

Access and Inclusivity Statement

Building P2

King's Cross Central
General Partner Ltd

May 2018

**KINGS
CROSS**

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Management Issues

1.0 Introduction

This document sets out the process adopted by the developer (King's Cross Central General Partner Limited) to create an accessible and inclusive environment in Building P2 and its associated public realm located within the King's Cross Central (KXC) Development site.

This statement has been prepared to accompany the Building P2 Reserved Matters submission, which includes the surrounding public realm as shown on Site Location Plan in Figure 1 below.

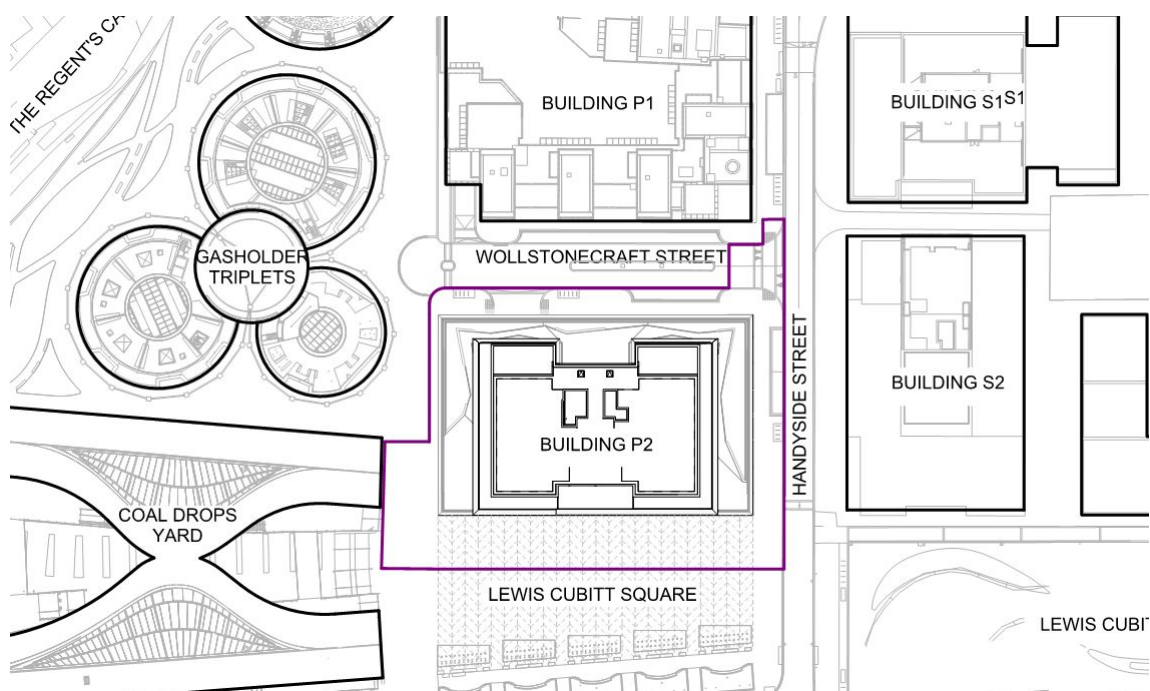


Figure 1 Building P2 location plan with the reserved matters boundary in purple.

Both the proposed building and the surrounding public realm are located within Development Zone P in the north-west portion of the KXC development.

Plot P2 is located in the north of the King's Cross Central ('KXC') development site. It is bounded by Lewis Cubitt Square and the Western Transit Shed to the east, Building S2, Lewis Cubitt Park and Building R1 to the north and north east, Building P1 to the west, and the heritage Coal Drops Yard ('CDY') and Gasholder Triplets buildings to the south and south west.

The proposed Building P2 is a ten storey office building with retail and theatre at ground levels.

It is formed of a unifying light pre-cast concrete frame, with inset glazing surrounded by contrasting midnight blue frames and spandrels at upper levels, and glazed oriel bays interspersed with concrete ribbed panels with benches and various entrances at

Ground Level. A large, wrap around terrace is proposed at Level 5, whilst inset terraces (loggias) punctuate a facade with strong horizontal and vertical emphasis, to respond to the surrounding buildings and in particular to the adjacent Lewis Cubitt Square.

The proposals also include a strip of paving to complete Lewis Cubitt Square, as well as landscaping along part of Handyside Street, Wollstonecraft Street and between the proposed building and the Gasholder Triplets.

This statement has been prepared in response to discharge Condition 19 (Access Statement) of the KXC Outline Planning Permission ('the Outline Planning Permission'), granted in December 2006 (with ref. 2004/2307/P) and Section V of the associated Section 106 Agreement on Access and Inclusivity.

1.1 Context

Documents which relate to access and inclusivity within KXC are developed under a document hierarchy as follows:

1. King's Cross Central Access and Inclusivity Strategy (Sept 2005)	A scene setting document establishing the principles and containing the master plan philosophy and over arching strategies, as referred to in the S106 Agreement.
2. Access Statement (this document)	A detailed document containing expanded descriptions explaining how the strategy has been implemented in the individual schemes.
3. Building Regulations Access Statement	A document which will accompany the building regulations application for the buildings. This document will contain a further level of detailed description to accompany the increased level of detail of the Building Regulations submission.

1.2 Scope

This Access Statement contains an explanation of measures that have been incorporated within the proposals for Building P2 and associated public realm to facilitate access and use by all people including disabled people. It indicates how the design meets the required design standards, good practice guidance and Building Regulations access requirements.

The statement takes into account the needs of people with mobility impairments including wheelchair users and those with sensory and cognitive impairments. However, it is recognised that the issues considered in this report will affect the convenience of access for all occupants, not just disabled people.

This Access Statement is based on the strategies set out in the King's Cross Central Access and Inclusivity Strategy (September 2005) and addresses the items set out in Appendix D of that document, including:

- Explanation of policy and approach to access;
- Sources of advice and guidance on accessibility;
- Details of consultations undertaken or planned;
- Details of access consultant involvement;
- Explanation of specific issues affecting accessibility and details of access solutions adopted; and
- Details of potential management policies and procedures to be adopted to enhance and maintain accessibility.

Areas where technical or other constraints have prevented or constrained the application of the principles set out in the above strategy are highlighted as appropriate.

The areas covered in the building include entrances, horizontal and vertical circulation, facilities and sanitary accommodation.

The plans include the indicative designs for the theatre and retail layout. These areas are not discussed in detail as they will be subject to Building Regulations applications and the arrangements for access for disabled people will be addressed in those applications.

This statement does not cover operational aspects in detail, since these will be addressed during Building Regulations approval, but it identifies and comments on areas where management procedures are likely to be required to ensure good accessibility.

Landscape considerations are discussed where relevant, including materials, routes, lighting, parking and street furniture.

This Access Statement is based on, and should be read in conjunction with, the submitted scheme drawings and information provided by Allford Hall Monaghan Morris Architects ('AHMM') and Townshend Landscape Architects ('TLA').

1.3 Role of Access Consultant

The access consultant has been actively involved in the preparation of the submitted proposals. The role of the access consultant is to advise the design team and appraise elements of the design at the relevant stages of the design process to ensure that the best possible level of access is achieved and that the proposals meet relevant legislation, the S106 Agreement requirements and recognised good practice guidance. The consultant also provides recommendations about measures that can be incorporated within the scheme to facilitate access and use by disabled people.

The access consultancy services have ensured the integration of accessibility measures into the building whilst also maintaining the overall concept of the design.

1.4 Criteria for assessment and design guidance references

The following documents and guidance have informed the proposals and are referenced where appropriate:

- Argent (King's Cross) Limited, King's Cross Central Access and Inclusivity Strategy, September 2005;
- GLA, Accessible London: Achieving an Inclusive Environment, April 2004;
- Building Regulations Part K, Approved Document K, 2013 edition;
- Building Regulations Part M, Approved Document M, 2015 edition;
- British Standard BS8300:2018 Design of an accessible and inclusive built environment Part 1: External environment — Code of practice;
- British Standard BS8300:2018 Design of an accessible and inclusive built environment Part 2: Buildings — Code of practice;
- BS 9999:2017 Fire safety in the design, management and use of buildings - Code of practice;
- DETR, Parking for Disabled People, Traffic Advisory Leaflet 5/95, 1995;
- Other currently recognised good practice design guidance including *Sign Design Guide*, (SDS, 2000); *Guidance on the use of Tactile Paving* (UK, DETR), *Inclusive Mobility* (DoT); *Designing for Accessibility* (CAE, 2004), *The Access Manual*, (Blackwell, 2006) and *Manual for Streets* (DfT and DCLG 2007).

It is also necessary to observe reasonable functional and financial practicalities and to take into account the nature of this and its neighbouring buildings. Wherever possible, the design team have gone beyond the minimum requirements of Part M (Building Regulations) and the guidance provided in the Approved Document M. This will assist the occupier(s) in meeting its/their duties under the Equality Act 2010.

1.5 Factors contributing to accessibility

This Access Statement considers accessibility at an early stage in the design. Detailed design issues such as fixtures, fittings, street furniture, play equipment, lighting, communication systems, management and other issues which contribute to the accessibility of the services and facilities provided will need to be considered in the future.

The individual needs of visitors cannot always be known in advance, thus it is acknowledged that further adjustments to estate management policy or procedure or to the physical features of the building and landscaping may become necessary. However, it is the intention of the design team to ensure that the need for further physical alterations and cost implication of this is reduced to a minimum.

1.6 Consultation

The proposals were presented to and discussed with the King's Cross Design and Access Forum at a meeting held on the 10th April 2018. These meetings have been developed as a method of ensuring that there is adequate user input into the design development process ahead of the planning submission.

Issues raised at this meeting have been noted and where possible, incorporated into the scheme, for example:

Questions	Responses
The accessible parking appears to be located around the back in Wollstonecraft Street. This is too far from the entrance of the building?	Two accessible parking bays have been located in Handyside Street (please see the Figure 3 in the building discussions) which are approximately 45-65 metres from the front entrance(s).
Is the parking close to the front entrance?	Yes – the new bays will be approximately 45-65 metres of travel along a level paved area to the theatre/office entrance. The scheme also benefits from on-street benches every 50 metres in the landscape, and the building itself has benches built into the façade of the building.
Will there be accessible cycle facilities?	Accessible changing and showering have been provided in the Upper Basement. For the office element of the building, this facility is reached via a single story platform lift from the rear entrance on Wollstonecraft Street to the basement level. For the theatre the passenger lift will access the Lower Ground Level where there is also storage for theatre (workers) bikes. The remaining cycle parking for visitors to the theatre and staff and visitors to the retail units will be located in the public realm.
How will the different entrances be differentiated sufficiently for people with partial vision?	The ground Level of the building has three entrances that face Lewis Cubitt Square. The retail element to the south, the office entrance at the centre, and the theatre entrance to the north. The theatre entrance is not directly on the corner of the building, in direct response to not being able to install a revolving door for accessible reasons. The actual entrance is therefore just to the south of the corner of the building, and this will be made evident with signage and colour signalling. The lighting of the interior spaces will also make it very clear where the theatre foyer is located and this will be expressed using very different lighting and decoration to that behind the office entrance. Much of the life of the theatre will become evident in the evenings when the nature of the building will become further differentiated by lighting and occupancy. The outside edge of the theatre is also reinforced by the use of spill out tables and chairs, which does not extend to the office entrance.

<p>The bike store appears to be accessed using a platform lift, which should not be used in new buildings?</p>	<p>The platform lift is proposed as it is covering only one floor change and is ONLY for users to access the bike store from the entrance level to the Lower Ground Level. This type of lift is more customisable to ensure good access for a range of mobility bicycles and is felt to be appropriate for this limited usage. The lift will be designed to accommodate the frequent movements of cyclists at the peak periods and provide an appropriate duty cycle. Should the lift fail then the cycle store is accessible using the main core lifts, including the goods lift from the front reception of the building.</p>
<p>Does the theatre have a platform lift?</p>	<p>The precise nature of the lift has not been fully resolved, but this lift will have a design that allows the same number of duty cycles as a passenger lift as it is essential for disabled people accessing their seating. The theatre proposal details are not a part of this application.</p>
<p>Will the theatre have an induction loop as there is a concern that it will talk to the other nearby buildings.</p>	<p>The precise nature of the theatre's audio enhancement has not yet been agreed, but care will be taken to avoid either interference and/or overspill of the signal.</p>
<p>The detail of the theatre has not been shown – but it will need to have accessible dressing rooms etc as well as seating front of house.</p>	<p>The theatre design has taken the needs of actors and employees into account and will have accessible dressing rooms (at stage level) an accessible stage door, stage, workshop and administration areas and will be subject to a Building Regulations application.</p>
<p>Can the building accommodate a changing places WC?</p>	<p>Changing places WCs are ideally accommodated in building which have long opening hours and can be advertised as available for long period so time day/evening. Unfortunately the nature of the theatre is that there will be periods of time during the day and seasons when it is closed and therefore not suitable for offering this type of facility.</p>

Further details are provided in the full assessment of the proposals set out in Section 2.0.

2.0 Public Realm

The proposals also include a strip of paving to complete Lewis Cubitt Square, as well as landscaping along part of Handyside Street, Wollstonecraft Street and between the proposed building and the Gasholder Triplets.

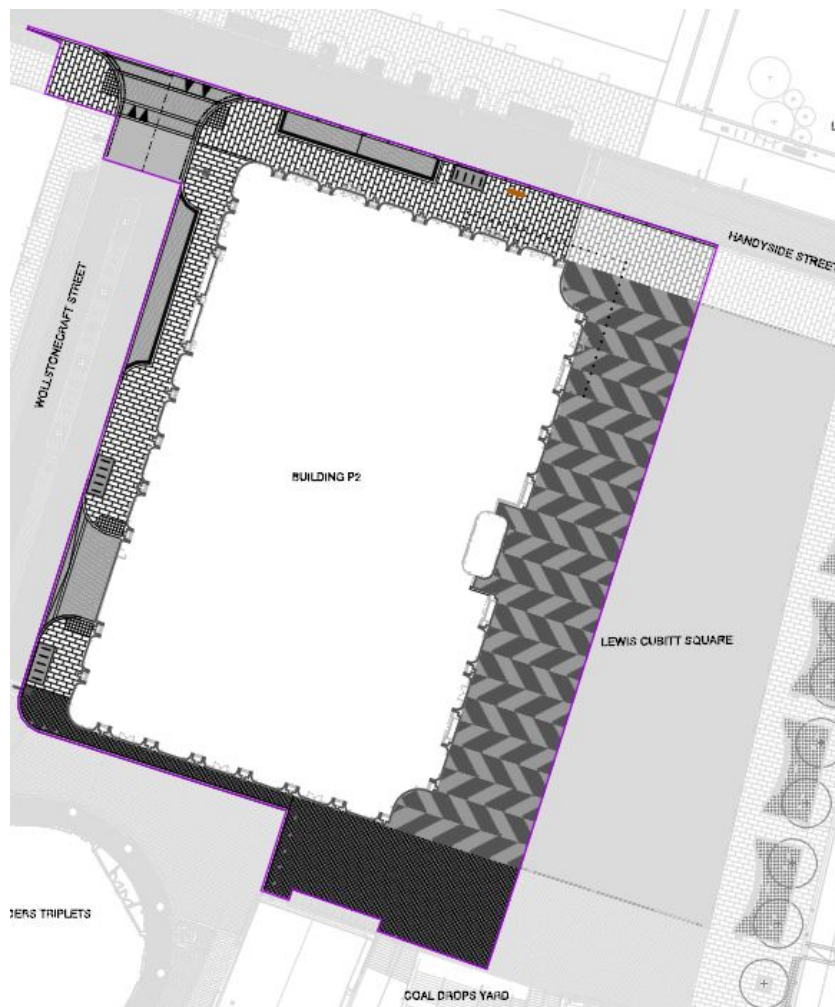


Figure 2 Proposed landscaping around Building P2

2.1 Materials

The public realm area will have a consistent palette of materials which have been utilised throughout the KXC scheme to provide a coherent and navigable landscape, whilst reinforcing pedestrian routes and crossings.

Paving already exists in the built Lewis Cubitt Square and Wollstonecraft Street, and these will be continued up to the threshold of the building.

The paved areas around the building will consist of sandstone pavements along Handyside and Wollstonecraft Streets and the granite setts in a chevron pattern to complete the square.

2.2 General Levels

The change in level north to south alongside the eastern elevation facing Lewis Cubitt Square is negligible, being about 200mm over a distance approximately 70 metres.

The landscaping along Handyside Street elevation has a change in level from the Lewis Cubitt Square corner of approximately AD 25.40 down to the northwest corner of the building at AD 24.80 (600mm) over approximately 45 metres, giving this street a slope of approximately 1:75 (level).

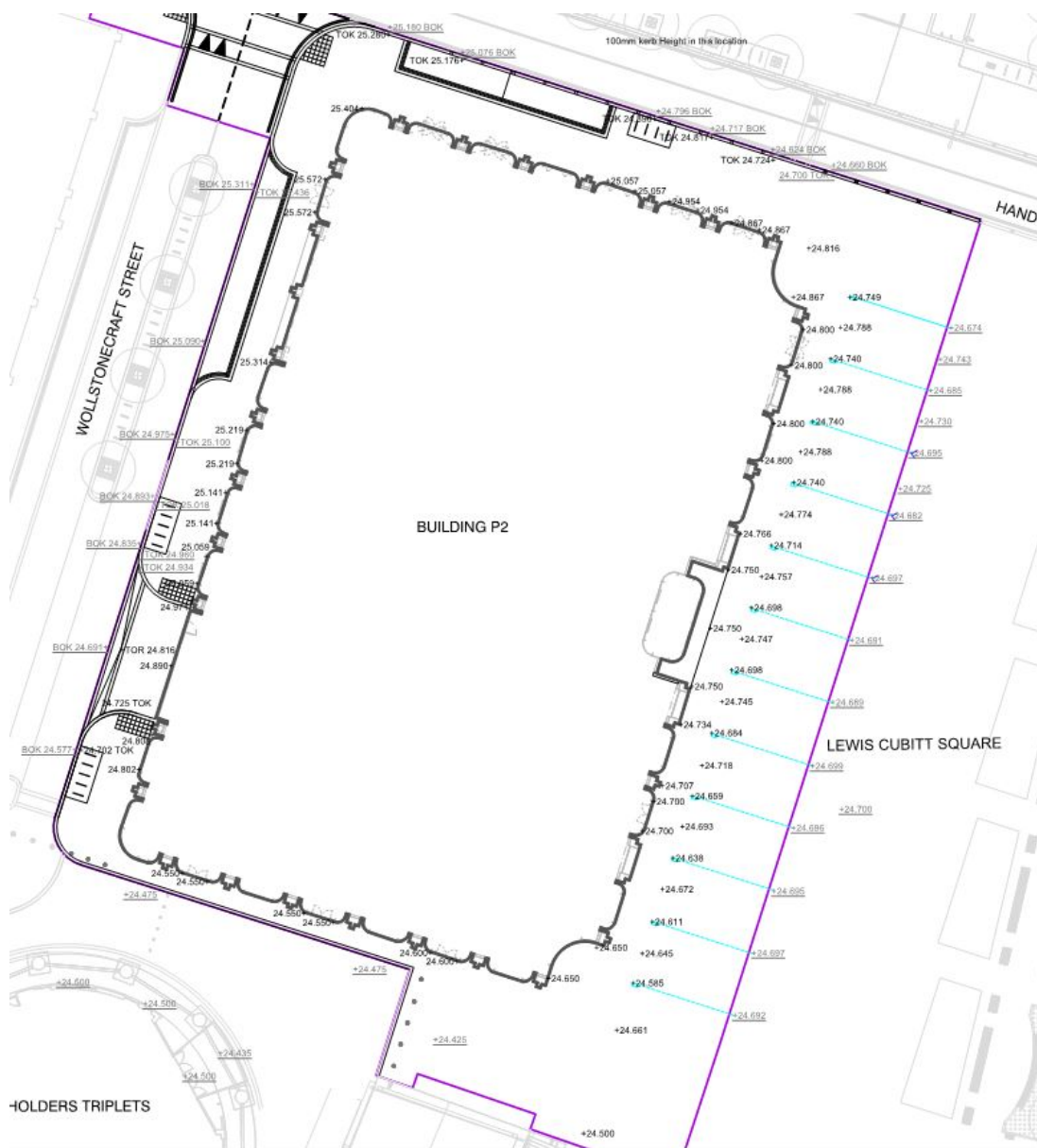


Figure 3 Proposed landscaping levels around Building P2

2.3 Threshold Treatments

All thresholds onto the public realm, terraces and roof terrace from the buildings will be flush, with no more than a 15mm level change, and with joints of no more than 10mm wide. Where possible, thresholds between materials will also have a visual contrast (using different materials) to assist people with visual disabilities.

2.4 Street Furniture

Sitting benches will be installed periodically within the design of the pavements and cycle stands located within the paved area as illustrated on Figure 3 above.

2.5 Security and Lighting

A comprehensive CCTV system will form part of the estate wide management strategy which will considerably improve user security across the site. The area will also be regularly patrolled by members of the estate management team throughout the day.

A lighting scheme will be developed to deliver light levels which support the safety initiatives in the area, and navigation by people with visual disabilities.

Night time lighting of the scheme has been specifically designed to improve safety and will also come from lighting within the buildings and from the amenity lighting in the public realm submissions.

Overspill of light from the theatre foyer will enhance the lit environment for those leaving the theatre.

2.6 Wayfinding

Navigational signage for the area has not yet been detailed. Much of the signage will be specified according to normal highway standards although there will also be signage that will form part of an overall Wayfinding Strategy across the KXC site.

3.0 Building P2

3.1 Building P2

The proposed Building P2 is a ten storey office building with retail and theatre at ground levels.

It is formed of a unifying light pre-cast concrete frame, with inset glazing surrounded by contrasting midnight blue frames and spandrels at upper levels, and glazed oriel bays interspersed with concrete ribbed panels with benches and various entrances at ground floor. A large, wrap around terrace is proposed at Level 5, whilst inset terraces (loggias) punctuate a facade with strong horizontal and vertical emphasis, to respond to the surrounding buildings and in particular to the adjacent Lewis Cubitt Square.

The upper floors provide highly flexible office space. The building is designed to allow for a single or multiple tenancies.

Amenity space is provided in the form of a terrace at Level 5, and loggias at Levels 1, 2, 3, 4 and 9.

Office occupiers are provided with secure cycle parking and changing facilities on the upper basement level with associated showers and changing facilities. Theatre employee cycle parking is also provided in the Lower Ground Level of the theatre's demise. Space for theatre and retail visitors is provided within the public realm, as is cycle parking for retail employees.

3.2 Parking

An overall site-wide strategy for parking has been approved under the KXC Outline Planning Permission, with maximum ratios of parking spaces agreed, and appropriate percentages of parking provision for disabled people.

The proposed Building P2 has no general parking, but does include two accessible spaces on Handyside Street as indicated in Figure 3, which are approximately 45-65 metres from the theatre and office entrances.

These are on-street blue badge spaces, which will be for general use, but most likely office use during the daytime and theatre use during the evening.

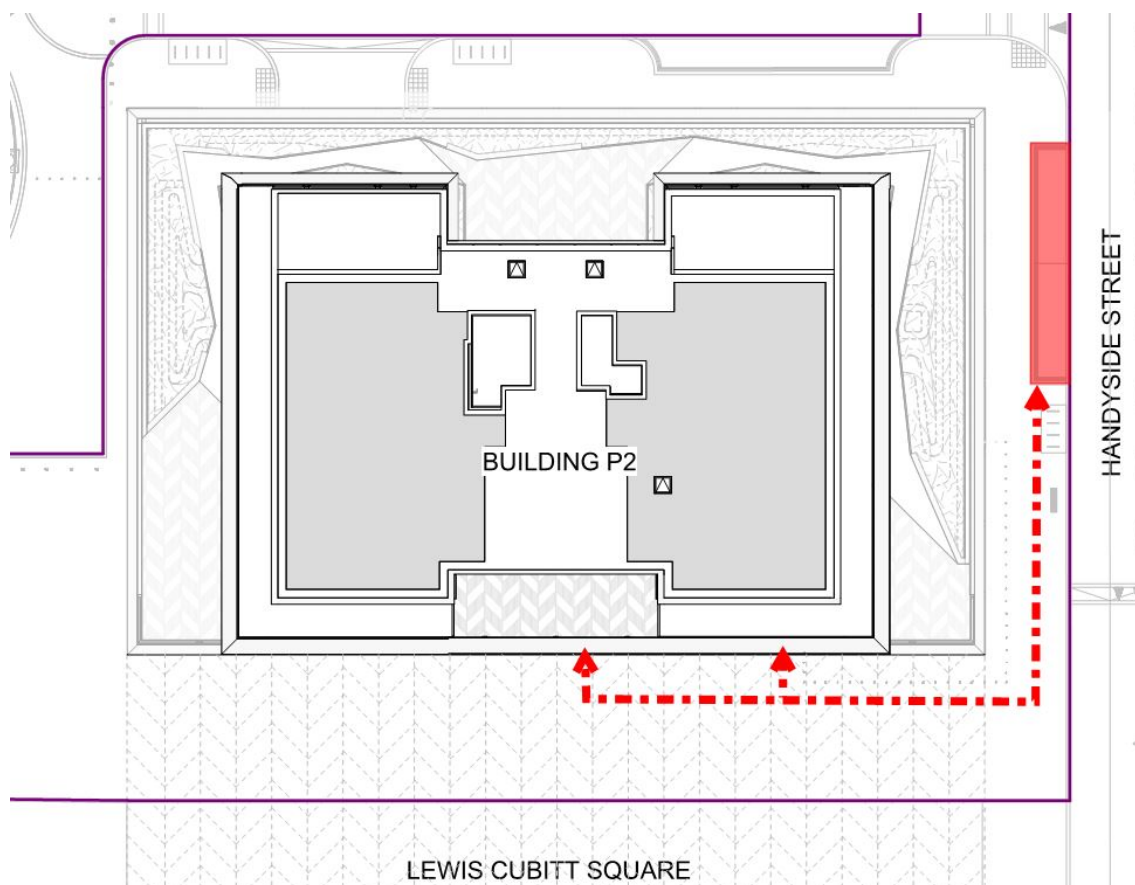


Figure 4 Accessible parking located to the north of the proposed Building P2

3.3 Entrances and exits

Proposed Building P2 will have several entrances (as shown on Figure 5);

- The main office entrance, which is centred on the Lewis Cubitt Square elevation;
- The Theatre entrance which is at the northern end of the Lewis Cubitt Square elevation;
- The retail unit has its main entrance on the southern corner of Lewis Cubitt Square elevation, with secondary entrance(s) opposite the Gasholder Triplets;
- The cycle parking and facilities entrance and theatre stage door are located in the centre of the Wollstonecraft Street elevation.

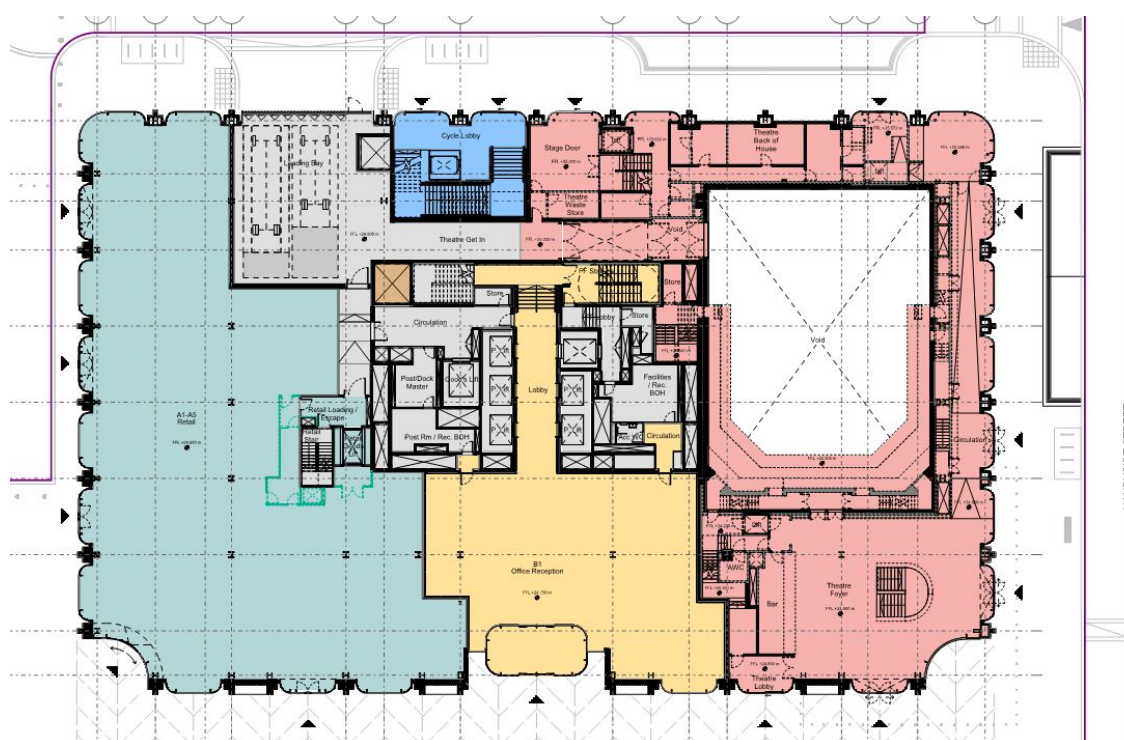


Figure 5 Entrance locations (black arrows) in the proposed Building P2

Entrances will provide level thresholds and solid entrance matting. The various entrances will be clearly articulated within the building elevation using lighting, materials and decoration.

The entrances will be differentiated in their lighting, (which will change throughout the day and night), décor and style and feel to ensure that they are easily located and differentiated.

Glazed doors and screens will have manifestation to meet the guidance in Part M/K.

All fire exits have level thresholds and openings in line with guidance in Part M.

3.4 Threshold Treatments

All external material thresholds will be flush and materials will have no more than a +/-5mm level change over the surface and joints of no more than 10mm wide.

Outside/inside thresholds will have a height/upstand no more than 15mm. Where possible, thresholds between materials will also have a visual contrast to assist people with visual disabilities.

3.5 Vertical Circulation

3.5.1 Lifts

Office Lifts

- The office use is served by a bank of 6 large passenger lifts that have car sizes of approximately 1600mm by 2200mm and will serve Levels 0 to 9;
- One of the passenger lifts is an emergency evacuation/fire-fighting lift and this serves all levels up to Level 9;
- The mezzanine floor on Level 9 is served by an additional platform lift that addresses only Level 9 and the Level 9 mezzanine. This will be a platform lift with automatic lift doors and a single push button for floor selection. The platform lift has a slower travel speed, but this lift is only operating over a single level change. The lift will have a minimum internal car of approximately 1400mm deep and 2000mm wide.
- The Cycle Parking and Facilities are served by a platform lift that is accessed from the Cycle Lobby at Ground Level. This will be a fully automatic lift which is sized to accommodate mobility bicycles and scooters.
- An additional goods lift serves Lower Basement to Level 9.

All lifts will meet or exceed the requirements of Part M of the Building regulations and BS/EN 81-70 2003. The retail area also has a goods lift that connects the basement, ground and mezzanine levels, and soft spots have been designed into the floor slab (illustrated in dashed lines) to allow tenants to install further lifts and stairs as part of future fit-outs.

3.5.2 Stairs

The proposed building incorporates two principal accommodation/circulation/escape stairs within the central core. These address all floors, from Ground to Level 9 mezzanine. One of the stairs continues down to the basement and Lower Ground levels. These staircases will meet all aspects of Part M and Part K of the Building Regulations and BS8300 guidelines for use by people with ambulant and visual disabilities.

3.5.2.1. Cycle Parking & Facilities Stair

A further staircase is proposed to the rear of the building to provide access to the Cycle Parking and Facilities area on the Upper Basement Level. (See Figure 6 below) It is proposed that this staircase, which is solely for the movement of bikes from the Ground Level to the Upper Basement Level, has two flights; one with 15 risers and one with 10 risers in line with BS8300:2018 section 10.1.2. The proposed rise per step is 155mm and the going 300mm.

The staircases will have cycle channels located on both sides of both flights to allow users to move their bicycles.

This arrangement will optimise the route for cyclists as it produces a shallow stair. In order to maximise the space for cyclists, the stair width is proposed to be 2200mm. A stair of this width would normally be subdivided by a central handrail (2000mm in Part K). In this instance it is proposed that the central handrail is not added, to provide the maximum width for cycle manoeuvring, and in recognition that this stair has a low traffic flow and is not for the use of the general public. For those that find this type of stairs difficult, there are alternative routes in the form of a lift, and/or the main office core stair and/or the main office core lifts.

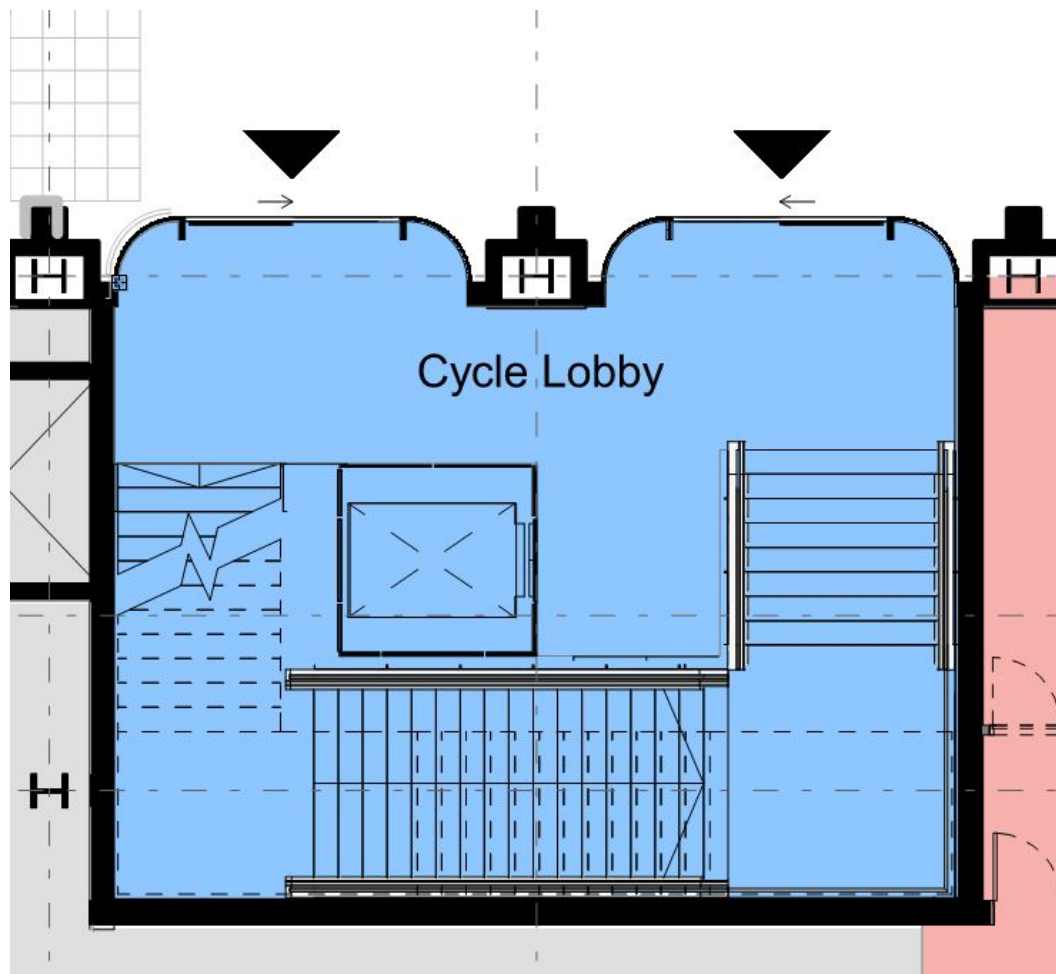


Figure 6 Cycle stair in the proposed Building P2

3.6 Doors

All main entrance doors comprise pairs of sliding doors in a draught lobby configuration.

All main accessible entrance doors will provide a minimum clear opening width of 1000mm and will be provided with visual manifestations where glazed, and/or vision panels where solid and on an access route.

3.7 Internal Floor Finishes

Floor finishes in the internal public areas will provide a slip resistance equal to or greater than R10 (to meet DIN51130:2004).

3.8 Sanitary Accommodation

An accessible WC has been provided behind the office reception area.

Accessible toilet accommodation has been provided throughout the core of the building on every floor. People with mobility disabilities are accommodated in the unisex cubicles.

On each floor an Accessible WC is alternately handed (handed = the side that the WC is located, left handed or right handed) and will comply with Part M of the Building Regulations and BS8300:2018 for an inward opening door arrangement as illustrated in Figure 7 below.

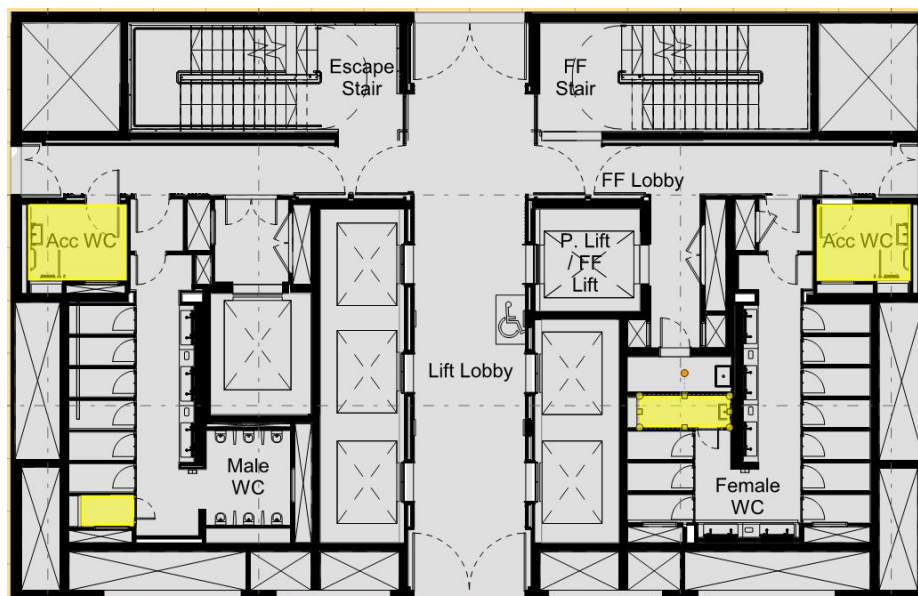


Figure 7 Core Accessible WC on upper levels

An accessible WC/Shower facility has been provided within the Cycle Parking and Facilities area on the Upper Basement Level and will meet the Part M requirement for a cubicle of 2400mm by 2500mm. This is highlighted in Figure 8 below.

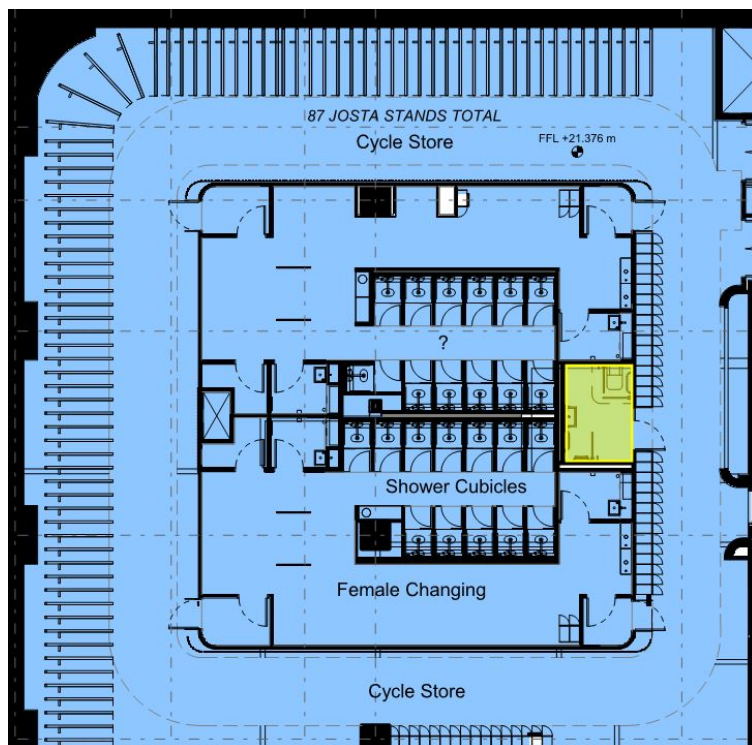


Figure 8 Accessible Cycle Change and Facilities shower/change Accessible WC

3.9 Escape Arrangements

3.9.1 General Arrangements

Areas of refuge to, BS9999:2008 Code of practice for fire safety in the design, management and use of buildings to accommodate disabled people have been provided at all levels within the core designs.

Management procedures will be put in place by the operator and the estate management to ensure that refuges are checked in the event of an emergency and/or for staff to respond to a disabled person in the refuge.

Staff will be suitably trained to assist disabled people and to assist with use of evacuation chairs where provided.

Operator and estate management policy, procedures and practices will be developed together with a means of escape strategy for disabled people, whether staff or visitors. Personal Emergency Egress Plans (PEEP) for individual disabled users will be developed as required.

3.10 General Details

Details of the following areas and how they will be made accessible shall be addressed as the scheme develops and form part of any Building Regulations Submission:

- decoration
- lighting
- service counters
- sanitaryware selection and layouts
- fire alarm details
- lift details
- toilet layout details
- signage
- furniture selection
- kitchen layout

In addition, Appendix A of this statement sets out the management issues which estate staff should be aware of to ensure access is achieved and maintained.

Appendix A

Management issues

The following management issues will be brought to the attention of relevant parties to ensure that access is achieved and maintained:

- **external routes** – keep in good repair and free of obstructions and leaves, ice, snow and surface water;
- **doors** – adjustment of door closers; ironmongery to be kept in good working order;
- **horizontal circulation** – keep routes free from obstructions and furniture layouts/seating arrangements accessible;
- **vertical circulation** – regular checking of lifts to ensure floor of car aligns with finished floor level;
- **WCs** – checks to ensure that manoeuvring space in accessible compartments is not obstructed by bins, sanitary disposal equipment etc; replenishment of toilet paper and paper towels in accessible WCs as well as other WCs;
- **communication** – new signs to integrate with existing sign system, no ad hoc homemade signs; all information to be kept up-to-date; signers and translation services to be provided as necessary; appropriate provision of accurate access information and other literature;
- **hearing enhancement systems** – advertising; regular checking and maintenance of systems;
- **alarm systems** – checking of systems; staff training in procedures;
- **surfaces** – ensuring cleaning does not cause slippery surfaces; maintaining junctions to avoid worn surfaces becoming tripping hazards; replacing surfaces like with like; maintaining colour contrast in redecoration;
- **lighting** – prompt replacement of bulbs; keeping windows and light fittings clean;
- **means of escape** – specific evacuation strategies to be devised for people who need assistance, including staff and visitors; staff training; regular practice drills; maintenance of fittings and equipment; reviewing evacuation procedures;
- **security** – ensuring security procedures do not conflict with accessibility good practice;
- **training** – staff training is critical to maintain access and to provide accessible services and employment opportunities. Training can cover areas such as disability awareness and equality, use of equipment such as platform lifts and induction loops, British Sign Language, hearing awareness, clear lip speaking, guiding people with visual impairments and general access awareness.

- **health and safety policies** – implementation of policies on access, risk assessment;
- **responsibilities for access** – identification of responsible people to approve improvements, set priorities, ensure access is included in maintenance and refurbishment programmes, provide auxiliary aids, review numbers of disabled people using a service and establish and run user groups;
- **funding for access improvements** – identification of specific access funds or grants; funds for specific employees such as 'Access to work'; use of the maintenance budget;
- **policy review** – regular reviews of all policies, practices and procedures affecting access.

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