



Proposal for:  
**3A CAMDEN ROAD,  
LONDON, NW1 9LG**

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# 1.0 INTRODUCTION

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## 1.1 Executive Summary

**The purpose of this report is to highlight the restorative design alterations to be made to the property at 3a Camden Road.**

The proposal comprises a rear extension, as well as a roof extension to provide additional floor area and improved quality of living for the residential aspect of the building.

The extension to the roof will be visually in keeping with it's neighbours, (the other properties within the terrace, at nos. 1a, 3, 5a, and 5b Camden Road), so as to not damage the heritage of the street visually.

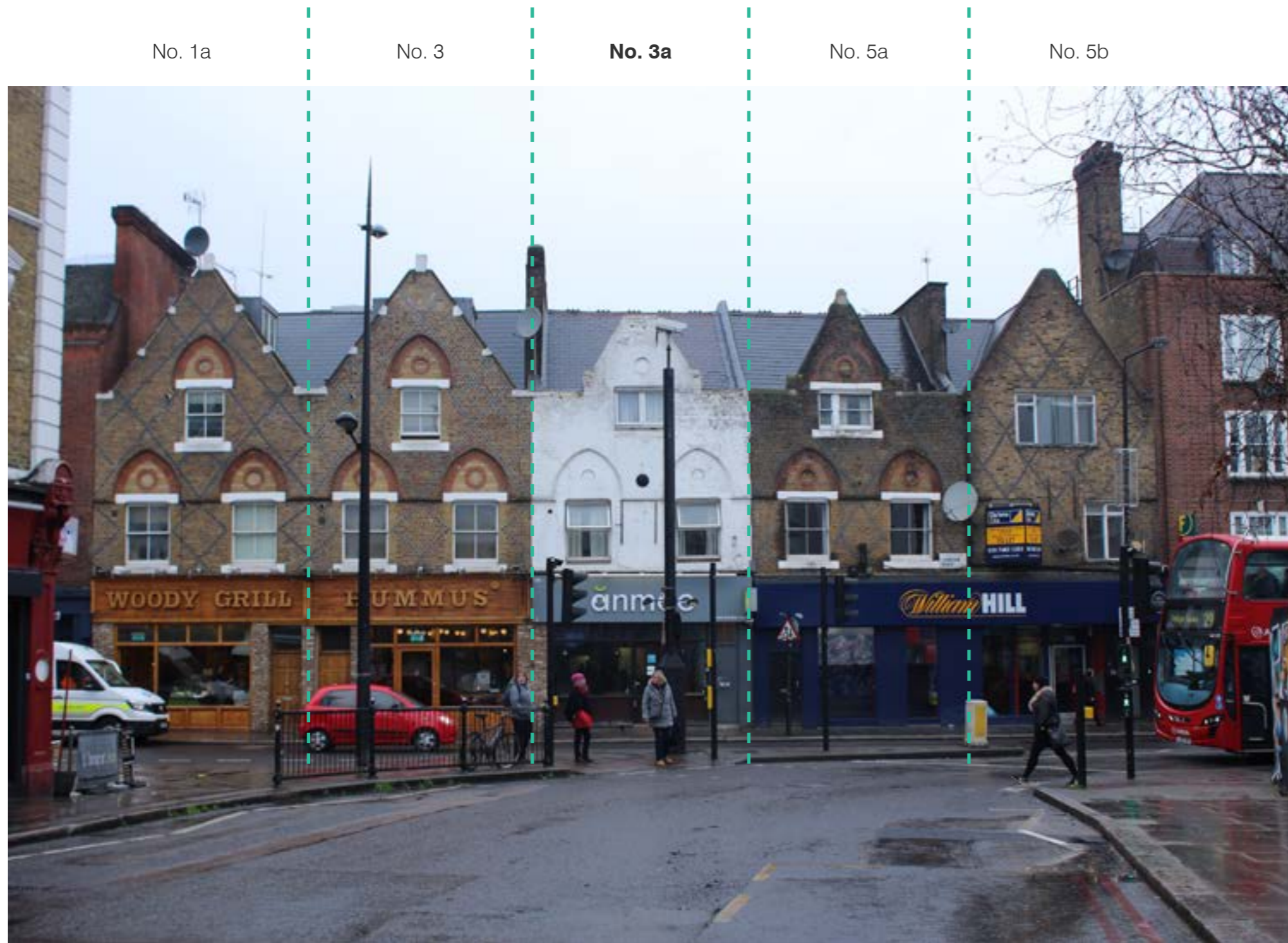
The site is well located and less than a minute's walk from Camden Town Underground Station. The station is also under review for potential redevelopment so it can 'properly support the vibrant local community and economy.' This will see a new entrance introduced on Buck Street, and the formation of new escalators and lifts, while the existing station is maintained. With the property at no. 3a being in such close proximity, there is likely to be a vast increase in footfall, positively affecting the retail unit at ground floor and accommodation on the above floors - hence a need for expansive works.

The proposed works will be carried out in a sensitive manner and will undertake the relevant procedures to ensure the outcome is a sustainably coherent design.

## 2.0 LOCATION

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- 2.1 The Site
- 2.2 Local Sites
- 2.3 Aerial View
- 2.4 Local Context
- 2.5 Conservation Area
- 2.6 Neighbour Planning History
- 2.7 Local Developments



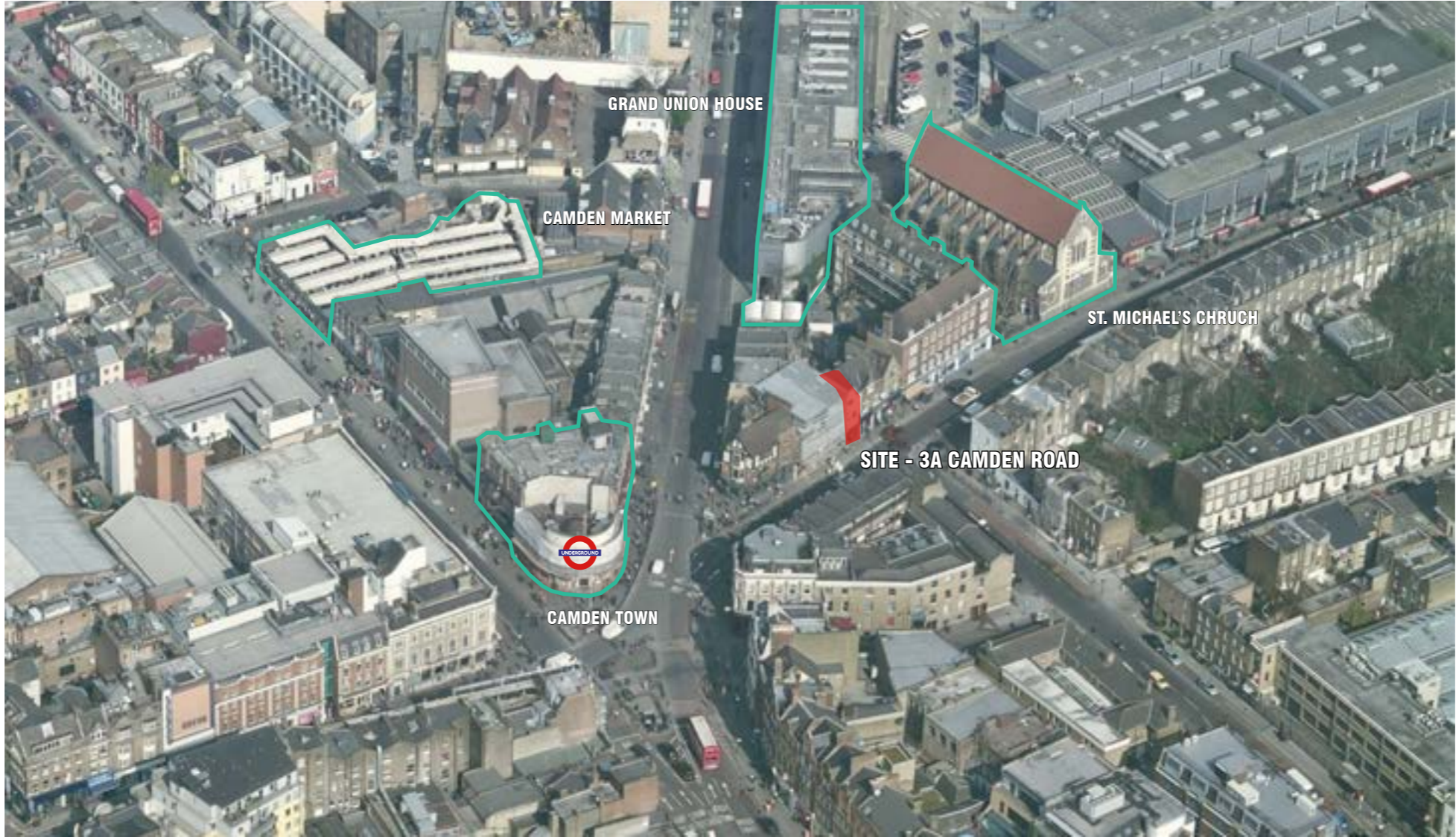
3a Camden Road is situated within a small row of 5 late Victorian 3-4 storey terraced properties. The gables vary in size and were inspired by the architecture of the Low Countries, whereby the terrace has a visual expression of properties being stitched together. With each individual property having it's slightly unique identity.

The ground floor currently operates as a Vietnamese restaurant (Class A3) and the property also houses two residential units above (Class C3).

The active pedestrian frontage is a common typology shared along this part of Camden Rd. Much of the immediate High Street is poorly maintained and there are strong signs that the surrounding area is undergoing a gradual transition of improvements. The majority of the immediate buildings vary in use with some residential and some office use at upper floor levels and mostly retail at ground floor.

A vast amount of buildings in the area date back to the mid-late 19th century with timber sash windows, yellow London stock brick in Flemish bond, brick arch lintels and cornice and corbelled detailing making up what is considered a typical Georgian London style. However, this terrace is distinctive, apparent through its alternative brick bond which is decorated with a diagonal cross hatch and defined by darker brick heads. No. 3a is unique in that the front facade has been painted white, and archival photographic evidence suggests it has been this way since before the 1970's.

Careful consideration will be paid to the front facade in terms of not diminishing it's character. The rear of the property, however, has been poorly designed, with various inefficient additions and extensions over the years. There is a duty to make corrections to this whilst maximising floor area, particularly at roof level.



- Site - 3a Camden Road
- Notable local sites

3a Camden Road is a prime location and renowned part of London. It is typically a well suited destination for tourists, with various markets, particularly well known for its food stalls. However, Camden is also starting to see an influx of tech start-ups and other groups making business ventures.

These things combined are reason for Camden having one of the highest rates of hotel occupancy in the country.









Regent's Canal



Camden Market



KOKO, concert venue



Camden Lock



St. Michael's Church



Camden Market

# 2.0 LOCATION

# 2.5 Conservation Area

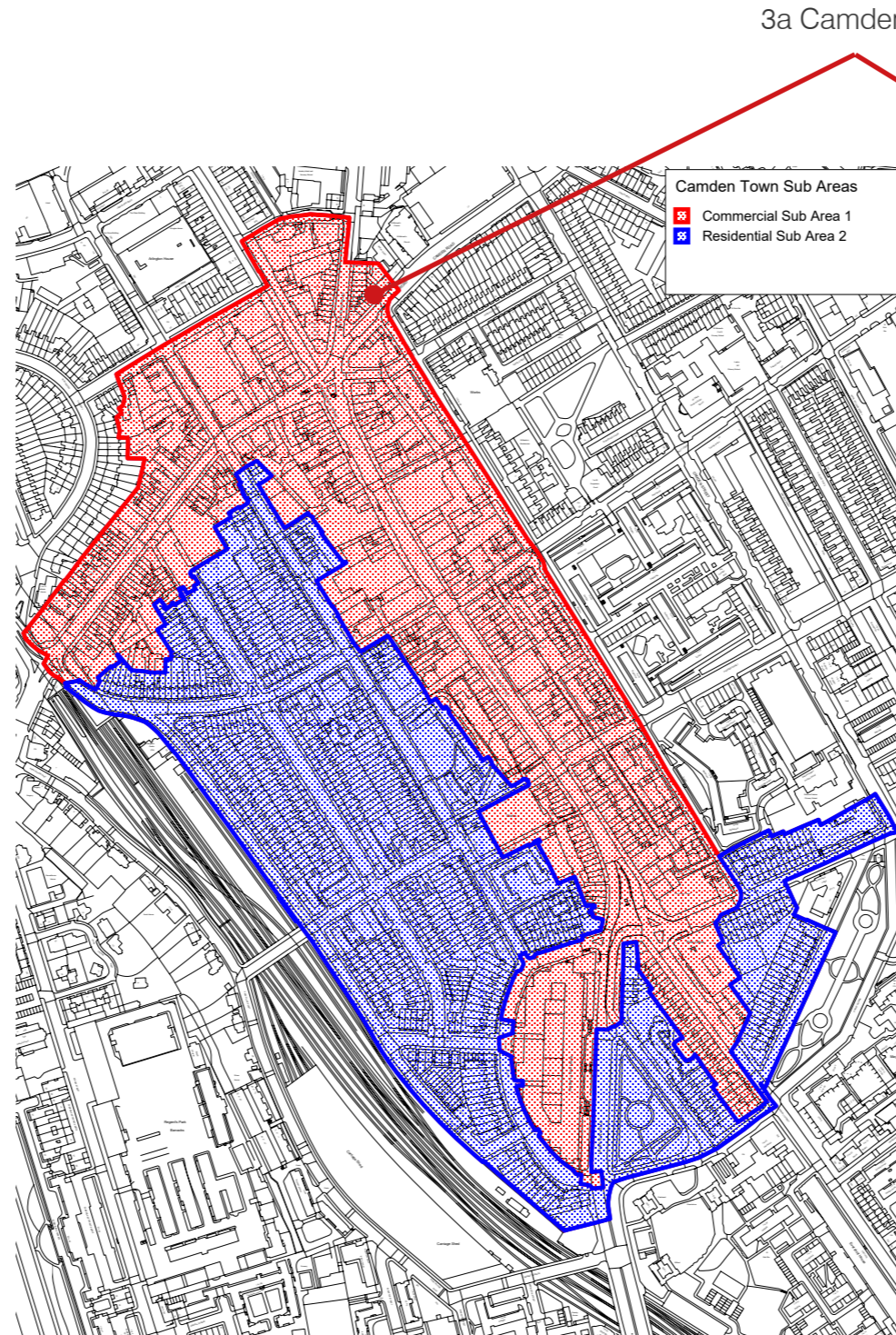


From left to right (highlighted): London, London Borough of Camden, Camden Town Conservation Area

Camden Road is located in the London Borough of Camden. Within the borough, the site is located within the Camden Town Conservation Area on the outer north-eastern edge. The conservation area itself is split up into two distinct areas, one being predominantly residential and quiet, whilst there is a contrastingly busy retail area which makes up the other half.

The site itself, located just 50m from Camden Town underground station, falls within the central hub of Camden, well known for its busy, dynamic and diverse characteristics.

Throughout the conservation area there is a high proportion of 19th century buildings which make a positive contribution to the historic character and appearance. 3a Camden Rd itself is not excluded from this, however, it has not achieved listed status. The reasoning for this is mainly due to the tendency for alteration within this commercial sub area. This derived from increased pressure for redevelopment since the end of the 18th century, which in turn has caused a wider variety of architectural styles.



Camden Town Conservation Area Townscape Appraisal, 2006



Camden Town Conservation Area Character Sub Area, 2006

## 2.0 LOCATION

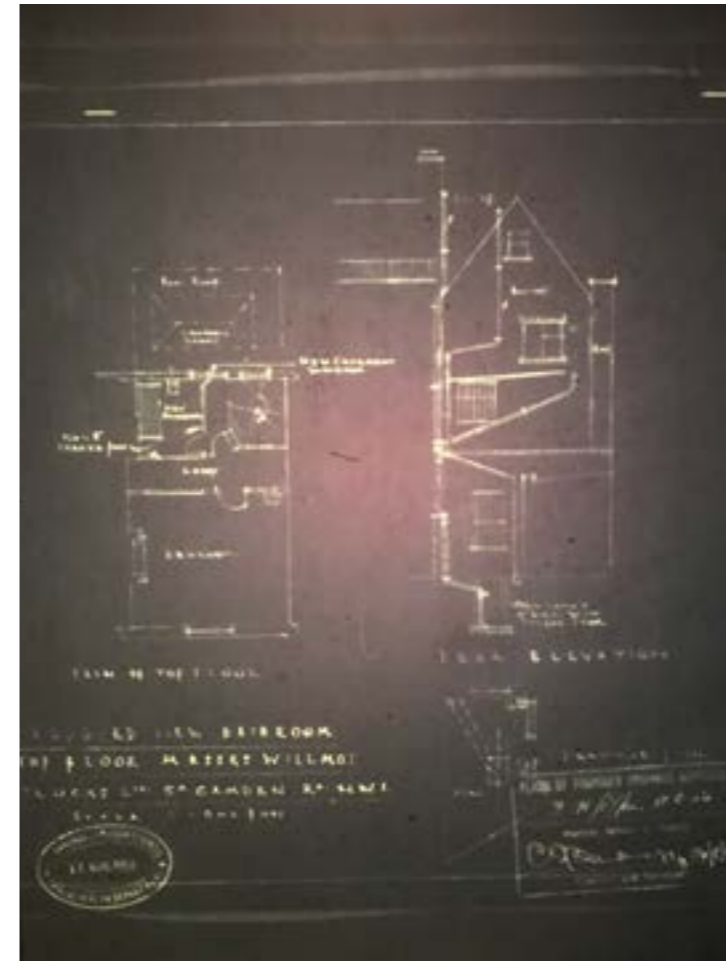
## 2.6 Neighbour Planning History



Front elevation of 5a Camden Rd from 1971



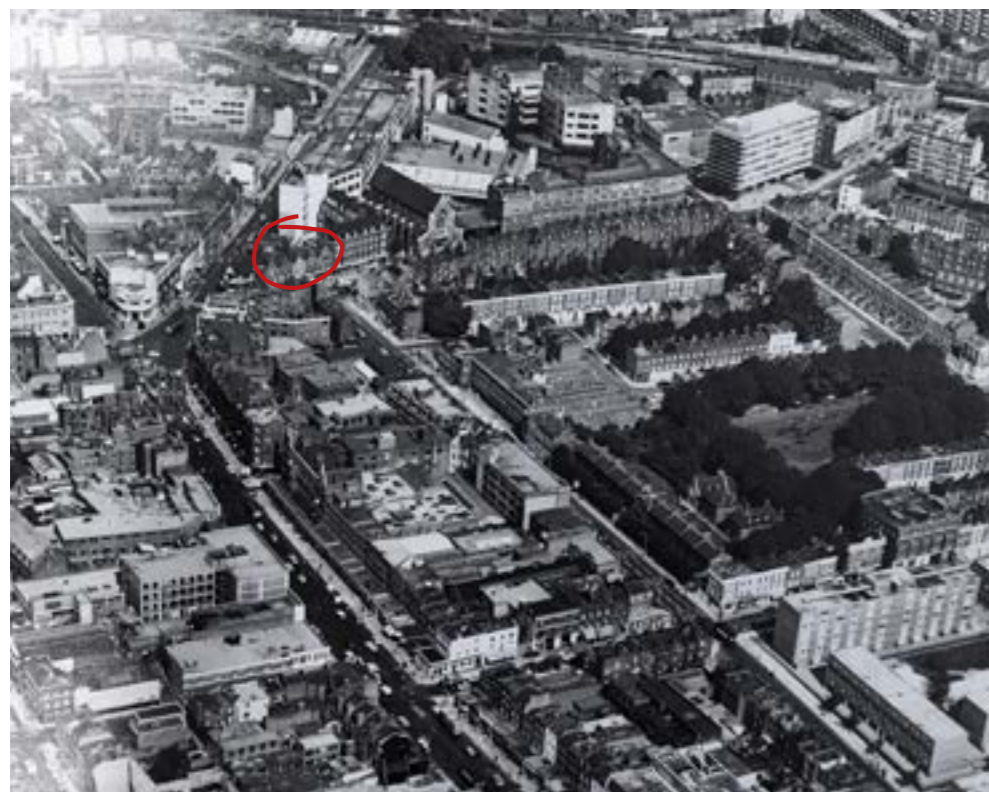
Current front elevation - 5a Camden Rd



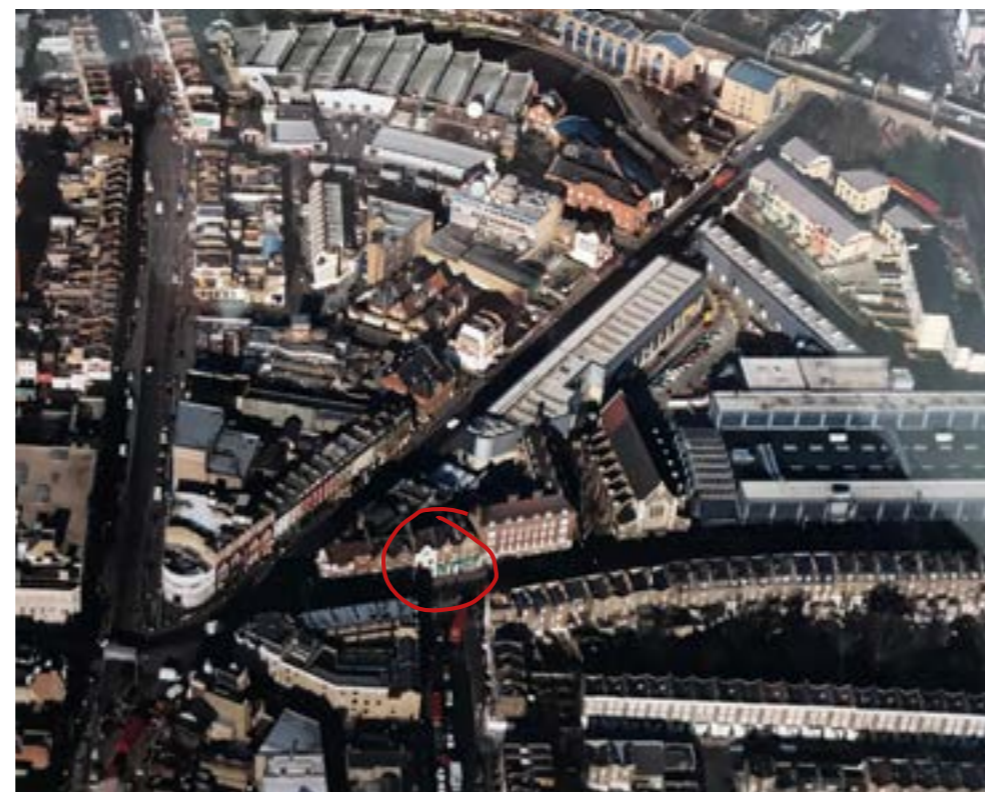
Rear elevation of 5a Camden Rd from 1954



Current rear elevation - 5a Camden Rd



Aerial photograph from 1971



Aerial photograph from 1985

### Front elevation (No. 5a):

There is evidence showing that the front facade has not changed since c.1971, however, it is unknown whether this is the original facade.

### Rear elevation (No. 5a):

It is evident from looking at the drawing from 1954 that the current rear facade at 5a Camden Road is not original. The window order has changed, as well as the roof structure.

We can conclude that historically there has been alterations to the property at 5a Camden Rd, particularly to the rear elevation.

### Aerial views (No.3a):

The image dating back to 1971 shows that No.3a is unique from the rest of the properties making up the terrace.



## Grand Union House

The Grand Union House development proposes a major new office building, new affordable homes and local shops, and a new street frontage bringing activity the local area, creating spacious links and improved public realm between Kentish Town Road and Camden Road. These elements together encompass 6,268 sqm of mixed-use development.

It is clear within the scheme's intention to take into account the evolving context of the surrounding area, acting as a catalyst for further improvements to the centre of Camden Town, alongside other proposed major developments - such as the proposed new Camden Town tube station at Buck Street.



View from Buck Street



View looking north along Kentish Town Road



View of entrance from Kentish Town Road

# 2.0 LOCATION



## Buck Street Station

The proposal for a new station entrance for Camden Town aims to ease congestion and increase space to allow for anticipated rise in footfall. The development will also include housing above the new station, and retail and commercial space will also be provided, contributing to the evident evolution that Camden Town is undergoing.

The site at 3a Camden Road is likely to benefit greatly from vast improvements to the pedestrian access in and out of Camden Town, with a strong focus within the scheme dedicated to enhancing the public realm and establishing a good mix of active street-level uses. This will be executed by widening the pavements at the station entrance and potentially restricting Buck Street of vehicular access.

It is with good speculation that once these works are completed, it is likely to set a precedent and be the catalyst for successive developments, as Camden begins to transform into a creative hub.



Existing and proposed station entrances

- Site boundary
- Proposed station entrance
- Proposed station footprint
- Proposed below-ground passenger tunnels
- Existing station entrance
- Existing below-ground passenger tunnels



Above right: Plan of the street and landscape around the new station and development



Right: 3D drawing showing the new forecourt and adjustments to Buck Street in front of the station.



View from Camden High street of station entrance



View facing north on Kentish Town Road with Buck Street on the left

## 3.0 DESIGN

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- 3.1 Pediment Modification
- 3.2 Rear Extension
- 3.3 Rear Elevation Development
- 3.4 Ground Floor Amendments
- 3.5 First Floor Amendments
- 3.6 Second Floor Amendments
- 3.7 Third Floor Amendments
- 3.8 Roof Amendments
- 3.9 Front Elevation Amendments
- 3.10 Rear Elevation Amendments
- 3.11 Section AA Amendments
- 3.12 Section BB & DD Amendments



Existing view looking east along Camden Road



Existing view looking east along Camden Road



Proposed exterior of 3a Camden Road (duplicating extension of nos. 1a and 3 Camden Road)



Proposed view looking east along Camden Road

### Front

It is our intention to extend the front pediment to align with its neighbours Nos. 1 and 3a. By duplicating the pediment and dormer window we will create more continuity between the individual properties of the terrace in a sensitive manner.



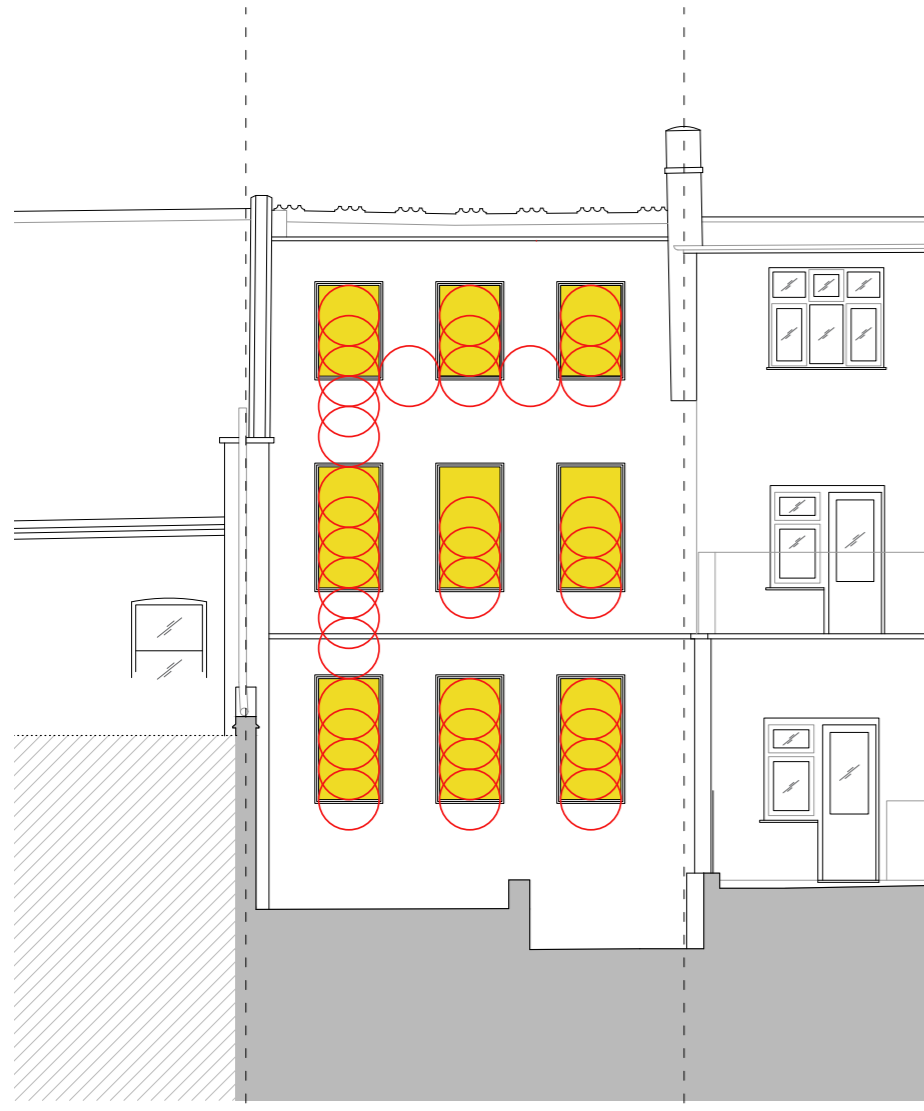


**Rear**

The diagram shows how we are using the existing mass to form our limitations for the extension and to follow the guidelines set by the current facade.

The extension at first floor is to align with each of it's neighbours, as they jut out further, and the height of this will reach that of No.3. The proposed roof extension will extrude to the existing boundary set by the previous extension, which has been poorly built.

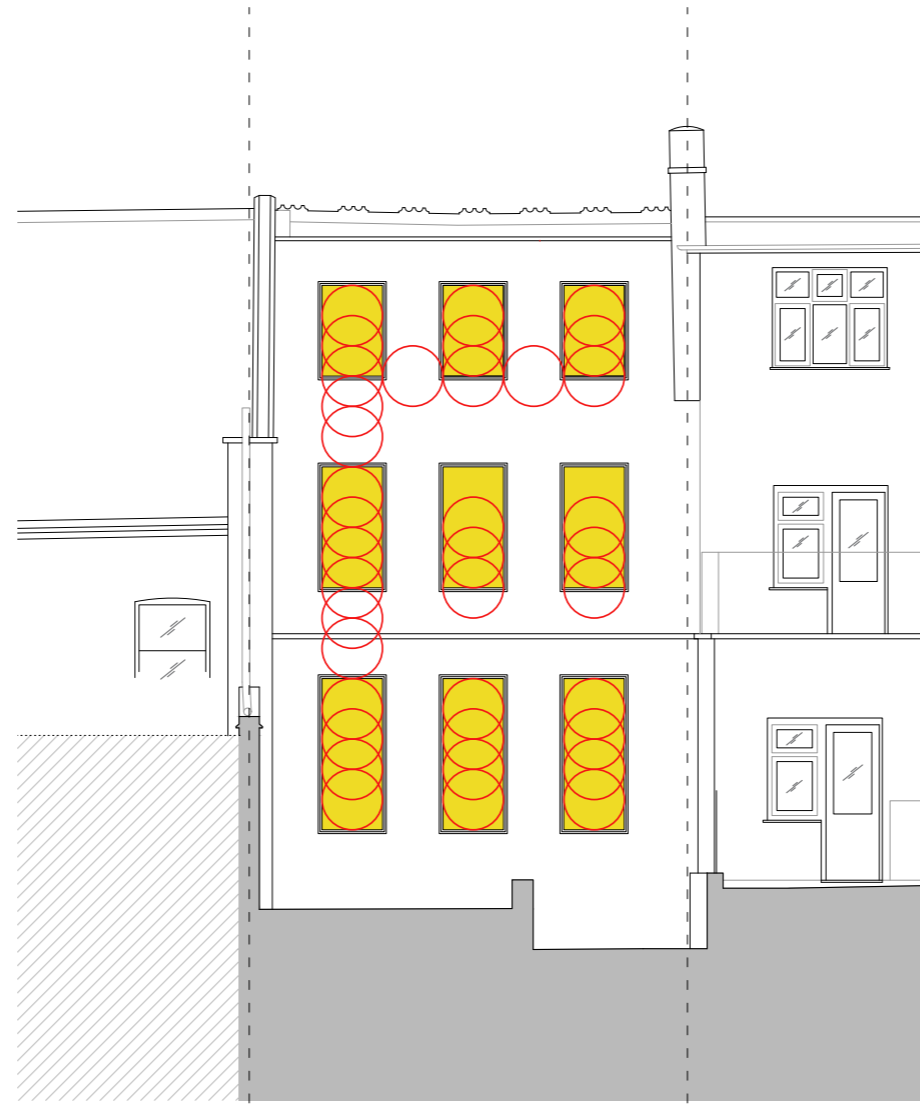
Currently the rear facade shows no particular order and over time has been modified in such a way that there is a lack of clarity of massing geometry and proportion, with the approach for extensions over the years being insensitive to the existing fabric. The proposed works intend to restore a this clarity.



### Option 1a

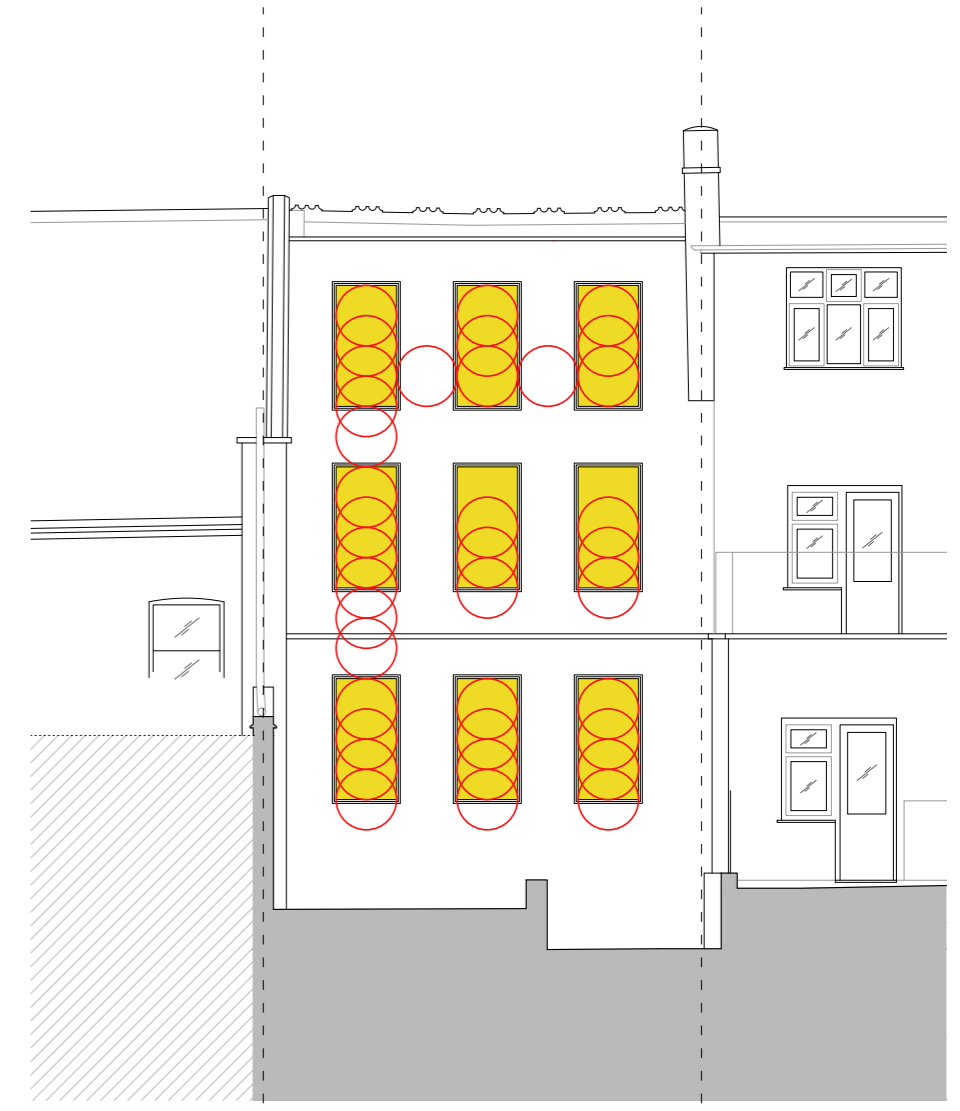
First and second floor windows are of equal proportion (2 circles), and the third floor windows are shorter (1.5 circles), depicting typical Georgian window hierarchy between floors, including both height of windows and spacing between.

However, the 3 column lines of windows is not the typical style for a rear facade, and would look out of place against the neighbours.



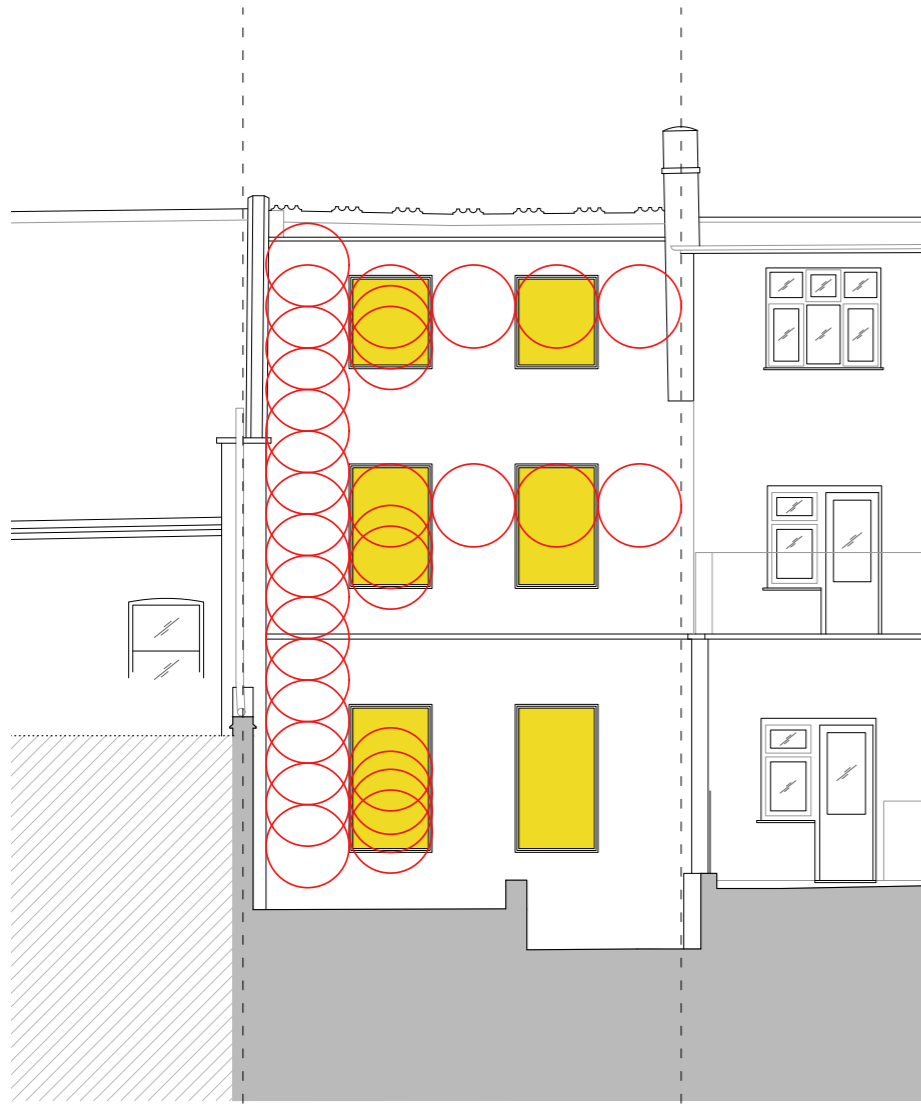
### Option 1b

Windows decreasing in height from first to third floor, showing a distinctive vertical hierarchy transition. First floor windows are taller than in Option 1a.



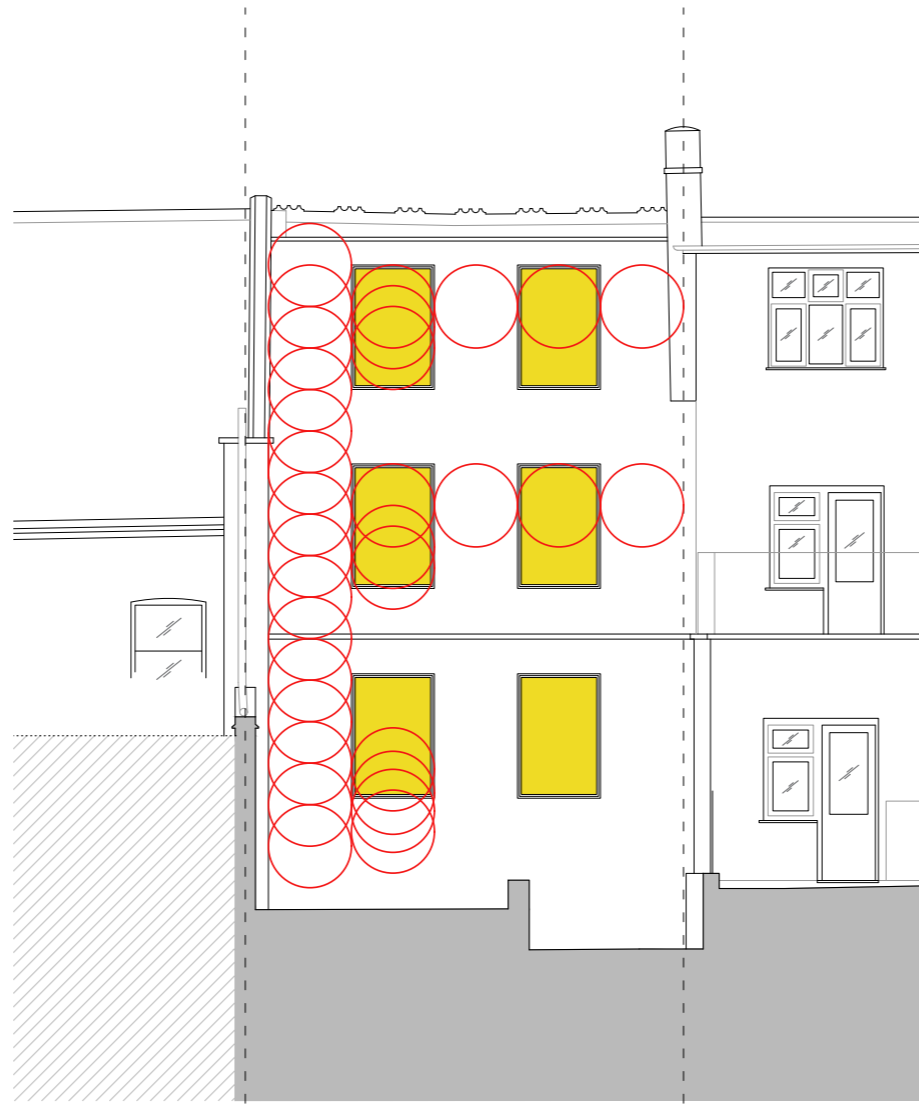
### Option 1c

All windows are equal in height and width, which deviates slightly from the hierarchical system shown in the previous two options. The ratio of brick to glass is slightly unusual as most of the area of the facade is dedicated to windows, This contrasts quite greatly to the neighbouring properties.



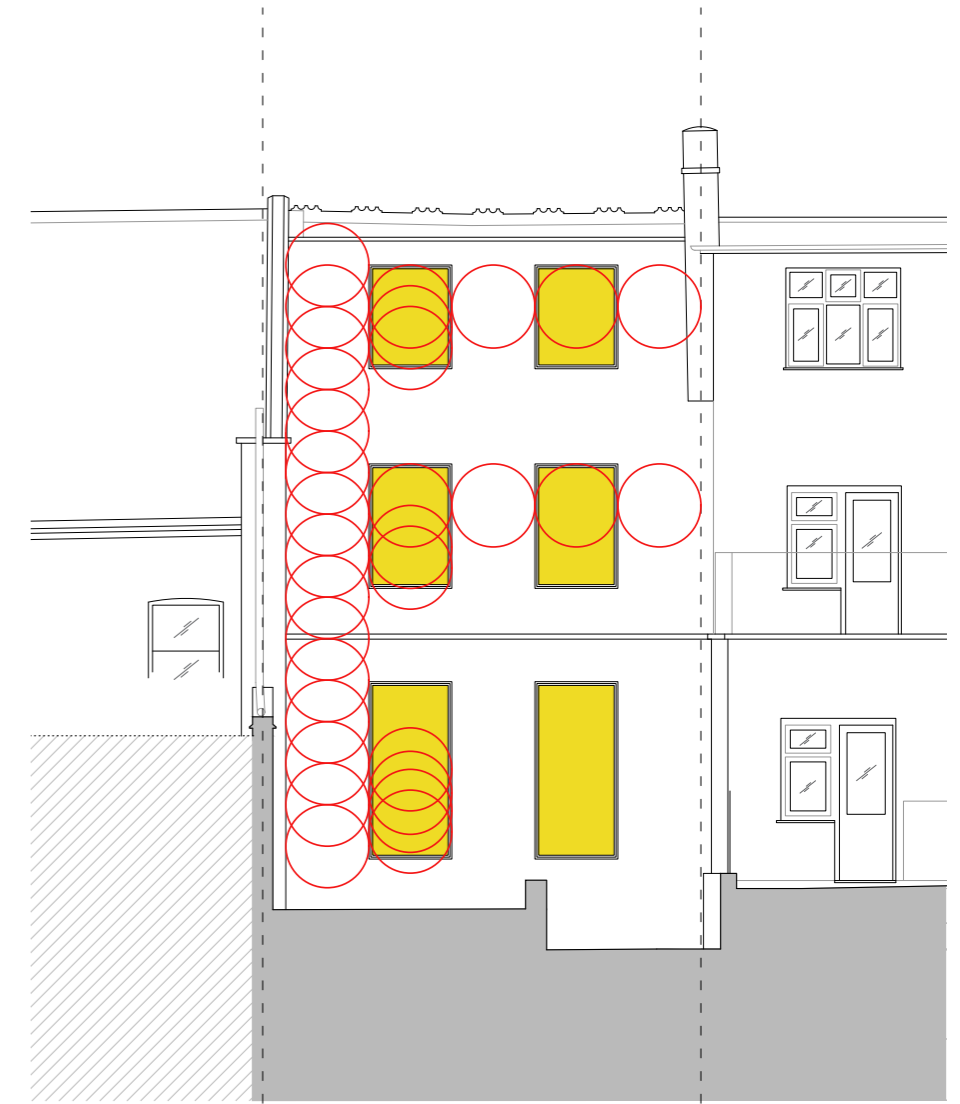
### Option 2a

With two columns of windows as opposed to three, the rear facade looks more complete and sits better within its context due to a more even brick-to-glass ratio.



### Option 2b

With equal window heights the facade looks more modern with a stylish window system which still remains true to a typical Georgian proportion.



### Option 2c

Increasing the height of the first floor windows allows vastly increased natural light into the property, however, it seems too overpowering of the facade - the transition between second floor and first floor windows is too dramatic.



### Option 3a

The windows on the side of the elevation closest to No. 3 Camden Rd have reduced in height and are equidistant from one another. This helps to break up the regularity of the facade and is a closer match to neighbouring façades.



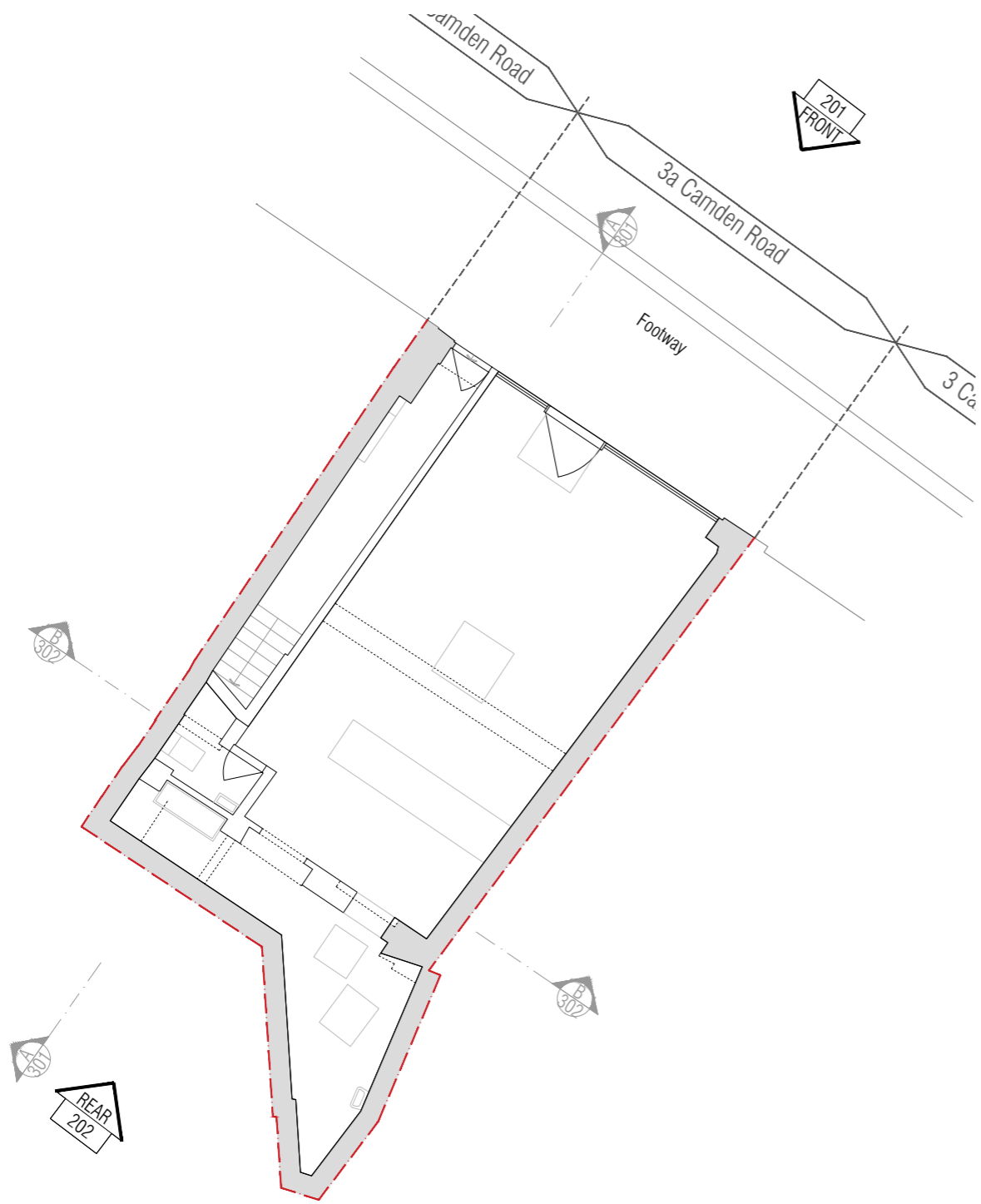
### Option 3b

By reducing the width of this column of windows (using the adjacent rear window of No. 5 as a guide), we gain more accurate proportions by reducing the window to brick ratio.

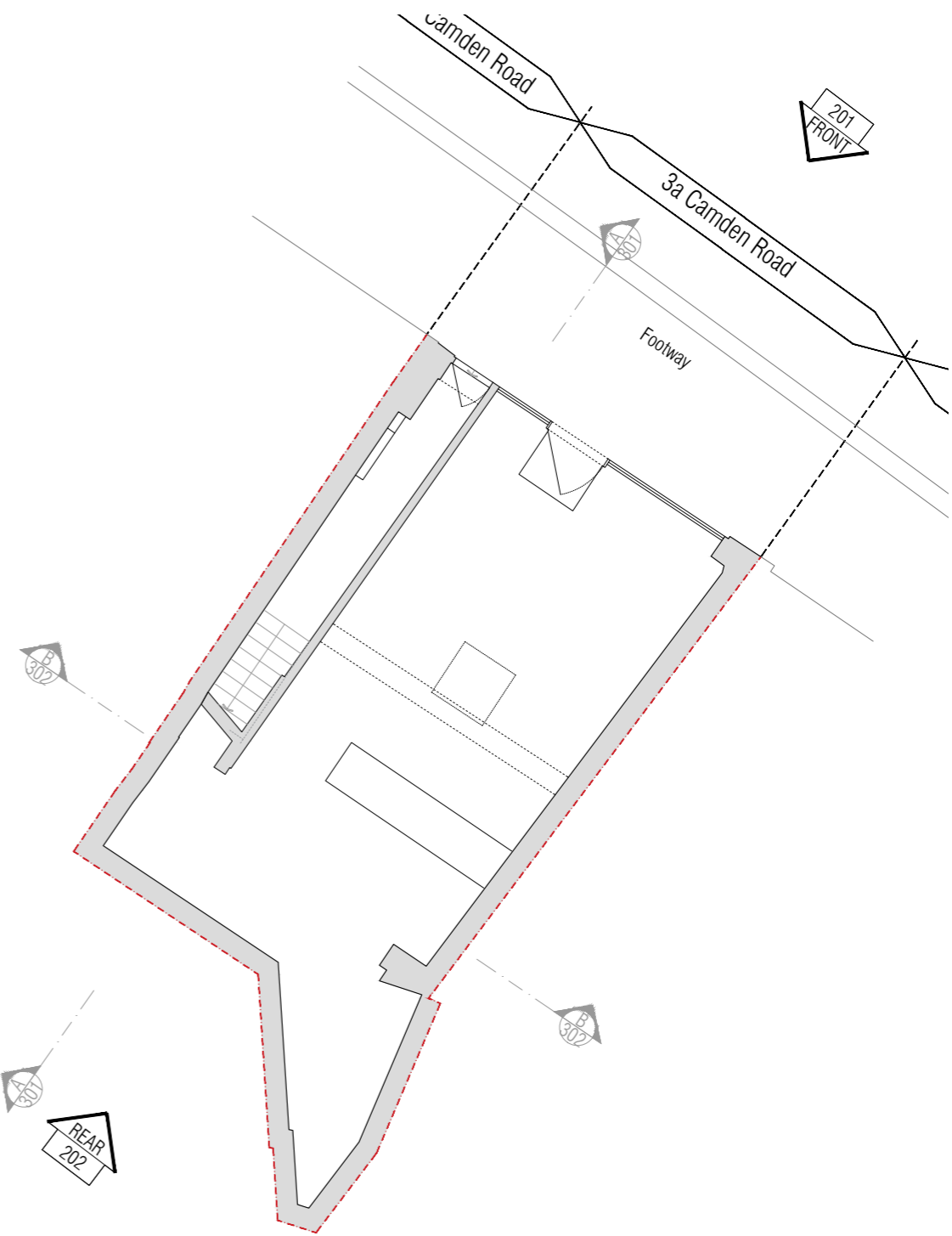


### Option 3c

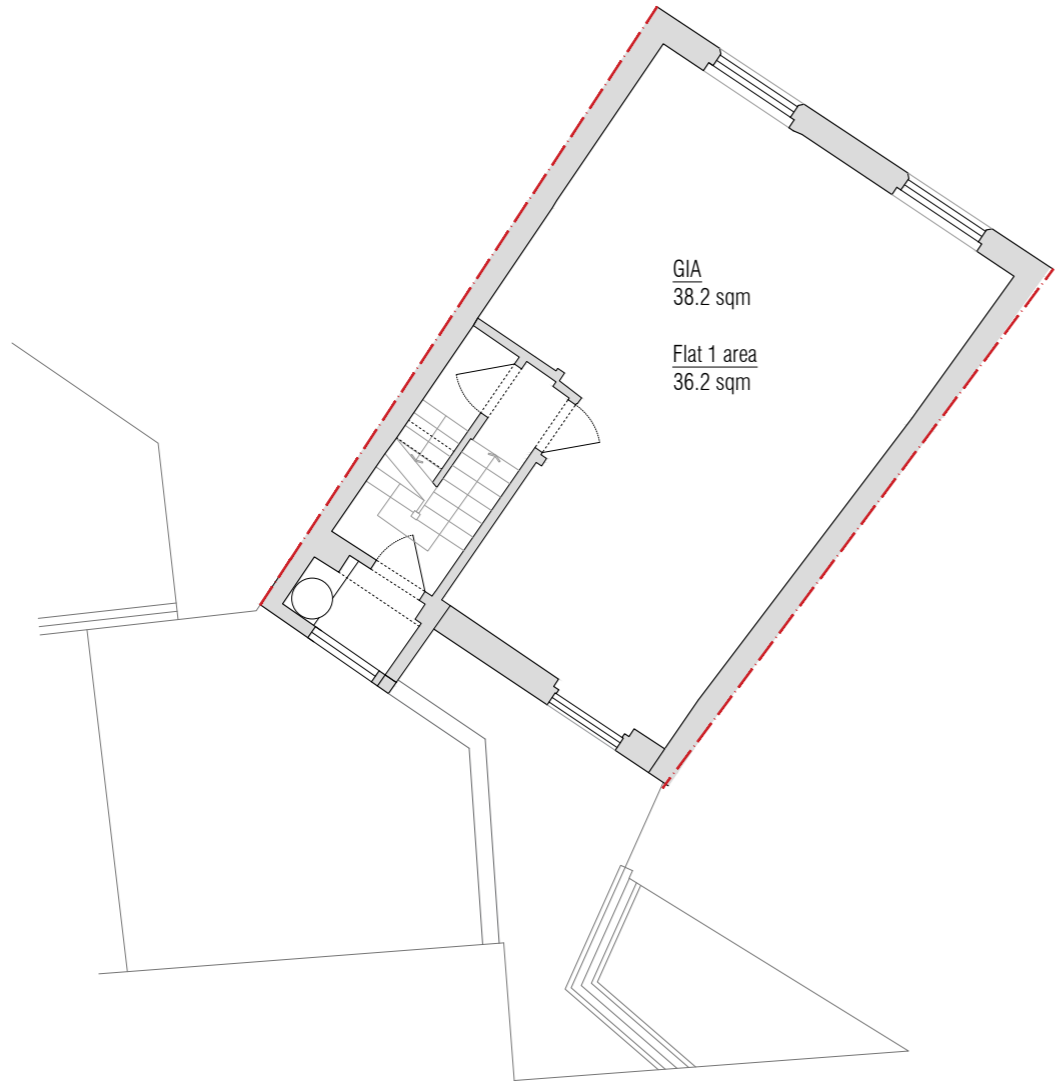
The final step to a satisfactory facade was to step the column of windows down which now feels like a more typical Georgian rear facade. This is closer to the neighbouring properties and seems a good fit given the context.



Existing Ground Floor  
1:100



Proposed Ground Floor  
1:100

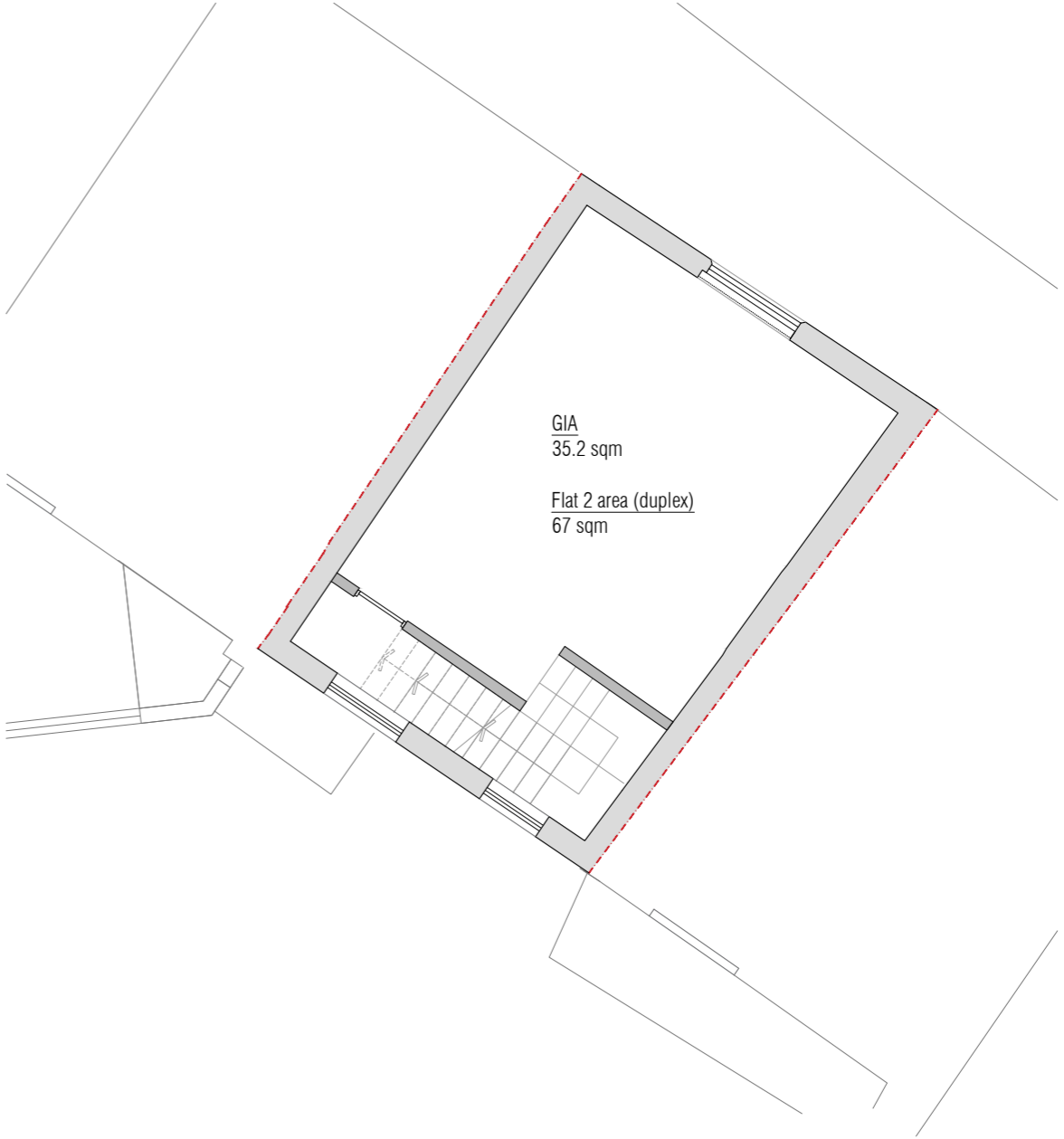


Existing First Floor  
1:100

Proposed First Floor  
1:100

GIA		
	Existing	Proposed
1F	38.2	34.8
2F	37.3	35.2
3F	20.7	31.8
<b>TOTAL</b>	<b>96.2</b>	<b>101.8</b>

Flat Area		
	Existing	Proposed
Flat 1	36.2	33.6
Flat 2	58	67

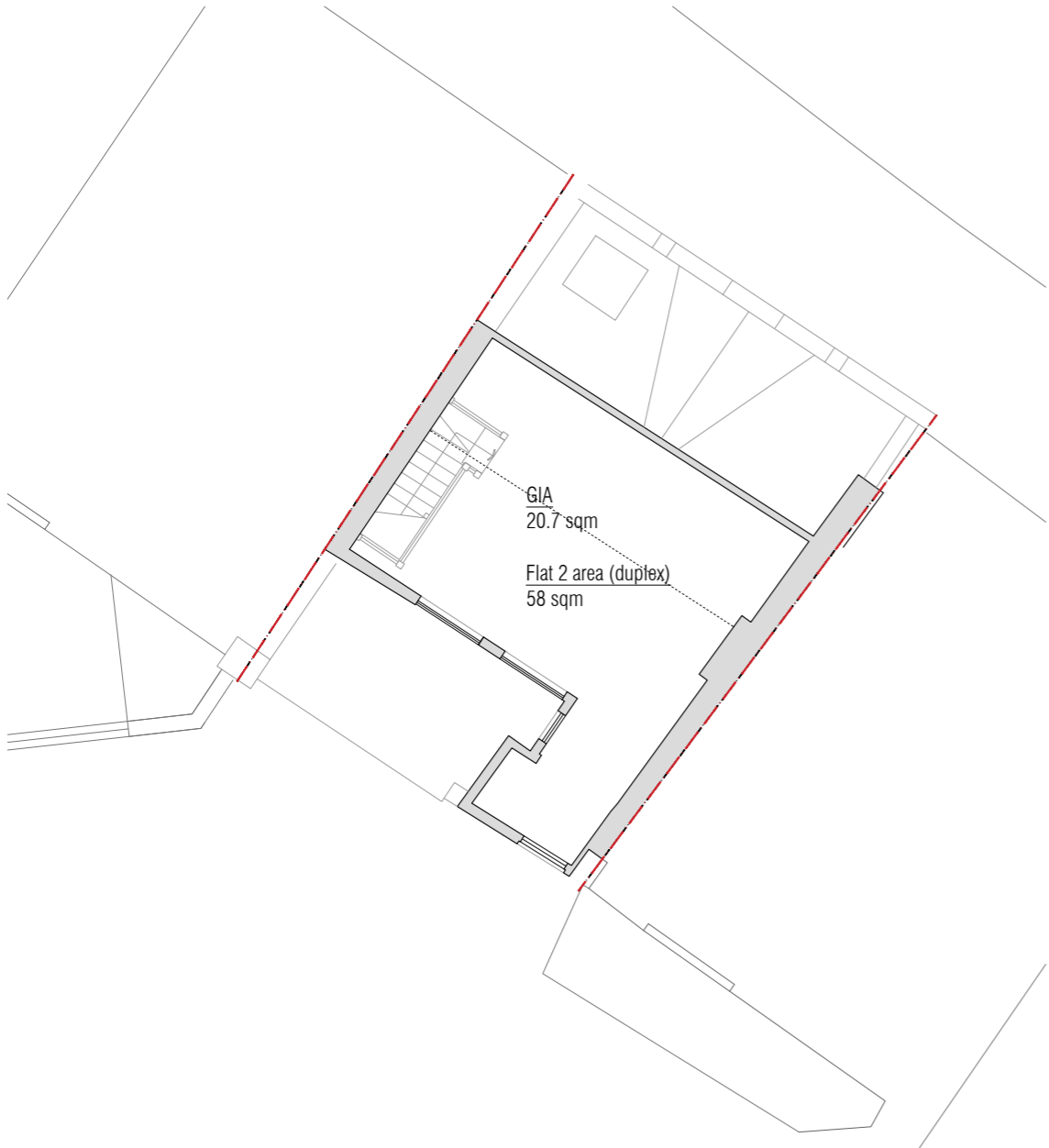


GIA		
	Existing	Proposed
1F	38.2	34.8
2F	37.3	35.2
3F	20.7	31.8
<b>TOTAL</b>	<b>96.2</b>	<b>101.8</b>

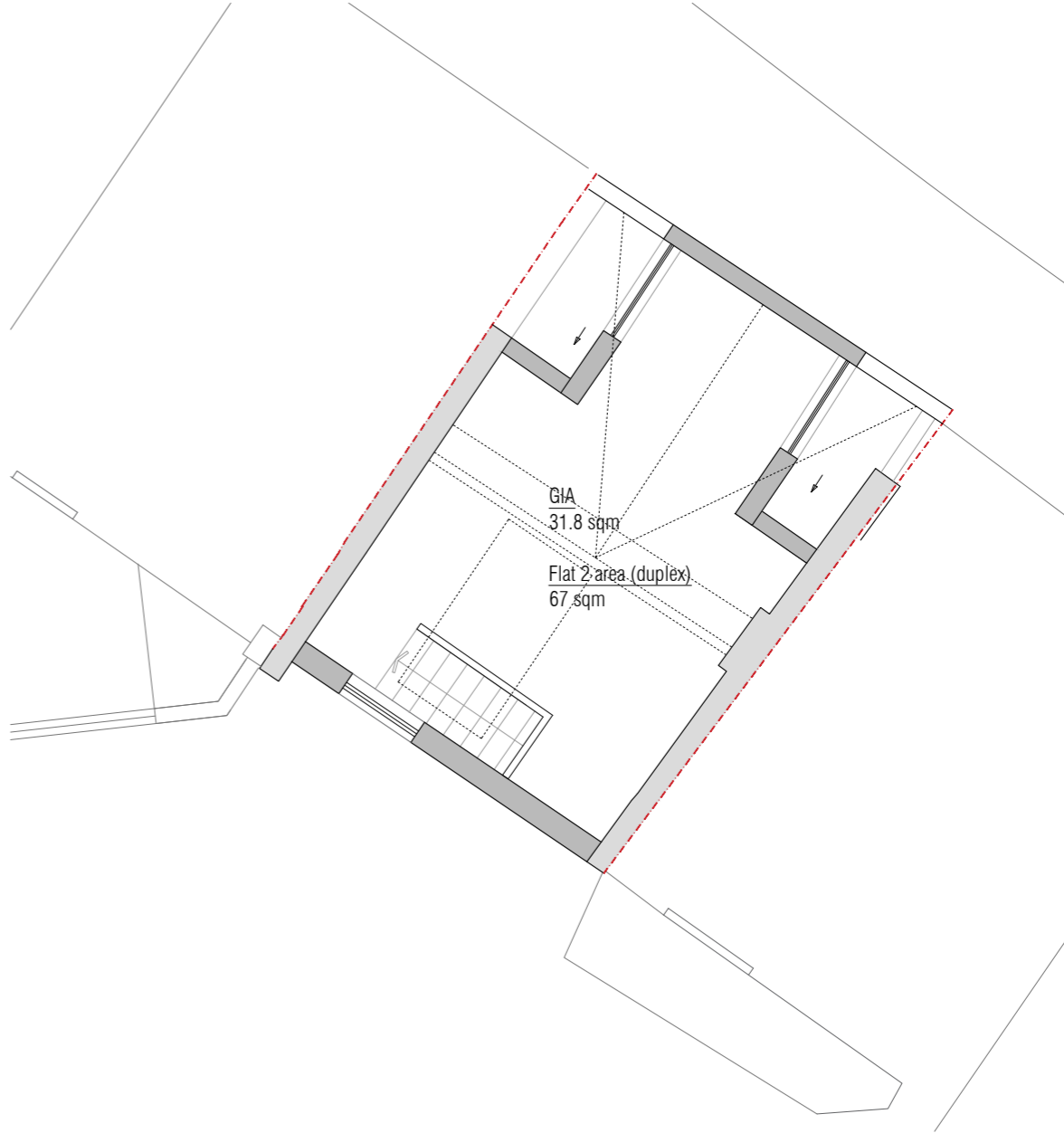
Flat Area		
	Existing	Proposed
Flat 1	36.2	33.6
Flat 2	58	67

Existing Second Floor  
1:100

Proposed Second Floor  
1:100



Existing Third Floor  
1:100

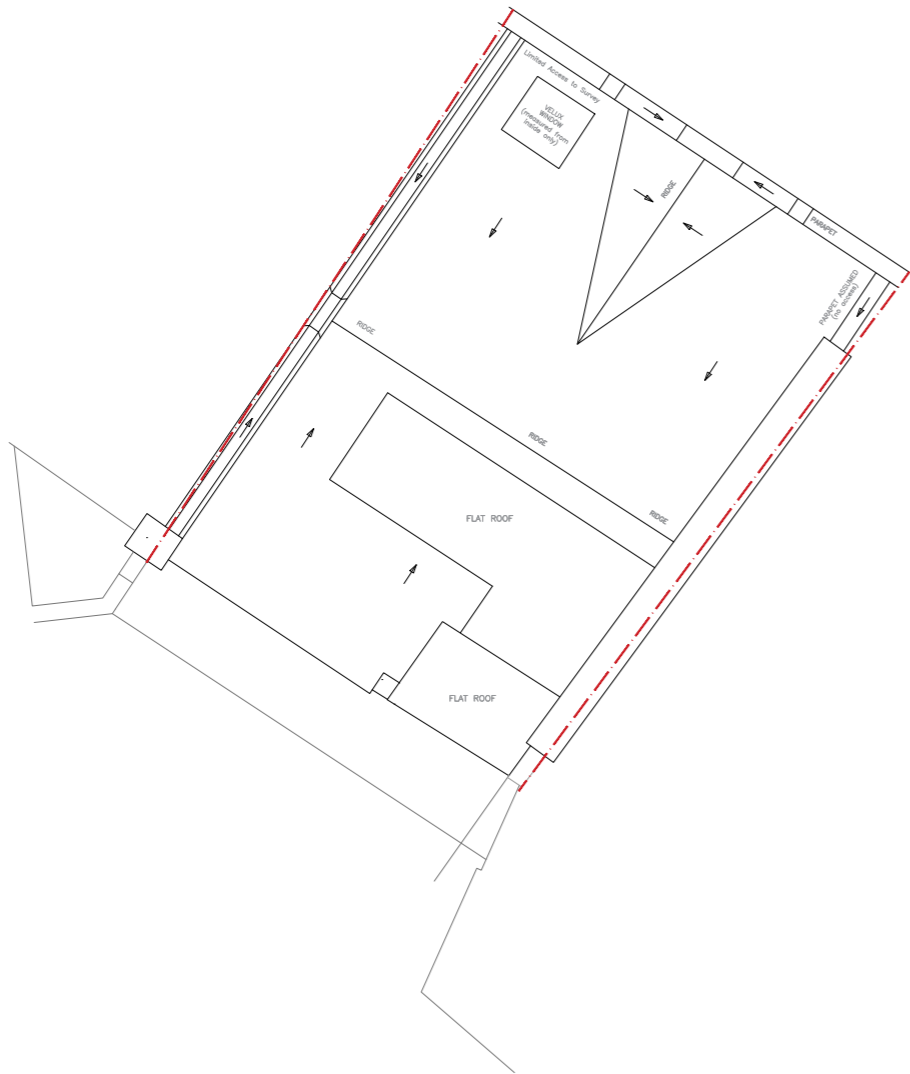


Proposed Third Floor  
1:100

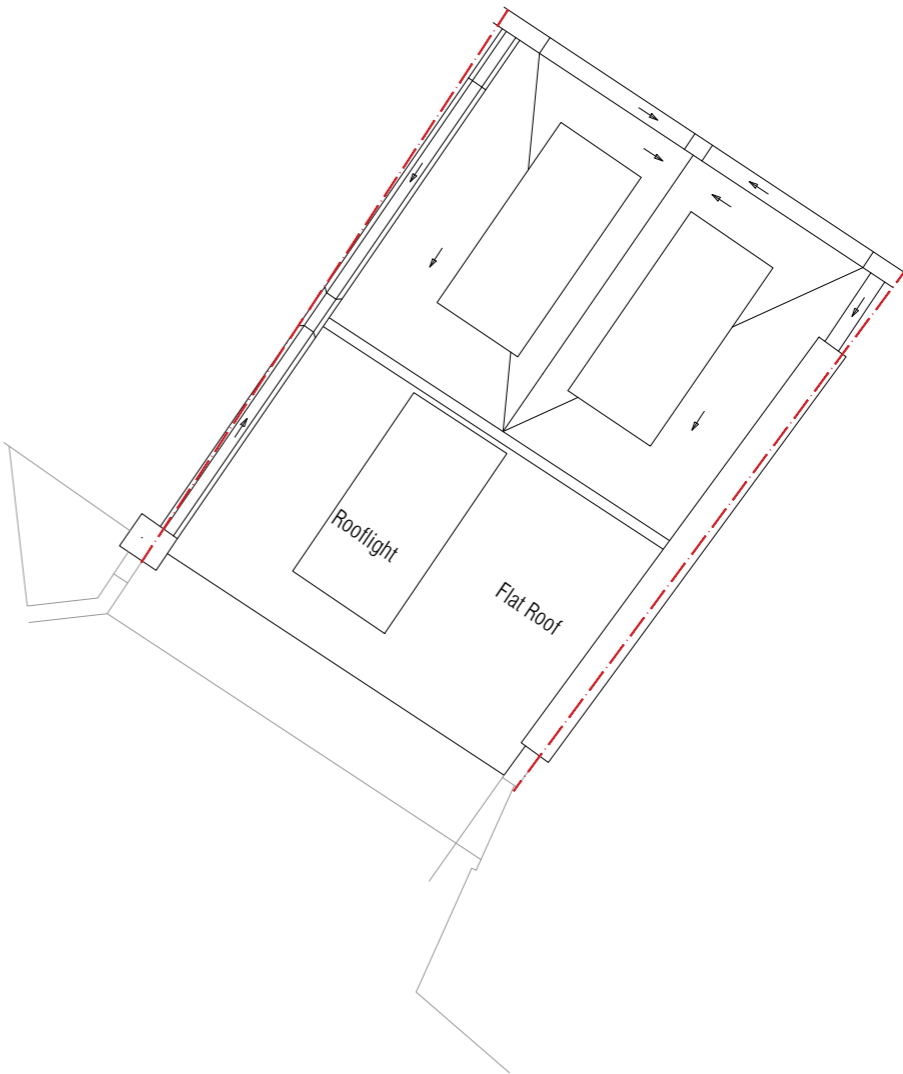
GIA		
	Existing	Proposed
1F	38.2	34.8
2F	37.3	35.2
3F	20.7	31.8
<b>TOTAL</b>	<b>96.2</b>	<b>101.8</b>

Flat Area		
	Existing	Proposed
Flat 1	36.2	33.6
Flat 2	58	67





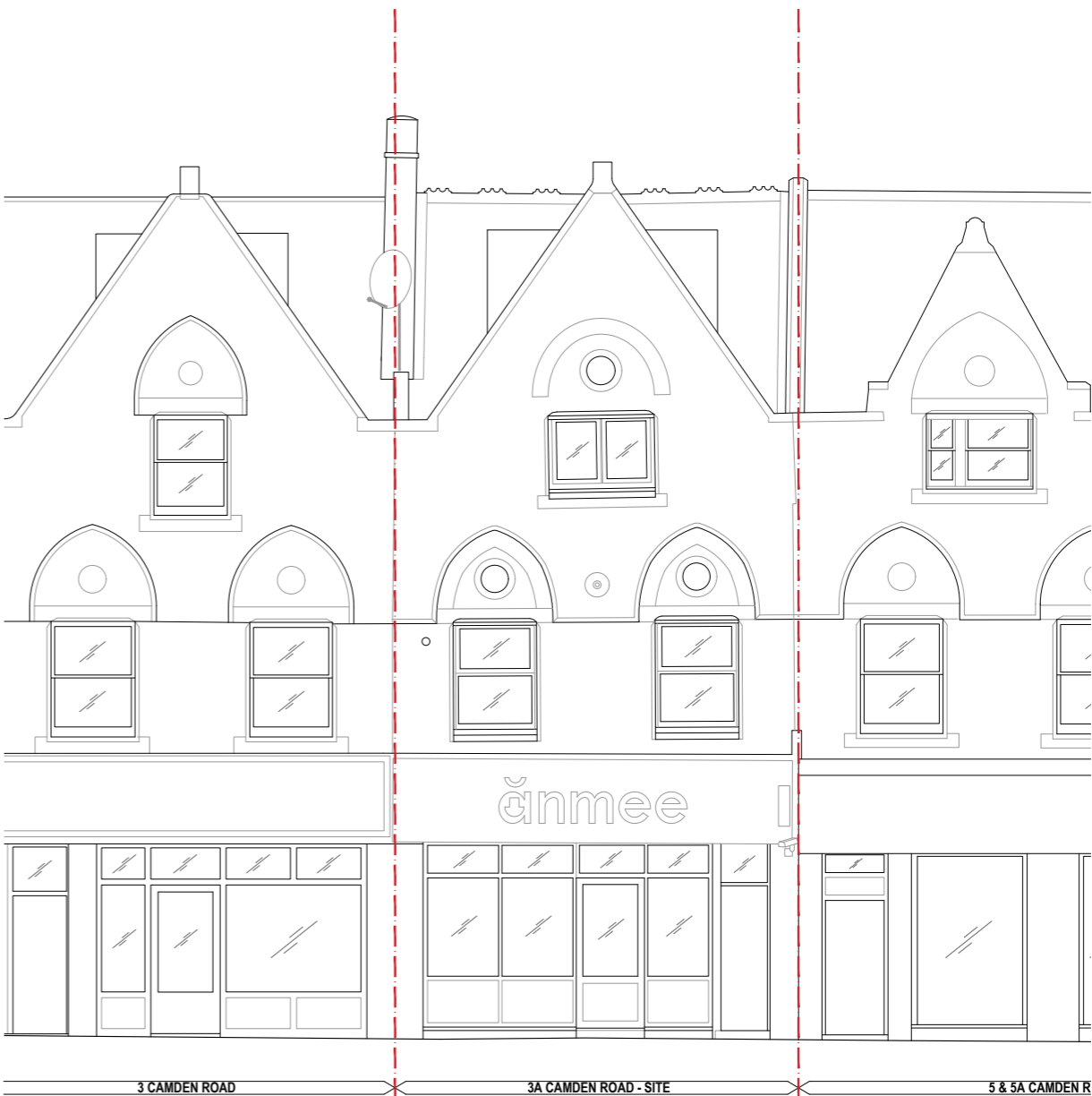
Existing Roof Plan  
1:100



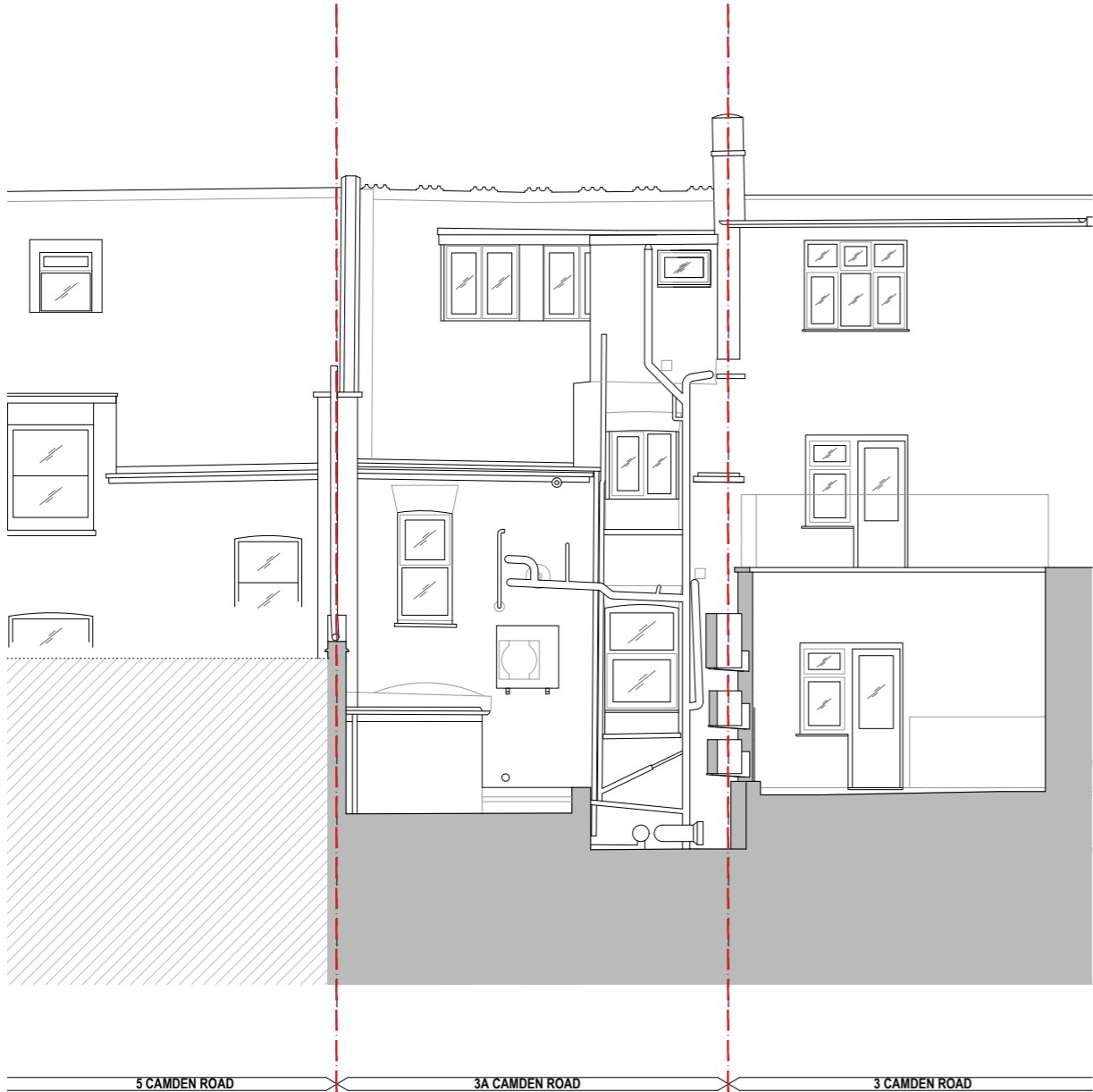
Proposed Roof Plan  
1:100



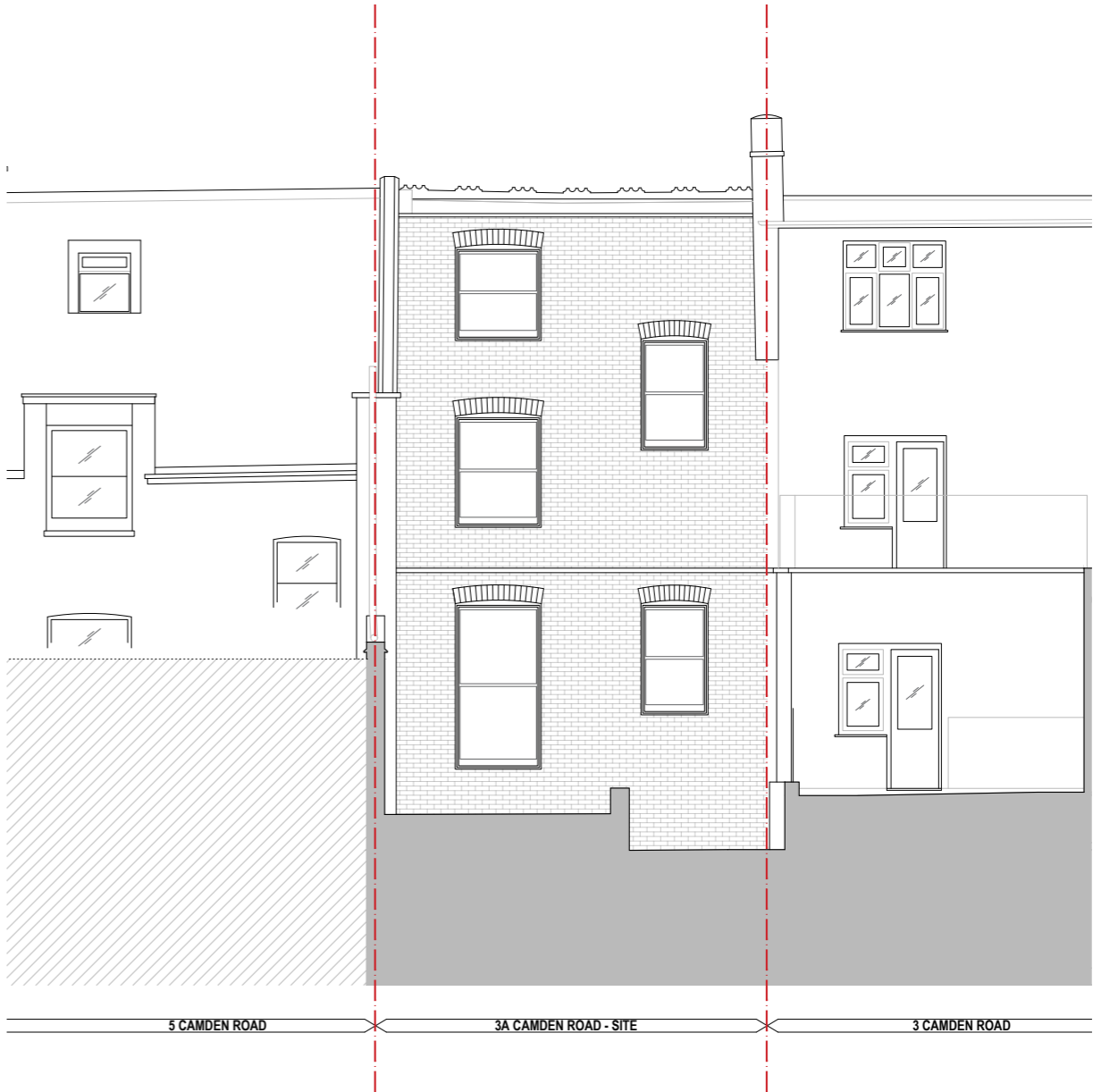
Existing Front Elevation  
1:100



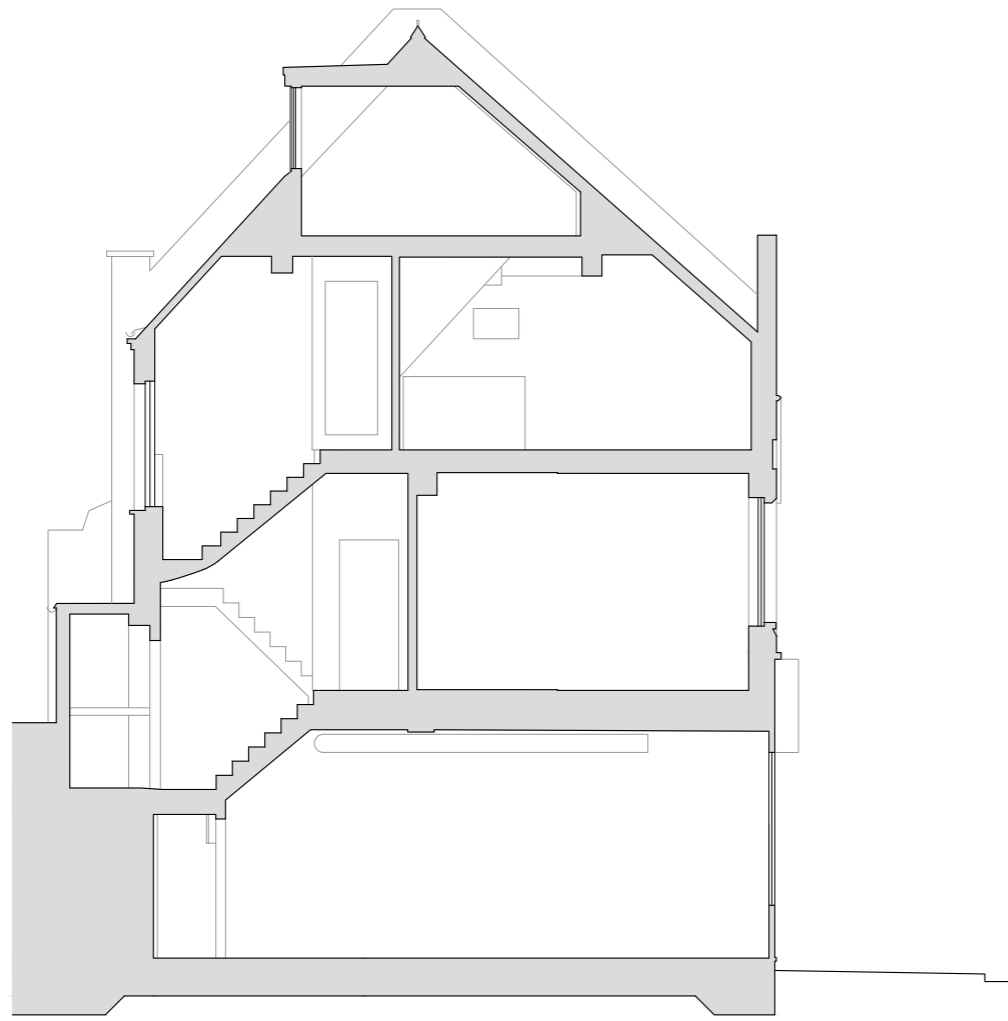
Proposed Front Elevation  
1:100



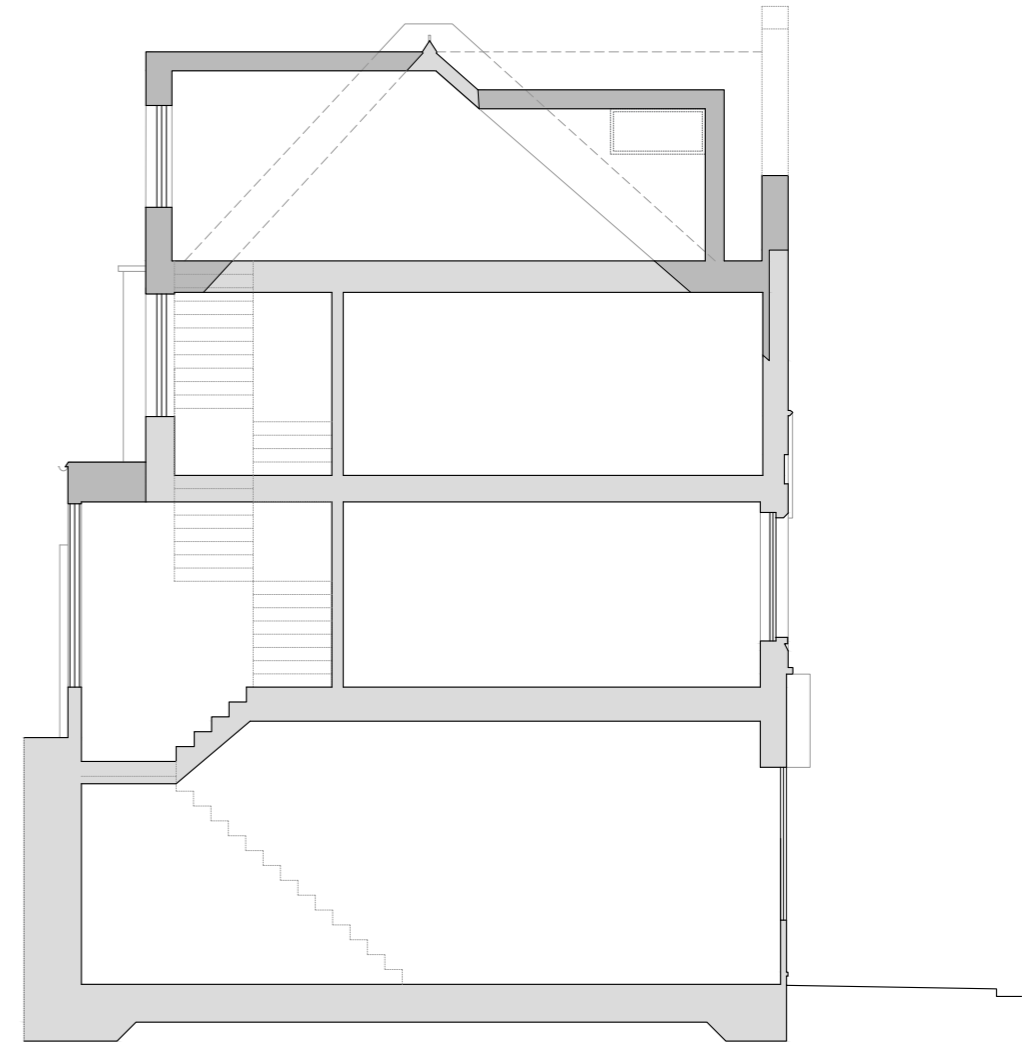
Existing Rear Elevation  
1:100



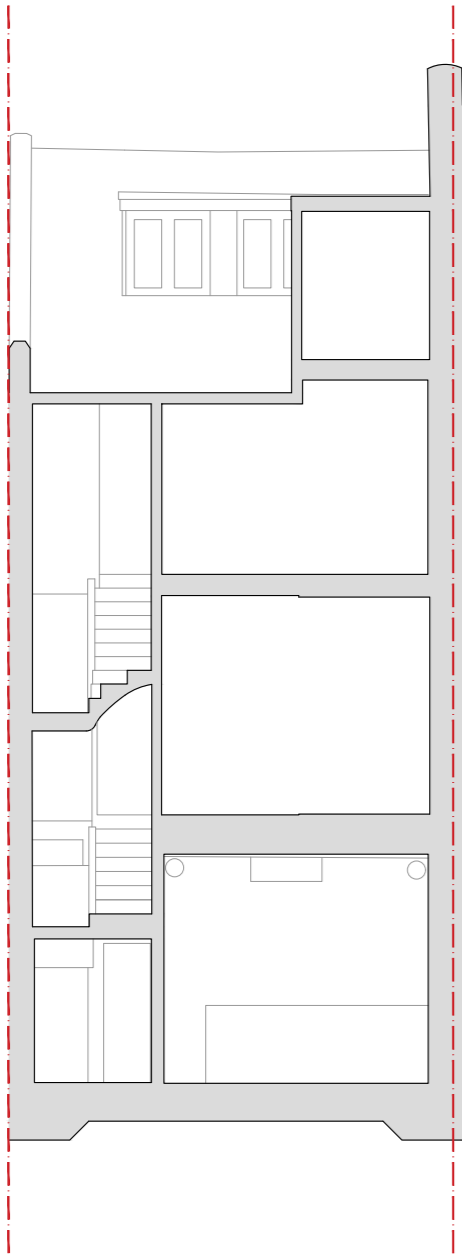
Proposed Rear Elevation  
1:100



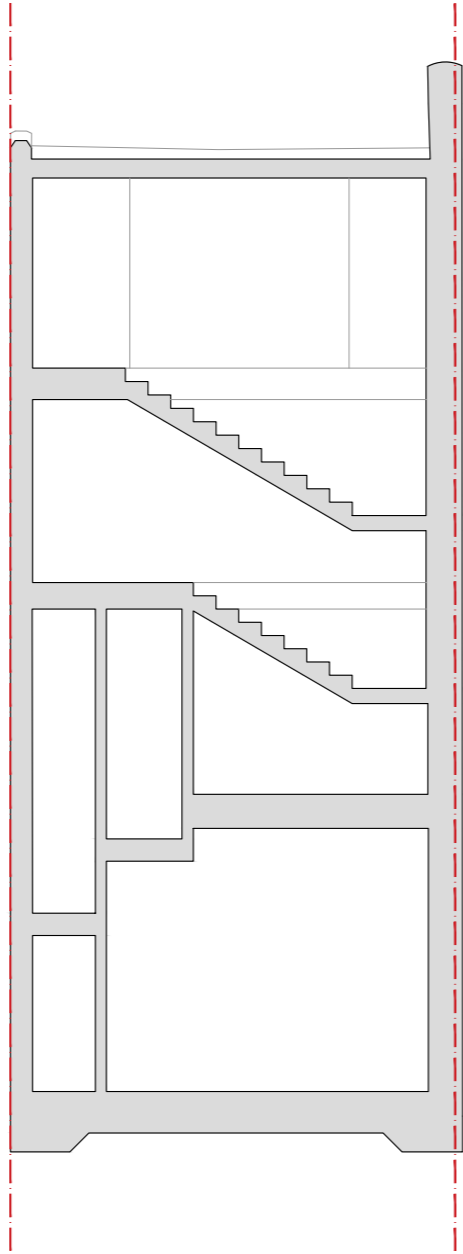
Existing Section AA  
1:100



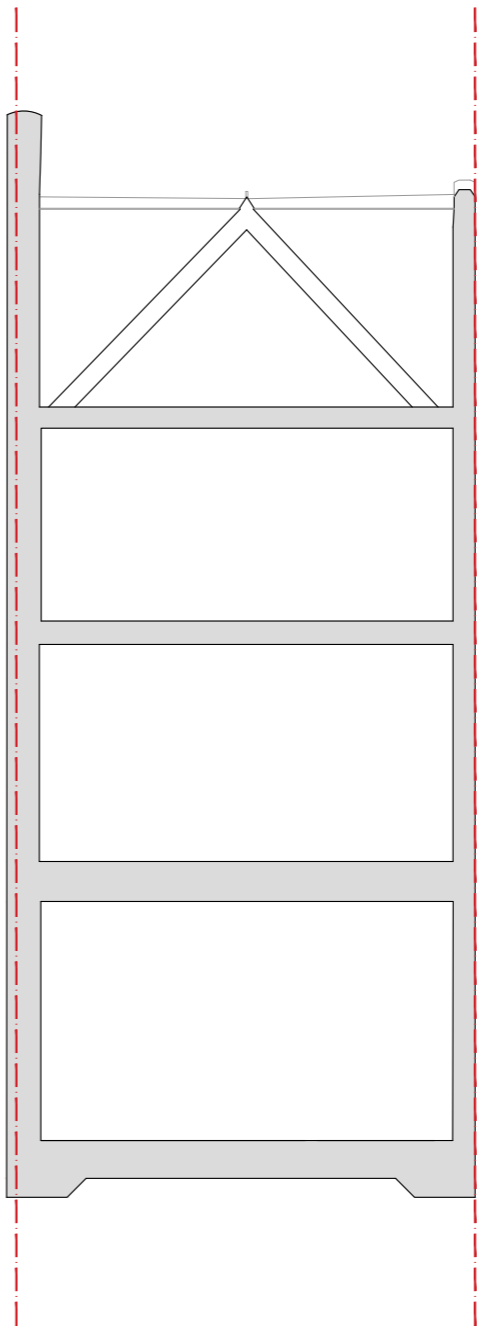
Proposed Section AA  
1:100



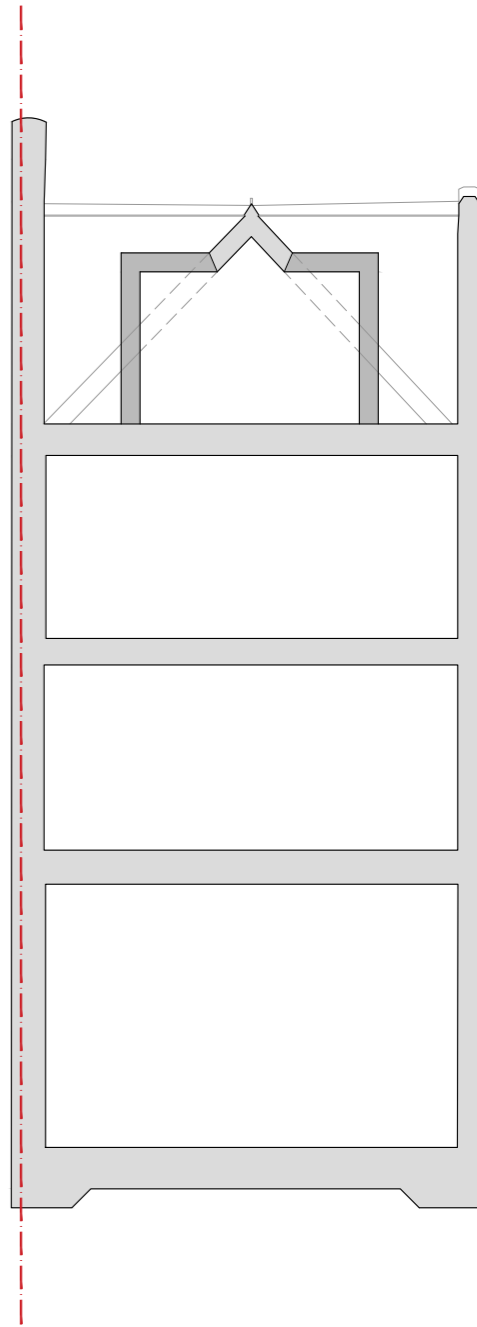
Existing Section BB  
1:100



Proposed Section BB  
1:100



Existing Section DD  
1:100



Proposed Section DD  
1:100

## 4.0 MATERIALITY

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- 4.1 Brick Restoration
- 4.2 Precedent Studies



Efforts will be made to remove the white painted facade and restore the original brick in keeping with the rest of the terrace. Whilst we are enlarging the pediment to match that of no. 3, to not restore the brick would compromise the application site's ability to blend in with its neighbours.



House in Hackney, Neil Dusheiko Architects



Landells Road, Alma-nac



Hawksley Heights, Gruff



Stoke Newington Loft, Brian O Tuama



