

Index

- 1 Vehicle sizes and routing of construction vehicles to and from site and holding areas for these on and off site
- 2 Vehicle parking of site operatives and visitors
- 3 Loading and unloading of plant and materials
- 4 Storage of plant and materials used in the construction of the development
- 5 Erection and maintenance of security hoardings
- 6 Wheel washing facilities
- 7 Measures to control the emission of dirt and dust during construction
- 8 Disposal and recycling of site waste as a result of demolitions and construction
- 9 CCS membership and site conditions

Appendix

- A Site location map
- B Draft Construction Management Plan

1. Vehicle parking of site operatives and visitors vehicle sizes and routing of construction vehicles to and from site and holding areas for these on and off site

The total works programme is expected to continue for a duration of 18 weeks with the heaviest traffic during the scaffold erection /dismantling, minor demolitions and principal material delivery periods

Traffic routing information;

Generally it is anticipated that vehicular access will be off Euston Road into Great Portland Street then left into Goodge Street and right into Whitfield Street to the site.

Departure is anticipated to be from Whitfield Street right into Windmill Street, right into Charlotte Street, right into Goodge Street then along Tottenham Court Road into Euston Road.

This is on the basis that traffic will generally be arriving either from the west or east along Euston Road.

Access is somewhat guided by the comprehensive one way system that is in place in the vicinity of the site.

Although access information will be issued to all drivers, as there are more than the above described routes, drivers will select those routes that are most appropriate in relation to their direction of travel and the size of their vehicles.

The aim will be to select the routes that are least disruptive to local amenity, avoid sensitive areas such as schools and provide ample road widths for manoeuvring of vehicles.

Travelling to site;

All drivers will call in ahead of their delivery slot to confirm the exact time they are expected in order for the site to make ready to receive them.

Travelling from site;

The vehicles will drive away from the site once it has been assessed that it is clear to leave.

2. Vehicle parking of site operatives and visitors

Although the site had not been arranged for on site vehicles, it is possible to access the site with small vehicles up to the height and width of a typical Ford Transit van.

Arrangements will therefore be made with the Local Authority to purchase a length of the single yellow line zone alongside the site for the location of a skip and provision for parking a commercial vehicle such as a lorry delivering scaffolding. A similar arrangement was made for a three year period during the construction of the development some six years ago.

Site operatives will be expected to travel to work either by bicycle or via public transport links which are excellent for the area.

A small lightweight bike storage shed will be provided on site so that those who choose to cycle will have reasonable and safe storage facilities for their equipment.

3. Loading and unloading of plant and materials

The existing site entrance is not large enough for construction vehicles therefore all deliveries to site will be by prior arrangement with the vehicle parking at a pre-arranged nearby location. A marshal will be present to ensure any oncoming traffic is properly managed in the interests of road safety.

Once alongside the site all materials will be offloaded from vehicles immediately and in a timely and ordered fashion into the designated areas of storage on site and within the boundaries to be agreed.

4. Storage of plant and materials used in the construction of the development

All plant and materials will be stored on site, either at ground floor in designated protected areas or at roof level accessed via the scaffolding system; also in designated protected areas.

It is important to note that materials will not be stored on site for prolonged periods and deliveries will be placed in a timely manner as and when materials are required, thus allowing the materials to be used in the construction process as quickly as possible: this will be done in order to minimise the amount of material ever stored on site for purposes of security as well as reducing the potential impact of weight of the materials both at ground and roof levels.

The following categories of plant and materials will be stored.

- A. Hard non leaching materials such as blocks, bricks and aggregates if required will be stored on pallets open to air within protected, designated areas.
- B. Weather sensitive non leaching materials such as timber will be stored on pallets open to air within designated areas and will be protected with temporary covering.
- C. Leaching materials such as paint and mortar will be stored in fully protected conditions and any hazardous or flammable materials within lockable metal cases.
- D. All plant material and tools will be stored within designated lockable metal strongboxes in fully protected areas.
- E. Site accommodation and welfare will be within 'portacabins' either at ground or roof level. Toilet facilities will be in a 'portaloo' adjacent to the 'portacabin' with provision of running water next to it for hygiene.

5. Erection and maintenance of security hoardings

As all construction activity will be totally on site, well away from any site boundaries, standard security hoardings will not be required.

There will, however, be some provision for security hoarding within the confines of the site to ensure the safety of the residents who have access to their properties along Crabtree Place.

Note: See Appendix B; Draft Construction Management Plan.

6. Wheel wash facilities

No vehicles are planned to enter the site or construction zone. As such there will be no way that the delivery of materials and collection of waste from the site could cause vehicles to sustain any muck on their wheels any more than regular driving on roads.

7. Measures to control the emission of dirt and dust during construction.

All demolition works will be water jetted in order to minimise the spread of dust to the local area and the water jetting will be undertaken by a specialist contractor.

No vehicles will enter the site but all vehicles and the site frontage will be checked prior to the delivery or waste vehicle leaving to ensure that no materials have dropped under the wheels where they could be carried and to maintain the clean appearance of the site and immediately adjacent areas. During the somewhat minor demolition works, in order to prevent the spread of demolition dust and debris over the local highways, extra time will be put to this maintenance task.

8. Disposal and recycling of site waste as a result of demolitions and construction

The appointed contractor will be expected to have a company policy in place to recycle 80% of the site waste and a process of sorting will be undertaken by the specialist waste disposal firm whereby skips or bagged materials will be removed from site and taken to an offsite materials sorting plant where materials of different categories will be sorted into recyclable rubble piles to be processed at a local recycling centre. Skip and bagged materials will be kept either in the agreed location in Whitfield Street or protected, designated areas within the site.

The aim is that skips or bagged materials will be taken on a regular basis but in circumstances where they are on site for longer periods of time, they will be covered and secured at the end of each day to prevent any birds or other animals entering them and will be checked each morning.

9. CCS membership and site conditions

The appointed contractor will be a member of the Considerate Contractors Scheme

CCS requirement signage and protocol posters will be displayed in appropriate locations including within site welfare facilities.

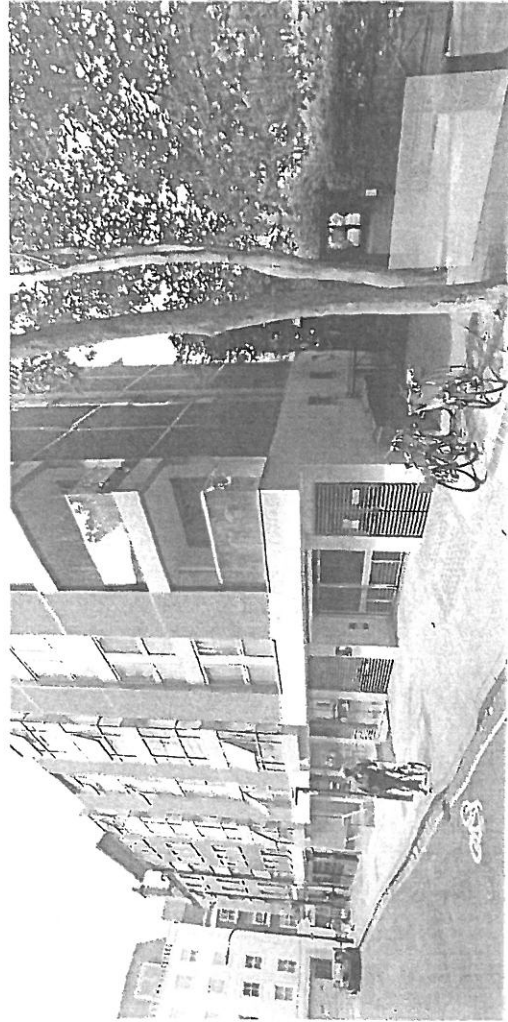
All sub-contractors and site operatives will be made fully aware of the requirements of this scheme and will be expected to adhere to those requirements throughout the works and will be directly held responsible if they repeatedly fail to do so.

A site CDM planning co-ordinator and Principal Designer will be appointed and they will advise on correct placement of notices and prepare a site safety file which must be adhered to.

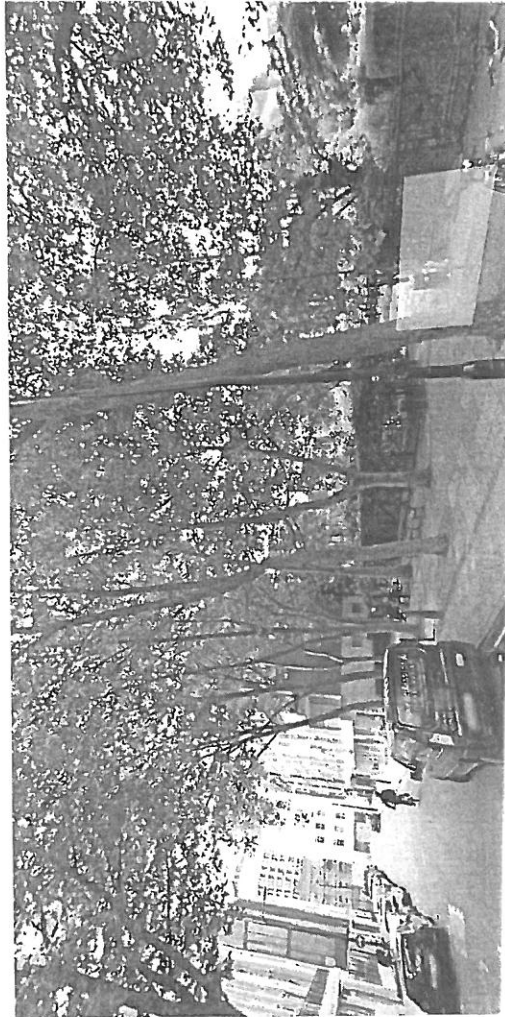
The appointed CCS member contractor will adhere to the site works guidelines and noise levels under Section 60 of the Control of Pollution Act 1974.

Noisy works will be limited within the following days and times;
Monday to Friday between 8.00 am and 6.00 pm
Saturdays between 8.00 am and 1.00 pm
Sundays and Public Holidays – No noisy works permitted

Any noisy works carried out during these allocated times will be done under the full knowledge and compliance of BS 5228 : 2009



A



B

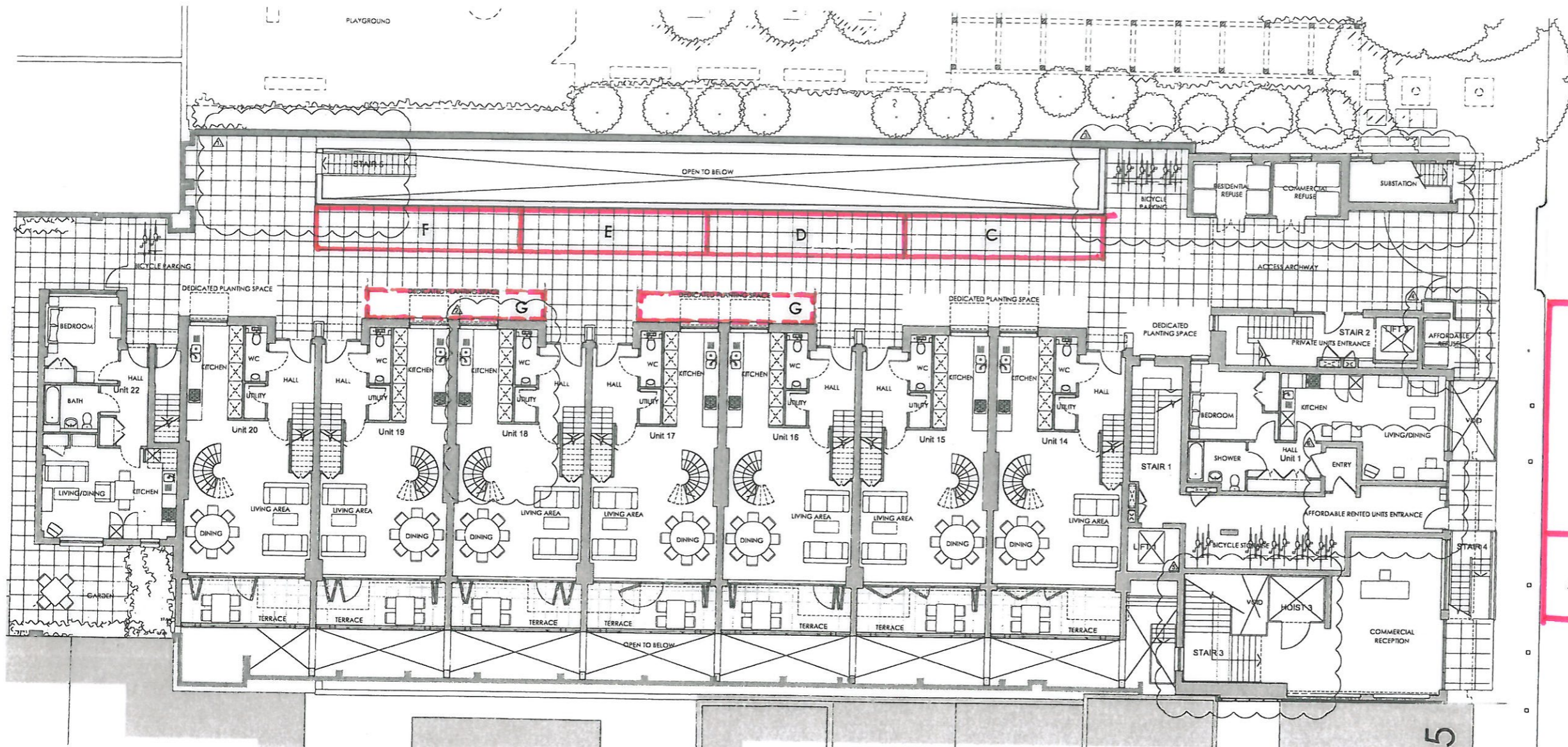


ADZ Architects

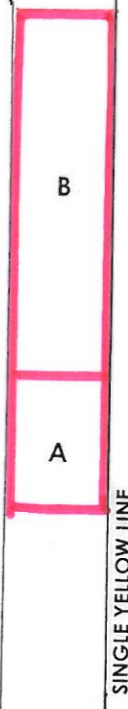
One, Bonny Street
London NW1 8PE
T: 020 7485 8029
F: 020 7472 0278

PROJECT	CRABTREE PLACE LONDON W1
TITLE	LOCATION PLAN SITE PHOTOGRAPHS

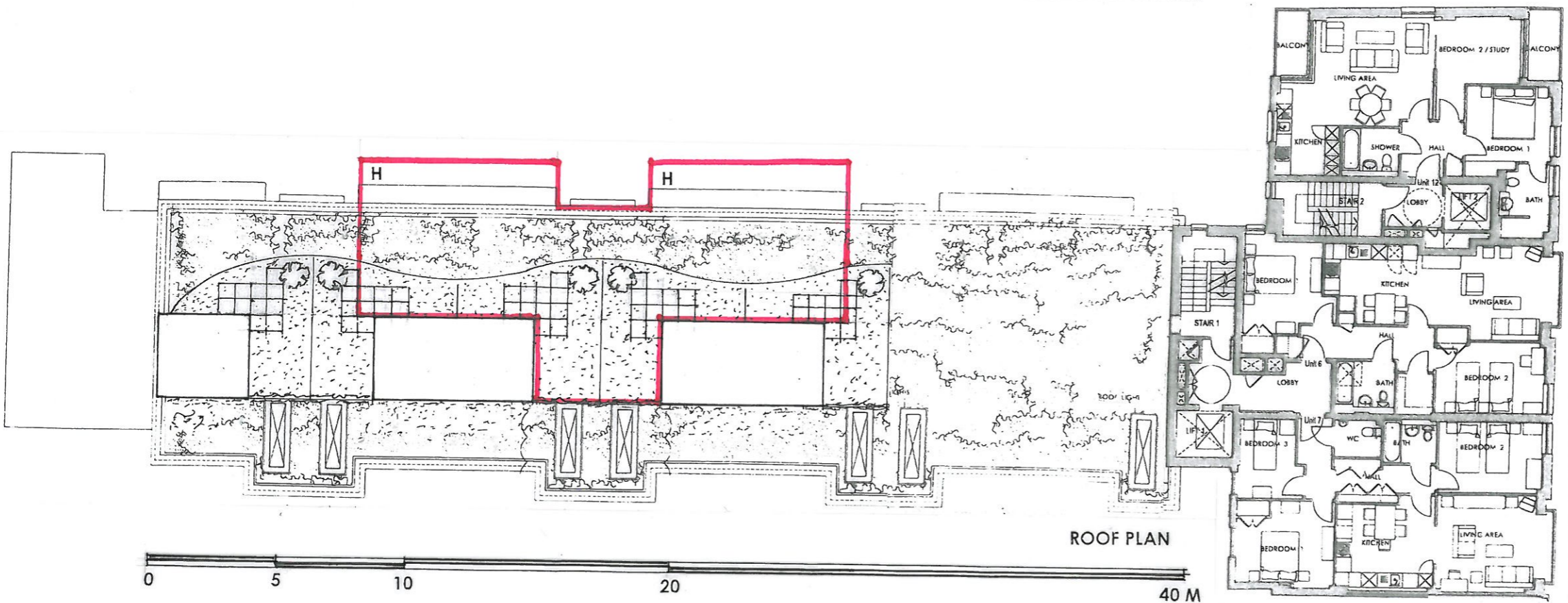
DATE	11.250
DATE	SEPT 2016
DWG NO	P09
REV NO	A15.126



GROUND FLOOR PLAN



WHITFIELD STREET



ROOF PLAN

- A PROPOSED SKIP LOCATION
- B PROPOSED LOADING BAY
- C SITE WASTE HOLDING AREA
- D SITE ACCOMMODATION & WELFARE
- E CONSTRUCTION MATERIAL HOLDING AREA
- F PLANT & TOOL STORAGE
- G SCAFFOLDING INCLUDING VERTICAL TRANSPORTATION
- H WORKING AREA

PROJECT: **CRABTREE PLACE**
7-15 WHITFIELD STREET
LONDON W1T 2AS

TITLE: **DRAFT CONSTRUCTION**
MANAGEMENT PLAN

 **ADZ Architects**

One Bonny Street
 London NW1 9PE
 T : 020 7485 9029
 F : 020 7424 0229

DRAWN	DATE	SCALE
	FEB 2017	1:200
PROJ No:	DWG No:	REV. No:
A.15.126	PL.19	A