

# Construction Management Plan

pro forma v2.2

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# Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
09/06/2017	Draft	Chris Mead MCIHT

## Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by
14/12/2017	Updated	CT/RT
07/02/2018	Updated	CT/RT
12/02/2018	Updated	CT/RT

# Introduction

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any **cumulative impacts of other nearby construction sites**, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) [6: Amenity](#) and [CPG](#) [8: Planning Obligations](#)).

This CMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Community Safety](#) (**CLOCS**) scheme) and [Camden's Minimum Requirements for Building Construction](#) (**CMRBC**).

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The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as for road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

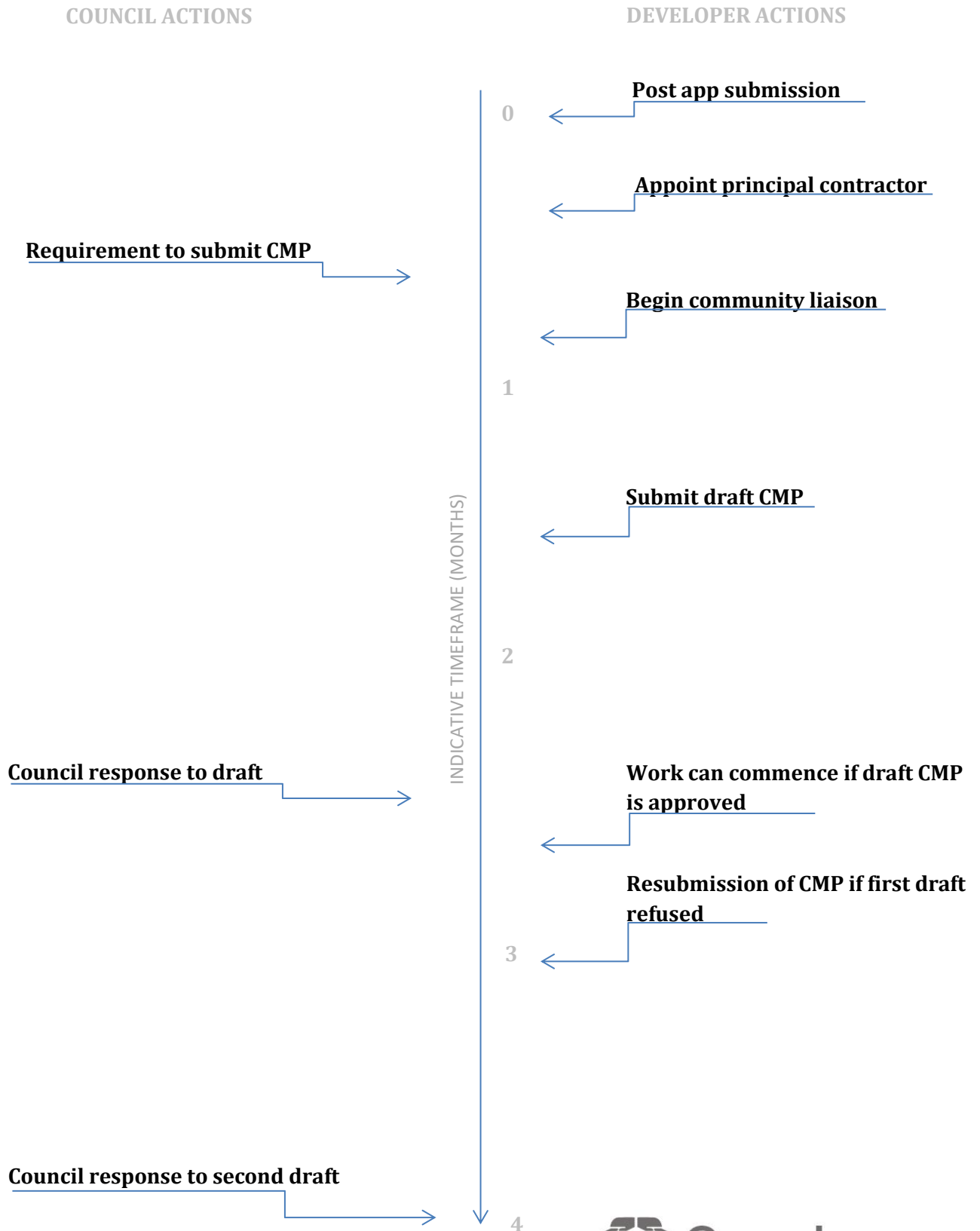
Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. **It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP.**

Please notify that council when you intend to start work on site. Please also notify the council when works are approximately **3 months from completion**.

(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction, etc.)

Revisions to this document may take place periodically.

# Timeframe



# Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: The Water House, Millfield Lane, London, N6 6HQ

Planning reference number to which the CMP applies: 2017/3692/P

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Chris Mead MCIHT

Address: Cannon Consulting Engineers

Email: [chris.mead@cannonce.co.uk](mailto:chris.mead@cannonce.co.uk)

Phone: 07469 853 387

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Spencer Keane

Address:

Email: [spencer@800group.net](mailto:spencer@800group.net)

Phone: 07966 020807

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of [Community Investment Programme \(CIP\)](#), please provide contact details of the Camden officer responsible.

Name: Stuart Minty – Pre planning

Address:

Email: info@smplanning

Phone: 07900 413080

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Vernon Moffit

Address: **800 Group** - Cranborne Road, Potters Bar, Herts. EN6 3JN

Email: Vernon@800group.net

Phone:



# Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

Please refer to the attached Land Registry Plan – NGL709805

The Water House is located with a primary access on Millfield Lane and a secondary access on Fitzroy Park. Millfield Lane is adjacent to Hampstead Heath.

7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

The proposed development will comprise:

Erection of a single storey side extension and a part single part two storey rear extension including facade alterations and associated works

The location of the dwelling will require the use of Millfield Lane for construction traffic. There is a precedent for the use of the lane for emergency vehicles in relation to the Ladies Pond as well as use of the lane by various vehicles associated to City of London activities. Examples of vehicles that use the lane on a regular basis include motorcycles, cars, small vans including the dog unit.

Aside from realising a technical solution that will ensure the integrity of the environment upon completion of construction and any necessary remedial works it is important to recognise the unique nature of the location.

Millfield Lane has been purposely constructed to the current finish of the track and advice from discussions is that the integrity of the track must be maintained through any construction phase.

Millfield Lane is uneven and there is a section of the track that carries overflow water from an adjacent pond. This area will need a specific intervention to ensure minimal impact to the existing surface.

The lane is half owned by City of London with the other side being in the ownership of adjacent land owners. Each of these land owners is being consulted as part of the development of the CMP.

The following excerpt is taken from a letter in relation to a previous application 2011/4390/P

‘There is no other swimming pond in Britain, that is a swimming place solely for women, and, importantly, one which is open to all women who wish to come. For this reason it is a place which has a very special meaning for its swimmers.’

Any construction management plan must seek to not only ensure that upon completion of the works that the lane is restored to its previous condition, or better but also respect during the construction phase the rights off all, but in particular those users of the exceptional Ladies pond.

8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

In terms of demolition and construction the works will result in a level of disruption. The adjacent dwellings and Ladies Pond will need due consideration in terms of reasonable times of day where deliveries and activity on site cannot occur.

9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.

A full topographical survey of Millfield Lane has been undertaken and is provided on drawings:

L 7940/1 - 5

10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal).

59.5 week programme

Assumed Start date Feb 2018

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

The working hours are to be agreed but initially would be

8.00am to 6pm Mon – Fri

However, there will be a need to agree additional restricted times to allow for the Ladies Pond and ensure that construction does not interfere with peak pedestrian movements on Millfield Lane. 800 Group are aware that there is potential for high number of visitors to the Ladies Pond at times which results in heavy foot traffic on Millfield Lane. 800 Group will consult with the Ladies Pond as necessary to ensure that vehicle movements do not interfere during those peak periods.

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

No impact to public utilities or statutory undertaker plant.

# Community Liaison

A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft. This consultation must relate to construction impacts, and should take place following the granting of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. **The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off.** This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

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## Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements and/or generate significant sustained noise levels should consider establishing contact with other sites in the vicinity in order to manage these impacts.

**The Council can advise on this if necessary.**

## 13. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents **prior to submission of the first draft CMP**.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation should be included. Details of meetings including minutes, lists of attendees etc. should be appended.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

A number of pre-application discussions have taken place with interested parties including local residents, residents associations, City of London and representatives of the Ladies Pond. A table with all consultation events is appended as well as initial comments from all parties.

Refer to supplementary CMP document

#### **14. Construction Working Group**

Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works, the way in which the contact details of the person responsible for community liaison will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

A construction working group will be in place and further details will be provided following appointment of a contractor. Overall the intention is to display information boards as well as provision of times and dates at both ends of the construction route on Millfield Lane to inform users of the lane of any additional information / variations.

The frequency of updates will need to be agreed and a list of key contacts will be held in relation to newsletter updates.

## 15. Schemes

Please provide details of your 'Considerate Constructors Scheme' registration, and details of any other similar relevant schemes as appropriate. Contractors will also be required to follow the "[Guide for Contractors Working in Camden](#)" also referred to as "[Camden's Considerate Contractors Manual](#)".

To be provided by appointed contractor

## 16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

It is not anticipated that there will be any other construction activity on Millfield Lane during these works.

# Transport

**This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.**

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the [CLOCS Standard](#).

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by the council to ensure compliance. Please refer to the CLOCS Standard when completing this section. Guidance material which details CLOCS requirements can be accessed [here](#), details of the monitoring process are available [here](#).

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.

**Please refer to the CLOCS Overview and Monitoring Overview documents referenced above which give a breakdown of requirements.**



## CLOCS Contractual Considerations

17. Name of Principal contractor:

800 Group

18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our [CLOCS Overview document](#) and [Q18 example response](#)).

CLOCS does not apply to 3.5T GVW vehicles

19. Please confirm that you as the client/developer and your principal contractor have read and understood the [CLOCS Standard](#) and included it in your contracts. Please sign-up to join the [CLOCS Community](#) to receive up to date information on the standard by expressing an interest online.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers:

Initial contact made by Chris Mead to CLOCS on 26 June 2017

Client notified of requirement to include CLOCS in contracts, subsequently removed due to proposed vehicle fleet

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.

## Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

**20. Traffic routing:** *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.” (P19, 3.4.5)*

Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of how vehicles will be routed to the [Transport for London Road Network](#) (TLRN) on approach and departure from the site.

Initial plan appended and based on construction traffic arriving from M1 / North Circular  
TLRN Network maps provided  
Will impact North West LoHAC & Central LoHAC

b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

Will form part of documentation for any procurements  
800 Group will manage all deliveries

**21. Control of site traffic, particularly at peak hours:** *“Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)*

Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the [Guide for Contractors Working in Camden](#)).

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.

3.5 T GVW vehicles for entire construction period. No more than 7 deliveries per day – Mon – Fri. Deliveries will be made during normal working hours, Monday to Friday 8am to 5pm and Saturday 8am to 1pm. During the winter months 800 Group will try and avoid deliveries after 4pm Monday to Friday.

b. Please provide details of other developments in the local area or on the route.

No other known developments

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

Vehicles will circulate on Millfield Lane in the event that vehicles arrive prior to banks persons in attendance. All vehicles are fitted with hands free communication.

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

Merton Lane or Millfield Lane. Only in suspended parking bays.

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

Entire refurbishment to be undertaken with 3.5T GVW

No Heavy goods vehicles.

A method statement has been prepared that sets out a monitoring and repair strategy for Millfield Lane. The letter containing the method statement is attached to this pro-forma

**22. Site access and egress:** *"Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles."* (P18, 3.4.3)

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic marshals must ensure the safe passage of all traffic on the public highway, in particular pedestrians and cyclists, when vehicles are entering and leaving site, particularly if reversing.

Traffic marshals, or site staff acting as traffic marshals, should hold the relevant qualifications required for directing large vehicles when reversing. Marshals should be equipped with 'STOP – WORKS' signs (not STOP/GO signs) if control of traffic on the public highway is required. Marshals should have radio contact with one another where necessary.

a. Please detail the proposed access and egress routes to and from the site

The site will be accessed from Millfield Lane for all vehicular traffic

Fitzroy Park will not be used for any vehicular traffic

b. Please describe how the access and egress arrangements for construction vehicles will be managed.

Due to the nature of Millfield Lane it will only be feasible to allow one vehicle movement to take place at any one time regardless of the direction of travel.

Vehicle speeds will be limited to 3mph – walking speed. Access and egress will be in forwards gear.

Banks people will be used at all times

c. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).

Swept path drawings are provided that demonstrate the area of frontage required for vehicles. All vehicle turning will take place on site.

Additional works will be required within the site to accommodate larger movements. Tracking for an ambulance and a pick up truck can occur with no further works required.

The contractor up on appointment will confirm the proposed vehicle fleet and whether onsite works are required to satisfy turning.

d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled.

Wheel washing will be provided on egress to ensure that no contaminate from site is passed onto Millfield Lane

**23. Vehicle loading and unloading:** *“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)*

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required.

All parking and unloading will take place on site as will all vehicle turning.

Arboriculture report identifies root protection area on site and appropriate mitigation.

# Highway interventions

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

If the site is on or adjacent to the TLRN, please provide details of preliminary discussions with Transport for London in the relevant sections below.

## 24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. **Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.**

Information regarding parking suspensions can be found [here](#).

No parking bay suspensions required

## 25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

- a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).

The construction phase will require the use of Millfield Lane using a robust movement strategy. As such it is key to ensure that vehicles do not dwell in the area adjacent to Fitzroy Park.

It may be necessary to undertake works to the traffic island at the junction of Merton Lane

b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

Further detail to be provided by appointed contractor

## 26. Diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

None

## 27. VRU and pedestrian diversions, scaffolding and hoarding

Pedestrians and/or cyclist safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc.



A secure hoarding will generally be required at the site boundary with a lockable access.

a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.

Access gates will open into site to avoid any conflict with users of Millfield Lane

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

None

 SYMBOL IS FOR INTERNAL USE

# Environment

To answer these sections please refer to the relevant sections of **Camden's Minimum Requirements for Building Construction ([CMRBC](#))**.

28. Please list all [noisy operations](#) and the construction method used, and provide details of the times that each of these are due to be carried out.

Screw Piling to be carried out by Mini Rig with a silencer pack - Duration of Screw Piling - duration 3 weeks -

Internal demolition of concrete elements to be carried out with percussive hand held tools -

29. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

A noise survey was undertaken in June 2007 and the results are provided in the acoustic report undertaken by ACA Acoustics and is submitted along with the planning application.

30. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

The noise level from Rig is 77DB at 10m

31. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

Any noise will sought to be reduced by the best practical means.

Given the proximity to the Ladies pond and the lane in general a further restriction on types of activity undertaken at certain times of day may be appropriate.

e.g. No cutting activity before 9am.

32. Please provide evidence that staff have been trained on BS 5228:2009

We do not envisage any excessively noisy operations but confirm that our Site Managers are all trained to SMTS Level and are therefore competent in the management of localised noise and dust control appropriate to this project

33. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

We would propose the use of local exhaust ventilation and water suppression techniques to suit the relevant dusty operations. The Site Manager will monitor dust on a regular basis to ensure that all equipment is being correctly utilised. Where Specialist Subcontractors are employed, RAMS will be provided which will where appropriate also demonstrate the proposed method for dust prevention

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Wheel washing for vehicles.

Other mitigation to be provided by appointed contractor

35. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

If required, a noise monitoring station can be installed for the duration of the noisy works (Demolition and Piling). In all instances, the Site Manager will monitor noise, vibration and dust levels throughout each day to ensure compliance and will have a procedure for stopping works which do not comply and/or require additional measures to be implemented

36. Please confirm that a Risk Assessment has been undertaken at planning application stage in line with the GLA policy. [The Control of Dust and Emissions During Demolition and Construction 2104 \(SPG\)](#), that the risk level that has been identified, and that the appropriate measures within the GLA mitigation measures checklist have been applied. Please attach the risk assessment and mitigation checklist as an appendix.

To be confirmed

37. Please confirm that all of the GLA's 'highly recommended' measures from the [SPG](#) document relative to the level of risk identified in question 36 have been addressed by completing the [GLA mitigation measures checklist](#).

To be confirmed

38. If the site is a 'High Risk Site', 4 real time dust monitors will be required. If the site is a 'Medium Risk Site', 2 real time dust monitors will be required. The risk assessment must take account of proximity to sensitive receptors (e.g. schools, care homes etc), as detailed in the [SPG](#). Please confirm the location, number and specification of the monitors in line with the SPG and confirm that these will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

To be confirmed

39. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

In an effort to prevent the attraction of Rodents, all canteen facilities will be within designated site cabins and all food will be kept in cupboards. Waste food will also be kept within the site cabins and doors will be kept shut. The Site Manager will police this procedure to ensure compliance by all operatives.

If Rodents are found, we will immediately inform a registered Pest Control Company to visit site and carry out remedial measures as required. All documentation will be kept on site for inspection

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey has been undertaken

Additional information to follow

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

No builders to dwell on Millfield Lane or adjacent to Ladies Pond during the construction phase.

Ideally all pedestrian access to the site from Fitzroy Park (agreement from Fitzroy Park Residents Association to be discussed further)

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

### **From 1<sup>st</sup> September 2015**

**(i) Major Development Sites** – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

**(ii) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

**From 1<sup>st</sup> September 2020**

**(iii) Any development site** - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

**(iv) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (mm/yy - mm/yy ): 10/17 - 09/18
- b) Is the development within the CAZ? (Y/N): No
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N): To be confirmed
- d) Please provide evidence to demonstrate that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: TBC
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection: TBC
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required: TBC

• SYMBOL IS FOR INTERNAL USE

# Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

**Please notify that council when you intend to start work on site. Please also notify the council when works are approximately 3 months from completion.**

**Signed:** Cecilia Thordardottir

**Date:** 12 Feb 2018

**Print Name:** Cecilia Thordardottir

**Position:** Senior Transport Planner – Cannon Consulting Engineers

Please submit to: [planningobligations@camden.gov.uk](mailto:planningobligations@camden.gov.uk)

End of form.

Date	Consultation Activity
28 April 2017	<b>One to one meetings</b> to discuss emerging plans and consultation strategy with: <ul style="list-style-type: none"> <li>• Bob Warnock (Superintendent of the Heath) – City of London</li> <li>• Jonathan Mears (Conservation and Trees Manager) – City of London</li> </ul>
4 May 2017	<b>One to one meeting</b> with Karen Beare, Chair of Fitzroy Park Residents Association (FPRA)
18 May 2017	<b>Open House Event</b> at The Water House (4-7pm) <ul style="list-style-type: none"> <li>• Invitation letters sent to all residents in local area (Millfield Lane, Fitzroy Park, Fitzroy Close and Millfield Place)</li> <li>• All local amenity groups, ward councillors and stakeholders invited</li> </ul>
19 May 2017	<b>One to one meeting</b> with Owner of Fitzroy Farm
28 May 2017	<b>One to one meeting</b> with Owner of Wallace House
30 May 2017	<b>One to one meeting</b> with Owner of 51 Fitzroy Farm
12 June 2017	<b>One to one meeting</b> with Owner of 1 Millfield Place
15 <sup>th</sup> June 2017	<b>Walking meeting down Millfield Lane</b> to discuss tree protection measures and construction methodology with: <ul style="list-style-type: none"> <li>• Bob Warnock (Superintendent of the Heath) – City of London</li> <li>• Jonathan Mears (Conservation and Trees Manager) – City of London</li> <li>• David Humphries (Trees Management Officer) – City of London</li> </ul>
17 June 2017	<b>One to one meeting</b> with Owner of 55 Fitzroy Park
17 June 2017	<b>Hampstead Heath Consultative Committee</b> visit to the Waterhouse <ul style="list-style-type: none"> <li>• Facilitated by the City of London, and attended by 12-15 representatives of local amenity groups who are interested in development bordering the Heath</li> </ul>
22 June 2017	<b>One to one meeting</b> with Owners of Apex Lodge to discuss tree protection measures and construction methodology.
22 June 2017	<b>Walking meeting down Millfield Lane</b> to discuss tree protection measures and construction methodology with: <ul style="list-style-type: none"> <li>• Karen Beare, Chair of Fitzroy Park Residents Association (FPRA)</li> <li>• Mary Powell and Nicky Mayhew, Ladies Pond</li> </ul>



## Chris Mead

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**From:** Stuart Minty <info@smplanning.com>  
**Sent:** 08 December 2017 18:26  
**To:** 'Zenab Haji-Ismail'  
**Cc:** 'Pat Gill'; Chris Mead; 'Adam Hollis'; 'Richard Solomon'; 'Matt Elms'; 'Vernon Moffitt'  
**Subject:** COL Engagement - Waterhouse  
**Attachments:** Agenda - Water House Stakeholder Meeting - Monday 4th December.pdf; Agenda and Minutes - Water House Stakeholder Meeting - Monday 25th September.pdf

Hi Zenab

Please see below engagement with COL since September. Next steps included at the bottom of email, which we may not want to include at this point as discussions are ongoing

Let me know if you need anything further. Final version to be shared with Chris for the CMP update submission

There have been emails etc with other stakeholders (KB, Colin and Christine etc) – but have focussed on COL and wider group interaction

### **25<sup>th</sup> September – Stakeholder Meeting. See attached agenda and minutes**

#### **Key Messages / Action Points**

- Various updates on CMP, as now revised with contractor on-board (800 Group)
- Meeting to be arranged involving Adam Hollis, City of London and suitable engineer to discuss products, and solutions in relation to protection of 3 veteran trees along the lane.
- Following this meeting, an engineer to be appointed to develop a solution, and a presentation to be made by project team to demonstrate how the trees would be protected.

### **17<sup>th</sup> October – Update email to all stakeholders**

#### **Key Messages**

- Notice that revised drawings submitted to Camden
- Notice that we wished to instruct an Independent Technical Consultant to fully understand the Impact on Millfield Lane / Veteran Trees (Andrew Dawson Identified)
- The draft brief for this appointed was attached for comment
- Advised that CMP and Arb report were to be revised to ensure consistency

### **30<sup>th</sup> October – Discussions with COL**

#### **Key Messages**

- Andrews Dawson report to be judged by an independent arboriculturist to ascertain impact on veteran trees. A single joint expert (SJE) was chosen as the best option by all parties
- Meeting arranged with COL, GEO Synthetics (COL preferred provider), and WH project team to discuss design solutions for the lane.

### **2<sup>nd</sup> November – Site Meeting with COL and Geo-Synthetics**

- Lane examined and discussed with view to a quote
- Geo-Synthetics would provide product, with 800 Group responsible for labour/installation.

### **Between 2<sup>nd</sup> November and 21<sup>st</sup> November – Email discussions on SJE**

- Discussion around the brief for the SJE agreed between team and COL
- Quote received from Geo-Synthetics

### **21<sup>st</sup> November – Meeting with COL**

**Agenda as below**

1. SJE Independent Arb Report – Brief and Consultant Appointment (Agreed at this meeting)
2. Quote from Geo-Synthetics and next steps/costs discussion. (Costs Agreed in Principle at this meeting)
3. Timetable
4. Communications with residents/stakeholders going forward

### **23<sup>rd</sup> November – Email to all Stakeholders with update on project**

#### **Key Points**

- Update on the Project timeline – Veteran and Mature Trees protection inline with what discussed with COL during November 17 (above)
- SJE draft scope shared for comment (as already agreed with COL)
- Invitation to next stakeholder meeting, and to view mock-up/sample of the Geo-Synthetic model

### **4<sup>th</sup> December – Stakeholder Meeting. See attached agenda. Minutes TBC**

- Action points include drainage plans / swept path drawings to be circulated
- Further work around comms to be undertaken in the form a leaflet / poster to be easily communicated to those outside of the stakeholder group
- SJE to be commissioned

#### **Next Steps**

- SJE Commissioned and report back on suitability of works to protect the Veteran Trees, including discussion surrounding whether this should be temporary or permanent solution.
- Decision on timing of the work, whether this should be temporary / permanent solution, and whether secured as part of planning permission (S106). TBC
- Preference for WH team likely to be temporary solution, with agreement outside the remit of planning. Perm works to follow on completion of project, however needs SJE to confirm best solution, and COL to confirm their acceptability
- Comms rolled out through leaflet/poster – distribution TBC but will be discussed with wider group as best method.
- Revised documents submitted to Camden and stakeholders for consideration
- SJE for structural engineer to look at Apex Lodge boundary structures. This has to be after the SJE results for trees.

Many thanks

Stuart

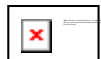
**Stuart Minty**

M: 07900 413080  
DDI: 0207 692 0643  
E: [stuart@smplanning.com](mailto:stuart@smplanning.com)



80-83 Long Lane  
London, EC1A 9ET

[www.smplanning.com](http://www.smplanning.com)

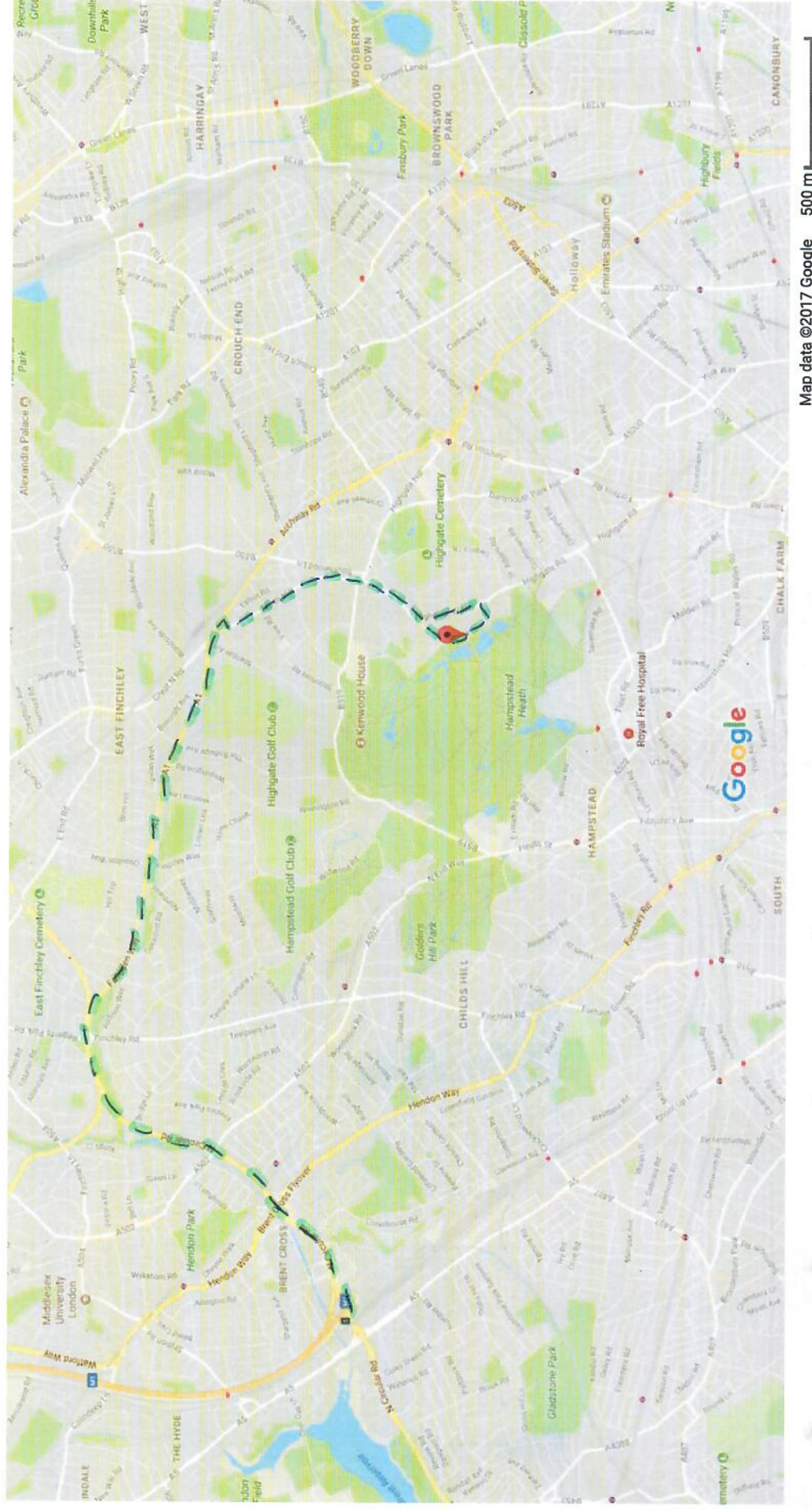


Virus-free. [www.avast.com](http://www.avast.com)

Google Maps

Millfield Ln

Construction Routing





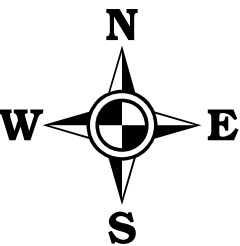
Transport for London - Central Area Red Routes (LoHAC)



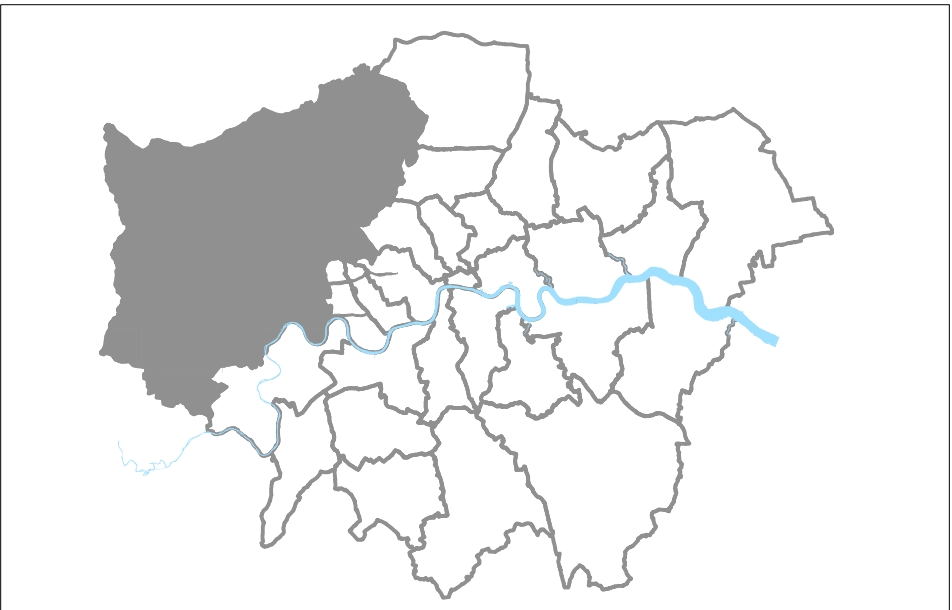




- KEY:**
- Red Route
  - A Roads
  - Motorway
  - Borough Boundaries
  - North-East Area
  - North-West Area
  - Central Area
  - South Area



To report a street fault call: 0343 222 1234



Project:	<i>The Waterhouse</i>
CCE Ref:	
Title:	Construction Management Plan
Prepared by:	CCE
Approved by:	CM
Date:	June 2017

The Construction Management Plan (CMP) is a live document that will establish the construction strategy for the site as well as acting as an enforcement tool for Camden Council. As such the document will need to result in a comprehensive and considered strategy for construction that responds to the local issues.

Whilst only a single dwelling the CMP will receive a high level of scrutiny at all stages including planning and construction.

The first stage tender must respond to the issues raised in this note which form part of the draft CMP. As the design and approach towards construction develops it will be necessary post planning for the appointed contractor to take responsibility for the matters raised and to continue to respond proactively.

This information has been compiled prior to any collaboration with the lead constructor to allow any response to stage 1 tender to be informed on these matters. At this time the CMP is still in development and will be further informed on appointment.

A CMP was produced as part of an application made in 2011 and this is appended. The CMP has been independently reviewed following the council refusing the application on the following grounds:

*It is considered that, on the basis of submitted CBR data taken from Millfield Lane, the proposed intensive use of the lane for construction traffic would cause physical damage to the Lane's surface and would harm the long-term survival of the adjoining trees along the lane. It is also considered that, in the absence of further information to demonstrate otherwise, necessary mitigation measures to facilitate construction access, such as ground guards and vegetation pruning, would be likely to be harmful to the rural landscaped character and appearance of Millfield Lane and the wider conservation area.*

*It is considered that the proposed access by construction vehicles at the site's entrance, by reason of the associated vehicular movements within Millfield Lane, is likely to cause harm to users of the Lane and Ladies Pond and may cause harm to adjoining trees and thus the landscaped character and appearance of the lane and conservation area.*

As the documentation has been deemed lacking by the Council there must be further consideration of how to respond with the construction methodology and programme.

Further to the attachments the following should be given consideration as part of any tender and commitment to assuring the CMP is delivered to the highest standard throughout the construction phase to avoid any enforcement issues.

## Key considerations

### **The Ladies Pond**

‘There is no other swimming pond in Britain, that is a swimming place solely for women, and, importantly, one which is open to all women who wish to come. For this reason it is a place which has a very special meaning for its swimmers.’

Any construction management plan must seek to not only ensure that upon completion of the works that the lane is restored to its previous condition, or better but also respect during the construction phase the rights off all, but in particular those users of the exceptional Ladies pond.

As such the tender will require that access to the Ladies Pond and Millfield Lane will be unfettered at all times and that the presence of construction traffic and site operatives is considerate to the location.

### Opportunities

Whilst only a small site, there may be training opportunities for those associated with National Association of Women in Construction (NAWIC).

As the site is connected to Fitzroy Park this could be offered as a pedestrian route should any activities deem it necessary to provide an alternative route. This could also provide observation windows.

### **Analysis of Millfield Lane**

Millfield Lane is best described as a ‘track’ and is maintained by City of London. Any tender should be supported by examples of maintaining and making good a route of this nature during the construction phase and an end of construction reparation.

A site visit is essential and team members will be made available to discuss the need for frequent reporting to City of London and Camden on condition surveys throughout the construction phase.

It is considered that whilst solutions are achievable to protect the lane itself and the underlying tree roots and drainage, that any interventions would need to be removable. There is an expectation that the character of the track remain unaltered, such that at times where no construction activity is taking place that the lane has no observable.

### Opportunities

Demonstration of maintaining similar surfaces during construction phase

### **Vehicle Classes**

The Ladies Pond has a need for emergency vehicle access and should this be required then the construction would need to ensure that this need can be met, if required.

Refer to the vehicle classes provided in the previous CMP and the vehicle specifications appended.

All turning will need to be accommodated on site and vehicles will not be allowed to proceed any further north than the boundary extension from The Water House to Millfield Lane.

Vehicle sizes are determined by the dimension of vehicles that can safely negotiate Millfield Lane and the potential impact of heavy vehicles on the lane. Both are a factor if considering the use of a vehicle not identified. Justification will be required.

Opportunities

Ways of working that maintain the environment of the lane out of construction hours / activity.

### **Entering and Exiting the Site**

Turning will need to be accommodated on-site and as can be seen from the topographical survey the entrance to the site rises at 1 in 3 at the steepest point.

As such any vehicles entering and exiting may be at risk of grounding and significantly harming Millfield Lane

Opportunities

Amendments to the access to ensure that there is no risk of grounding / damage to the lane as a result of this movement.

Consideration of a vehicle fleet where such grounding would not be a risk.

### **Other Matters**

Whilst there are a number of constraints and the previous CMP was substantially developed it may be that there are alternative solutions to deliver these proposals. Along with a fee supporting the outline approach supported by this document any reasonable alternatives will be considered if they can be demonstrated as feasible in terms of time and cost to deliver.

In the event of these works taking place, the site will continue to be under intense scrutiny throughout the construction phase and any actions not in accordance with the final agreed CMP will likely be met with enforcement action.

Therefore any agreed construction approach must be realistic and deliverable and in full accordance with the strategy as agreed for planning.



Charles Thuaire  
Senior Planning Officer  
Development Management  
London Borough of Camden  
N1C 4AG

Email to: [charles.thuaire@camden.gov.uk](mailto:charles.thuaire@camden.gov.uk)

Date: 05 February 2018

Dear Mr Thuaire,

**RE: The Water House - Millfield Lane Mitigation**

This letter provides a brief summary of the findings by technical consultants and outlines how construction impact will be managed to mitigate against harm to the character and the root protection areas of veteran trees along Millfield Lane.

Revised Construction Management Plan (CMP) and Arboricultural Report were submitted to the Council on 19 December 2017 in response to concerns raised by neighbouring residents. The revised pack included a report produced by an international rural road expert, Andrew Dawson of Nottingham University Consultants (NUC) who was independently appointed to comment on the construction and suitability of the lane, the report is appended to the CMP produced by Cannon Consulting Engineers.

Andrew Dawson concluded:

- It is very unlikely that deep-seated (subgrade) rutting will occur under the planned traffic;
- In specific reference to the veteran trees, at the start / top of the lane, he anticipates a negligible soil deflection at c. 200mm depth of no more than 0.5mm, which as he suggests (and our tree expert agrees) is unlikely to have any perceptible effect on the veteran trees themselves or their rooting environment; and
- These negligible effects at root depth (of c. 200mm and below) are to some extent predicated on maintaining the integrity of the upper paving surface as a wearing course.

Andrew Dawson's report recommends monitoring and reactive repair. The report found that given the scale of the proposal, the size of vehicle (3.5T GVW) being used and other mitigation measures such as the movement of vehicles at 3mph, there is no need for further intervention required to maintain the integrity of the lane or root protection areas. Other than 'spot' reparation there is no evidence to suggest that Millfield Lane will suffer from the use of the lane for refurbishment of The Waterhouse using 3.5T GVW vehicles.

He notes the existing surface *'will probably be satisfactory or, at least, won't degrade rapidly (giving time for appropriate intervention as required during use).'* Comparable vehicles (albeit at lower densities) currently use the lane at speeds in excess of the proposed construction traffic, in all weather conditions. The effects of these ad hoc 'tests' by taxis, delivery and CoL vehicles are evident in the current condition of the carriageway (recorded by Dawson as satisfactory).

The Applicant is assured that elaborate tree protection measures with extensive use of resources (plastic web, geotextile, timber stone and gravel) are unnecessary for the purposes of the proposed development at the Water House. However, the Applicant is prepared to support the City of London in their long term proposal for the lane in the spirit of cooperation and neighbourliness. This support is relevant to the future upgrade of the lane, rather than the immediate use for construction.

### **Method Statement**

This method statement clarifies our approach to monitoring and maintenance of the lane. The project team will take photographs of the lane and record its condition prior to commencement of the works. This record will be circulated to the Stakeholders as evidence of its pre existing condition.

Banks persons will be given the relevant training by the Project Team and made aware of the key issues and thereafter will be responsible for visually checking the condition of the lane each day. Any issues will be recorded and reported to the Site Manager. The Site Manager will make his/her own thorough visual inspection of the lane on a Fortnightly basis and keep relevant records, which may involve photographs if applicable. Any significant issues would be reported to the City of London Corporation accordingly.

The wearing surface of the lane is currently an MOT Type 1 sub-base material, the project team will be keeping a stock of this material so they are able to carry out any localised repairs as required, which would be recorded and photographed.

Based on evidence produced by experts, this approach should suffice. However, should there be any obvious and significant signs of damage to the lane as a result of this construction, the project team would consider implementing a long term solution such as cell web before proceeding any further with construction.

We hope this along with the supporting documents submitted on 19 December 2017 clarifies our approach to construction management. Do get in touch should you have any queries relating to this letter.

Yours sincerely,

Zenab Haji-Ismael