

Construction
Vehicle Access

knight
harwood



Athlone House, Hampstead Heath

Introduction

This paper outlines the proposed routes for construction traffic in relation to the works at Athlone House. It considers the proposed access routes and general access requirements for all vehicles including, large HGVs such as articulated lorries, eight-wheel tipper trucks and concrete delivery wagons.

This strategy includes details of how we expect to manage the deliveries/traffic movements, taking into account the local residents and traffic, schools and the general public. It provides a balance of incorporating restricted delivery/vehicle access/egress times whilst minimising the overall duration of the project which consequently minimises any disruption that the works may cause. We are aware of the blanket condition whereby the London Borough of Camden requires construction vehicle movements to be considered and restricted during peak hours, relating to drop off and pick up times associated with local schools, however we would seek opportunities to adapt these hours as we feel this would provide a number of benefits and would certainly avoid the programme having to be extended by a number of months, unnecessarily so.

Athlone House is situated within the London Borough of Camden. However, access into its grounds is via the B519 Hampstead Lane. At this point the perimeter boundary wall to Athlone House denotes the boundary between the London Boroughs of Camden and Haringey with the B519 being the responsibility of the London Borough of Haringey.

In developing the proposed construction traffic routes, the current local traffic restrictions have been considered as well as the proximity of Highgate School which has its entrances located along Bishopswood Road, to the north of Hampstead Lane and is situated within the London Borough of Haringey.

The proposed construction traffic routes have also been reviewed in relation to the various London Boroughs that they pass through.

Overview

Athlone House is an existing building situated within its own extensive grounds. The building is currently in a derelict condition having been unoccupied for over 10 years. Planning consent from the London Borough of Camden has been granted for the scheme and includes for extensive refurbishment and extension works to restore the property back to its original use as a single occupancy residential property. The scope of works incorporates:

- The internal and external restoration of the existing building along with structural modifications.
- A new extension incorporating a basement and containing a swimming pool.
- A new build garden Pavilion structure.
- Extensive landscaping to the existing gardens retaining and restoring key features and historic landscape structures. This includes the redevelopment of the existing pond area.

It is anticipated that the construction works will last for a period of approximately 24 months, commencing in late 2017.

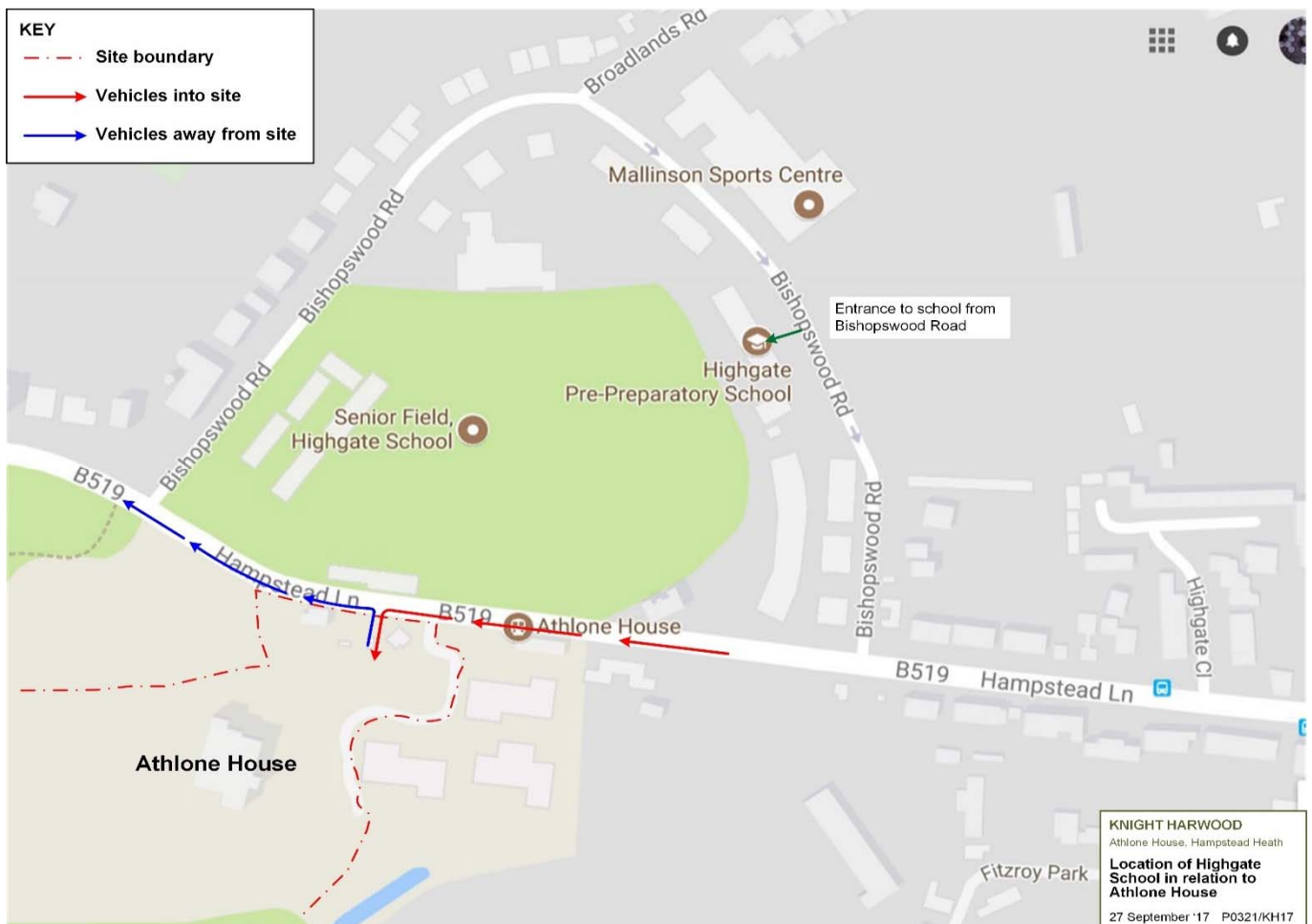
CONSTRUCTION VEHICLE ACCESS

The construction workforce is expected to reach a peak of 150 operatives on site with up to 30 vehicle movements per day. Operatives will be encouraged to access the site via public transport rather than driving, although a small amount of car parking may be permitted for loading and unloading purposes, to allow transportation of materials and equipment to and from the building. Car parking will be restricted and managed by Knight Harwood.

Approach to Managing Safe Vehicle Access to and From Site

Vehicle routes have been considered in order to provide safe access/egress routes and to minimise traffic congestion, disruption and interface with the local residents, schools and the general public. The close vicinity of Highgate School which has a number of buildings and playing fields located close to the site, has been a primary consideration when detailing the logistics strategy for this project.

Highgate School and its playing fields are situated immediately opposite the main entrance to Athlone House on the north side of Hampstead Lane. This school is located in the London Borough of Haringey and its entrance is situated on Bishopswood Road. There is no access to the school for drop offs and pick ups directly from Hampstead Lane. The playing fields are segregated from the road via metal railings. The proximity of the school and playing fields to Athlone House are shown below.



Location of Highgate School in relation to Athlone House

CONSTRUCTION VEHICLE ACCESS

Proposed hours of vehicle access to and from site

To avoid congestion and disruption to Highgate School and the local area in general, we would like to propose the following hours for construction traffic movements as noted below:

8:00am to 8:30am	Construction vehicles allowed to enter site only. Once within the site demise vehicles will be held within the private grounds of Athlone House for unloading/loading. This is completely remote from the public highway. No vehicles will be permitted to leave site until after 9:30am.
8:30am to 9:30am	No construction traffic permitted to enter or leave site.
9:30am – 3:00pm	Normal construction traffic movements permitted to and from site.
3:00pm – 4:00pm	No construction traffic permitted to enter or leave site.
4:00pm – 6:00pm	Normal construction traffic movements permitted to and from site, although it should be noted that it is anticipated that the majority of vehicle movements will be leaving site only.

Vehicles Entering and Leaving Site

The proposed logistics strategy allows for all site vehicles to be able to enter the site for loading/unloading via a newly formed entrance/crossover along Hamstead Lane.

Access/egress will be via left turn only, to and from Hampstead Lane. All vehicles will be controlled via a gate man/traffic marshal. The traffic marshal will control the gate and ensure that suitable barriers are in position when vehicles enter and leave the site.

Once within the site boundary, vehicles will be able to drive around within the site demise along dedicated vehicle access routes to enable exit onto Hampstead lane in a forward direction only. This mitigates the need for vehicles to reverse directly out of the site demise and onto Hamstead Lane.

All loading and unloading associated with the site, will take place within the site demise and vehicles will not be permitted to wait on any public highways within the vicinity to the site.

The proposed logistics drawings for the project are included below.

The first logistics plan (drawing KH15) shows the area of the site where construction vehicles are permitted to drive within the site boundary. The second plan (drawing KH16) shows the overall boundary of the site, which highlights that the construction works will be entirely located within the private grounds.

Primary access routes to and from the North Circular road have been detailed on the KH14 drawing as below. It is anticipated that the majority of site vehicles will utilise these and all subcontractors will be requested to adhere to these routes as far as is reasonably practicable. These routes also take into account local restrictions on HGV access.

Dedicated Logistics Manager and Booking In Regime

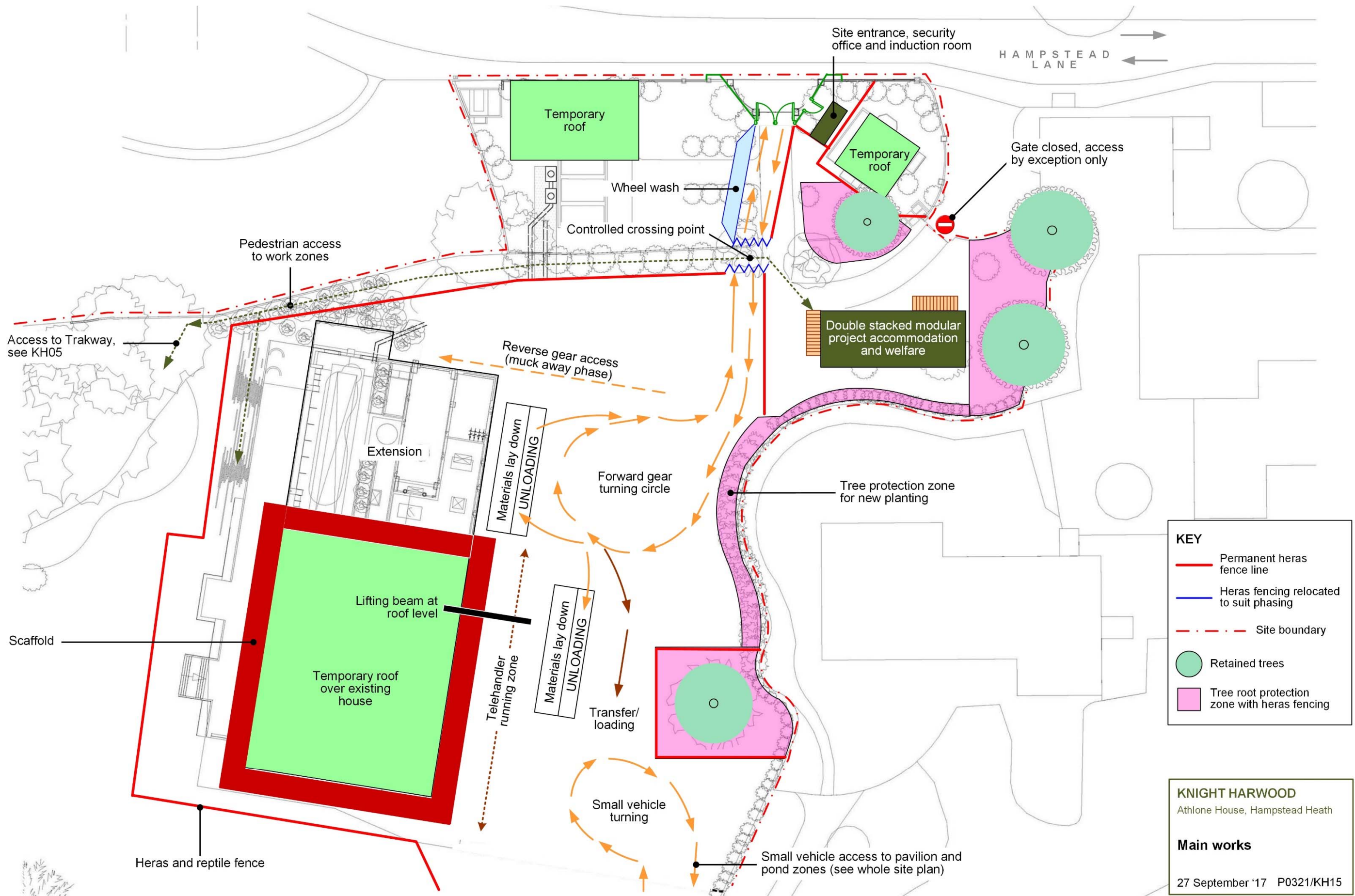
Deliveries and traffic management will be managed by a dedicated Logistics Manager, who will operate a carefully coordinated delivery schedule. Deliveries will be arranged to meet the above timings. A very strict policy will be maintained in regard to late or early delivery arrivals and any vehicles turning up outside of these hours which will be sent away to discourage complacency towards the delivery restrictions.

Regular reviews of the efficiency and effectiveness of the traffic management for the project will take place. A delivery schedule will be presented and discussed at a site weekly progress meeting to ensure that the proposed delivery schedule meets the programme requirements of the scheme and is within the allowed delivery time slots available. Suppliers and subcontractors will be encouraged to optimise transport efficiency by avoiding part load deliveries and adhere strictly to the allocated delivery times. All deliveries will be sequenced to ensure the site is able to accommodate each assigned delivery. No vehicles will be allowed to make a delivery or collection outside of the agreed times, thereby eliminating unnecessary traffic congestion and nuisance especially for the local traffic and residents.

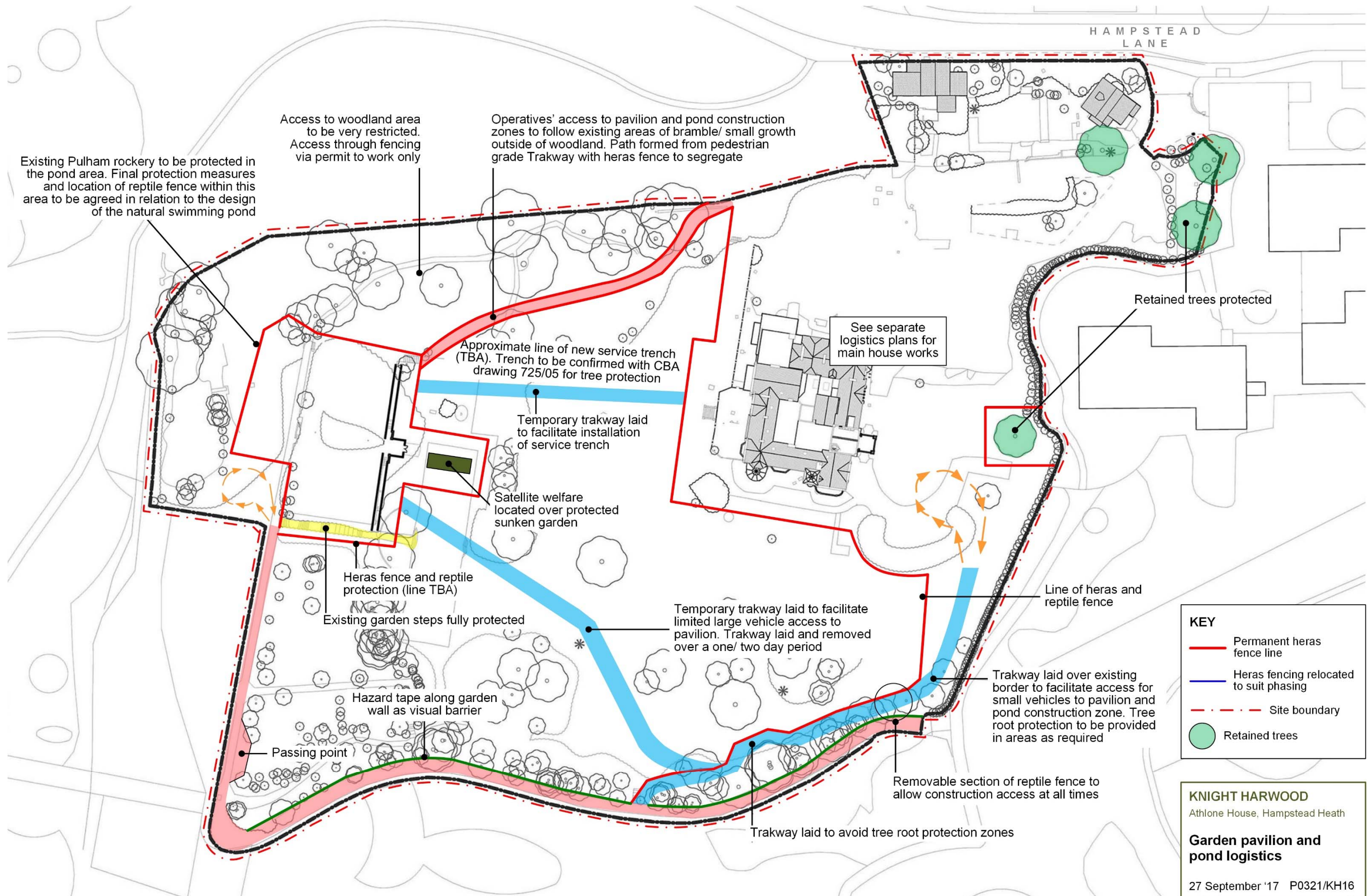
Maintaining Clean Public Highways

A wheel cleaning facility will be provided at the site exit gate. This will drain into a settlement tank to ensure no silting up of the local drains results from the washing down. It will be the responsibility of an operative stationed at the gate, to ensure the wheels of all vehicles leaving site are clean, prior to their exit. In addition, a small road sweeper unit will be utilised at certain times during the project, such as during the groundworks phase to ensure the surrounding roads are kept clean.

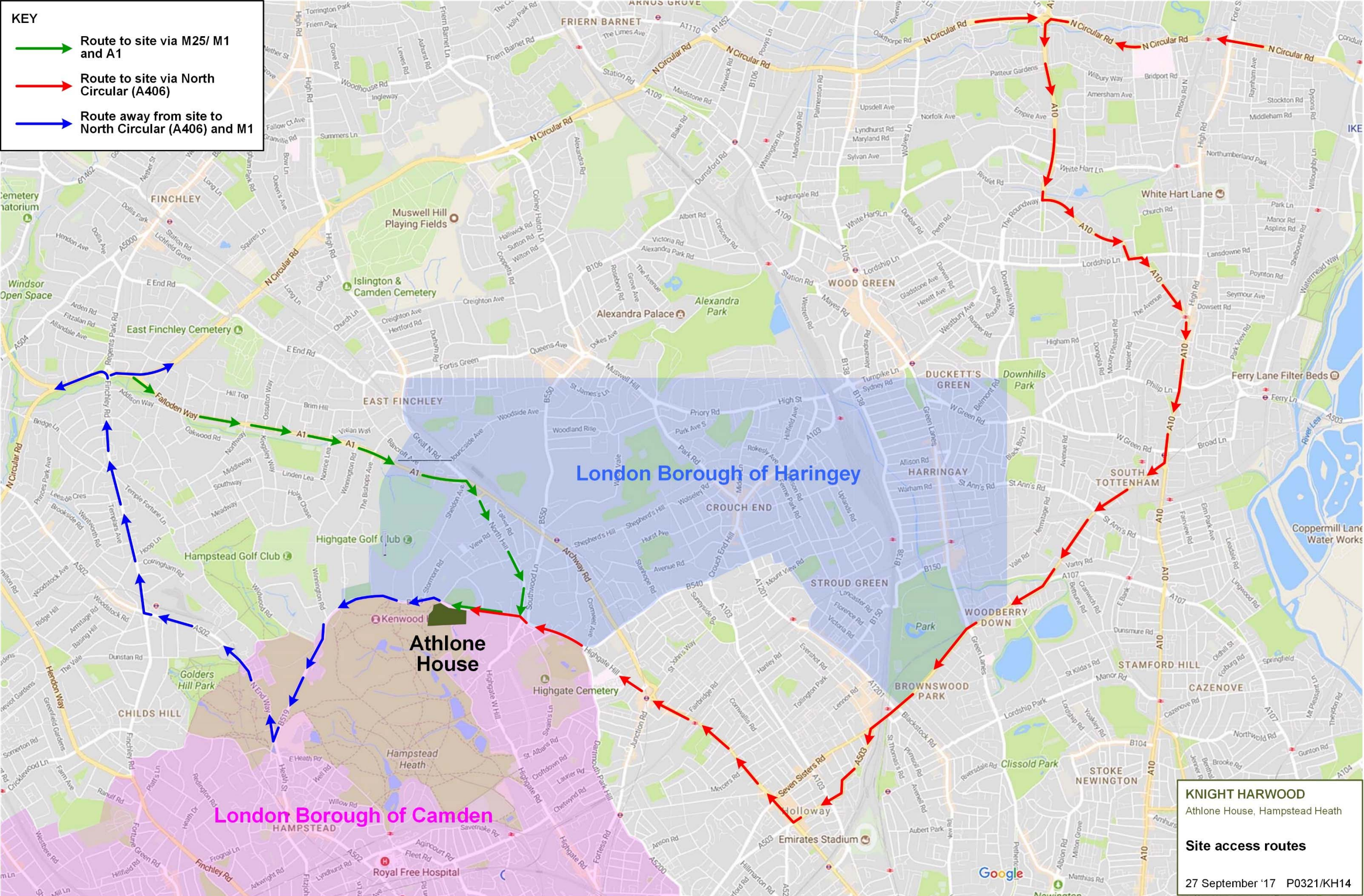
Logistics plan showing area of private grounds off the public highway for construction vehicle movements



Site wide logistics plan showing boundary of site



Traffic management routes to and from site in relation to Local Authority boundary



Benefits of Knight Harwood's proposed access regime

In the context of the proposed works at Athlone House, which take place entirely within the private grounds, we consider that these arrangements will be mutually beneficial for both the local community and also the requirements of the project. Some of the primary benefits include:

- A safe system of works is maintained in respect to traffic management. All vehicles entering and leaving the site will be under the control of a fully trained traffic marshal, who will safely guide each vehicle into and out of the site.
- Use of local residential roads are avoided.
- Impact on traffic movements to and from Highgate School at busy periods such as drop off and pick up times is minimised. As the entrance to Highgate School is not located along Hampstead Lane and no site vehicles will be permitted to park or wait along Hampstead Lane, the likelihood of congestion or impact on the school drop offs/pick ups is minimised.
- Vehicles pulling up or waiting along roads in the vicinity to the site is avoided. As the site demise has a substantial area for vehicle movements within the private grounds of the site demise, there will be sufficient space for vehicles to enter site, park with their engines switched off and be loaded/unloaded safely. This will eliminate the need for any construction vehicles to pull up/wait in the local area and hence mitigate congestion.
- Vehicle movements are planned, controlled and coordinated at all times. Vehicle movements will be carefully monitored through a strict pre-booking schedule. All movements will need to be pre-booked in a set time slot with 48 hours' notice provided. This will be controlled and monitored by Knight Harwood's logistics manager.
- Knight Harwood's logistics manager will strictly control the vehicle entrance gate to ensure that it remains closed and locked at all times between 8:30am to 9:30am and again between 3:00pm to 4:00pm, to coincide with busy periods/school drop off and pick up times.
- Vehicles will be routed so that they make a left turn into the site entrance and also a left turn out when exiting. This will avoid the need for vehicles to cross the carriageway at the site entrance which will be safer and reduce the likelihood of congestion. The Knight Harwood traffic marshal will supervise this process to manage pedestrians, cyclists and general traffic. The use of barriers and signage will also make this process safer.
- The public highways will be kept clean and tidy, free from construction dirt/debris.
- Lengthy restricted site access/egress hours for delivery vehicles are avoided which will in turn avoid unnecessary prolongation of the overall project duration.