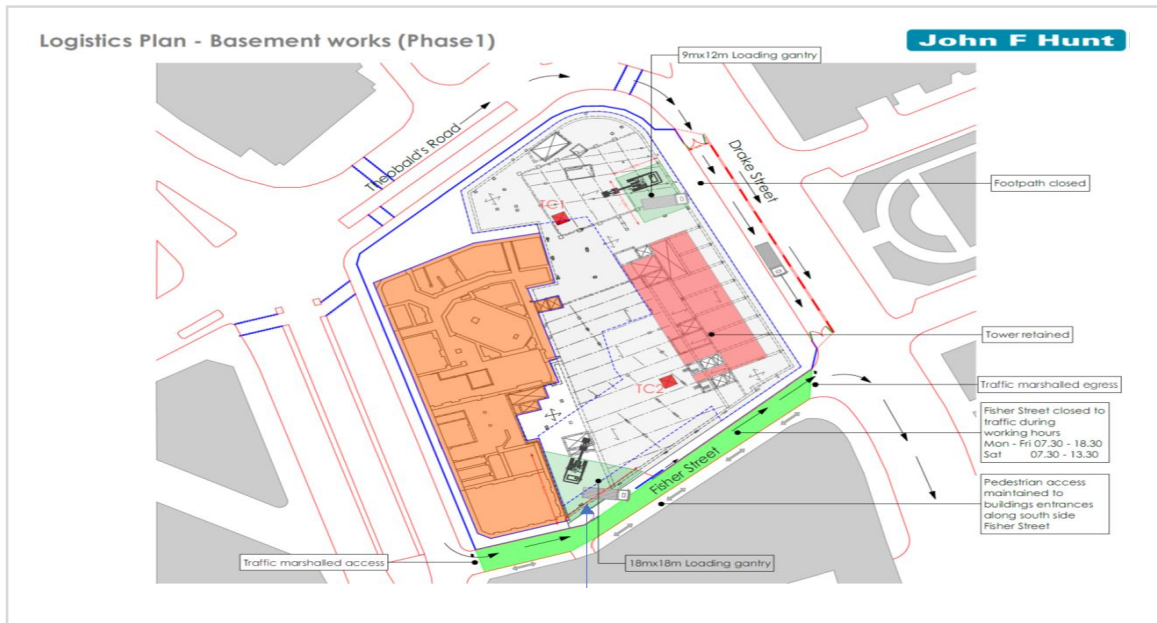
Wheel washing facilities will be fitted by the site exit to reduce the dirt and dust from leaving site.

21. Vehicle loading and unloading: *“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)*

This section is only relevant if loading/unloading is due to take place off-site on the public highway. If loading is taking place on site, please skip this section.

a. please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will

access and egress the site. If this is attached, use the following space to reference its location in the appendices. Please outline in question 24 if any parking bay suspensions will be required.



b. Where necessary, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded. Please provide detail of the way in which marshals will assist with this process, if this differs from detail provided in Q20 b.

Please refer to Q20.b

Street Works

Full justification must be provided for proposed use of the public highway to facilitate works. Camden expects all options to minimise the impact on the public highway to have been fully considered prior to the submission of any proposal to occupy the highway for vehicle pit lanes, materials unloading/crane pick points, site welfare etc.

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

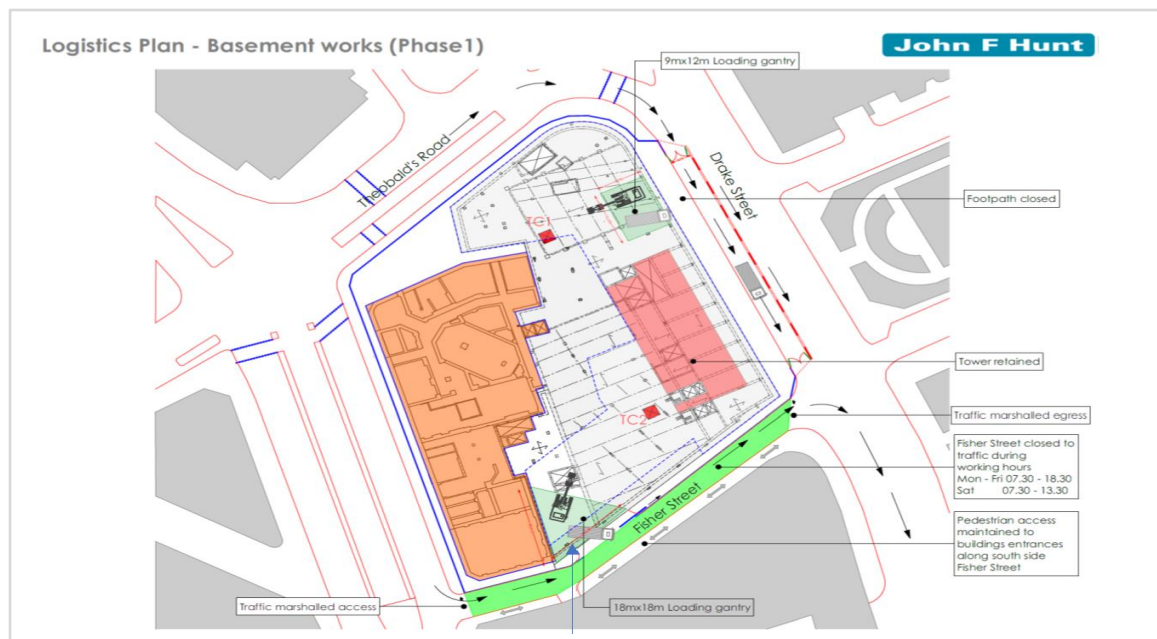
Please note that there is a two week period required for the statutory consultation process to take place as part of a TTO.

If the site is on or adjacent to the TLRN, please provide details of preliminary discussions with Transport for London in the relevant sections below.

If the site conflicts with a bus lane or bus stop, please provide details of preliminary discussions with Transport for London in the relevant sections below.

22. Site set-up

Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents, relevant street furniture, and proposed site access locations. If these are attached, use the following space to reference their location in the appendices.



23. Parking bay suspensions and temporary traffic orders

Parking bay suspensions should only be requested where absolutely necessary and these are permitted for a maximum of 6 months only. For exclusive access longer than 6 months, you will be required to obtain a [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and/or TTO's which would be required to facilitate the construction - include details of the expected duration in months/weeks. Building materials and equipment must not cause obstructions on the highway as per your CCS obligations unless the requisite permissions are secured.

Information regarding parking suspensions can be found [here](#).

N/A

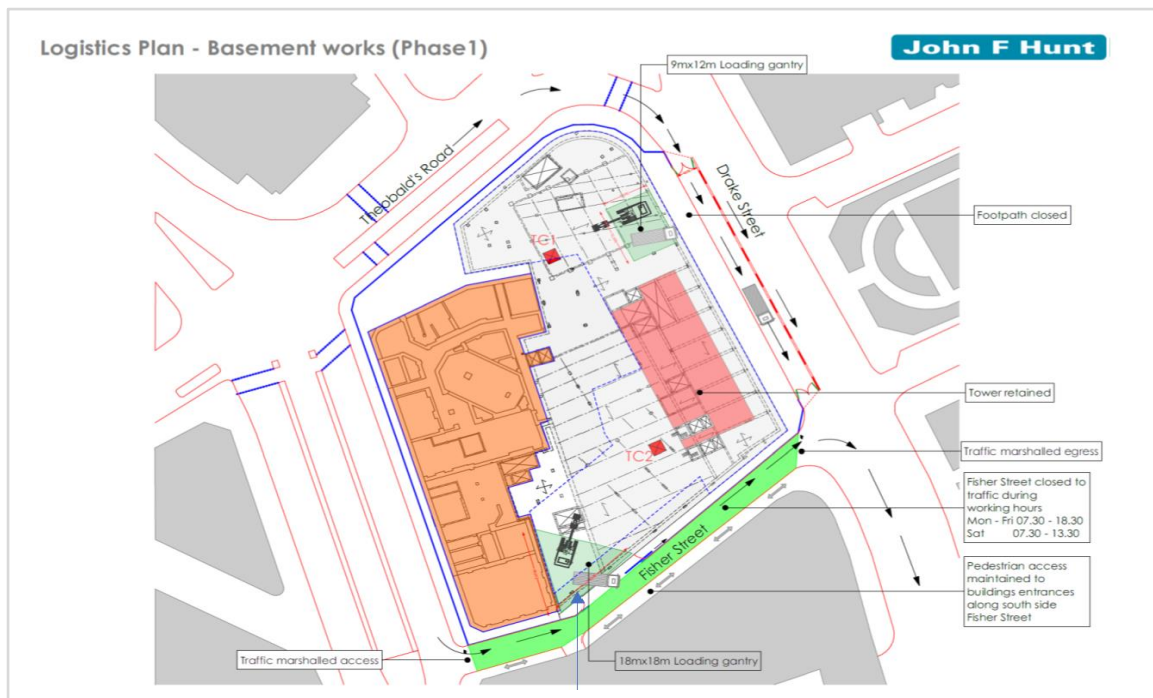
24. Occupation of the public highway

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

a. Please provide justification of proposed occupation of the public highway.

Due to the complexity of the basement excavation and piling, we have no option but to seek the closure of the footpath along Drake Street for a period of time to be agreed with the Council

b. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses, removal of street furniture etc). If these are attached, use the following space to reference their location in the appendices.



25. Motor vehicle and/or cyclist diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period. Please show locations of diversion signs on drawings or diagrams. If these are attached, use the following space to reference their location in the appendices.

The only diversion applicable, will be directing pedestrians to the other side of Drake Street whilst demolition of the Tower block is being carried out. All appropriate signage will be provided nearer to the time.

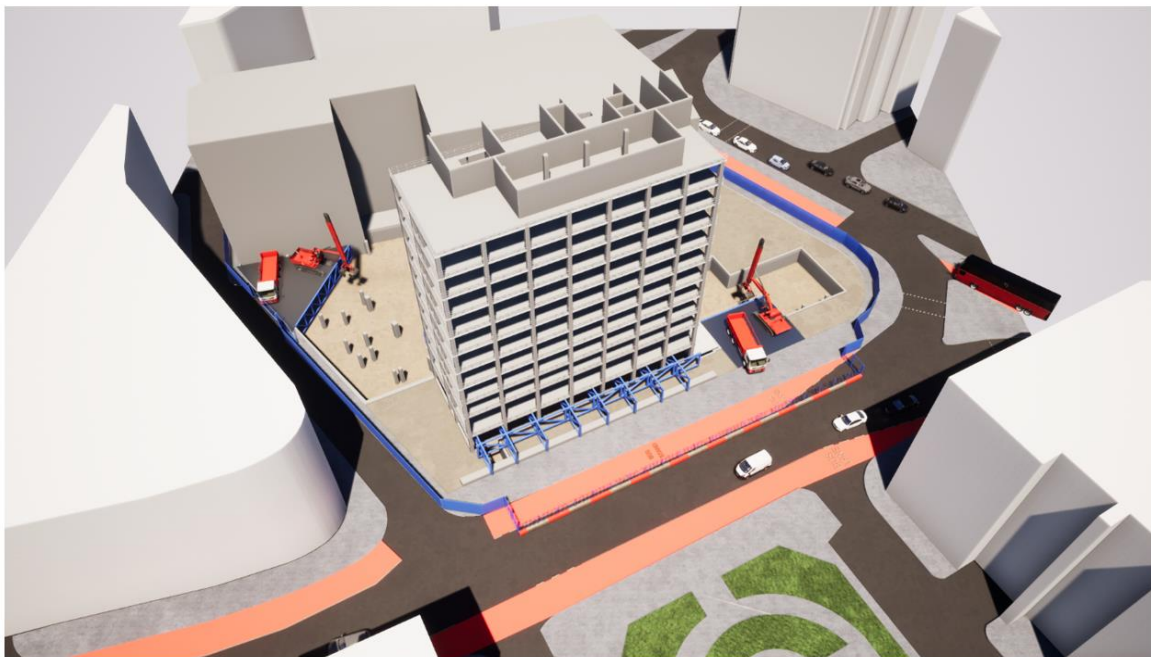
26. Scaffolding, hoarding, and associated pedestrian diversions

Pedestrians safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramps must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions, and hoarding should not restrict access to adjoining properties, including fire escape routes. Lighting and signage should be used on temporary structures/skips/hoardings etc.

A secure hoarding will generally be required at the site boundary with a lockable access.

a. Where applicable, please provide details of any hoarding and/or scaffolding that intrudes onto the public highway, describing how pedestrian safety will be maintained through the diversion, including any proposed alternative routes. Please provide detailed, scale drawings that show hoarding lines, gantries, crane locations, scaffolding, pedestrian routes, parking bay suspensions, remaining road width for vehicle movements, temporary vehicular accesses, ramps, barriers, signage, lighting etc. If these are attached, use the following space to reference their location in the appendices.



b. Please provide details of any other temporary structures which would overhang/oversail the public highway (e.g. scaffolding, gantries, cranes etc.) If these are attached, use the following space to reference their location in the appendices.

Please see images in 26.a. The S106 CMP will provide firmer details once the contractor is appointed

27. Services

Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

The existing UKPN substation will be stripped out and removed, although it may be retained to provide a temporary builder's supply during the early phases of works. A new UKPN incoming high voltage (11kV) installation will serve the site. This will include a new high voltage (HV) incoming service cables and switchroom at ground floor level, entering the site from Fisher Street. The building electrical services will be bulk-metered at this location at 11kV. The 11kV supply feeds directly into a Landlord-owned and operated high voltage switchroom, located at basement 03. The main switchpanel will comprise of a 4-cubicle vacuum switchgear assembly, rated for 250MVA 200A 11kV panel with integral protection relays. The room will also include 2No. 1MVA dry-type AF transformers. From this location a set of new 1600A busbars will feed the main LV switchroom immediately adjacent. A new LV distribution network will extend from this point to support LV power services in all parts of the development except the residential building.

Subject to survey, we envisage that the existing incoming mains cold water supplies associated with the site will be stripped back to the utility connection points in adjacent streets and new services installed in suitable locations to serve the new development. The new incoming mains water supplies will be provided with the appropriate isolating valves, check valves and BMS linked water meter.

Discussions with BT etc are ongoing and are not known at this stage.

Environment

To answer these sections please refer to the relevant sections of **Camden’s Minimum Requirements for Building Construction (CMRBC)**.

28. Please list all noisy operations and the construction method used, and provide details of the times that each of these are due to be carried out.

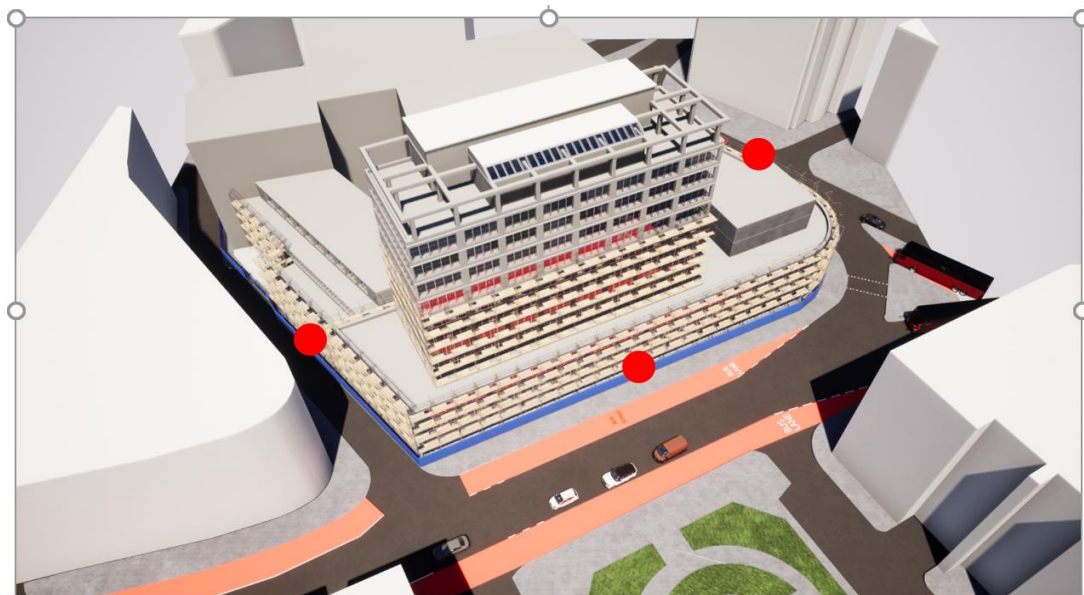
Demolition works will be carried out with demolition specification excavators equipped with rotating concrete crackers, concrete pulverisers and hammers. As far as reasonably practical crackers will be utilised in lieu of hammers. This methodology will significantly reduce the amount of noise and vibration generated by the works. Works will be carried out between the hours of 8am - 6pm, Monday to Friday and 8am - 1pm on Saturday.

To lessen noise migration from the site the site boundary will be enclosed by 2.4metre high erected hoarding installed.

All site noise, dust and vibration levels will be recorded by company owned monitoring

29. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

A noise survey will be carried out prior to any works being carried out and will be shared with Camden. See below proposed locations of noise and air quality points



30. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

This is not available at present, but will be issued in due course.

31. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

All demolition works will be undertaken by plant using 'quiet' hydraulic powered demolition pulverising attachments thus minimising the use of percussive impact breakers.

To lessen noise migration from the site, the site boundary will be enclosed by 2.4metre high erected hoarding installed.

Each section of the project will be planned to ensure all noisy working requirements are identified along with the timescales so such information can be advised to all concerned parties. There will be no site working during any anti-social hours.

The use of fully serviced plant with fully operational exhaust systems.

Ensuring all plant engine covers are kept closed at all times.

All site plant not in use will be shut down and not left idling on site.

All provided site generator plant will be of the new 'whisper' operational type.

The shouting out of instructions on site will be strictly forbidden, all site management and supervisors will be issued with site communication radios.

There will be no noisy working during any 'anti-social' hours or hours determined by the contract or in liaison with the London Borough of Camden Environmental Protection Team.

The playing of radios etc on site will be strictly forbidden at all times.

The sounding of vehicle hooters on site or in any adjacent street will be strictly forbidden at all times.

No commercial vehicles will be allowed to park in the adjacent streets waiting for access to the site, particularly with engines left 'ticking over'.

Where possible all site plant will be effectively silenced and located in such areas of the site so as to cause the minimum amount of noise migration to areas beyond the site boundary.

Maximum noise generation levels will be determined for each major item of plant from such information as supplied by manufacturers or company noise monitoring records. This will enable the potential level of noise generation to be anticipated.

A full noise level management programme will be developed in liaison with the Camden Council Environmental Protection Team.

Where appropriate to minimise noise emissions from within the building work areas all glazing will remain in place for as long as possible.

There will be no site activities or plant engines started or lorry movements to and from the site made before 8am and not after 6pm.

All plant deliveries and collections plus all waste management requirements will be coordinated to ensure the noise impact from all such vehicle's movements on the community is kept to a minimum and is within agreed times. This will be particularly relevant to the unloading and collection heavy plant.

32. Please provide evidence that staff have been trained on BS 5228:2009

All paperwork/certification will be provided prior to any works starting onsite.

33. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

Suppression of dust using water will be carried out at all key stages of activity –

- Damp down the floor area in advance of demolition activity starting
- Direct hose water jet dust suppression at the demolition work face during demolition
- Damp down the transit routes for plant involved in moving debris around the site along with any stockpiles of waste materials
- Damp down hard-core debris resulting from demolition activity before it is handled, moved or loaded into lorries

The access / protection scaffold enclosing the site perimeter of the building for demolition will be fully wrapped in Fire Retardant 'Monarflex' polythene sheeting while the site boundary will be protected by a combination of the existing retained 2.5-3mtr high masonry walls and a substantial 2.4mtr height plywood covered timber framed hoarding.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

All construction waste vehicles will be sheeted over prior to leaving site.

Installation of temporary water supply infrastructure for vehicle cleaning, jet wash and dust suppression will be used.

35. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

Due to the sensitivity of the site location site noise-monitoring will be undertaken for the duration of the project. With the noise levels being monitored by installed noise monitoring data loggers placed in predetermined locations around the site boundary. Spot noise levels will be taken on a regular basis by the visiting company Environmental manager, or appointed representatives, using hand held noise monitoring equipment at selected locations around the site where various items of plant are working.

The noise level assessments will include:

- Noise emissions from the individual demolition site processes
- Noise levels from operated demolition plant and plant moving around the site
- Noise emissions from traffic accessing and egressing the work site
- Noise emissions throughout piling, ground works and RC elements.

Baseline noise monitoring will be undertaken by the PC who will monitor the equipment being used and shall collect and collate all recorded results which will be issued to both Site management teams

36. Please confirm that a Risk Assessment has been undertaken at planning application stage in line with the GLA policy. [The Control of Dust and Emissions During Demolition and Construction 2104 \(SPG\)](#), that the risk level that has been identified, and that the appropriate measures within the GLA mitigation measures checklist have been applied. Please attach the risk assessment and mitigation checklist as an appendix.

A Risk Assessment will be carried out during the planning application and can be issued once in receipt. All measures listed within the Pro forma, should identify all known risks and how these can be mitigated.

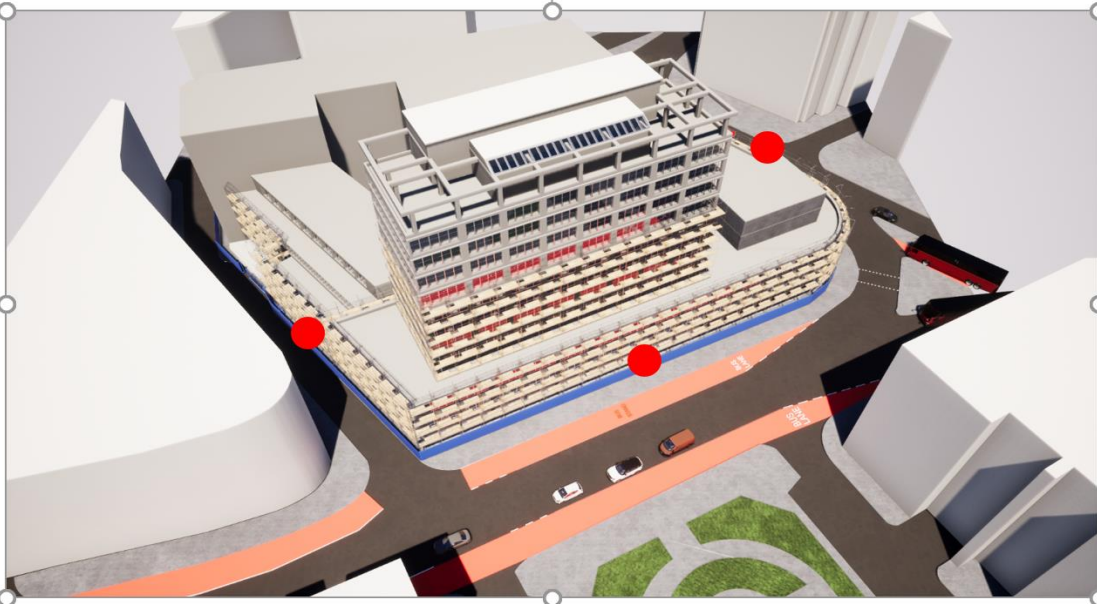
37. Please confirm that all of the GLA's 'highly recommended' measures from the [SPG](#) document relative to the level of risk identified in question 36 have been addressed by completing the [GLA mitigation measures checklist](#).

As per item 36. All risks will be identified and mitigation measures put in place.

- 38. If the site is a 'High Risk Site', 4 real time dust monitors will be required. If the site is a 'Medium Risk Site', 2 real time dust monitors will be required. The risk assessment must take account of proximity to sensitive receptors (e.g. schools, care homes etc), as detailed in the [SPG](#). Please confirm the location, number and specification of the monitors in line with the SPG and confirm that these will be installed 3 months prior to the commencement of works,

and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

See proposed locations below. The visiting Group Safety & Environmental Manager will use monitoring data to assess & report on the implemented impact control measures as part of the regular site safety inspections



39. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

To minimise the adverse impacts from pests and rodents the following control measures will be implemented on site in the following order;

- All drainage systems and access points will be kept secure to prevent rodent access
- All generated rubbish particularly food waste will be cleared as it is generated and placed into secure containers and removed off site for disposal on a continuous basis
- A high level of good housekeeping will be maintained on site and in all facilities
- Site rules will be implemented to prevent the feeding of such pests as pigeons and seagulls
- All food stuffs brought on site will be within storage containers
- Where all other control measures have been actioned then pest control management will be implemented on site by a reputable pest control company

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

A refurbishment and demolition survey was carried out on the 17th Dec 2018.

Asbestos was found in the building and will be removed by a licenced contractor prior to any works being carried out.

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

The PC will be responsible for the conduct of builders. This is usually implemented by use of a yellow/red card system. All operatives will be briefed in the site induction as to how they are expected to behave in and around the site. Designated smoking areas will be provided in locations so as not to impact the surrounding area.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

From 1st September 2015

(i) Major Development Sites – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

(ii) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

From 1st September 2020

(iii) Any development site - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

(iv) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period **Q3/2020 – Q3/2023**
- b) Is the development within the CAZ? **(Y)**
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? **(Y)**
- d) Please provide evidence to demonstrate that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: **To be provided at a later date.**
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection:
Agreed
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required:
Agreed

 SYMBOL IS FOR INTERNAL USE

Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed: *Ian Jones*.....

Date: 17/04/2020.....

Print Name: Ian Jones.....

Position: Project Manager.....

Please submit to: planningobligations@camden.gov.uk

End of form.