

<b>Address:</b>	41 - 43 Chalton Street London NW1 1JD		<b>1</b>
<b>Application Number:</b>	2016/6599/P	<b>Officer: Fergus Freeney</b>	
<b>Ward:</b>	St Pancras & Somers Town		
<b>Date Received:</b>	01/12/2016		
<b>Proposal: Erection of full height rear extension at 41-43 Chalton Street frontage; erection of 4 storey plus basement infill building, following demolition of existing single storey building at Chuchway frontage and replacement two storey link building to connect the Charlton Street and Churchway frontages across the site; alterations at roof level of 41 and 43 Chalton Street to include dormer and terrace, and installation of plant equipment at roof level; to provide additional office space (Class B1 Use Class).</b>			
<b>Background Papers, Supporting Documents and Drawing Numbers</b> 100 00; 101 03; 102 04; 103 03; 104 03; 105 03; 106 03; 107 02; 121 02; 123 01; 124 01; 122 01; 111 07; 112 08; 113 05; 114 05; 115 05; 116 06; 117 06; 131 03; 132 01; 133 02; 134 02; SK104 02; Design and Access Statement Rev 00; Daylight Analysis; Planning Statement (November 2016); Sustainability Statement 30/11/2016; Energy Statement 30/11/2016; BREEAM Pre Assessment 30/11/2016			
<b>RECOMMENDATION SUMMARY: Grant Conditional Planning Permission Subject to a Section 106 legal agreement</b>			
<b>Applicant:</b>		<b>Agent:</b>	
NJA/Pealkwalk C/O Agent		DPP Planning Second Floor 1 City Square Leeds LS1 2ES	

#### ANALYSIS INFORMATION

<b>Land Use Details:</b>			
	Use Class	Use Description	Floorspace
Existing	1.1	<b>B1 Business</b>	2103m <sup>2</sup>
Proposed	1.2	<b>B1 Business</b>	2671sqm (568m <sup>2</sup> extension)

## 2 OFFICERS' REPORT

- 2.1 **Reason for Referral to Committee: minor development where this involves the construction of either 5 or more single dwelling houses or upwards of 500 sq. mtrs of non-residential floorspace; [clause II]**

## 3 SITE

- 3.1 The site is located north of the Euston Road within Somerstown; the site comprises a frontage on the west side of Chalton Street (41 and 43) and spans across to a frontage on the east side of Churchway.
- 3.2 No. 41 is a red brick building with a pitched roof that forms part of a pair with No.39 (not subject to this application). No 39 was constructed in 1884 and No 41 in 1900. Both properties are in Class B1 office use. Historically Nos 39-43 were used for industrial purposes as the 'Connolly Bros Carriers' leather works who vacated the site in 1980.
- 3.3 The building comprises basement, ground and three upper storeys and a mezzanine floor on the Chalton Street frontage. At basement and ground floor level, the property extends the length of the site with a single frontage and separate access on Churchway. The rear element comprising ground and basement level was rebuilt in the mid-20thC and is now bookended by five storey residential properties on Churchway with blank gable walls facing the site.
- 3.4 No. 43 Chalton Street was built pre-WWII and is a red brick art-deco building comprising basement, ground, first, second and third floor levels with a flat roof. There is a roof terrace and lift and stair enclosure at roof level.
- 3.5 The site is located within the St Pancras and Somerstown Ward. The properties are not listed and the site is not located in a conservation area. However Nos. 39-41 and No.43 Chalton Street and the Seymour and Winsham Buildings on Churchway (adjacent to the site) are identified as 'non designated heritage assets' in Camden's Local List ('Locally listed buildings'). The properties are considered to contribute to a sense of place and local distinctiveness.
- 3.6 Whilst Chalton Street is a designated Neighbourhood Centre, the site is excluded from this designation. Chalton Street lies outside the boundary of the Central London Area and the Camden Central Activities Zone.
- 3.7 This part of Chalton Street is covered by an Article 4 Direction and therefore the permitted development rights to change from office to residential use without the formal grant of planning permission have been withdrawn. The designation came into force on 5 November 2015.

## 4 THE PROPOSAL

- 4.1 Permission is sought for the redevelopment of the site to include:
- Erection of a full height rear extension at 41 Chalton Street,

- Demolition of existing link and building fronting Churchway;
- Erection of 4 storey building plus basement and two storey link building;
- Alterations at roof level of 41 and 43 Chalton Street to include dormer and terrace; and;
- Installation of plant equipment at roof level; to provide additional office space (Class B1 Use Class)

### Chalton Street

- 4.2 On the Chalton Street frontage the proposal involves refurbishment and internal reconfiguration at basement to third floor levels of both properties. On the front elevation, the basement lightwells are to be reinstated with walk on paving lights.
- 4.3 At the rear of the Chalton Street element, a partially glazed extension is proposed at rear first to fourth floor levels to provide a lift and staircase for access to all floors. At first floor level to the rear, in a small void between the new full height extension and number 39 Chalton Street a terrace would be created.
- 4.4 The flat roof of No. 43 at top floor level will also be reconfigured and would continue to be used as a terrace, as existing, for the occupants of both 41 and 43. A dormer roof extension, with green roof would provide access to the roof terrace.

### Churchway

- 4.5 The existing single storey building (plus basement) fronting onto Churchway would be demolished, and replaced with a 4 storey plus basement building within the space between the neighbouring mansion blocks.
- 4.6 The Churchway elevation would comprise red brick facings with a double height glazed entrance at basement and ground floor level. A front lightwell would be created protected by railings and a bridge over the lightwell to access the entrance door. The front elevation will project slightly beyond the building line of the two adjoining properties. At third floor level a rendered panel is proposed. The rear elevation of the building would comprise a brick central panel and glazing on either side.
- 4.7 The existing two storey link element between Chalton Street and Churchway would be demolished and rebuilt with a similar height link building with butterfly roof. The new link building would contain green roofs and a central walkway linking the Chalton Street and Churchway buildings.
- 4.8 The proposed extensions will provide an additional 568.15sqm of office floorspace.

## **5 RELEVANT HISTORY**

2005/0660/P - Erection of 2 storey extension at 2nd and 3rd floor levels to provide a 3 bed self-contained maisonette. *Granted 25/04/2010*

2016/4658/PRE - Creation of additional 580sqm office space within infill building fronting Churchway, extension to rear of Chalton Street building and low level link building between Churchway and Chalton Street frontages. *Advice issued 01/12/2016*

## **6 CONSULTATIONS**

Statutory Consultees – No statutory consultees  
Conservation Area Advisory Committee – No CAAC  
Local Groups – No Local Groups

### Adjoining Occupiers

23/12/2016-13/01/2016: 1x site notice displayed on Chalton Street and 1x site notice displayed on Churchway – *No comments received*

### Other consultees

*Thames Water* – No objection subject to a condition requiring a piling method statement is submitted and approved by the local planning authority (in consultation with Thames Water).

*London Underground* – No objection subject to a condition requiring detailed design and method statements (in consultation with London Underground) for all the foundations, basement and ground floor structures and piling have been submitted and approved by the local planning authority

## **7 POLICIES**

### **NPPF**

#### **The London Plan 2016**

#### **LDF Core Strategy**

CS1 Distribution of Growth

CS5 Managing the impact of growth and development

CS13 Tackling climate change through promoting higher environmental standards

CS14 Promoting high quality places and conserving our heritage

#### **LDF Development Policies**

DP16 The transport implications of development

DP17 Walking, cycling and public transport

DP22 Promoting sustainable design and construction

DP23 Water

DP24 Securing high quality design

DP26 Managing the impact of development on occupiers and neighbours

DP27 Basement and lightwells

DP28 Noise and vibration

#### **Camden Planning Guidance 2015**

CPG1 Design

CPG4 Basements and lightwells

CPG6 Amenity  
CPG7 Transport  
CPG8 Planning Obligations

## **Emerging Local Plan Policies**

The emerging Camden Local Plan is reaching the final stages of its public examination. Consultation on proposed modifications to the Submission Draft Local Plan began on 30 January and ends on 13 March 2017. The modifications have been proposed in response to Inspector's comments during the examination and seek to ensure that the Inspector can find the plan 'sound' subject to the modifications being made to the Plan. The Local Plan at this stage is a material consideration in decision making, but pending publication of the Inspector's report into the examination only has limited weight.

G1 Delivery and location of growth  
E1 Promoting a successful and inclusive Camden economy  
A1 Managing the impact of development  
A3 Protection, Enhancement and management of biodiversity  
A4 Noise and vibration  
A5 Basements and lightwells  
D1 Design  
D2 Heritage  
CC1 Climate change mitigation  
CC2 Adapting to climate change  
CC3 Air quality  
CC5 waste  
T1 Prioritising walking, cycling and public transport  
T2 Car-free development and limiting availability of parking  
T3 Improving strategic infrastructure  
T4 Promoting the sustainable movement of goods and materials  
DM1 Delivery and monitoring

## **8 ASSESSMENT**

8.1 The principal considerations material to the determination of this application are summarised as follows:

- Land Use
- Design
- Amenity
- Basement
- Sustainability
- Economic Development
- Planning Obligations and CIL
- Other matters

## **9 Land Use**

- 9.1 The proposal seeks to refurbish the existing office space and increase the amount of office space in a new extension by 568sqm. The increase and enhancement in office provision is supported by various development plan policies including CS1, CS8 and DP13.
- 9.2 The overall approach to growth and development as set out in Policy CS1 (Distribution of growth) is to focus growth in the most suitable locations, particularly the highly accessible growth areas of Kings Cross, Euston, Tottenham Court Road and Holborn. It goes on to state that the Council will promote the most efficient use of land and buildings in Camden by, among other things, seeking development that makes full use of its site, taking into account quality of design, its surroundings, sustainability, amenity, heritage, transport accessibility and any other considerations relevant to the site.
- 9.3 Policy CS8 (Promoting a successful and inclusive Camden economy) seeks to promote the provision of additional office floorspace in the borough, and whilst accepting the majority of demand will be met at Kings Cross, expects the remaining demand to be met by significant but smaller levels of office development at Holborn and Tottenham Court Road.
- 9.4 Policy DP13 supports the delivery of the Core Strategy by ensuring that sufficient sites are retained to enable a variety of commercial and industrial business to find premises and continue to operate.
- 9.5 Although not specifically within the Central London Activity Zone the site is within 200m of its boundary and given its proximity to both King's Cross St Pancras and Euston Stations it reads as an extension to the central London area and is a considered an acceptable location for office uses.
- 9.6 Policy DP1 states that The Council will require a mix of uses in development where appropriate in all parts of the borough. The benefits of mixed uses include reducing the need to travel between homes, jobs and services, providing a range of activities through the day and so increasing community safety and security, and allowing an efficient use of land. Given the nature of the site with an existing office building fronting Chalton Street, which will be unaltered externally as a result of the proposal it is not considered appropriate to require mixed use (i.e. retail at ground floor) on this side of the development. Churchway is residential in nature and it would not benefit from are more intensive retail or commercial element as it could impact upon residential amenity. The secondary office entrance proposed is considered to be an appropriate use given the nature of the surrounding area, therefore mixed uses are not considered to be necessary or appropriate for the site.
- 9.7 Policy DP1 also requires that where an uplift of more than 200sqm of floorspace is proposed within the Central London Activity Zone (CAZ) up to 50% housing will be required (or a payment in lieu). However, the site is not within the CAZ, as such neither the provision of housing or a payment is required.

## **10 Design**

### Churchway

- 10.1 At present there is a gap between the large residential mansion blocks on Churchway (the Seymour and Winsham Buildings), where a single storey plus basement commercial/industrial building is located.
- 10.2 The Seymour and Winsham Buildings are locally listed and are described within Camden's Local List description as "Social Housing blocks built by the London County Council Housing Branch to designs by E.H. Parkes, built 1899-1900. Red brick blocks with attractive detailing in an arts and crafts style including rendering at top floor level with rows of casement windows". They are 4-5 stories in height and have animated and articulated facades fronting Churchway.
- 10.3 The principle of infilling this gap is broadly acceptable, given the history of the site which saw a larger building on the plot. It also appears that the neighbouring buildings have been designed to allow for some form of future infill as their flank elevations have been left undecorated and utilitarian in appearance compared to their much more ornate and animated street elevations.
- 10.4 It is proposed to erect a full width 5 storey (plus basement) building, comprising a front lightwell with railings and top floor front terrace. The building would bridge the existing residential properties on either side with a brick/render element; slightly forward of this would be a floating façade and projecting windows to provide articulation. At ground/lower ground level green glazed bricks would be used to pick up detailing found in the neighbouring mansion blocks (also echoed by the render band at top floor).
- 10.5 The proposal is considered to be acceptable, the design responds well to the neighbouring mansion blocks without being a pastiche, rather it would read as an understated contemporary infill which respects and does not dominate the important locally listed neighbouring buildings.
- 10.6 The use of floating façade and projecting windows echoes the articulation in the facades also seen in the neighbouring buildings and is considered to be an appropriate approach to take.
- 10.7 The use of glazed green bricks at lower ground floor level, to reflect the glazed bricks used on the neighbouring buildings provides visual interest and draws on the materials used in the wider vicinity. The use of white banding at the parapet level is also considered to relate well to the neighbouring buildings, similarly metal railings at ground floor and balustrade at top floor also reflects wider elements.
- 10.8 In order to ensure a high quality outcome conditions will be added to require details (including samples where appropriate) of all facing material (including brickwork, cladding, tiles and render), windows (including jambs, head and cills) ventilation grills and external doors be submitted to and approved by the Council.

## Chalton Street

- 10.9 To the rear a stairwell core and lift lobby extension would be erected to infill a void between the neighbouring stairwell and the host building, the stairwell would project out to the rear of the building. The lift lobby would measure approx. 6.6m in depth x 5.4m in width and would extend the full height of the building, it would be mostly contained within an existing set-back void formed at the boundary of the application site and neighbouring building. The stairwell would project out further from the lift lobby, measuring 3.6m in depth x 2.4m in width; similarly it would extend the full height of the building.
- 10.10 The design would consist of glazed stairwell with brickwork with a set-back concrete wall with creeping plants.
- 10.11 The rear lift lobby/stairwell extension would not be visible from the public realm, but would be overlooked by the rear windows of the residential blocks on Churchway. The design reads as a simple, contemporary addition constructed from plain concrete and glazing, with a large area given over to a green wall. It complies with CPG1 in that it does not result in the removal of historic fabric and it is secondary to the building being extended, the green wall will help to soften the appearance of this built up courtyard type area.
- 10.12 The building fronting Chalton Street would see a dormer erected to allow access onto the flat roof of the building. The dormer would measure approx. 3.2m in height x 5.5m in width x 5.8m in depth. It would be set back from the front of the building by approx. 4.1m and would be 50cm below the roof ridge. Given the height of the building and the positioning of the dormer well back from the front of the building it would not be overly visible from the street or in long views, it is therefore considered to be acceptable.
- 10.13 The dormer would allow access out to an existing roof terrace which will be remodelled and would contain planter boxes and ornamental water features; these are considered to be acceptable and would not be visible from the surrounding area. This design approach is considered to be acceptable. The dormer would be set back from the front of the building, given the height of both 41 and 43 and narrow street it would not be overly visible in long views furthermore it does not result in the loss of any historic features and it complies with CPG1 in terms of dimensions.
- 10.14 An existing brick with glazed roof link structure (of a warehouse/industrial type design) between Chalton Street and Churchway at ground and basement level is proposed to be demolished and replaced with a similar height link, albeit with a butterfly roof with planting on the pitched elements. A footway between the two buildings will be located in the valley between the butterfly roof pitches, balustrades would ensure users do not stray from the footpath; a terrace at first floor level would be created in the space between the lift lobby/stairwell extension and 39 Chalton Street to the south.
- 10.15 The replacement link is considered to be an acceptable feature, it is broadly similar in height to the existing structure and the introduction of planting helps to soften its appearance. The existing link building is not of any architectural merit and its replacement is acceptable, the butterfly roof element with planting creates visual



interest whilst maintaining the existing urban grain of higher buildings fronting Chalton Street and Churchway surrounding an inner courtyard type area. The link building is a simple, yet contemporary design which complies with CPG1 and DP24.

- 10.16 The green roofs at upper level would not be visible from the public realm or surrounding buildings given their height, nonetheless green roofs are considered to be important feature in terms of sustainability and we welcome them where they can feasible be installed; The green roof on the link building, the first floor rear terrace and green wall would be visible from the rear of the neighbouring residential buildings, the green wall would help to provide visual interest on a blank façade whilst the terrace/green roofs would provide additional elements of vegetation in a hard urban landscape, they are considered to be attractive features which will contribute to the private views of neighbouring residents.
- 10.17 Conditions will be added requiring details of all green roofs and walls to ensure planting material, species and maintenance are appropriate to the site.
- 10.18 Photovoltaic panels would be located on the south facing roof slope of 41 Chalton Street in 2x arrays of 22 panels. They would not be visible from the public realm given the height of the building and that number 39 to the south is also a similar height pitched roof building – thereby hiding the panels within what is effectively a valley roof form. They positioning of the PV panels is therefore considered to be acceptable.

## **11 Basement/Lightwells**

- 11.1 The proposal would include a basement level and would see new paving lights installed at Chalton Street and new lightwells created on Churchway however there would be no new excavation proposed as there is already an existing basement across the entire site.
- 11.2 Replacing the lightblocks on Chalton Street is considered to be acceptable as there are existing lightblocks at present; details of the type of treatment proposed would be conditioned in to ensure compliance with Camden's Streetscape manual and to uphold pedestrian safety (a non-slip type would need to be selected, for example).
- 11.3 On Churchway the new lightwells would be set behind metal balustrades and would not be overly visible on the street scene. The metal balustrades would be representative of the railings used at both the Seymour and Winsham buildings.
- 11.4 These proposals are considered to be acceptable, given that glazed paving blocks already exist on Chalton Street and that the proposed lightwells on Churchway would be set behind metal balustrades which are characteristic of the wider streetscene. Details of the proposed metal balustrades will be secured by way of condition to ensure a high quality product is chosen. It is not considered that the proposed works require a basement impact assessment in accordance with DP27 as these are minor works above an existing basement level.

## 12 Amenity

12.1 The surrounding area is characterised by commercial and residential properties. The adjoining properties in Chalton Street are in office use. In Churchway, to the rear, there are residential flats in the adjoining properties; Seymour and Winsham House.

### Sunlight/daylight

12.2 The extension to the rear of Chalton Street would not have potential to impact upon sunlight/daylight levels of surrounding properties given that it's direct neighbours are 43 Chalton Street (part of the application site) to the north and 39 Chalton Street (an office building) to the south.

12.3 The infill building fronting Churchway would be between two residential blocks, to the north and south. It would project out from the rear building line of these residential properties by approx. 3m at the northern boundary and 6m at the southern boundary.

12.4 Given the track of the sun is to the south, the southern residential block would not experience a loss of sunlight/daylight.

12.5 The residential block to the north extends away from the application site in a north eastern direction (therefore the rear elevation faces to the south east – towards the sun). The nearest windows to the application site on the northern residential block are approx. 2.3m from the proposed extension, these appear to be secondary windows, and larger windows adjacent to these are approx. 4.3m from the proposed extension.

12.6 Given the modest projection of 3m of out from the rear building line and that each flat would have more than one set of windows facing the rear courtyard it is not considered that the proposal would be as detrimental in terms of sunlight/daylight loss as to warrant refusal.

12.7 Furthermore, informal diagrams showing shadowing of the infill building on the residential block, based on the average track of the sun in both summer and winter, have been submitted. Although not a formal sunlight/daylight report they are based on an accurate track of the sun and they indicate that overshadowing will be minimal.

### Overlooking

12.8 The proposed rear lift lobby/stairwell extension and first floor terrace to the rear of Chalton Street is located approximately 17m from the nearest residential window in Winsham House (the northern block) and 18m from Seymour House (the southern block), the terrace would be located within a void between 39 Chalton Street and the proposed full height rear lift lobby/stairwell extension, therefore it would not allow for clear overlooking to the residential properties.

12.9 In addition, CPG6 (amenity) states that, to ensure privacy there should normally be a minimum distance of 18m between the windows of habitable rooms of different

units that directly face each other, when applied to the terrace this measurement is broadly complied with.

- 12.10 The proposed terrace and walkway would only be used during working hours, rather than evenings when the flats are more likely to be occupied. The lift lobby/stairwell extension would not allow for any additional overlooking which is not already possible from the rear of 41 and 43 Chalton Street. The development would not have a detrimental impact on the privacy of residents of properties in Churchway in comparison to the existing situation.

#### Noise

- 12.11 The proposed plant equipment is located at roof level on the Chalton Street and Churchway buildings within plant enclosures. An acoustic report has been submitted and reviewed by the Council's environmental health officer, no concerns have been raised.
- 12.12 The equipment is located at roof level away from the nearest residential windows located on the upper floors of Winsham House, Churchway. The operation of the plant equipment will not cause nuisance to the nearest residential properties a condition is recommended to ensure noise levels comply with Camden's requirements and in accordance with the submitted noise report.
- 12.13 The proposals will not adversely affect the amenity of the nearby residents and the application complies with Core Strategy Policy CS14, Development Policy DP26 and CPG6.

### **13 Transport**

#### Employment space

- 13.1 The site is located within an area that has a PTAL rating of 6b (the best), being located close to King's Cross and Euston Station and multiple bus routes. It is not considered that an extension of 568sqm would result in a significant increase in the number of employees such as to require a Travel Plan. It is highly likely that workers will arrive at the site by public transport and the modest numbers would be able to be accommodated on the network without harm.

#### Cycle parking

- 13.2 The London Plan requires that 7 long stay and one short stay cycle parking spaces are provided. Camden's LDF requires 4 staff parking spaces and 2 visitor spaces. The applicant is proposing to provide 12 cycle parking spaces in the form of Sheffield stands within the basement vaults under Chalton Street. These would be accessed via a lift and level access from ground floor level, the stands would be fully compliant with CPG7.
- 13.3 The number and type of stands proposed are considered to be acceptable and in accordance with the London Plan and Camden CPG. It is recommended that the 12 cycle parking spaces will be secured by way of condition.

### Demolition/Construction management

- 13.4 The proposal would involve a significant amount of construction work which is likely to generate a large number of construction vehicle movements during the construction period. Traffic congestion is already a significant problem in this part of the Borough, and it would be necessary for construction vehicle trips to be managed so as to avoid the peak periods. The proposal is also likely to lead to a variety of amenity issues for local people such as noise, vibration, air quality.
- 13.5 A detailed CMP would need to be prepared once a principal contractor has been appointed. This should consider a comprehensive range of points including construction vehicle routes, relevant highways licenses to be obtained, safety for pedestrians and cyclists, and travel planning guidance for construction workers. The CMP should acknowledge any major developments within the area and the applicant is expected to liaise with the contractors to produce a coordinated traffic management plan.
- 13.6 The CMP would need to be approved by the Council prior to works commencing on site as an obligation under the Section 106 agreement. The associated implementation fee would also be secured as part of the Section 106 Agreement.

### Highway repair contribution

- 13.7 A financial contribution will be required in order to repair any damage caused as a result of construction to the public footway or highway, in line with policy DP21. This will be secured as part of the section 106 legal agreement.

## **14 Sustainability**

- 14.1 Pursuant to Core Strategy policy CS13 and Development Policies DP22 and DP23 all developments in Camden are required to make the fullest contribution to the mitigation of and adaptation to climate change, to minimise carbon dioxide emissions and contribute to water conservation and sustainable urban drainage.
- 14.2 This application is accompanied by an Energy Statement, Sustainability Statement and BREEAM Assessment. The Sustainability Statement demonstrates that the anticipated CO2 improvement would be 36.28% on Building Regulations 2013 when the overall scheme is taken into account; when just the new build element is considered a reduction of 22.98% is achieved. This will be achieved through the incorporation of passive design and energy efficiency measures, an Air Source Heat Pump (ASHP) and a Photovoltaic Panel array. Water efficiency measures will also be incorporated into the proposed development.

- 14.3 The energy requirements of the proposed development have been modelled to comply with Part L2A and L2B of Building Regulations (2013). The design of the proposed development has the energy hierarchy as set out in the London Plan and local guidance in CPG 3; Be Lean, Be Green. Full details are set out in the Energy Statement and are summarised below:
- 14.4 Be Lean: The building will improve thermal performance of the new building fabric beyond Part L2A and upgrade the thermal performance of the existing building fabric in accordance with L2B 2013.
- 14.5 Be Clean: There are no available Combined Cooling Heating and Power (CCHP) distribution networks to connect to and the installation of a gas fired Central CHP Plant on site would not be appropriate. However there could be potential to connect to the Somerstown District Heating Network, currently being developed by Vital Energy, in the future depending on the viability and feasibility of connecting to the network, as such a future proofing option will be secured by way of a s106 legal agreement.
- 14.6 Be Green: A range of low or zero carbon technologies have been considered. The use of Air Source Heat Pumps and PV panels are the most feasible given the nature of the development. This will reduce carbon emissions associated with the development.
- 14.7 The proposed development will achieve a BREEAM 2014 rating of 72.51% which is 'Excellent'. This complies with the Council's requirement for non-domestic buildings to achieve BREEAM Excellent by 2016.
- 14.8 However, as the new build parts of the development do not achieve the 35% carbon reduction target, the developer will be required to contribute £3,357 towards the Carbon Offset Fund, to support the delivery of carbon reduction measures elsewhere in the borough this will be secured by way of an s106 legal agreement.
- 14.9 Clauses within the S106 Legal agreement are recommended to ensure compliance with the Sustainability Plan and Energy Efficiency Plan.

## **15 Economic Development**

- 15.1 The proposed development is large enough to generate significant local economic benefits. Policy CS19 and Camden Planning Guidance state that in the case of such developments the Council will seek to secure employment and training opportunities for local residents and opportunities for businesses based in the Borough to secure contracts to provide goods and services.
- 15.2 In line with CPG8, a range of training and employment benefits are to be secured in order to provide opportunities during and after the construction phase for local residents and businesses. This package of recruitment, apprenticeship and procurement measures will be secured via S106 and will comprise the following clauses:

- The applicant should work to CITB benchmarks for local employment when recruiting for construction-related jobs as per clause 8.28 of CPG8.
- The applicant should advertise all construction vacancies and work placement opportunities exclusively with the King's Cross Construction Skills Centre for a period of 1 week before marketing more widely.
- The applicant should provide a specified number (to be agreed) of construction or non-construction work placement opportunities of not less than 2 weeks each, to be undertaken over the course of the development, to be recruited through the Council's King's Cross Construction Skills Centre.
- If the build costs of the scheme exceed £3 million the applicant must recruit 1 construction or non-construction apprentice per £3million of build costs and pay the council a support fee of £1,700 per apprentice as per clause 8.25 of CPG8. Recruitment of construction apprentices should be conducted through the Council's King's Cross Construction Skills Centre.
- If the value of the scheme exceeds £1 million, the applicant must also sign up to the Camden Local Procurement Code, as per section 8.30 of CPG8.
- The applicant provide a local employment, skills and local supply plan setting out their plan for delivering the above requirements in advance of commencing on site.

15.3 The proposed development has been designed in accordance with London Plan guidance and Development Plan Policy 22 and CPG3.

## **16 Community Infrastructure Levy**

- 16.1 The proposal will be liable for the Mayor of London's Community Infrastructure Levy (CIL) and the Camden CIL as the additional floorspace exceeds 100sqm GIA
- 16.2 Based on the information given on the plans, the Mayor's CIL Charging Schedule and the Camden Charging Schedule, the charge is likely to be £28,400 (568sqm x £50) for the Mayor's CIL and £25,560 (568sqm x £45 using the relevant rate for uplift in that type of floorspace ) for the Camden CIL.

## **17 CONCLUSION**

- 17.1 The proposal would provide a greater amount of modern, more flexible office space in an area which is appropriate for such use.
- 17.2 The proposed infill extension would respect the neighbouring locally listed buildings and replace an unattractive feature on the street scene; it would not harm the amenity of residents by way of overshadowing or overlooking. The proposal would have a minimal impact on local transport conditions and construction will be controlled by an appropriate construction management plan agreed with the Council.
- 17.3 Planning Permission is recommended subject to a S106 Legal Agreement covering the following Heads of Terms:-
- Construction Management Plan
  - Construction Management Plan Fee (£11140)
  - Sustainability Plan
  - Energy Efficiency Plan
  - Carbon Offset Fund Contribution (£3357)
  - Future proofing to connect to Decentralised Energy Network
  - Local employment and training contributions
  - Local Procurement
  - Local Employment and Training Plan
  - Highways and Streetworks Contribution

## **18 LEGAL COMMENTS**

18.1 Members are referred to the note from the Legal Division at the start of the Agenda.

Condition(s) and Reason(s):

- 1 The development hereby permitted must be begun not later than the end of three years from the date of this permission.

Reason: In order to comply with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).

- 2 The development hereby permitted shall be carried out in accordance with the following approved plans:

100 00; 101 03; 102 04; 103 03; 104 03; 105 03; 106 03; 107 02; 121 02; 123 01; 124 01; 122 01; 111 07; 112 08; 113 05; 114 05; 115 05; 116 06; 117 06; 131 03; 132 01; 133 02; 134 02; SK104 02; Design and Access Statement Rev 00; Daylight Analysis; Planning Statement (November 2016); Sustainability Statement 30/11/2016; Energy Statement 30/11/2016; BREEAM Pre Assessment 30/11/2016

Reason: For the avoidance of doubt and in the interest of proper planning.

3 Detailed drawings, or samples of materials as appropriate, in respect of the following, shall be submitted to and approved in writing by the local planning authority before the relevant part of the work is begun:

a) Details including sections at 1:10 of all windows (including jambs, head and cill), ventilation grills, external doors and gates;

b) Plan, elevation and section drawings of all proposed balustrades;

c) Manufacturer's specification details of all facing materials including brickwork, cladding, tiles and render (to be submitted to the Local Planning Authority) and samples of those materials (to be provided on site).

The relevant part of the works shall be carried out in accordance with the details thus approved and all approved samples shall be retained on site during the course of the works.

Reason: To safeguard the appearance of the premises and the character of the immediate area in accordance with the requirements of policy CS14 of the London Borough of Camden Local Development Framework Core Strategy and policy DP24 of the London Borough of Camden Local Development Framework Development Policies.

4 Detailed drawings, manufacturer's specifications and samples of materials as appropriate, in respect of the proposed glazed paving blocks, shall be submitted to and approved in writing by the local planning authority before the relevant part of the work is begun:

Reason: To safeguard the appearance of the premises and the character of the immediate area in accordance with the requirements of policy CS14 of the London Borough of Camden Local Development Framework Core Strategy and policy DP16, DP17, DP21 and DP24 of the London Borough of Camden Local Development Framework Development Policies.

5 Prior to the first occupation of the building a plan showing typical details of the green roofs, planting on the terraces and all green wall(s) including species, planting density, substrate and a section at scale 1:20 showing that adequate depth is available in terms of the construction and long term viability of the green roofs and green wall(s), in the areas indicated on the approved plans; and a programme for a scheme of maintenance shall be submitted to and approved in writing by the local planning authority. The green roofs and green wall(s) shall be fully provided in accordance with the approved details prior to first occupation and thereafter retained and maintained in accordance with the approved scheme of maintenance.

Reason: To ensure that the green roofs and green wall(s) are suitably designed and maintained in accordance with the requirements of policies CS13, CS14, CS15 and CS16 of the London Borough of Camden Local Development Framework Core Strategy and policies DP22, DP23, DP24 and DP32 of the London Borough of Camden Local Development Framework Development Policies



- 6 The 12x cycle parking spaces as shown in the approved plans shall be provided in their entirety prior to the first occupation of any of the new units, and permanently retained thereafter.

Reason: To ensure the development provides adequate cycle parking facilities in accordance with the requirements of policy CS11 of the London Borough of Camden Local Development Framework Core Strategy and policy DP17 of the London Borough of Camden Local Development Framework Development Policies.

- 7 Prior to commencement, detailed plans showing the location and extent of photovoltaic cells to be installed on the building shall have been submitted to and approved by the Local Planning Authority in writing. The measures shall include the installation of a meter to monitor the energy output from the approved renewable energy systems. The cells shall be installed in full accordance with the details approved by the Local Planning Authority and permanently retained and maintained thereafter.

Reason: To ensure the development provides adequate on-site renewable energy facilities in accordance with the requirements of policy CS13 of the London Borough of Camden Local Development Framework Core Strategy and policy DP22 of the London Borough of Camden Local Development Framework Development Policies.

- 8 Prior to commencement of any development other than site clearance & preparation details of the feasibility of rainwater and greywater recycling proposals should be submitted to the local planning authority and approved in writing. The development shall thereafter be constructed in accordance with the approved details.

Reason: To ensure the development contributes to minimising the need for further water infrastructure in an area of water stress in accordance with policies CS13 (Tackling climate change through promoting higher environmental standards), DP22 (Promoting sustainable design and construction) and DP23 (Water) of the London Borough of Camden Local Development Framework Core Strategy.

- 9 The development hereby permitted shall not be commenced until detailed design and method statements (in consultation with London Underground) for all of the foundations, basement and ground floor structures, or for any other structures below ground level, including piling (temporary and permanent), have been submitted to and approved in writing by the local planning authority which:

- Provide details on all structures.
- Accommodate the location of the existing London Underground structures and tunnels.
- Accommodate ground movement arising from the construction thereof; and
- Mitigate the effects of noise and vibration arising from the adjoining operations within the structures and tunnels.

The development shall thereafter be carried out in all respects in accordance with the approved design and method statements, and all structures and works comprised within the development hereby permitted which are required by the approved design statements in order to procure the matters mentioned in paragraphs of this condition shall be completed, in their entirety, before any part of the building hereby permitted is occupied.

Reason: To ensure that the development does not impact on existing London Underground transport infrastructure, in accordance with London Plan 2015 Table 6.1 and 'Land for Industry and Transport' Supplementary Planning Guidance 2012

- 10 No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to impact on local underground sewerage utility infrastructure. The applicant is advised to contact Thames Water Developer Services on 0800 009 3921 to discuss the details of the piling method statement

- 11 The external noise level emitted from plant, machinery or equipment hereby approved shall be lower than the lowest existing background noise level by at least 5dBA, by 10dBA where the source is tonal, as assessed according to BS4142:2014 at the nearest and/or most affected noise sensitive premises, with all machinery operating together at maximum capacity.

Reason: To ensure that the amenity of occupiers of the development site/ surrounding premises is not adversely affected by noise from mechanical installations/ equipment

- 12 Prior to use, machinery, plant or equipment and any associated ducting at the development shall be mounted with proprietary anti-vibration isolators and fan motors shall be vibration isolated from the casing and adequately silenced and maintained as such.

Reason: To ensure that the amenity of occupiers of the development site and surrounding premises is not adversely affected by vibration.

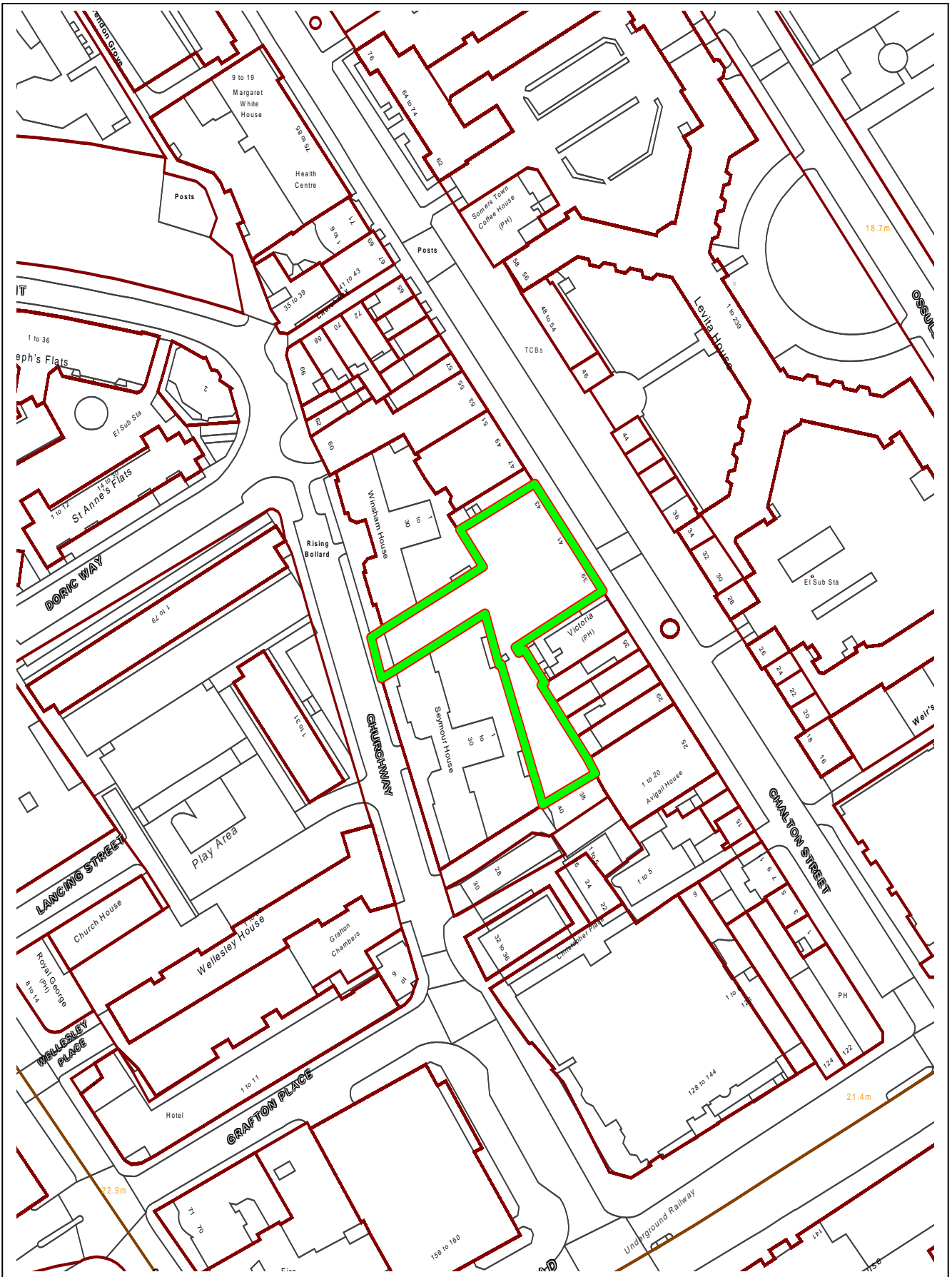
Informative(s):

- 1 The applicant is advised to contact London Underground Infrastructure Protection in advance of preparation of final design and associated method statements, in particular with regard to: demolition; excavation and construction methods;
- 2 Thames Water recommend the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

- 3 The proposal will be liable for the Mayor of London's Community Infrastructure Levy (CIL) and the Camden CIL as the additional floorspace exceeds 100sqm GIA

Based on the information given on the plans, the Mayor's CIL Charging Schedule and the Camden Charging Schedule, the charge is likely to be £28,400 (568sqm x £50) for the Mayor's CIL and £25,560 (568sqm x £45 using the relevant rate for uplift in that type of floorspace ) for the Camden CIL.

- 4 Your proposals may be subject to control under the Building Regulations and/or the London Buildings Acts which cover aspects including fire and emergency escape, access and facilities for people with disabilities and sound insulation between dwellings. You are advised to consult the Council's Building Control Service, Camden Town Hall, Argyle Street WC1H 8EQ, (tel: 020-7974 6941).
- 5 Your proposals may be subject to control under the Party Wall etc Act 1996 which covers party wall matters, boundary walls and excavations near neighbouring buildings. You are advised to consult a suitably qualified and experienced Building Engineer.
- 6 Your attention is drawn to the fact that there is a separate legal agreement with the Council which relates to the development for which this permission is granted. Information/drawings relating to the discharge of matters covered by the Heads of Terms of the legal agreement should be marked for the attention of the Planning Obligations Officer, Sites Team, Camden Town Hall, Argyle Street, WC1H 8EQ.
- 7 Noise from demolition and construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You are advised to consult the Council's Noise and Licensing Enforcement Team, Camden Town Hall, Argyle Street, WC1H 8EQ (Tel. No. 020 7974 4444 or on the website <http://www.camden.gov.uk/ccm/content/contacts/council-contacts/environment/contact-the-environmental-health-team.en> or seek prior approval under Section 61 of the Act if you anticipate any difficulty in carrying out construction other than within the hours stated above.
- 8 If a revision to the postal address becomes necessary as a result of this development, application under Part 2 of the London Building Acts (Amendment) Act 1939 should be made to the Camden Contact Centre on Tel: 020 7974 4444 or Environment Department (Street Naming & Numbering) Camden Town Hall, Argyle Street, WC1H 8EQ.



**Application No: 2016/6599/P**

**41-43 Chalton Street  
London, NW1 1JD**

**Scale:  
1:1250**

**Date:  
3-Mar-17**

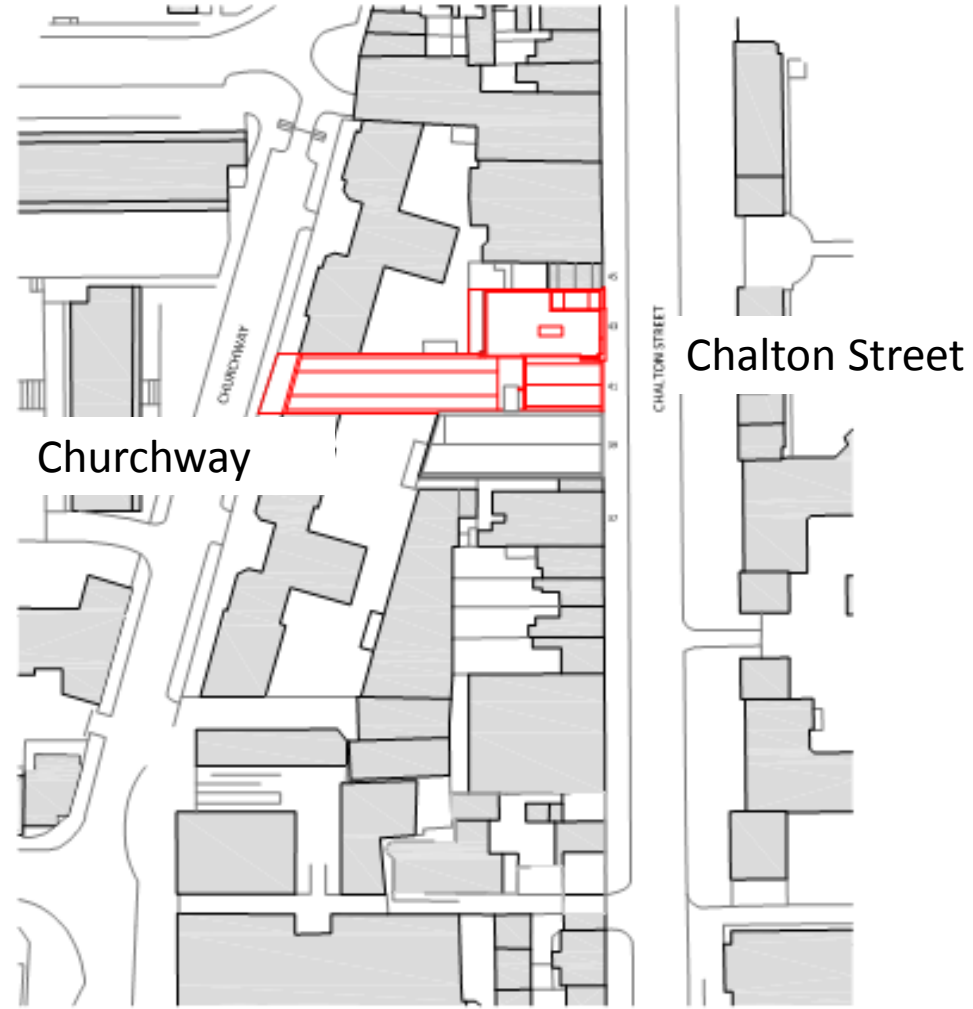


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# 41-43 Chalton Street 2016/6599/P



# Site location plan



# Chalton Street looking south



# Chalton Street looking north





# Churchway Looking North



# Churchway looking south



# Existing front and rear elevations Chalton Street



# Proposed front and rear elevations Chalton Street



# Existing front and rear Churchway elevations



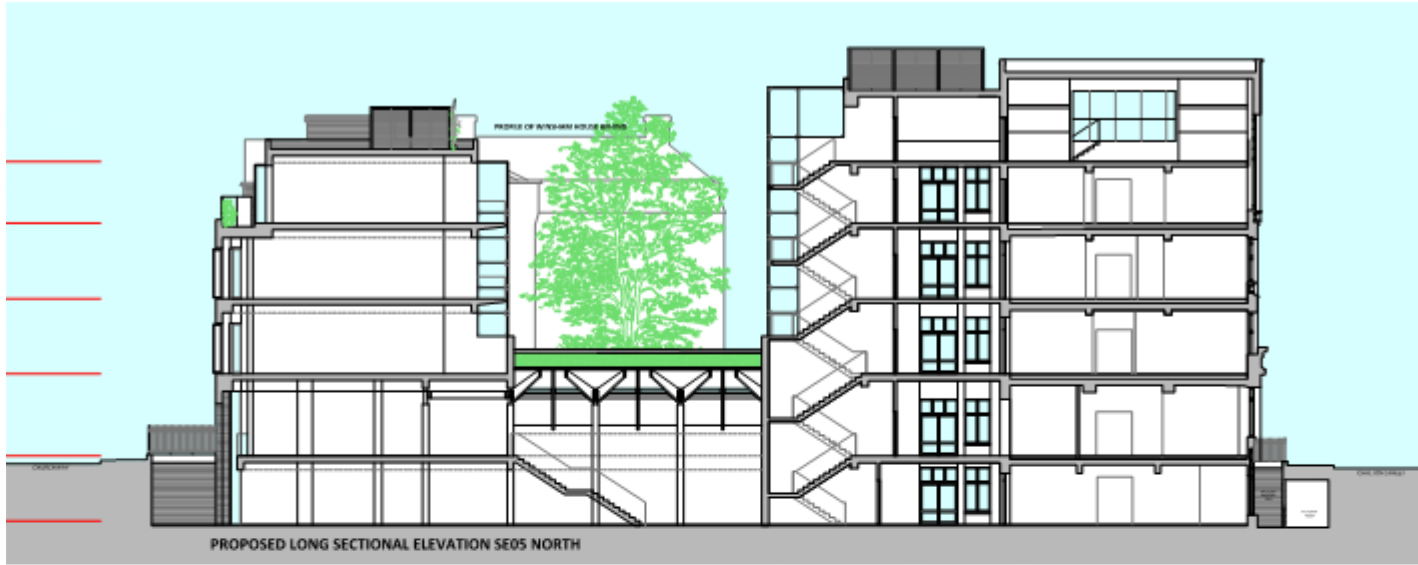
# Proposed front and rear Churchway Elevations



# Existing long sections

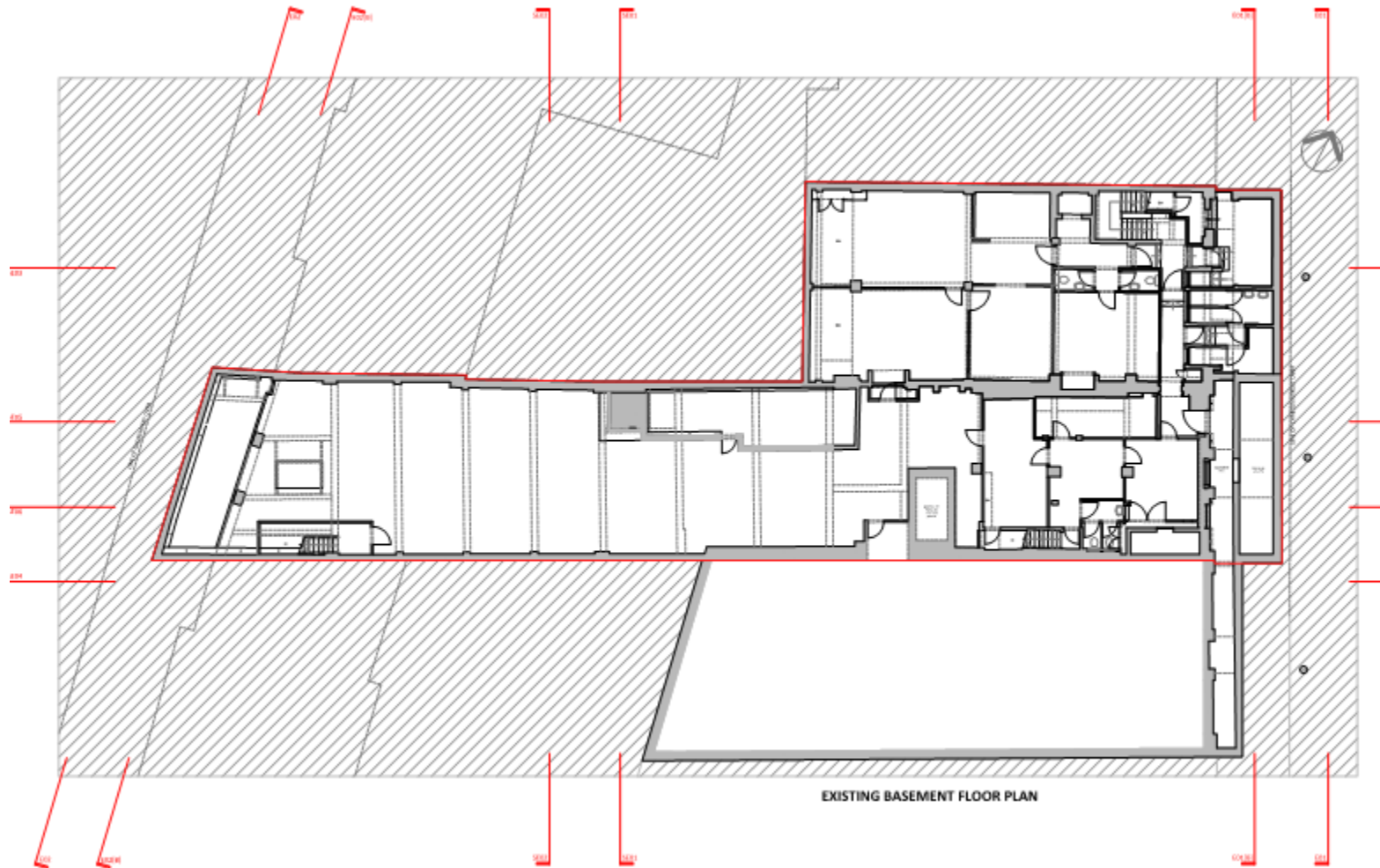


# Proposed long sections

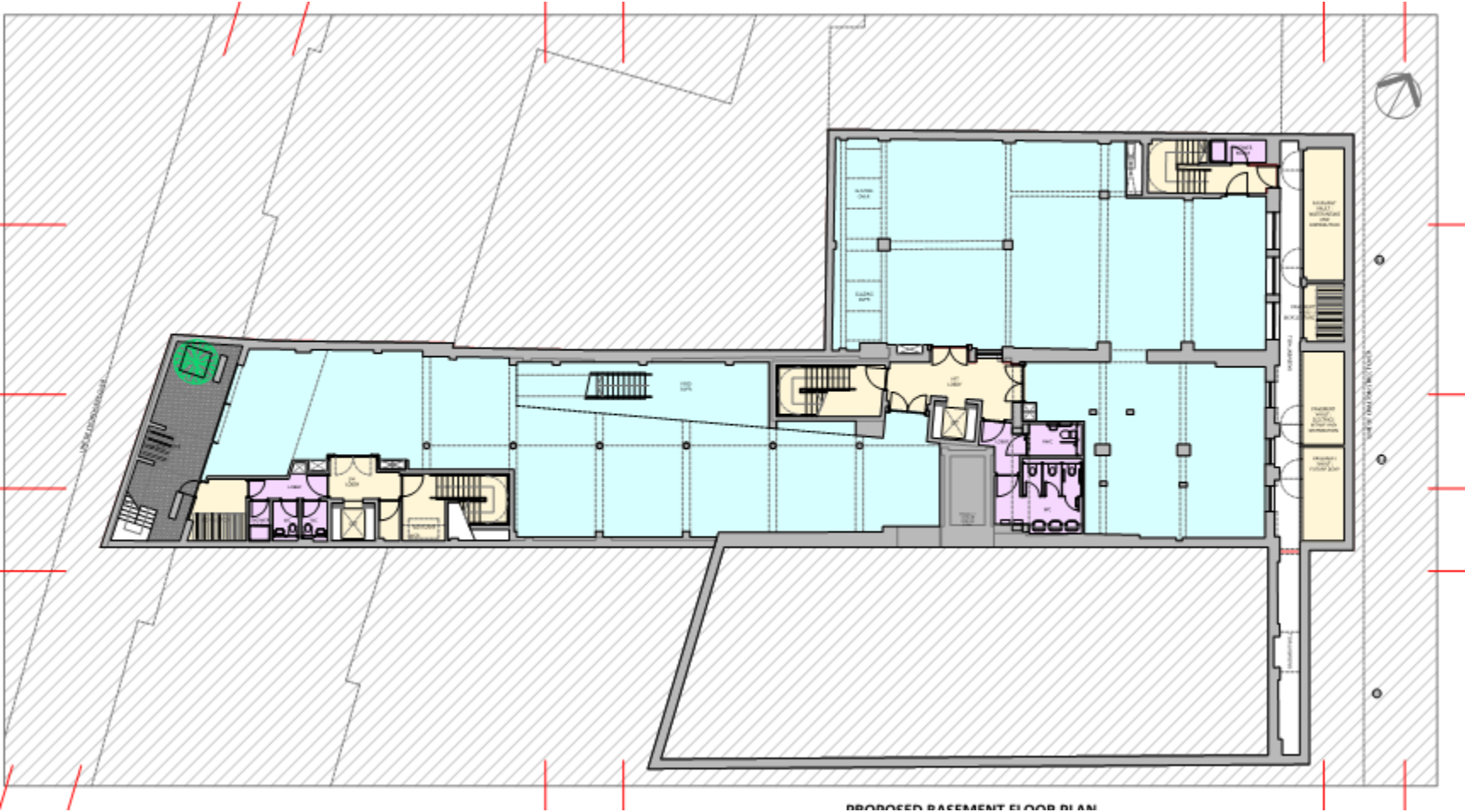




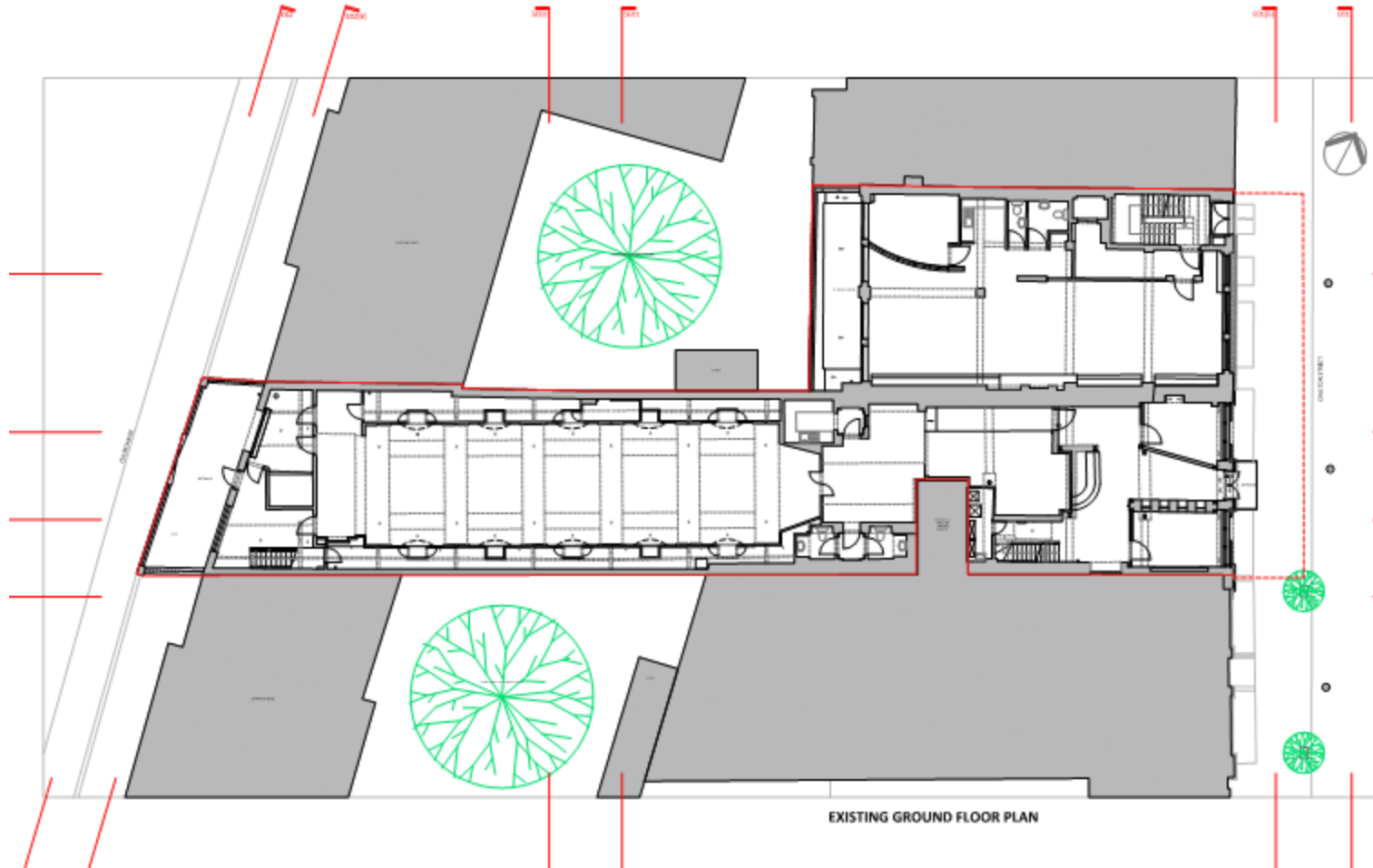
# Existing Basement Floorplan



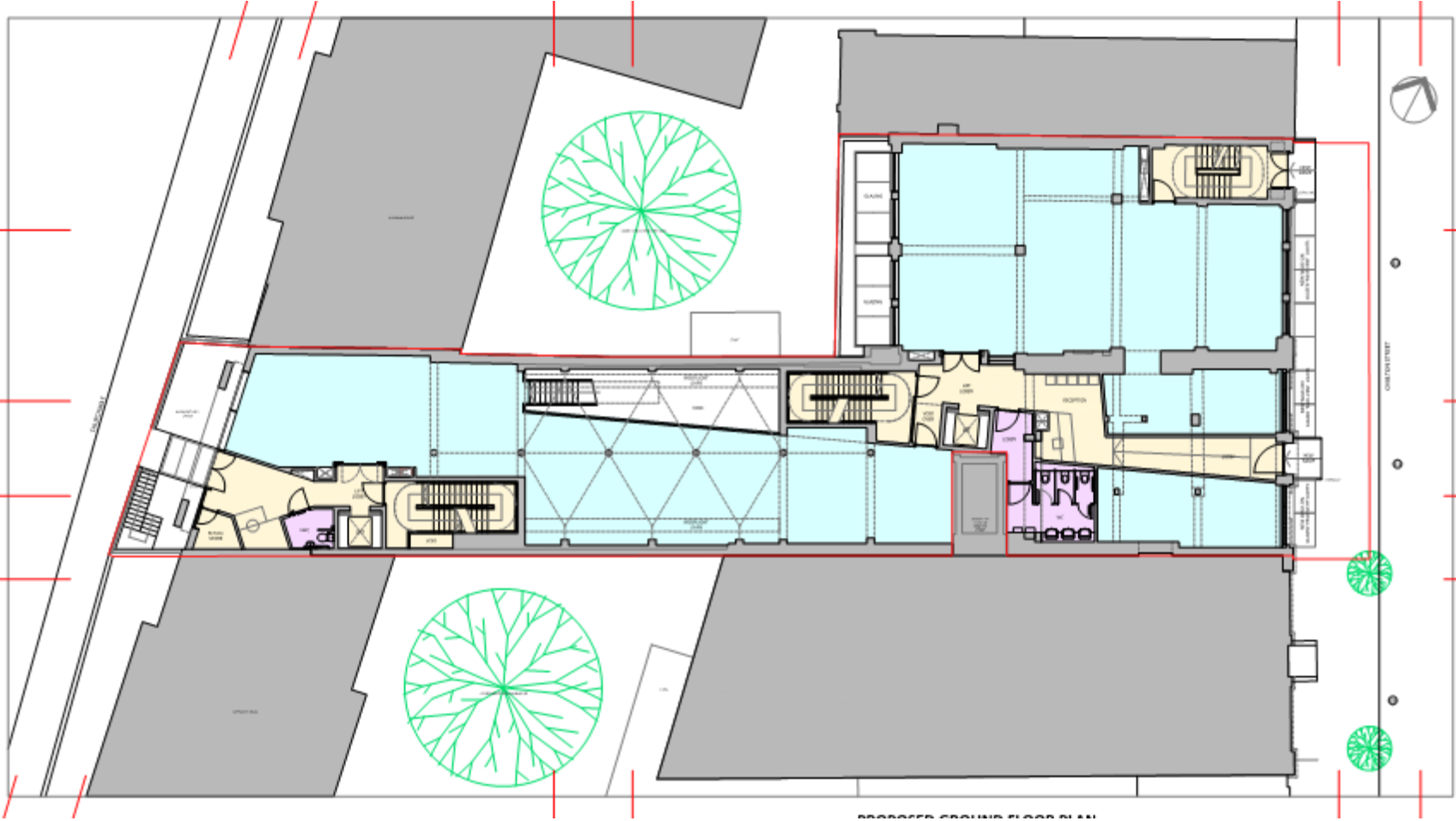
# Proposed Basement Floorplan



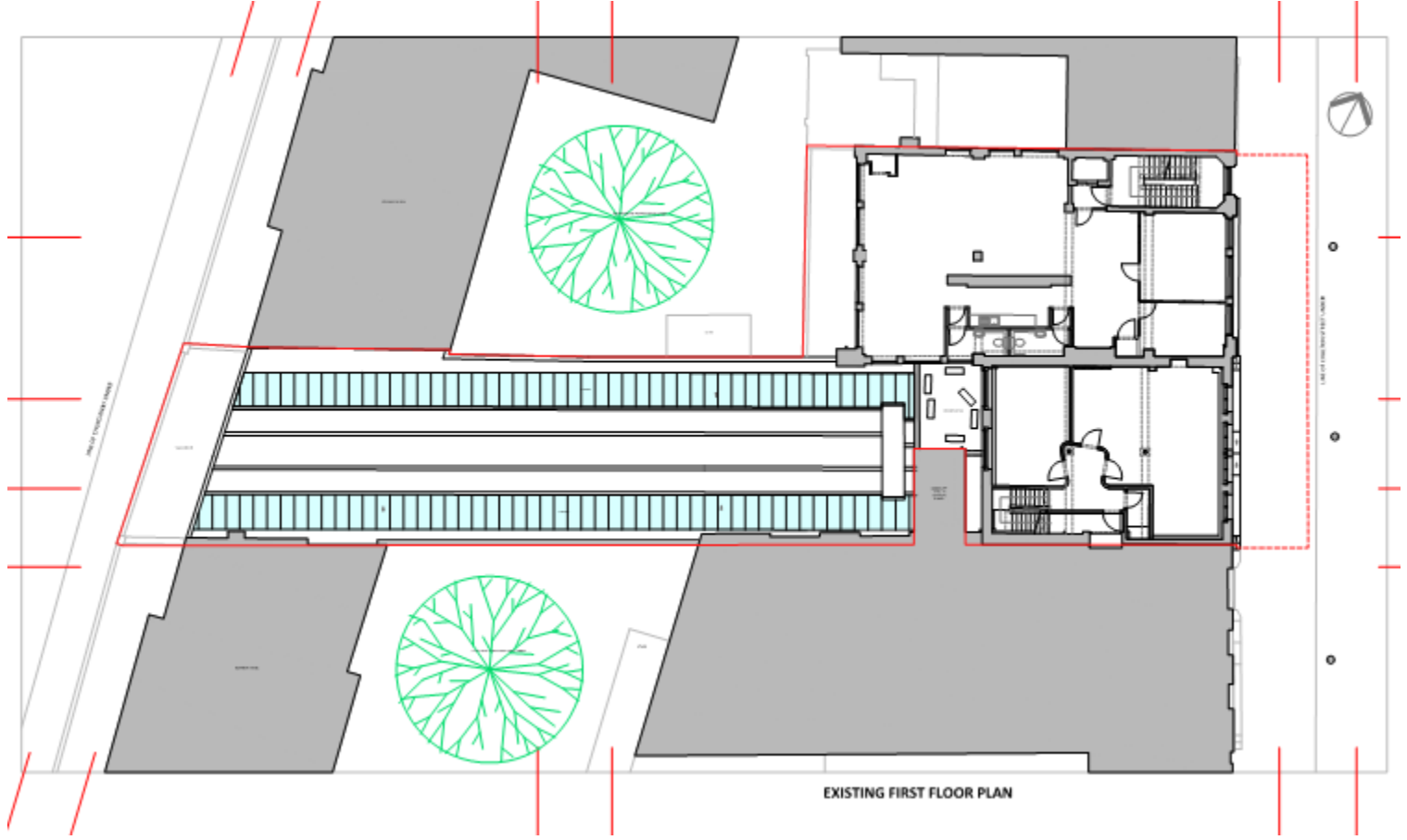
# Existing Ground Floor plan



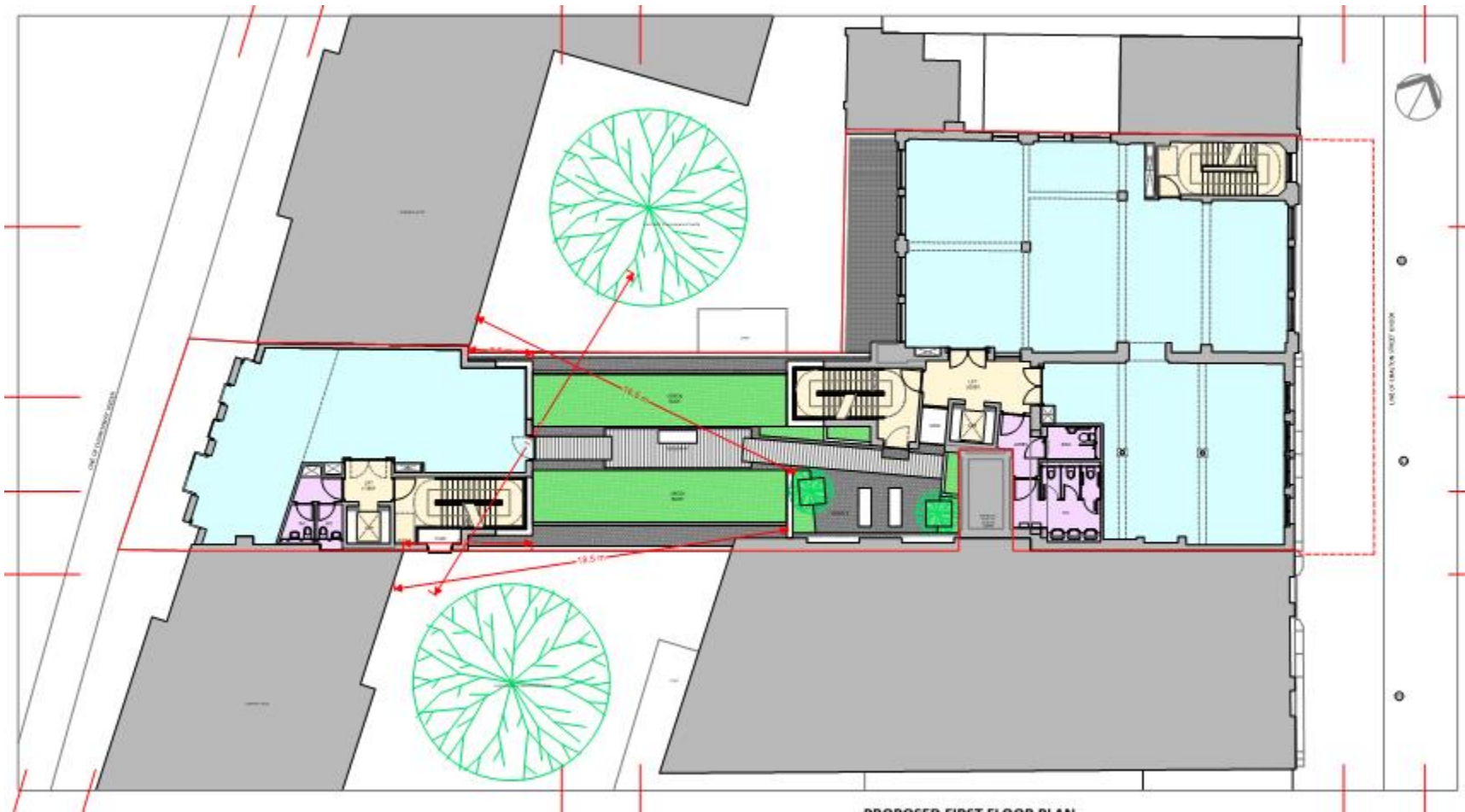
# Proposed Basement Floorplan



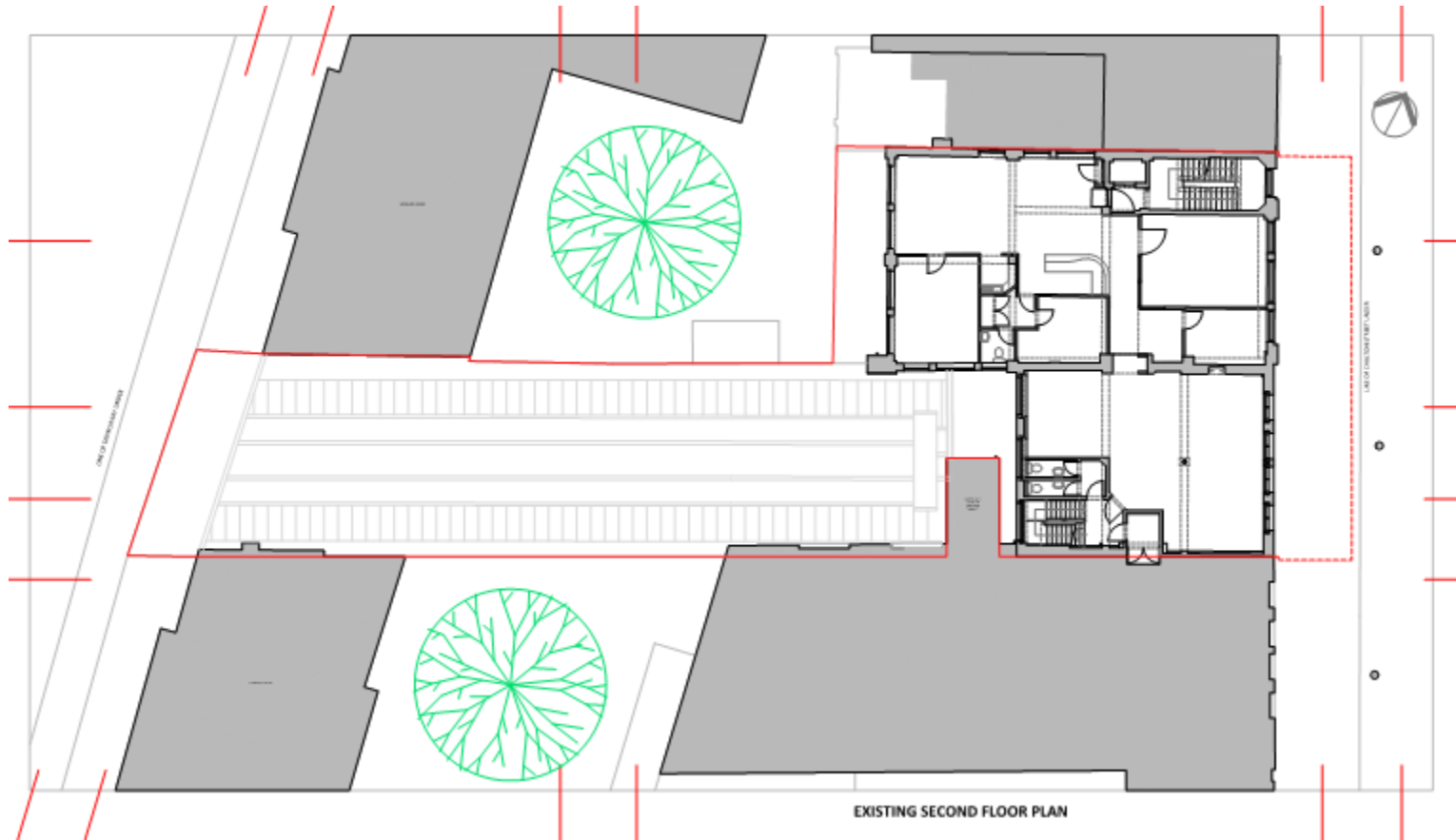
# Existing First Floor Plan



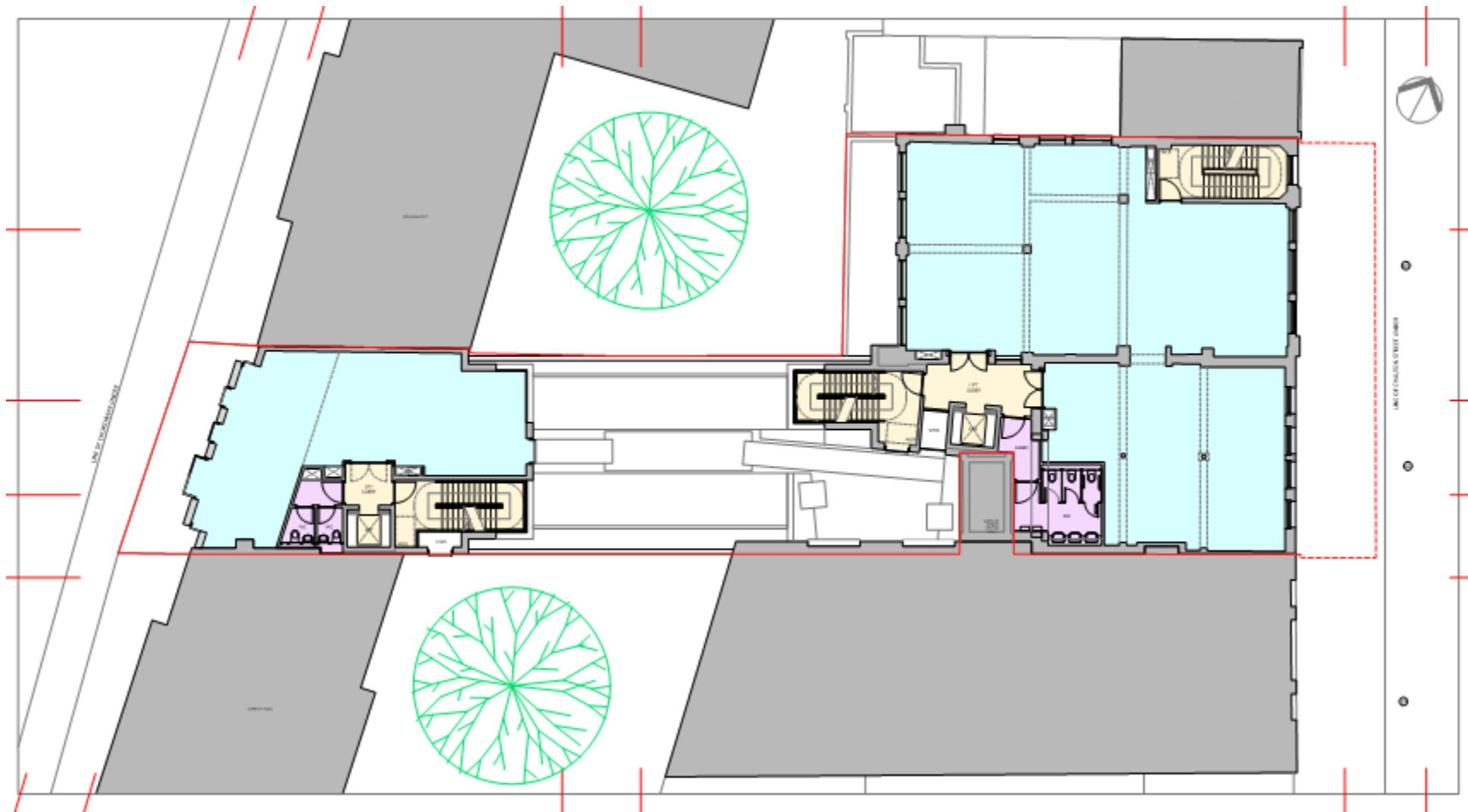
# Proposed First Floor Plan



# Existing Second Floor Plan

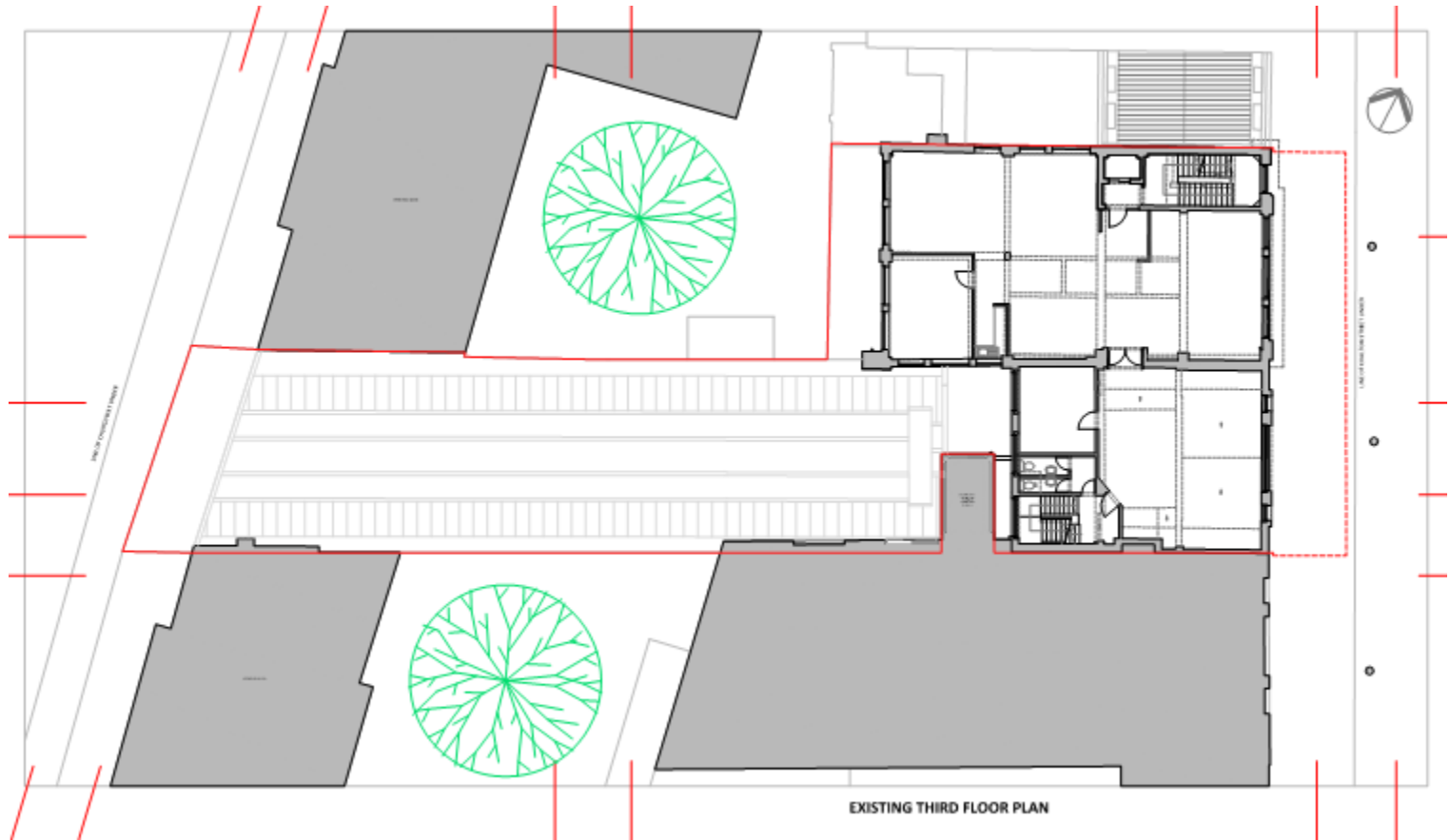


# Proposed Second Floorplan

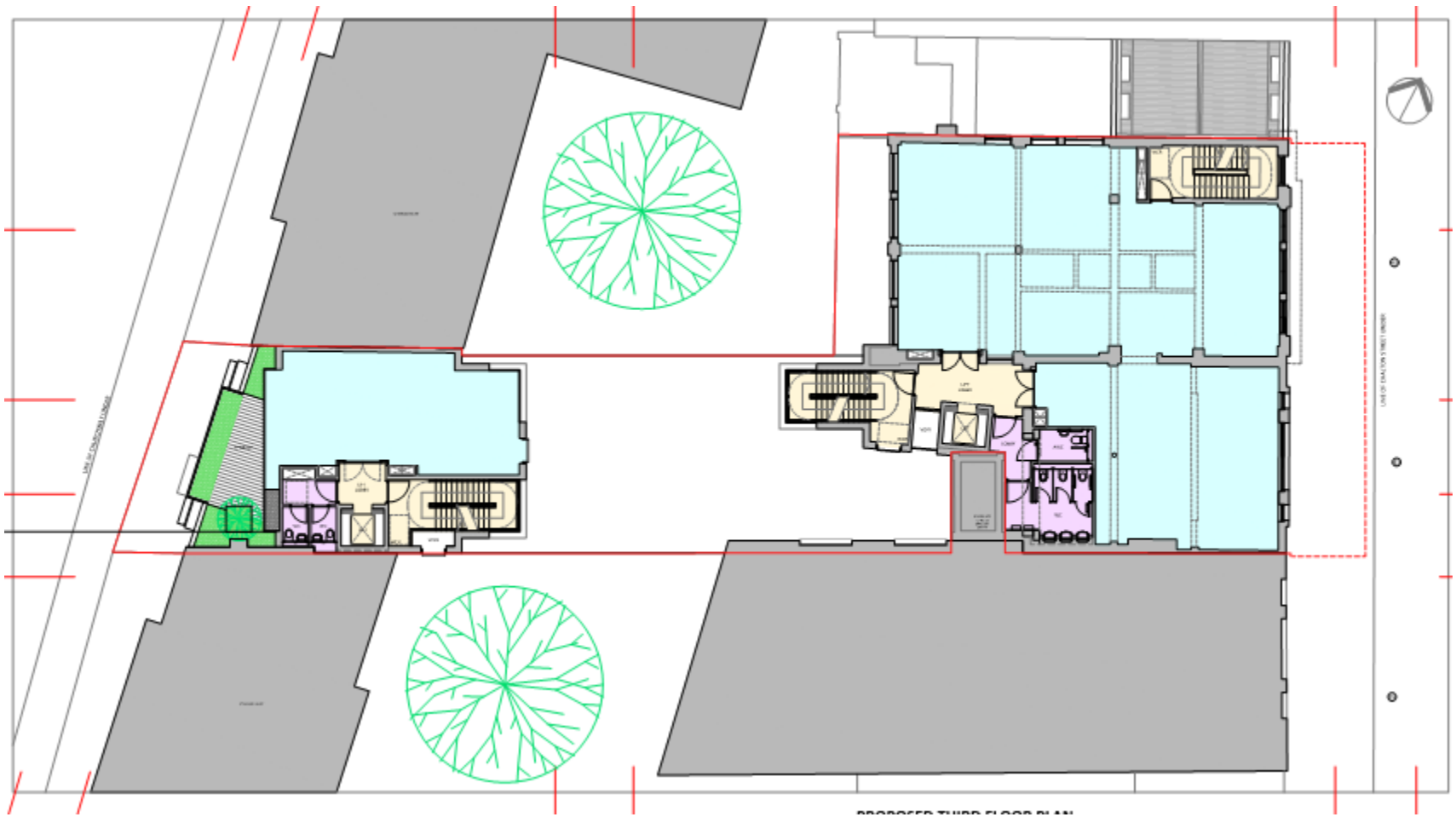




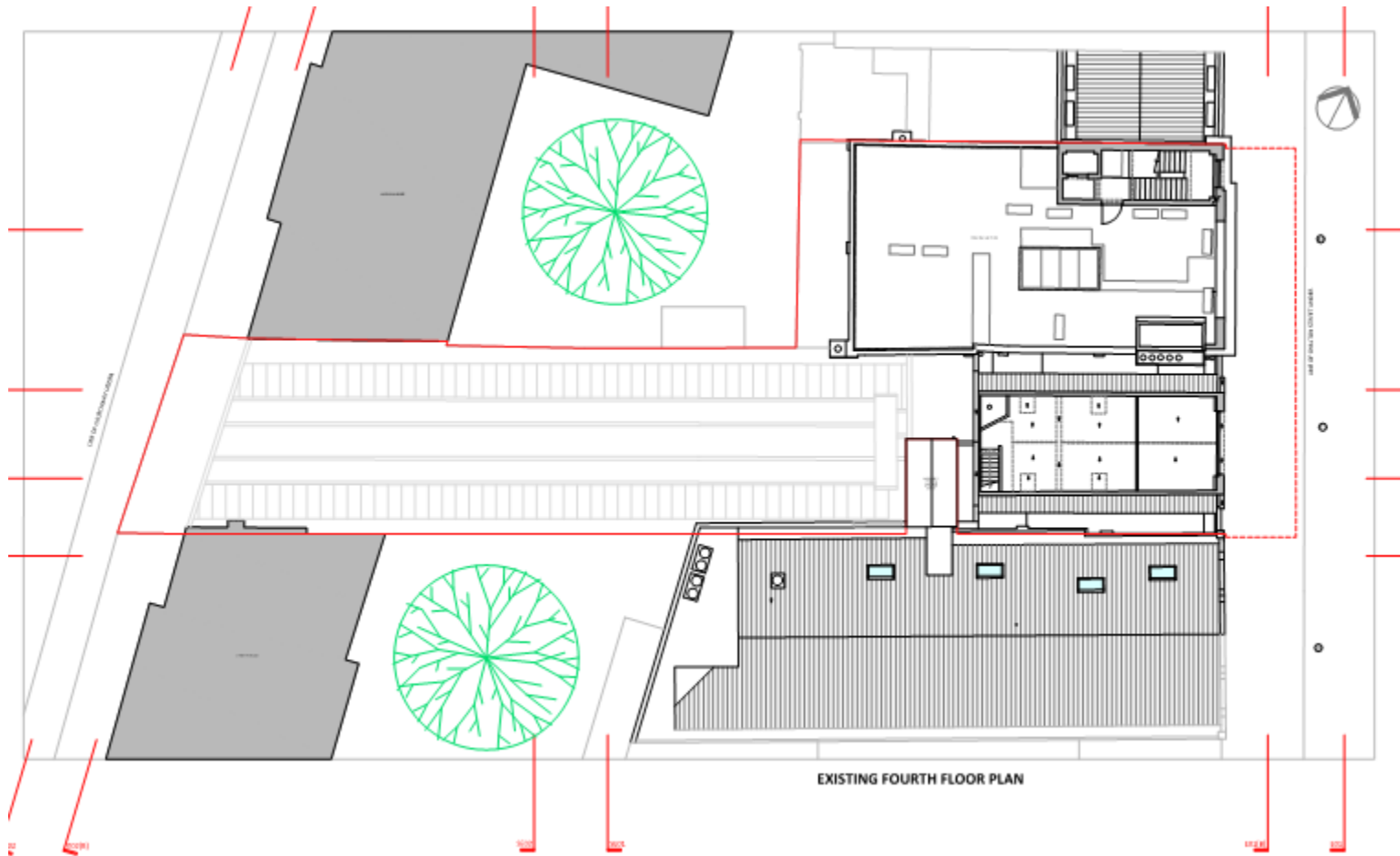
# Existing Third Floor Plan



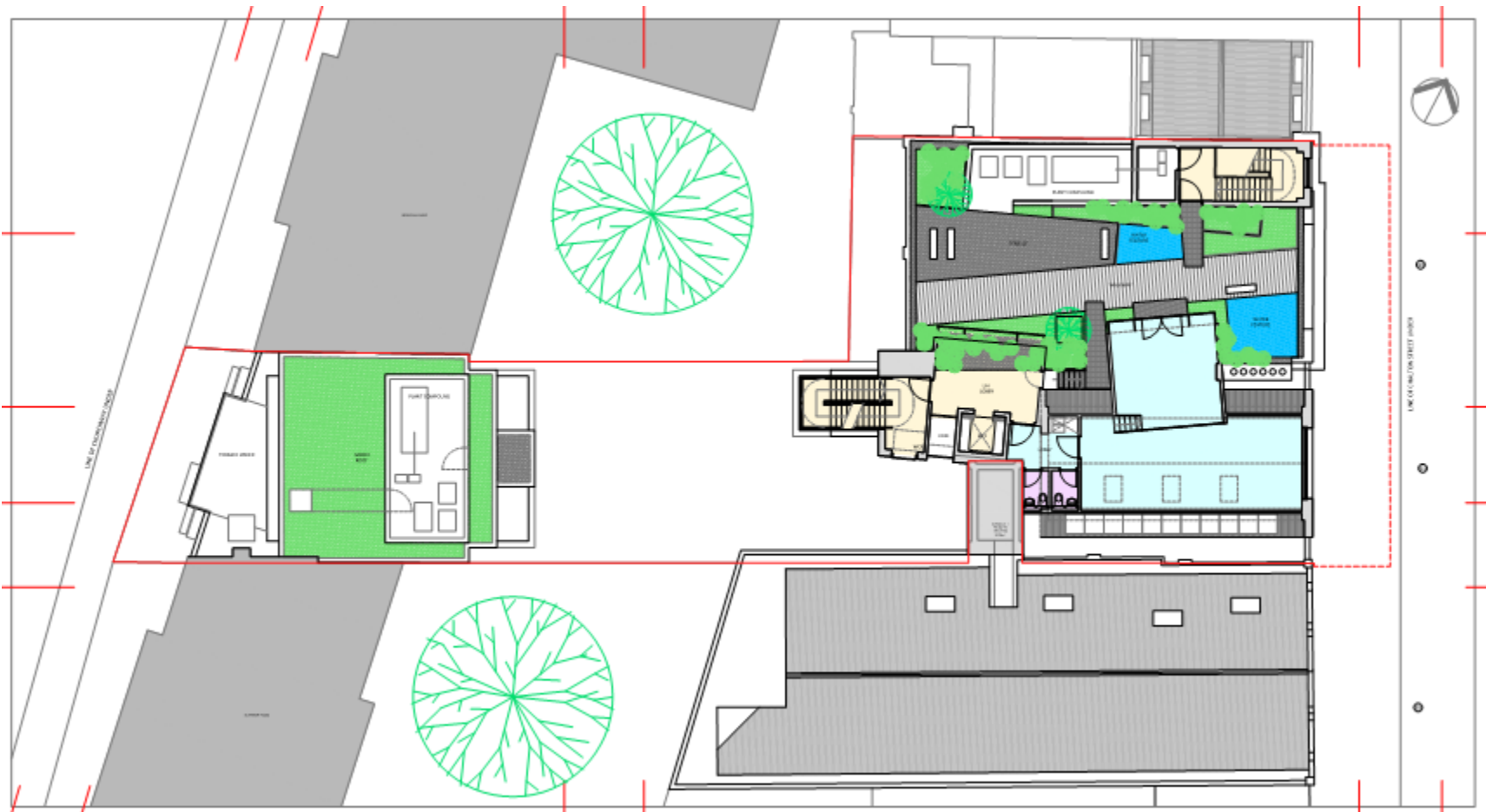
# Proposed Third Floorplan



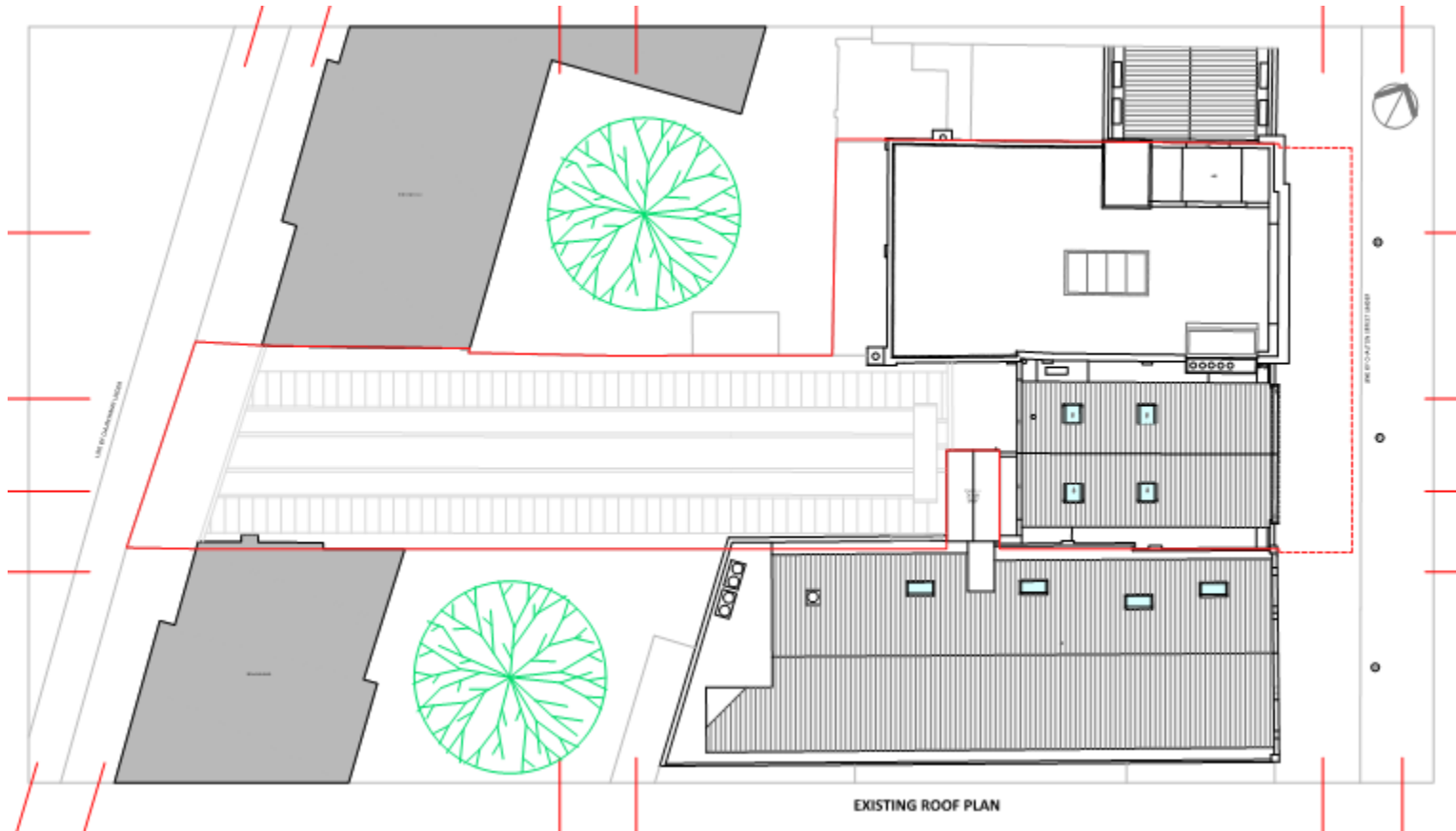
# Existing Fourth Floor Plan



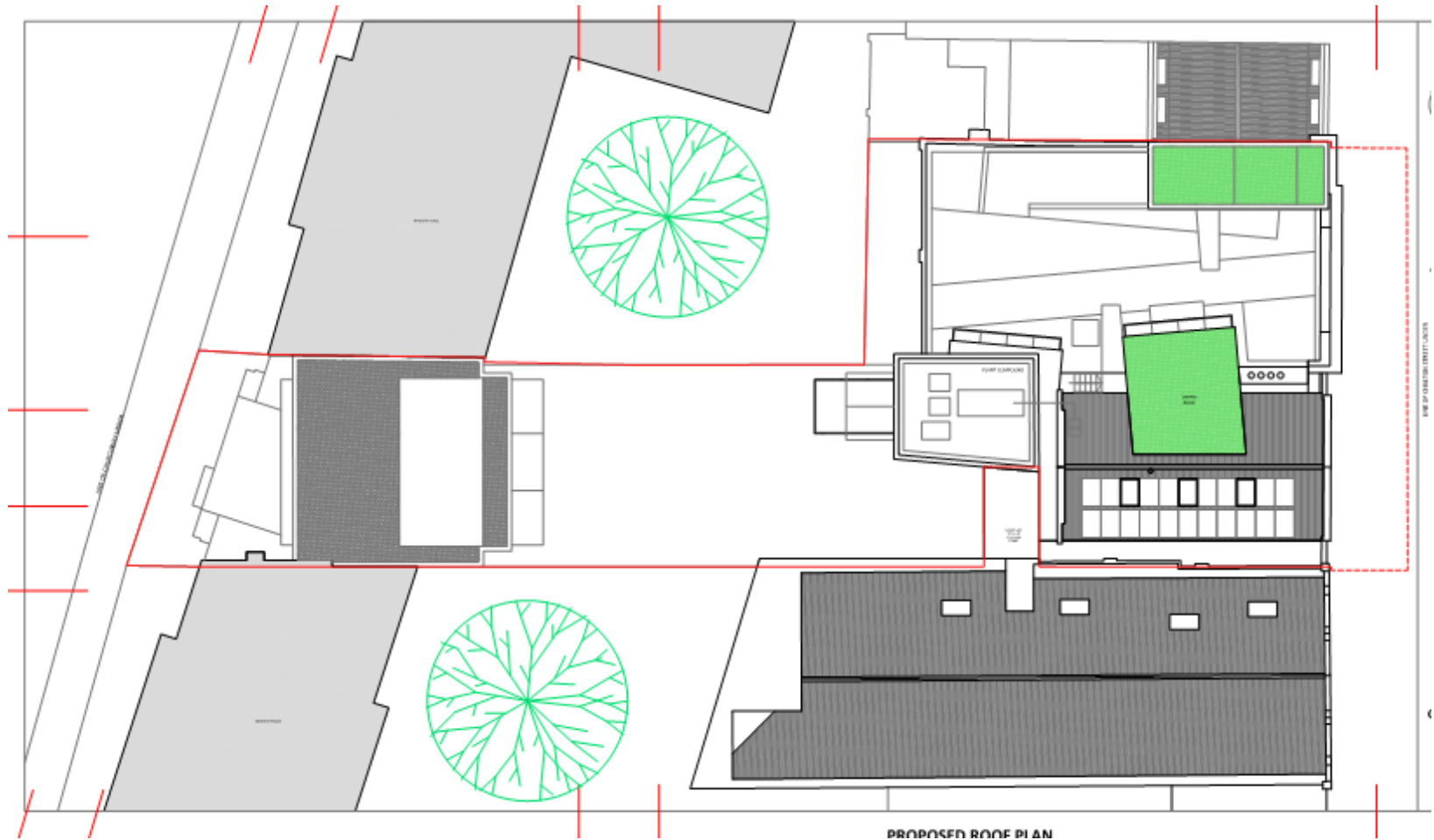
# Proposed Fourth Floor Plan



# Existing Roof Plan



# Proposed Roof Plan



# Rear Chalton Street Elevation CGI



# Churchway elevation (pre-app proposal and current proposal)



Pre App Proposal

Get back third floor clad to match slate grey colour adjacent locks

Glazed balcony size to mirror adjacent claustrary windows.

Glazed brick entrance mirrors materials used at entrances and walls on the estate.

ENTRANCE

CHURCHWAY.

Railings extend across new frontage.

