



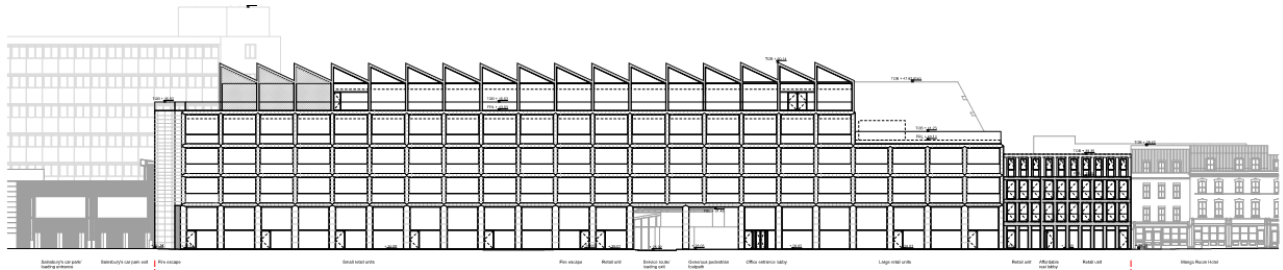
Grand Union House, London NW1
Construction Management Plan
December 2018

R.P.M.

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Grand Union House



Construction Management Plan

Grand Union House

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Contents

1.0	Introduction	3
2.0	Site location and Access	4
2.1	Local area	4
2.2	Site access and egress	7
3.0	Site set up and logistics	13
3.1	Logistics principles	13
3.2	Logistics outline proposals	13
3.3	Logistics management & Liaison manager	13
3.4	Condition Surveys	14
3.5	Site security, access & traffic management	14
3.6	Delivery / vehicle management	15
3.7	Existing services	15
3.8	Protection of existing trees	15
4.0	Details of the works	16
4.1	Outline demolition and construction sequence	16
4.1.1	Site set up and welfare facilities	17
5.0	Indicative Site logistics plans & programme	18
5.1	Logistics Plan	19
5.2	Summary Programme	21
6.0	Environmental considerations	22
6.1	Site working hours	22
6.2	Considerate Constructor Scheme	22
6.3	Noise, vibration and dust	22
6.4	Neighbour and community liaison	24

Appendices

1. TfL Primary Road Network Plan

1.0 Introduction

This document has been prepared to support the Planning Application being submitted to the London Borough of Camden for the redevelopment of Grand Union House. Included are details the works required to carry out the demolition/enabling and construction activities involved whilst outlining their anticipated timescales and identifying the environmental impact of the works and proposals for how these are to be mitigated.

The Development being undertaken relates to an existing building located on Kentish Town Road in the Borough of Camden and involves the redevelopment of the existing car park and commercial spaces into a mixed use commercial and residential scheme incorporating retail units to the ground floor façade on Kentish Town Road.

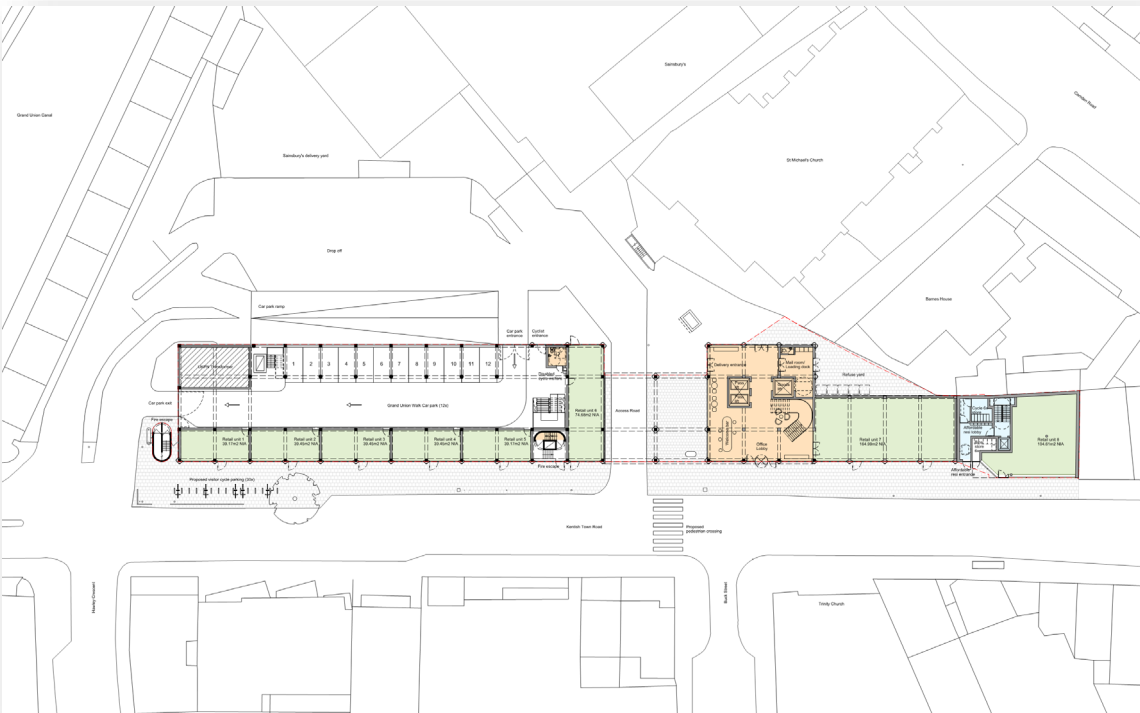


Figure 1 – Existing Building Plot (Redline)

2.0 Site location and access

2.1 Existing Site

The site can be found within the London Borough of Camden at 16-20 Kentish Town Road, Camden NW1 9NX and is accessed by vehicles from the northbound section of the A400 via Hawley Crescent.

The site sits in is a restricted location with adjoining owners on the north, east and southern boundaries; detailed as follows;

- Residential properties on Grand Union Walk to the Northern boundary.
- Sainsburys supermarket (customer and goods vehicle access) and St. Michaels Church to the Eastern boundary.
- Terraced retail and residential properties to the Southern boundary.



Figure 2 – Local site location plan

The plot lies outside the London Congestion Charging zone with vehicle access possible via the A400 Kentish Town Road from the south which provides links from the south which provides links to the TfL Primary Road Network (TfL PRN).

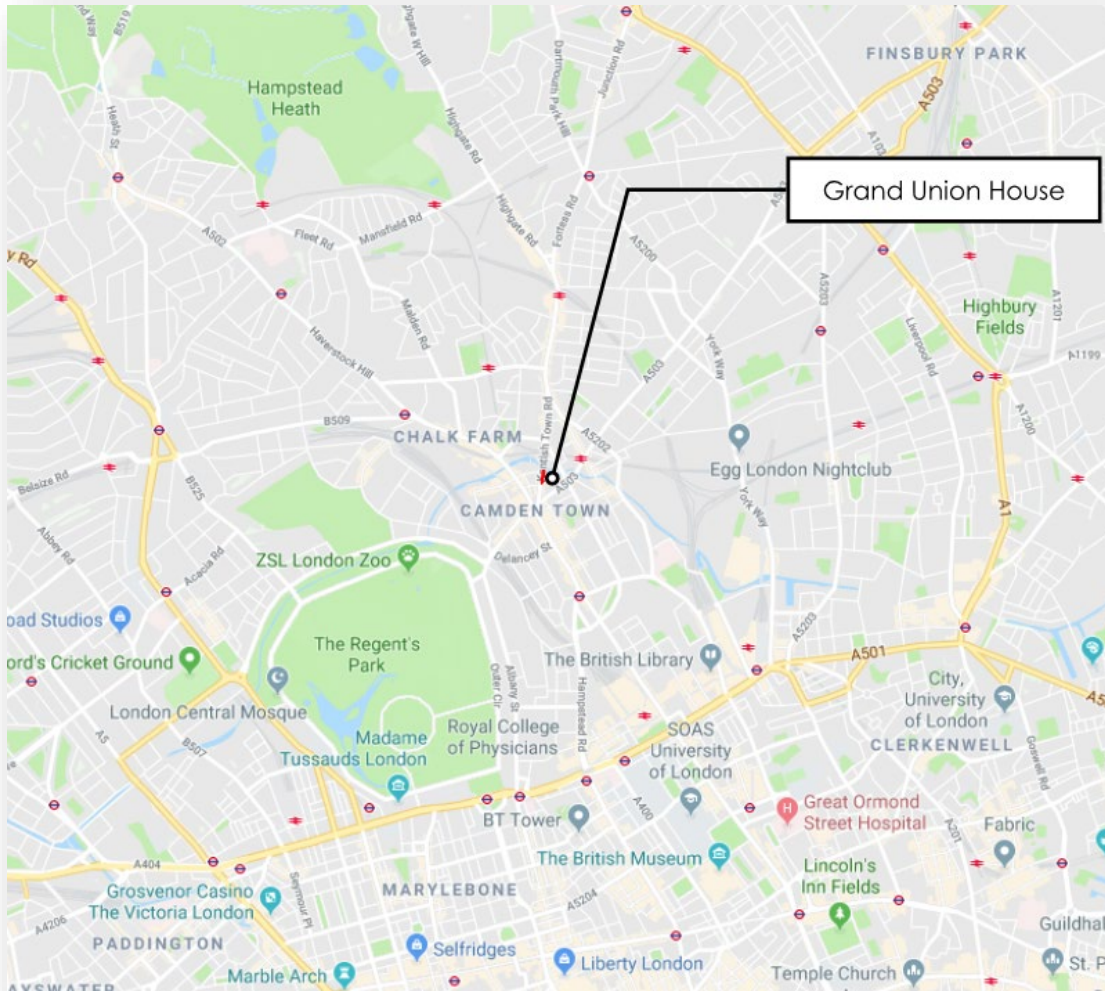


Figure 3 – General site location plan

2.2 Local area

As highlighted in section 2.1, the site is bounded by residential properties to the north, Kentish Town Road to the west, retail and residential properties to the south and the Sainsburys Superstore and St. Michaels Church to the east; all of which combined dictate a delicate approach to assessing the construction vehicle access and logistics approach.

The interface with the Sainsburys customer car park located at basement level and the goods yard at ground level also require careful consideration; details of the proposed approach to both are detailed further within this document.

The following are a summary of other buildings and local receptors noted as part of the access route review;

- Residential properties on Kentish Town Road (KTR) and Grand Union Walk.
- Sainsbury Supermarket off Kentish Road.
- St. Michaels Church and gardens off Camden Road to the east.
- Camden Council Residential block to the south east of the site accessed from Camden Road.
- Properties on the Western side of Kentish Town Road;
 - Devonshire Arms Public House
 - Trinity United Reformed Church.
 - Residential properties off Buck Street opposite 16 KTR.

The primary construction vehicle access routes have been prepared taking these into consideration where practicable.

2.3 Site access and egress

The primary construction access and egress route to the site for demolition and construction HGV's has been considered carefully to reduce the impact of vehicle movements on the local community and road network alike. Following review of the physical location of access nodes to the site potential routes during demolition and construction stage have been identified.

Following this assessment and review of the local traffic movements we have identified vehicle access and egress routes from the north and south to ensure efficient links back to the TfL PRN.

The TfL PRN Map can be found in Appendix 1 and Figure 4 below indicates the Grand Union House site and its location relative to the primary routes A503 and A501 respectively.

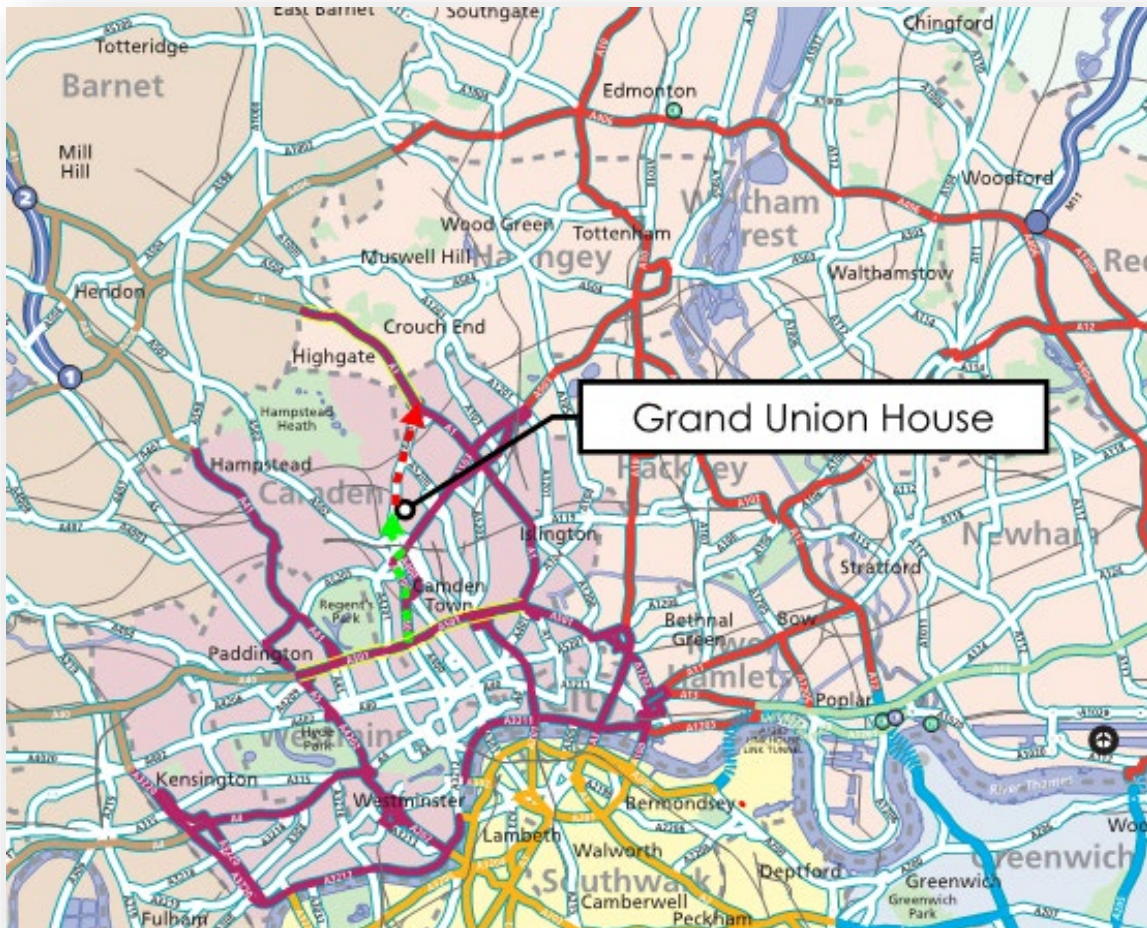


Figure 4 – Site location in relation to the TfL Primary Road Network

The extract of the logistics plan below indicates the overall location of the site, existing buildings and proposed buildings (highlighted blue) together with construction vehicle access point via Kentish Town Road.

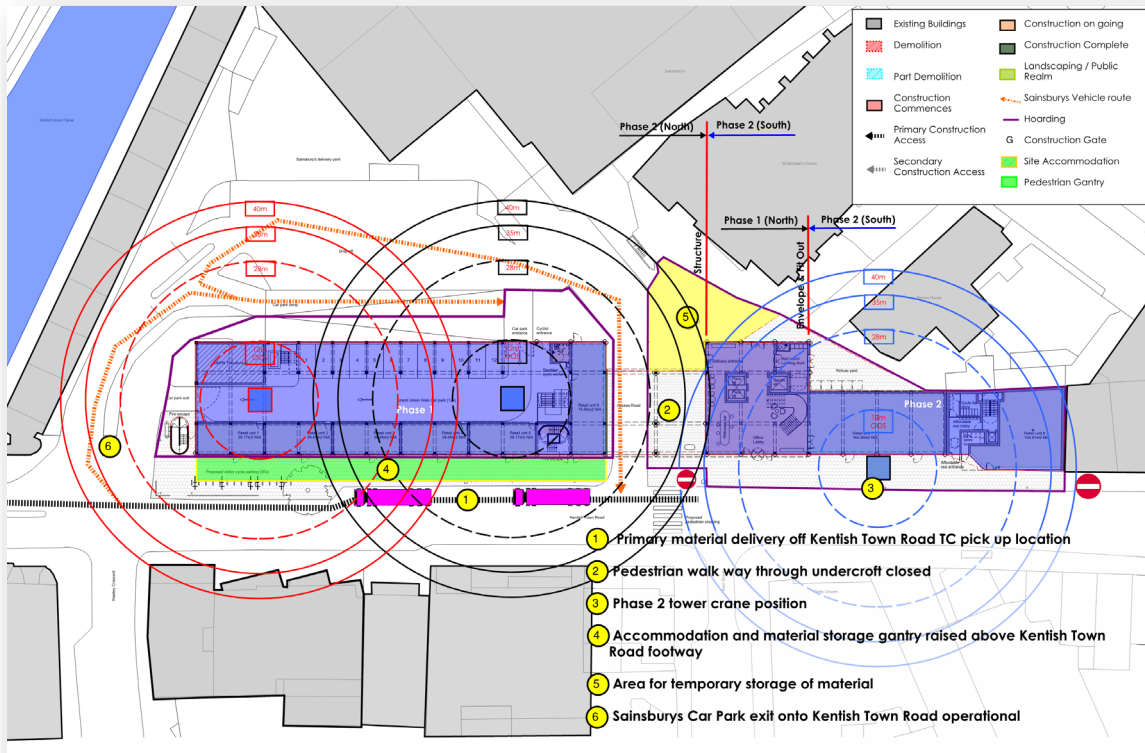


Figure 5 – Plan indicating construction vehicle access/egress locations

2.3.1 Pedestrian access

To maintain safe pedestrian access and provide segregation of pedestrians and vehicle access to the Sainsbury's Supermarket and mitigate the impact of construction traffic on the nearby residents, the pedestrian access off Kentish Town Road and through the undercroft will be closed to pedestrians for the majority of the construction period. Access from Kentish town road for pedestrians will also be closed to ensure construction.

The primary pedestrian access to Sainsbury's main entrance off Camden Street will remain operational together with access at basement level from the existing Sainsbury's customer car park.

2.3.2 Car Park access arrangements

During the works at the junction of Kentish Town Road and Hawley Crescent the current vehicle entry and exit arrangements will remain operation for the majority of the works, however to complete the façade to the east elevation use of the 'Up' car park ramp will need to be closed for a period towards the completion of the main façade installation; this will lead to the exit road closing and a perimeter hoarding to provide sufficient space for an external scaffold to be located.

Precise timings of the temporary closure will need to be developed to ensure that the proposed pre-cast façade to this can be completed with minimum disruption to the car park exit.

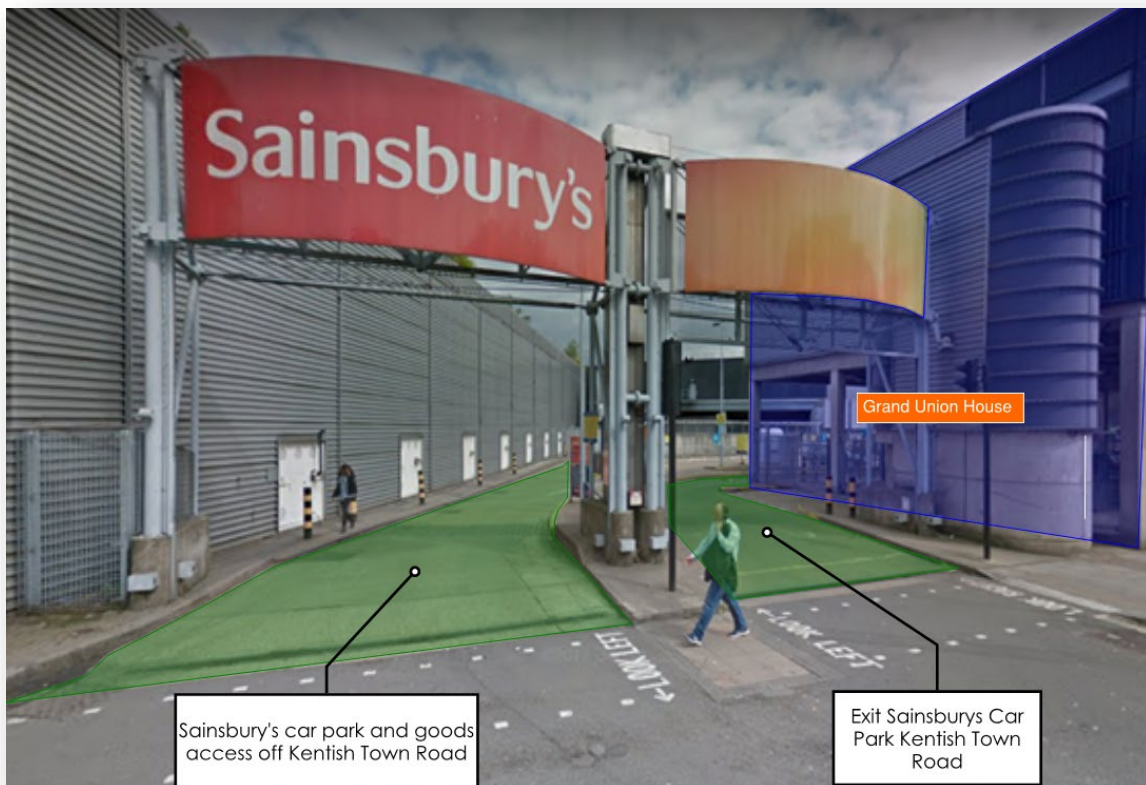


Figure 6 – Illustration depicting proposed Sainsburys access arrangements

To enable car park access and egress a scaffold crash deck will be installed from the level 1 slab that will initially comprise a fully encapsulated demolition scaffold that on completion of demolition down the level 1 slab level e adapted to an access scaffold for the installation of cladding brackets prior to the installation by tower crane of the unitised/pre-cast façade to the east elevation.

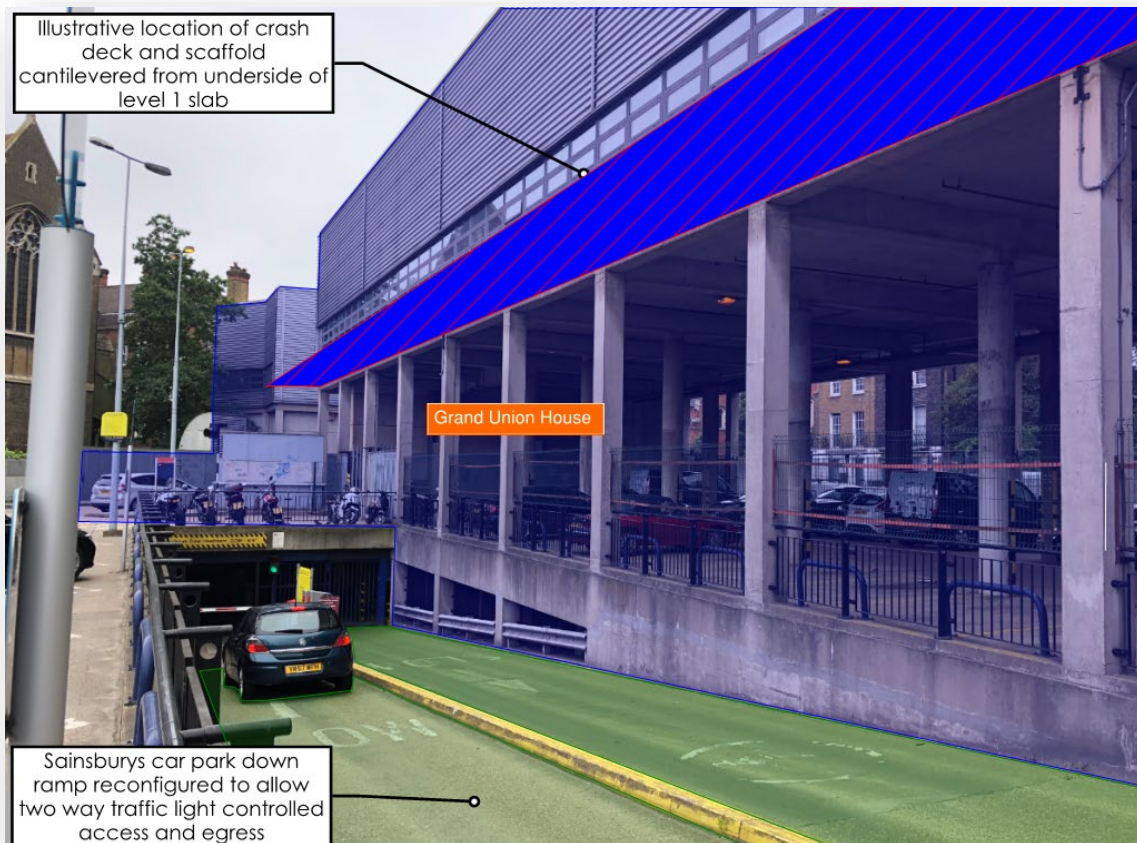


Figure 7 – Illustration depicting proposed Sainsburys car park ramp

The existing kerb layouts to suit the two-way car park access will require local realignment to achieve the exit route from the two-way car park access route to provide vehicle access onto Kentish Town Road beneath Grand Union House.

To facilitate the site set up, enabling works and external perimeter demolition scaffold works on the east elevation a cantilevered scaffold will be positioned at level 1 such that the ramp can remain open in both directions.

As illustrated in Figure 7 below, this arrangement will dictate that the current customer pedestrian access will be closed and vehicles existing the car park will leave via the undercroft onto Kentish Town Way.

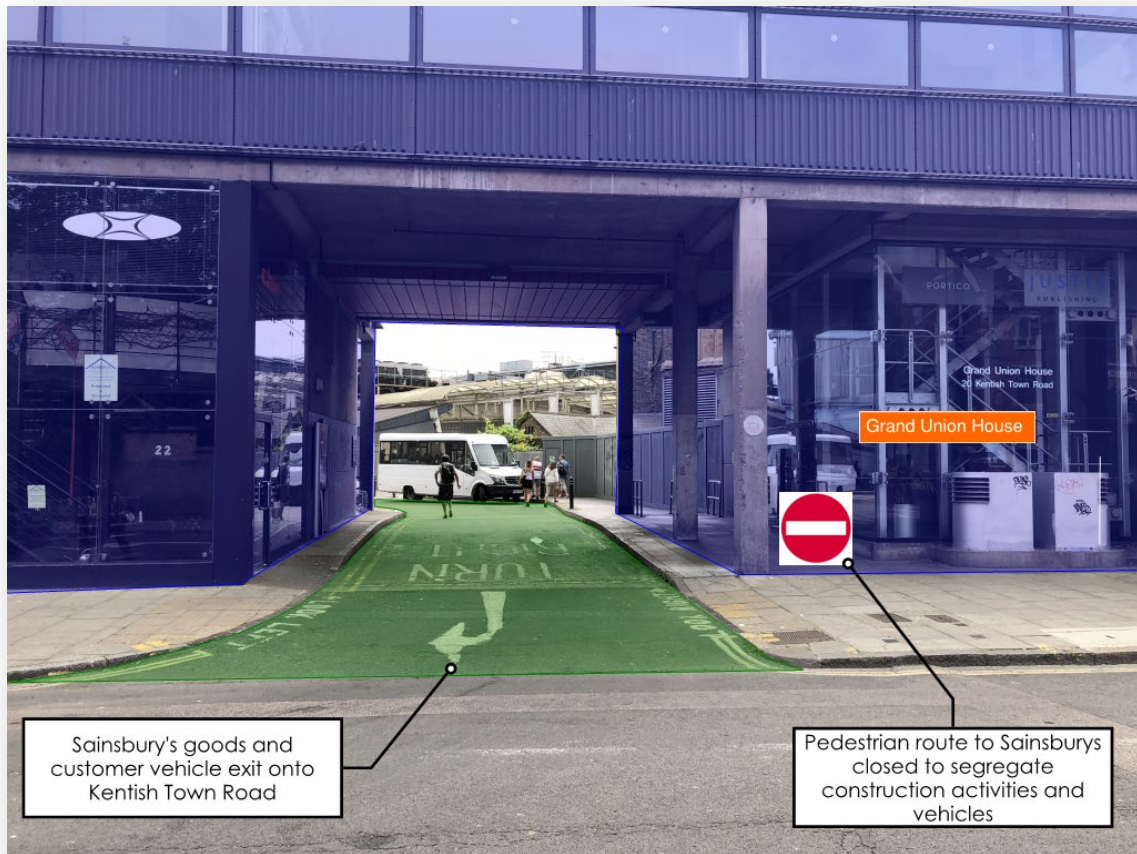


Figure 8 – Illustration depicting Sainsburys car park exit route

2.3.3 Sainsburys Goods In Delivery access

The current arrangements for Sainsburys goods deliveries and waste removal vehicle movements via access from Kentish Town Road will remain in place and stay unchanged for the duration for the works.

2.3.1 Access route from the South TfL PRN

Due to the restricted nature of the site and local road arrangements, Kentish Town Road is the only route that can be used for construction vehicle access and egress. The road is two carriageways heading in a one-way direction; south to north and providing link to the TfL PRN

The primary route detailed below identifies the optimum route for not only HGV's, but all other vehicle types arriving and leaving the site.

- HGV traffic (shown green dotted in Fig. 6 below).
This route directs all HGV traffic approaching from the southern section of the TfL PRN (A400 Camden High Street)
A501 Euston Road;
A400 Hampstead Road
A400 Camden High Street Northbound;
A400 Kentish Town Road Northbound;
Right into Hawley Crescent or Tower Crane pick up locations on Kentish Town Road.
(Note: Parking restrictions to be introduced to provide area).

Arrive at Grand Union House Site.

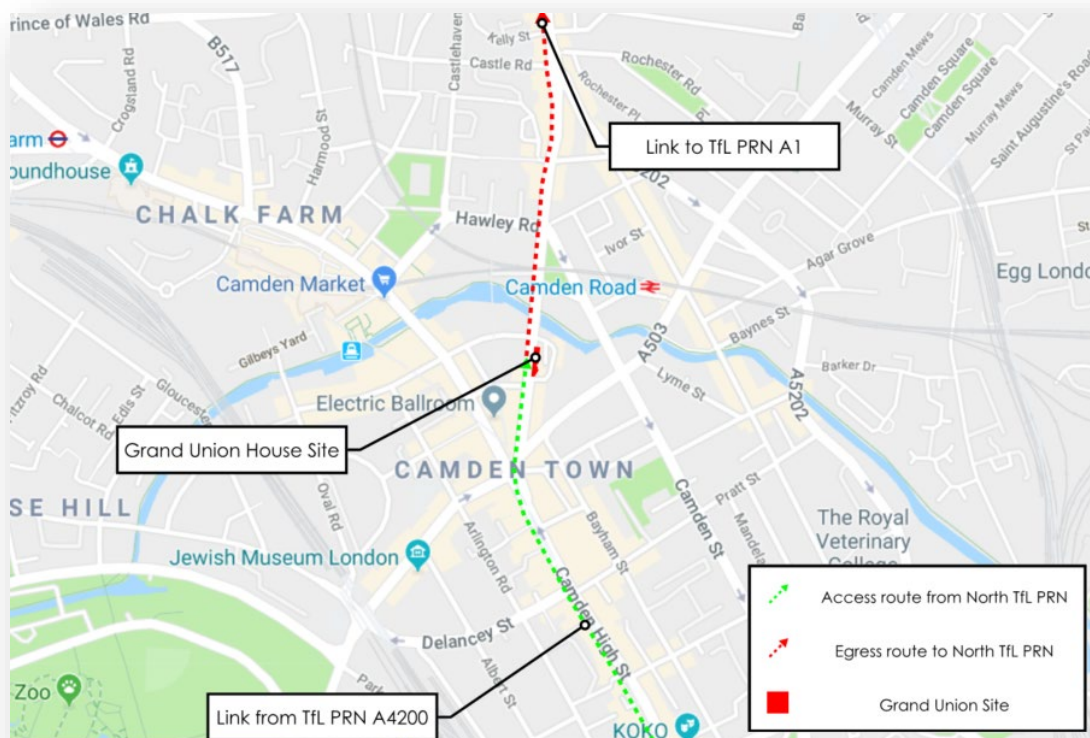


Figure 7 – HGV Construction vehicle access routes from south TfL PRN.

To ensure the Principal Contractors compliance during the construction phase, details of the construction traffic access routes indicated above will be issued as part of the Employer's Requirements.

3.0 Site set up and logistics

3.1 Logistics principles

The logistics plan within this document has been developed using the constraints as guiding principles and are intended to illustrate access to and from the site during the sub/super-structure and envelope and fit out phases of the project.

The plan included within the document details the vehicle access and egress locations during the various phases of the build and identify the tower crane locations and illustrate the pick-up locations necessary for the construction of the basement and super-structures.

3.2 Logistics outline proposals

Details of the logistics arrangements are illustrated within Section 5.0 of this document with the following indicating the proposed outline of how the project will be established.

As detailed within section 2.0 to maintain access to the adjacent Sainsburys Supermarket during the demolition and construction phases, the entrance at the Kentish Town Road junction will be maintained and managed by the construction logistics team who will manage the safe access for construction vehicles together with Sainsburys store and customer deliveries.

The entire site perimeter will be contained by a 2.4m high solid timber hoarding with appropriate energy efficient safety lighting and low-level bulk timbers where vehicle interfaces exist.

In summary, the primary access & egress for HGV's will be from Kentish Town Road and only smaller rigid axle vehicles will access the rear of the site unless for special deliveries including the installation and removal, of the tower cranes.

Due to the road layout ALL vehicles will make a right turn into the site and a left turn onto Kentish Town Road, when exiting the site.

3.3 Logistics management and Neighbourhood Liaison manager

The successful contractor shall provide a nominated directly employed member of staff to act as Logistics and Neighbourhood Liaison Relationship Manager. This individual will be responsible for managing and co-ordinating the material access / egress to the site through the operation of a vehicle / material booking in system. It will be their responsibility to manage and update the contractors Construction Traffic Management Plan in conjunction with the Project Construction Phase Health and Safety Plan. Both documents will be reviewed, updated and distributed as necessary.

As part of the role, early contact will be made with key neighbours, particularly with the adjacent Church, residents and Sainsburys Supermarket to ensure regular contact is maintained throughout the demolition and construction process. This will allow the site team to log any complaints received by the neighbours or any other affected third parties. Responses to those complaints can then be managed. To facilitate this, a Freephone contact number will be displayed on the external hoarding.

The cleanliness of the site boundary will be maintained. The areas adjoining and used for access to cross the public highway will be kept clean and free from obstructions. Where there is a need to run cables, hose or any other potential hazard for the users of the footway, suitable ramping together with appropriate signage must be employed.

These external areas will be the responsibility of the banksmen and outside of their role of policing deliveries to and from the site they will be on hand to offer help and assistance to vulnerable users of the footway.

The deliveries outside of the site hoardings will be 'by exception'. All other deliveries will take place within the confines of the site and not off the highway. All deliveries crossing the footway and entering the site will be managed into the site using trained banksmen who will maintain the flow of the footway, particularly during peak times. Suitable well maintained 'Chapter 8' barriers will be used to segregate the public from construction traffic movements wherever necessary.

3.4 Condition surveys

As soon as the Contractor is appointed a series of photographic condition surveys will be carried out during the lead in period before the start on site date.

These will take place to the immediate highway including local access routes, Sainsburys car park access road and goods in/out area and details issued to LBC and Sainsburys PLC for record purposes.

3.5 Site Security / Access / Egress & traffic management

The site will be covered 24/7 by security cameras located around the perimeter. During working hours entrances and exits will be manned by security personnel throughout the project period. Lighting will also be provided to the hoarding to enhance security.

All staff and visitors to the site will use an electronic system to access / exit from the site.

With the site well served by the following public transport links;

- Camden Town Underground Station (Northern Line) a 9-minute walk.
- Kentish Town (London Overground Line) a 7-minute walk.
- Camden Road (London Overground Line) a 9-minute walk.

There will be **no parking** for staff or workforce construction vehicles on site.

The secondary access (Gate A) Hawley Crescent off Kentish Town Road will be manned by trained and certified banksmen who will receive deliveries to the site and manage the construction traffic interface with temporary barriers to prevent pedestrians crossing during vehicle movements.

Any deliveries not booked into this system or arriving outside the permitted working hours will not be permitted onto site and will be turned away. Waiting vehicles in the local residential streets will not be permitted.

3.6 Delivery / vehicle management

3.6.1 General vehicle movements

In order to minimise the impact the increase on local construction traffic will have on the local area; all vehicle movements both in and out of the site will be managed and monitored by the Logistics Manager who will ensure vehicles do not wait on Kentish Town Road or other local highways at any time.

All vehicle movements to and from the site will be subject to a delivery booking system managed by the contractor's Logistics and Neighbour Liaison Manager and this system will incorporate any special events for the neighbours. The system will also ensure that material deliveries are rationalised to reduce vehicle movements to the site generally. To ensure bottle necks and waiting vehicles are avoided a system will be implemented to ensure that each delivery calls into the site.

The appointed Contractor must have a proven track record for developments for this nature and operate an online booking in system for ALL deliveries and material removal from the site.

A detailed heavy goods vehicle analysis has not been undertaken at this stage but based on experience we envisage peak vehicle numbers during the basement construction and excavation to reach 45 for a limited period of time. One vehicle movement relating to a single vehicle entering and existing the site via the previously noted primary HGV routes.

To ensure that all vehicles leaving the site are suitably cleaned at the key demolition and sub-structure stages of the programme, a dedicated logistics team will be in place to wash down vehicles prior to re-entry to the highway. This team will use jet-wash lances at a specific 'wash down area' to prepare the vehicles before they enter the highway together with regular road sweeper visits to sweep and wash the primary egress route local to the site.

3.7 Existing incoming services

Specific details of the existing site services are not known at this stage, however, prior to the demolition works commencing existing statutory services will be isolated and arrangements in place for them to be relocated to suit the new construction.

3.8 Protection of existing trees

As part of the site set up and enabling works activities, physical tree and root protection measures and ongoing site management controls will be implemented to ensure the trees adjacent to the site are suitably protected and maintained for the duration of the works

A tree survey and report have been commissioned to support the Application and its detail and recommendations can be found elsewhere, however as matter of course regular review visits would be made by the project Arboriculturist to ensure the measures and controls are being maintained.

4.0 Details of the Works

The Grand Union House site comprises a new commercial office space over 5 floors together with residential space to the southern section of the building.

4.1 Outline demolition and construction sequence

This section provides an indicative construction sequence incorporating the commercial and residential units.

Site Set up

- Initial surveys and ground investigations. (Some have already been undertaken to inform the design).
- Secure the perimeter of the site and security.
- Asbestos surveys, reports and associated HSE ASB5 notifications.
- Asbestos removal.
- Tree protection measures installed where applicable.
- Install initial site accommodation and welfare cabins.

Demolition

Phase 1 – Southern section of Grand Union House

- Initial soft strip, plant and services removal.
- Demolition of existing single storey building.
- External scaffold erection to existing building perimeter.
- Hard demolition of the main concrete framed building to level 2.
- Preparation of hard standings for scaffold as required and mobile crane location.

Phase 2 – Northern Section of Grand Union House

- Closure of ground floor car park.
- Initial soft strip, plant and services removal.
- Preparation of hard standings for scaffold as required and mobile crane location.
- External scaffold erection to existing building perimeter.
- Delivery of main demolition plant including craneage to roof.
- Hard demolition of the main building concrete framed building to level 2.

Sub-structure works

- Installation of propping (if required) to perimeter retaining wall.
- Basement excavation and temporary sheet piling to as required.

Super-structure works

- Traditional core construction.
- Reinforced concrete frame, floor slabs and columns.

Envelope works

- Unitised curtain walling system.
- Ground floor facades will be installed from grade using a combination of MEWP access and mini crane for glass handling.

Fit-out works

- School classroom and support space fit out.
- Lifts, toilet and core fit out.
- Residential entrance/reception fit out.
- Commercial and Residential fit out works.

4.1.1 Site set up and welfare facilities

As described above, to allow the works to commence a fully decorated 2.4m hoarding will be provided to the entire perimeter and vehicles gates installed to vehicle access off Woodlands Avenue. The line of the hoarding will generally follow the site land registry and stopping up boundary line. This would incorporate appropriate tree protection measures to ensure the hoarding and temporary accommodation does not impair the existing tree structure and growth.

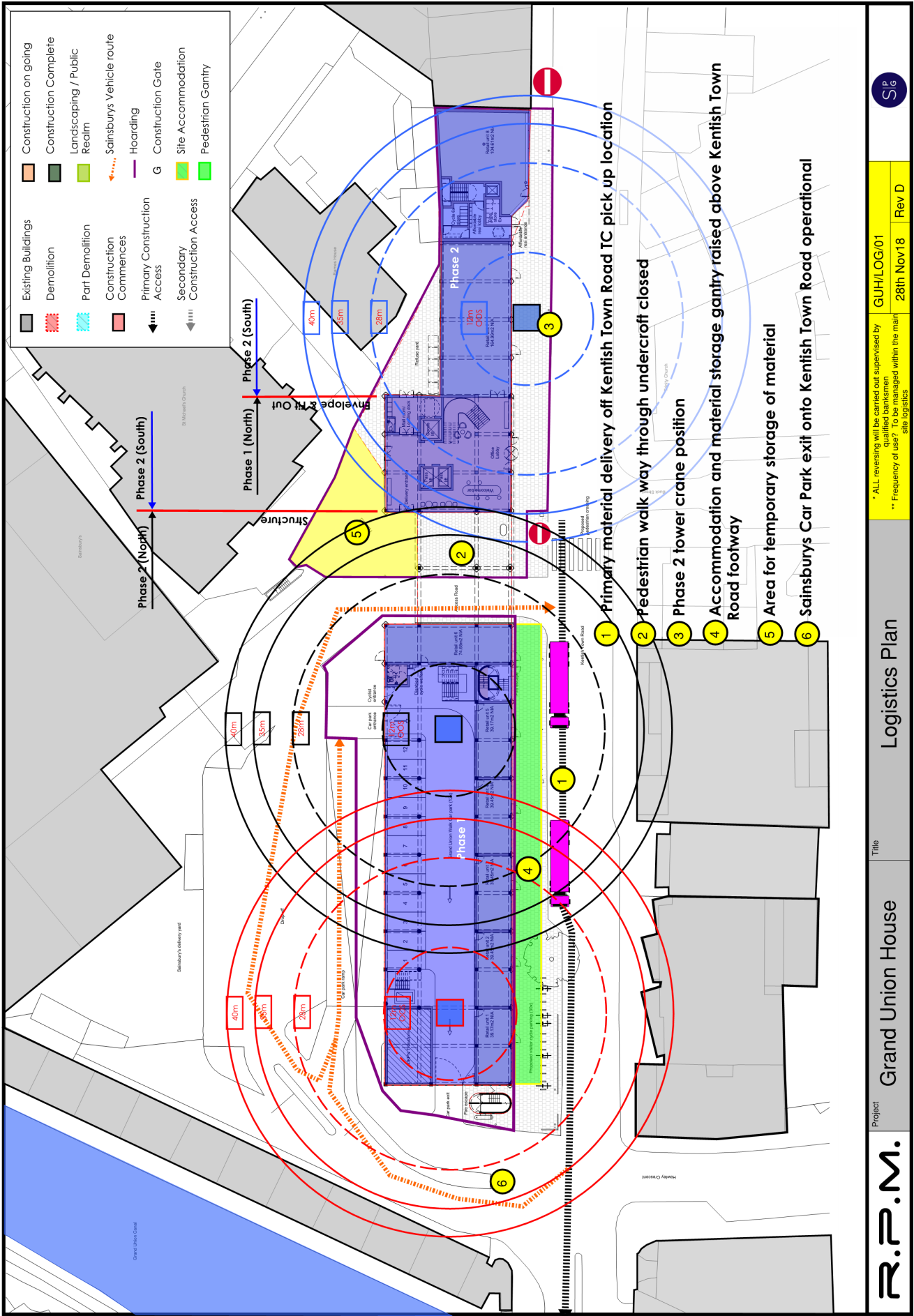
The hoarding will be lit with energy efficient LED lighting and the access roads illuminated sited so to minimise visual intrusion and light spillage/ pollution at the nearby properties but will comply with regulations to ensure safe passage around the perimeter. The hoarding is to include (where appropriate) public viewing panels that allow children as well and adults to observe operations and a Freephone contact number to the Neighbourhood Liaison Manager. These will be located on the

Initially we are proposing that welfare and staff accommodation for demolition will be sited in the southern area of the site within mobile units that can be relocated as works progress. There is potential for the retained engineering block to be used as a short-term alternative but until the position on asbestos can be confirmed we have discounted at this stage. This will allow flexibility in the short term whilst the enabling work are progressed to provide access and temporary services for the longer-term position for welfare provided in the area finally designated for external play.

The site hoarding line will remain in place throughout the project and in the main will only be removed as the external finishing works require it, in particular the interface with the adjacent Church and Sainsburys Supermarket. As the façade and external works are completed at the interface with hoardings will be locally relocated.

Finite details of progressive hoarding moves will be provided within the contractors detailed logistics and phasing plans as part of the Construction Phase Health and Safety Plan.

5.0 Logistics plans



5.1 Summary Programme

Programme under review and update for issue separately

6.0 Environmental considerations during Construction

6.1 Site Working Hours

The programme is based upon the following proposed working hours;

- Monday to Friday 0800hrs – 1800 hrs.
- Saturday 0800hrs - 1300hrs.
- No noisy works between 0800hrs and 0900hrs.
- No Sunday or Bank Holiday working planned unless by prior approval for specific works. i.e. Tower crane and installation and removal.

Noisy operations will not take place outside these times.

With the scope of demolition planned, it is expected that the Principal Contractor will make an application to the LBC Environmental Protection Team for prior consent for works through Section 61 of the Control of Pollution Act 1974.

6.2 Considerate Constructors Scheme

The development will be undertaken in accordance Considerate Constructor Scheme (CCS) with target set for minimum performance for the project, contractors and suppliers.

The Contractor will be required to register the Project under the Considerate Constructor Scheme with a minimum target score of 7 in each section of the Schemes site code for Considerate Practice.

As part of the CCS scheme regular inspections will be carried out and subsequent reports will be distributed as part of the contractor's monthly report.

6.3 Noise, vibration and dust

In order to mitigate construction noise vibration and dust the works will employ the 'Best Practicable Means'. Including application of methods recommended in BS 5228: Noise Control on Construction and Open sites, for example, undertaking works to ensure minimum disturbance, using muncher attachments to excavators in lieu of pneumatic breaker and utilising separation cut lines to minimise vibration transfer where applicable.

Construction processes will be monitored using air quality monitors to record particulates and the results compared to London wide monitoring stations. A site action level of 250µg/m3 over 15 minutes will be set and mitigating action taken if this level is approached.

Where appropriate, the following measures to minimise noise and vibration levels will be adopted:

- Using modern, quiet and well-maintained equipment;
- Using low impact techniques, such as munchers where applicable;
- Using electrically powered equipment (mains or super silenced generators);
- Use of screws and drills rather than nails installing the hoarding;
- Careful material handling such as lowering rather than dropping items;
- Isolating the deconstruction works from sensitive neighbours, to minimise the

- transfer of vibration and structure borne noise;
- Avoidance of unnecessary noise between operations, shouting, loud radios or excessive revving of engines by effective site management;
- The use of radios on site, shouting, swearing, singing; sitting outside the site is not to be permitted at any time.
- No idling engines to reduce noise and pollution.

The distance between noise and vibration sources and sensitive neighbours will be maximised and the sound path obstructed, where practical, by considerate siting of stationary plant and loading/unloading areas.

The suitability of specific noise limits is highly dependent upon the individual situation. The factors to be considered include the characteristics of the potentially affected neighbours, baseline ambient noise levels and the nature and duration of the works.

The following measures are to be undertaken for the control and monitoring of dust, fine particles and odours:

The contractor will develop a Code of Construction Practice (CoCP) in relation to construction noise and fugitive dust which must detail:

- The type of works to be undertaken.
- Construction techniques to be used.
- The site layout and access arrangements.
- Times and duration of site operations.
- An inventory and timetable of all dust-generating activities.
- Principle Contractor(s) Statutory Obligations and duties.
- How staff will be trained in the use of noisy machinery.
- How materials are to be handled to minimise the potential for noise nuisance.
- Times and durations of any abnormal noise and how the public will be kept informed.
- The appropriate range of dust suppression and control measures to be implemented in accordance with a 'high risk' site as defined under the Best Practice Guidance for 'The Control of Dust and Emissions from Construction and Demolition' (Greater London Authority and London Councils)
- The on-site storage of fuels or chemicals.
- Identify the Site Environmental Management Representative (SEMR).

All vehicle loads entering / departing the site are to be covered and material sprayed with water on all unsealed or exposed areas via watering carts at regular intervals during dry-weather.

Erect temporary solid hoardings along all site boundaries to act as a windbreak and to limit lateral dust 'escape'.

Ensure that suitable training and awareness are provided to construction personnel so that they understand their responsibilities regarding environmental management, particularly the control of fugitive dust.

The appointed neighbourhood liaison/relationship manager will undertake regular liaison to ensure all residents and users of adjacent premises have been given advance notice to minimise dust concerns.

6.4 Neighbour and Community Liaison

The appointed Logistics and Neighbourhood Liaison Relationship Manager will liaise with the LBC, local primary schools, residents, businesses and other interested parties to keep them informed of progress on site and forthcoming activities which may affect them.

As part of the demolition and construction processes, the Principal Contractor will meet with the LBC Environmental Health and Highway representatives and key members of from the local community to fine tune methods of working and the measures to minimise disruption. As part of this liaison, regular meetings will be held to ensure they are kept informed of the progress and any comments received logged and actioned as a result of the works.

To mitigate disturbance the Principal Contractor will identify all residential properties and other sensitive occupiers in close proximity to the site prior to commencing work.

Prior to starting work, all occupiers in the vicinity of the site will be informed of the start date, the duration and nature of the project, the principal stages of the project and contact names and numbers of appropriate personnel via hand delivered mailed Project newsletter.

Further newsletters notifying neighbours of progress and forthcoming activities, particularly those which may cause disturbance, access difficulties and the like, are to be hand delivered to all adjoining occupiers and other neighbouring occupiers who may be affected by the works, on a monthly basis throughout the duration of the works.

A 'display board' will be erected outside the site, which will identify key personnel, contact addresses and telephone numbers as well as a full copy of the planning permissions and any forthcoming activities relating to the works.

Appendix 1 - TfL Primary Road Network Plan

