



**STABLES MARKET
LONG STABLE**

**DESIGN AND ACCESS
AND
HERITAGE STATEMENT**

for new stairs and internal changes

February 2017

**Prepared for
Stanley Sidings**

By



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Stables Market: Long Stable –Heritage Statement

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1 INTRODUCTION

Executive Summary

This Design and Access Statement and Heritage Statement has been prepared as supporting documentation for the application for Planning and Listed Building Consent for alterations to the Long Stable (Building C). The building is Grade II listed and located within The Stables Market, Chalk Farm Road, London NW1 8AH.

The proposed works to the Long Stable comprise:

- a new spiral staircase between ground and first floor
- the removal of modern internal partition walls on the ground and first floors
- alterations in layout to the freestanding kitchen on the first floor, preserving the view of the exposed roof
- two extraction voids between the ground and first floors
- the reconfiguration of toilets on the first floor to create a designated disabled WC and a female WC
- an extension to the bar area with food preparation area on the ground floor, an additional bar area on the first floor
- an internal refit on both floors

The Long Stable is on the Heritage at Risk Register as it is a group listing for all stables (A, B, C and D). The stables are registered as Category F (Repair scheme in progress...) in the priority ranking, which is downgraded from the previous category C.

Priority for action is assessed on a scale of A to F, where 'A' is the highest priority for a site which is deteriorating rapidly with no

solution to secure its future, and 'F' is the lowest priority. Buildings in category F are assessed as: *"Repair scheme in progress and (where applicable) end use or user identified; or functionally redundant buildings with new use agreed but not yet implemented."* (English Heritage, HAR 2016, p. XVIII).

SITE NAME: Stanley Sidings: stables (inc. stables A, B, C, D), Chalk Farm Road NW1

DESIGNATION: Listed Building grade II, CA

CONDITION: Fair

OCCUPANCY: Occupied/in use

PRIORITY CATEGORY: F (C)

OWNER TYPE: Commercial company

LIST ENTRY NUMBER: 1258101

Important complex of industrial stabling built 1883-1895 for the Camden Goods Yard of the London and North Western Railway. Planning Permission and Listed Building Consent have been granted for the repair of all four buildings. Work to the Tack Room is now complete and work on the other buildings is in progress.

The Design Documents have been prepared by and with the input of the Project Team who are:

Client: Stanley Sidings Limited

Local Authority: London Borough of Camden

Architects: KEANE

Heritage Consultants: Stephen Levrant Heritage Architecture

Structural Engineers: Sinclair Johnston

M&E Engineers: Alpollo

1.1 Planning History

The proposals within this application represent the continuation of a programme of works to the four grade II listed buildings within the Triangle site in the Stables Market, the objective being to conserve and modernise the building in line with contemporary conservation principles and user expectations. Refer to Appendix II. In summary:

Description	Date granted	Planning ref.	LBC Ref.
Replacement of external stair, link and canopy to Long Stable Building.	10.08.16	2016/3673/P	2016/3779/L
CCTV	03.11.16	2016/S1919/P	2016/S960/L
Signage	11.09.15		2015/4302/L
Works to Cuban Building including installation of mechanical and electrical services, refurbishment of roof, replacement of public toilets and installation of 4x CCTV units to east and west	06.11.15	2015/4958/P	2015/5826/L

elevations.			
Refurbishment of Long Stables building staircase and Chalk Farm building walkway.	06.11.15	2015/4960/P	2015/5825/L
Replacement windows and associated works.	08.12.15	2015/3775/P	2015/5483/L
Alterations in connection with replacement doors and associated works.	08.12.15	2015/3777/P	2015/5484/L

1.1 Reference to other documentation

This document should be read in conjunction with:
Information prepared by KEANE, December 2016:

- 000:** Ground Floor Existing Floor Plan
- 001:** First Floor Existing Floor Plan
- 002A:** Existing External Elevations
- 004G:** Proposed GF Plan
- 006G:** Proposed FF Plan
- 007D:** Extract Details
- 008B:** GF Lighting Plan

009B: FF Lighting Plan

017A: GF & FF Floor and Wall Finishes

018: GF & FF Small Power

019B: Proposed Entrance Door Details

20A (4 sheets): GF Fixed Seating Details

21A (2 sheets): Screen Details

23A (4 sheets): GF Serveries Plan, Sections and Elevations

24A (2 sheets): FF Bar Plan and Elevations

25 (2 sheets): GF Proposed Interior Elevations

26B: FF Female Toilet Details

27A: Disabled Toilet Details

30: Door Schedule

Alpollo Air extraction plans:

The Long Stables - 004 GF Proposed EXTRACTION Plan

The Long Stables - 006 FF Proposed EXTRACTION Plan

Structural Engineer's Statement and annotated drawings:

CONSERVATION STRUCTURAL ENGINEER'S REPORT

8767_001A_Structural Scheme Rev A

8767_001B_Structural Scheme

8767_001B_General Notes

1.2 Planning Policy Guidance and Legislation

The assessment of the proposals on the building and conservation area has been prepared taking into account the information contained in planning policy guidance and legislation:

- National Planning Policy Framework (NPPF), 27 March 2012.
- DCLG Online Planning Guidance to the NPPF (NPPG)
- Conservation principles, policies and guidance for the sustainable management of the historic environment, English Heritage (now Historic England), April 2008.
- Understanding a Place: Conservation Area Designation, Appraisal and Management, English Heritage Guidance (now Historic England), 2011.
- Historic England. Historic Environment Good Practice Advice in planning note 3 March 2015.
- Seeing the History in the View, English Heritage (now Historic England), May 2011.
- Historic England Good Practice Advice in Planning – Notes 1-3, 2015.
- BS 7913:1998 Guide to the Principles of the Conservation of Historic Buildings.
- Camden Development Policies 2010 – 2025, Local Development Framework
- Local Development Framework - Camden Core Strategy 2010 – 2025

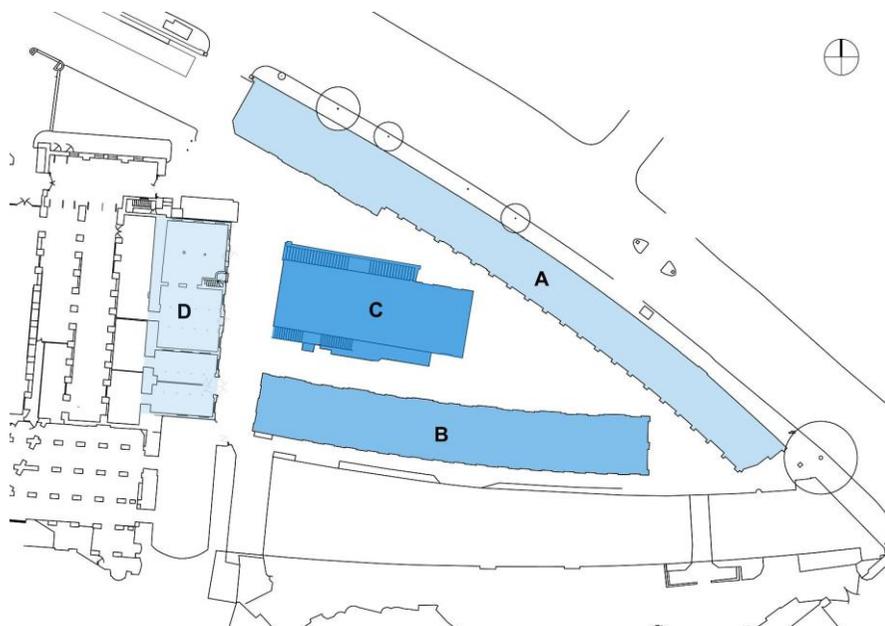


Figure 1 Stables Market - Four western ranges, subject site Building C. The Triangle site is formed of buildings A, B, C and D.

2 CONTEXT

2.1 Location

The Long Stable (Building C) lies within the triangular site, as

seen in fig.2, is part of the internationally recognised Stables Market, in the northwest corner of Regents Canal Conservation Area. (Fig. 1). The area is bounded to the north by Chalk Farm Road (Hampstead Road as it was known until 1862), and is separated from the former lands of the goods yard to the south by the railway viaducts (Fig. 2).

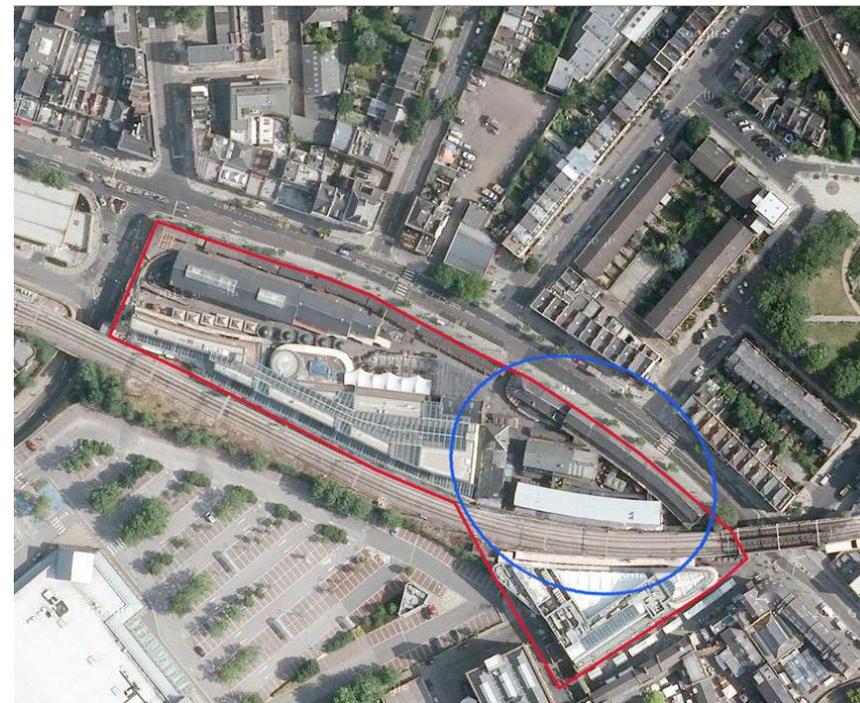


Figure 2 The Stables Market shown in red line; the stables range circled in blue.

2.2 Regents Canal Conservation Area

The Regent's Canal Conservation Area was originally designated

in 1974 and subsequently extended in 1981 to include the Stanley Sidings and the Stable Buildings. The conservation area was designated due to its unique character. "It is an important feature of historic and visual interest in the wider townscape and, following the decline of traditional canal-related commercial activities, has been increasingly recognized as a valuable resource for water-based leisure activities, for its tranquil seclusion, for its ecological value and its potential for transportation and informal recreation" (Regent's Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.5).

Many of the industrial buildings and structures are fine examples of industrial brickwork, illustrating styles of engineering construction characteristics of the 19th and early 20th centuries and using various types of brick, some produced in London and others brought in by the railways from their respective regions. Cast iron and wrought iron are also represented on the site.

2.3 Setting

The National Planning Policy Framework (NPPF) defines 'setting of a heritage asset' as 'the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral' (NPPF, Annex 2: Glossary, p.52)

Historic England's 'Good Practice Advice in Planning, Note 3', March 2015, highlights para 114 of the NPPF: 'The NPPF makes it clear that the setting of a heritage asset is the surroundings in which a heritage asset is experienced. Its extent is not fixed and

may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral (NPPF glossary).'

The site as a whole falls within the Regent's Canal Conservation Area, a heritage corridor of unique character, but the elements which contribute towards the neighbourhood identity, are not necessarily confined to the conservation area or other boundaries.

The character of the environs developed over the years and, today largely derives from the presence of four distinct but interrelated elements: the Regent's Canal, Chalk Farm Road, the Railway viaduct and the market activities.

Chalk Farm Road is the principal thoroughfare of the area. The sequence of diverse views along Chalk Farm Road, defined by the built form and the curve of the road, prevents long distance views.

The built environment is defined on the East side by 19th century terraced buildings which have shops, café, and restaurants at the ground floor; and on the West side by predominant structures, such as the Roundhouse, the 'Camden Wall' and the railway bridge, which come into view on a northerly progression.

The Roundhouse is a major focus point, almost providing a gateway to the area from the north.

The 'Camden Wall' confers to Chalk Farm Road a sense of continuity and calmness with its regular progression of openings, but it is also a distinct barrier with its non-active frontage. A strong change of character occurs behind the Stables Market gate, where the original industrial stables subject of this application, still remain. The market is enclosed and separated from the public realm by the impenetrable wall.

The railway bridge, crossing diagonally across Chalk Farm Road, is not a simple barrier. It creates a visual break, emphasised by the traffic island and one-way traffic system. It also frames with its portal structure the view behind that change again in character. In fact, the buildings aligned on this stretch of the street have a narrow pavement establishing a different relationship with the street.

The immediate setting of the subject site is the larger Stables Market at the heart of the area widely known as Camden Market. The complex comprises the industrial horse stables and tightly enclosed courts leading off one another; their plan form being influenced by the horse towpath and stabling serving the vast railway goods yard and interchange traffic.

The redevelopment of the underused site after the decline of canal-related activities and the conversion of wharves for the craft markets in the 1970s, contributed greatly to the economic and physical regeneration of Camden and to the present character of the area.

The craft markets developed into one of London's top tourist attractions giving a new iconography to Camden. Today the place is mostly known for its famous market, restaurants, café and entertainment facilities but this was never part of any planned development policies.

This market attracts large numbers of Londoners and tourists because of the character, the goods on sale and the uniqueness of the location. Boat trips, walks along the canal, and watching the barges pass through the lock gates from the Dingwall's beer garden, are important parts of the attraction of the Camden Lock area. Sunday trading was permitted on this private site while disallowed in many places elsewhere, and this also contributed to its success.

2.4 Historical Background

This complex of multi-storey stables, also known as Stanley Sidings, was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

Bounded to the north by Chalk Farm Road (known as Hampstead Road before 1862), the triangular site was separated from the goods yard to the south by the viaducts of the North London Railway but was linked to it by underground tunnels.

A 22 acre site at Chalk Farm was acquired by the London and Birmingham Railway company soon after its formation, for a terminus and depot. When development began in 1835-37 it was as a goods depot, linked to the Thames by the Regent's Canal and subsequently to the docks by the North London Railway, built in 1850-1.

In the 1840s and 1850s substantial buildings were constructed around the edge of the site, including the Southern Goods Shed, built in 1845 and rebuilt after a fire in 1857, and a goods shed north of the canal, replaced by the Interchange Warehouse in 1900-5. The Chalk Farm stables were linked to both of these buildings by underground tunnels.

Horses were required for a wide range of haulage duties in the yard as well as for distributing goods outside; others were needed for shunting locomotives. Early provision for the horses was apparently in makeshift accommodation or below ground in the vaults of the goods station. By 1849 some horses were being stabled in the south eastern corner of the goods yard, and it seems likely that the expansion of this part of the yard in 1855 led

to the construction of the stables at the triangle site.

The first phase of stabling, the four western ranges forming the triangular group A-D in the modern site plan, was built between 1854 and 1856, as indicated as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072). They were designed by the LNWR staff on the Stafford office and had a capacity of 148 horses. The ranges were mainly one and a half storeys, with ground floor stabling and haylofts above.

In the early 1880's the whole site underwent a phase of expansion and alteration with additional storeys added to the western ranges between 1880-3.

The site was vacated in 1973, and many of the buildings were demolished, tracks lifted, and various parcels of the land were sold into different ownerships. After 1973 the sidings and Camden Lock were used for various light industrial and storage uses including garaging. The market activities started at this time in a rather ad hoc fashion, commencing with small stalls located externally to the stable buildings. Gradually shop units and canopies were added to the buildings and by the 1980s it was a fully-fledged market place.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright.

2.5 Long Stable

The Long Stable building was built in 1853-5 as a single storey stable block with hay lofts. In 1881-3 it was raised (builders, Bradney and Company) to provide an upper level of stabling and a ramp was provided on the south side for horses to gain access to the upper storey (Fig.3). A second ramp to the north was added c1895 to provide access to the upper storey of the newly raised

Building A. Only the latter ramp now survives (Fig.4).

The building is seven bays long, with the centre bay breaking slightly forward on both sides; there was originally an entrance and a loophole to the south. On the ground floor there are round-headed openings with high level sills to light the stalls from above, as seen elsewhere on the site. The later first-floor openings have segmental heads. The east gable end has a small upper-storey hatch over a widened ground-floor opening. The west gable end has had its upper-storey opening enlarged as a goods hatch.

The 1880s 'horse road' to the south was not strictly a ramp as it had steps with 4 ½ inches risers; its position is evident on the south wall. The ramp to the north is brick built with concrete-paved road surfaces and an intermediate landing.

Interior:

There were originally 26 stalls in a single undivided space giving rise to the naming of this block as the 'long stable', even though this is the shortest of the stable blocks built on the site. The upper floor rests on cast-iron stanchions with moulded heads along the north and south walls; these bear marks that they were made by Barton & Son, of Darlaston. They support wrought-iron I-section griders carrying rolled joists to brick jack arches. There are timber king-post roof trusses, but no traces of the original stable fittings.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables, Report by Peter Guillery, September 1995. Crown Copyright.

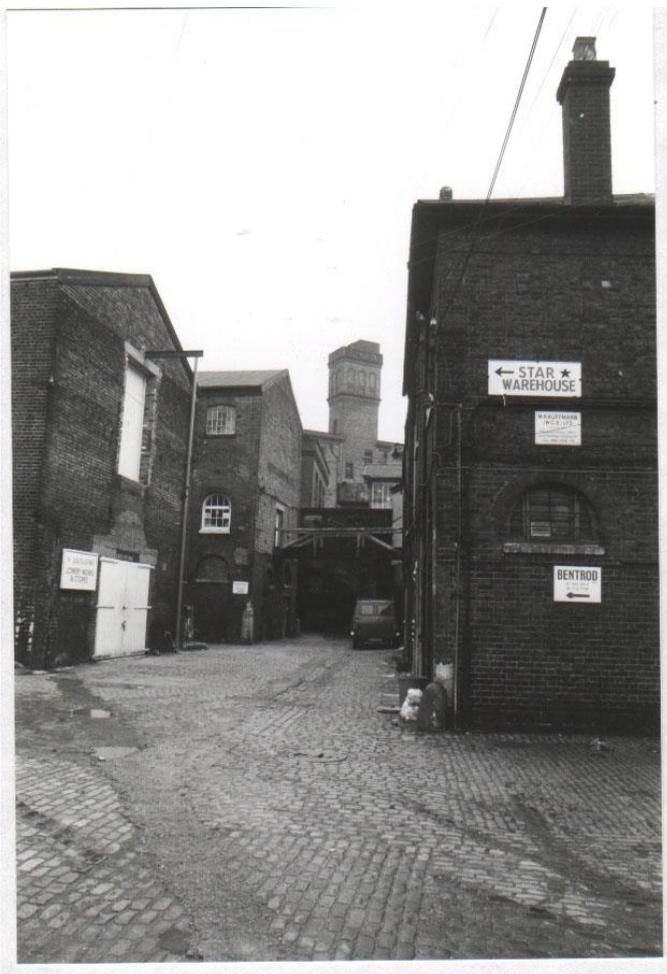


Figure 3 The Long Stable in 1975 (Front cover page and Pic. 3 & 4 by M.T.Tucker).



Figure 4 Long Stable in 1975.

3 ASSESSMENT OF SIGNIFICANCE

3.1 Criteria

As recommended by NPPF (March 2012) proposals for the alteration or redevelopment of listed building or buildings within a Conservation Area should be considered and be based on an understanding of the site's significance.

Paragraph 128 of NPPF states that 'In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should also be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance'.

The criteria for the assessment of significance according to English Heritage's 'Conservation Principles' consists primarily of:

Evidential Value – relating to the potential of a place to yield primary evidence about past human activity;

Historical Value – relating to ways in which the present can be connected through a place to past people, events and aspects of life;

Aesthetic Value – relating to the ways in which people derive sensory and intellectual stimulation from a place;

Communal Value – relating to the meanings of place for the people who relate to it, and whose collective experience or memory it holds.

3.2 Long Stable

Evidential Value

There are numerous surviving features providing evidence of the original function of the building, although disturbed by many alterations. The legibility of the former use is however clear.

The extent of historic fabric preserved contributes greatly to the special interest and character of the Long Stable and to that of the group in general.

Evidential value is therefore **high**.

Historical Value

The historical value of the Long Stable is recognised by its statutory Grade II listing, as part of a distinctive group of former stables.

In this case the historic value is closely associated to the evidential value, providing a significant example of stables construction of their historic period beyond the numerous alterations suffered through the years.

Historical value is therefore **medium to high**.

Aesthetic Value

The Long Stable, as well as the other historic stable buildings, is purpose-made and utilitarian. There is no artistic 'design ethos' in these buildings; utilitarian buildings are economical: there are no added embellishments, wasted space, nor selection of materials but those easily sourced.

Although the Long Stable is an exemplary Mid-Victorian stable

building, it has little artistic or architectural merit as such.

Aesthetic value is therefore **low**.

Communal Value

The Stables Market and its listed buildings have a high communal value as there is a strong sense of identity with the place, nurtured by the historical former industrial use.

The original purpose of the stables has been superseded. There are no possible horse-related activities that may be carried out within the site, and there have not been since the 1950's/60's. The market use, established since the late 1970's/80's, is the reason why this area of Camden achieved worldwide recognition and attracts over 100.000 visitors per week. Furthermore, a poll carried out by NOP World in late 2003, shows that 95% of visitors agreed that the Camden markets are a valuable tourist attraction and 89% agreed the markets are a valuable amenity to the local community as well as visitors (London Borough of Camden, the role the markets play in the vitality & viability of Camden Town, 2006.). The communal values of the site can be associated to the later but firmly established market function, and as proved by the studies and polls carried out since its inception. The fact that the building is still in use brings together past and present common uses by the community, reaffirming the communal value as **high**.

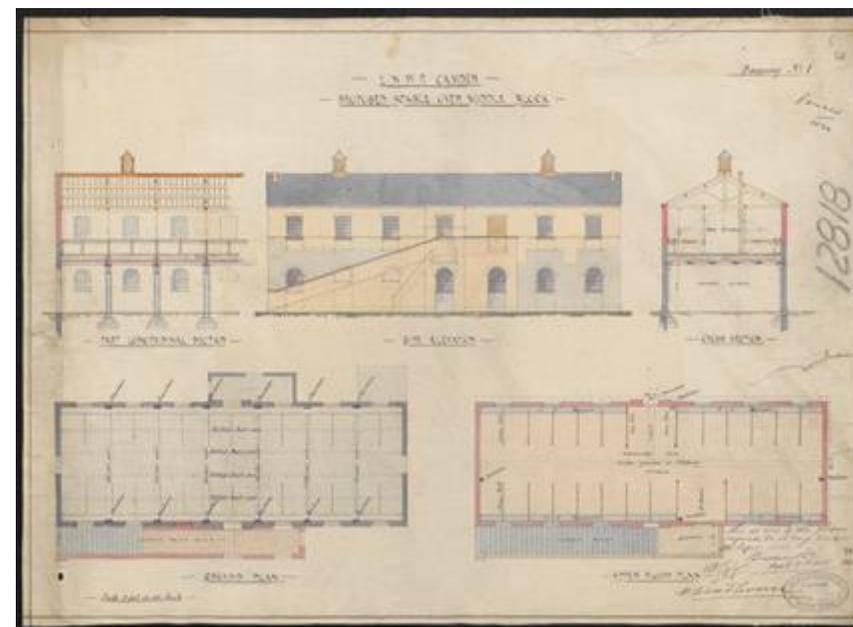


Figure 5 Drawing dated 20th September 1881 showing first floor extension to Long Stable.

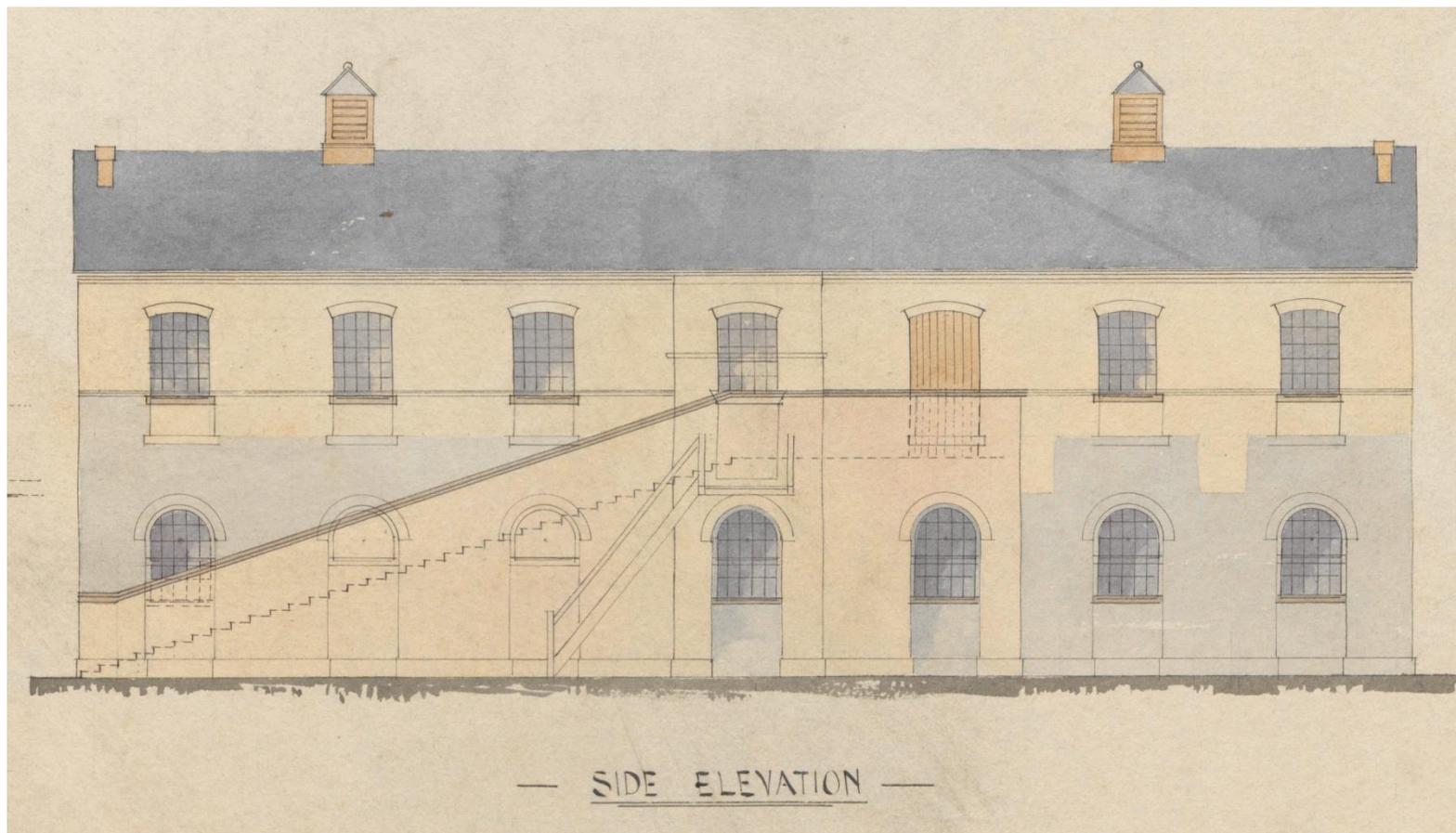


Figure 6 Detail of drawing. Long Stable Elevation.

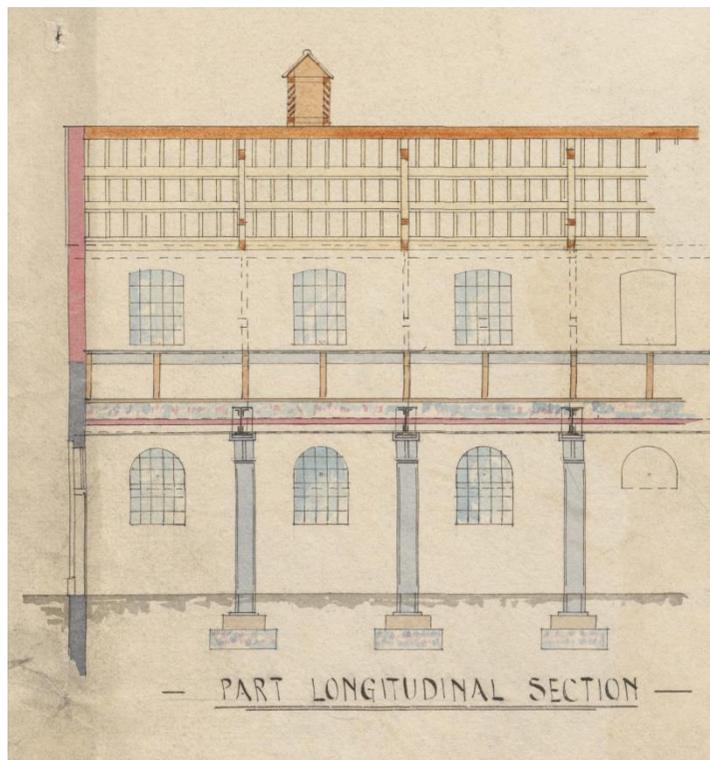


Figure 7 Detail of drawing. Long Stable - Longitudinal Cross Section.

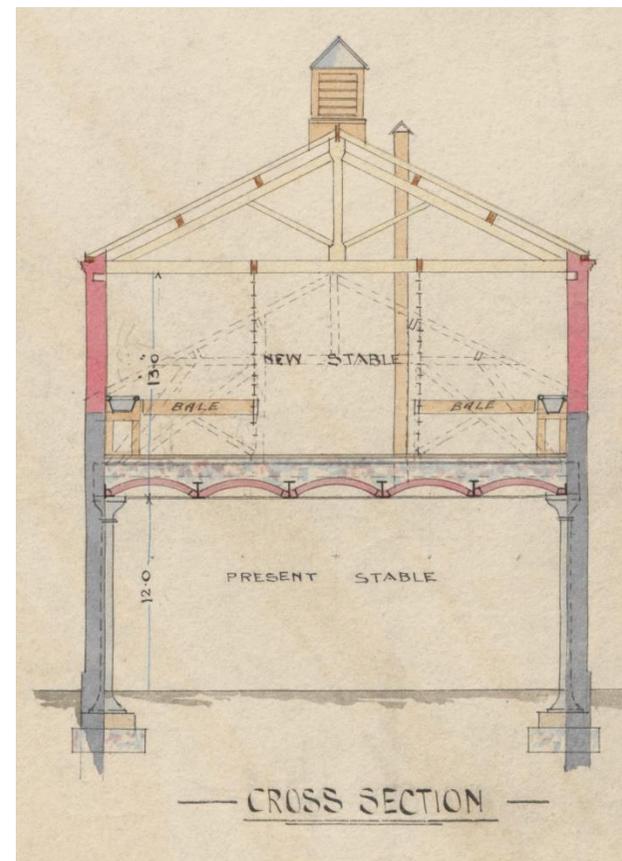


Figure 8 Detail of drawing. Long Stable - Cross Section.

4 PROPOSED WORKS

This chapter should be read in conjunction with the Design and Access Statement, architectural drawings and specifications prepared by KEANE.

The proposals seek to rationalise and improve the building's current existing state through reinstatement of original features where possible, repairs, new fittings and new lighting scheme for the restaurant/bar.

Layout:

Minor alterations to the layout are proposed, including the part removal of internal modern partition walls on the first floor, which will open up the space and give a better sense of the building's scale [figs. 11, 12 & 13]. The toilets will be reconfigured on the first floor to create a designated disabled WC and a female WC. The proposal also includes small storage cupboards adjacent to the existing lift at both ground and first floor levels and a new partition for a food finishing area at ground floor.

Replacements:

Part of the proposal is to replace the existing modern timber internal entrance doors. This aims to rectify the unauthorised works 'item no. 66' from Camden's 'Schedule of Unauthorised Works' (2013). The unauthorised works (item no. 66) is described as 'modern timber framed doors installed behind outer sliding doors' with required action to 'check record drawings and photographs for evidence of previous or original doors to inform on appropriate follow up action'. There are no historic records of the internal entrance doors, however, the proposal is to install black steel Crittall style doors, which are in keeping with the historic industrial character of the building and which have been

used throughout the market in other stables buildings.

There will also be a new timber floor installed on both levels. At ground floor, the new flooring will sit on battens to protect the original brick flooring.

Repairs:

The building will also undergo general repairs to the front and rear entrance elevations and internal brickwork. Following removal of the existing floorboards, repairs will be carried out to the brick flooring as necessary.

New Fittings:

Both ground and first floor will have new fitted furniture to accommodate the new seating arrangement in the bar and restaurant. The fittings will be a combination of upholstered and hard furnishings, which will include seating, retractable screens and radiator covers. The fitted furniture will be offset from the walls with fixtures on battens fixed to mortar joints only. This will prevent physical damage to original fabric. Most of the existing original walls will be left exposed, however, a new wall finishes are proposed to new partitions in the kitchen, WC and bar areas. These finishes include tiles, reclaimed scaffold board cladding, wall foliage, mirror and steel cladding with copper capping.

Lighting:

The range of proposed light fittings includes decorative pendants, suspended rods, wall mounted lights, strip LEDs and down lights. All ceiling lights are to be on track/cable tray including the store rooms, i.e. none will be directly attached to the existing ceilings. The downlights proposed in the first floor kitchen will be fixed to the existing, non-original suspended ceiling.

Services:

The works further include for the new installation of mechanical and electrical services, which comprises new switch sockets and power socket outlets and a new fire alarm system. The fire safety equipment will include extinguishers, alarms, fire escape notices, illuminated exit boxes and alarm panels.

Two new extractor fans are to be installed in the proposed 'food preparation' area on the ground floor. These will run through to the first floor and connect to existing ducts. The ducts will be boxed out with a fire-rated finish. This proposal will necessitate the removal of a small section on existing floor structure (removal of fabric: 450x450mm for each void).

The proposed services will work in line with the consented works (2015/4958/P, 2015/5826/L) for services in the Long Stable, which are as follows:

"All existing services are to be removed, including those cables and conduits currently fixed externally in an adhoc manner to the building's facades [...]. A new electrical mains distribution system (LV & ELV) will be routed within a horizontal compartment trunking system to run around the building internally at ground floor high level. The building will also be provided with internal small power services as required including BT, data and power points. A new gas and water supply will enter the building at the north east corner at ground level and rise to the first floor Plant Room. [...]"

Staircase

There is currently no internal staircase connecting ground and first floor. A new internal staircase is proposed, in order to improve the circulation and to encourage people to visit the upper floor of the listed building. It aims to create a circulation core that will allow for

the provision of future uses whilst improving circulation and means of escape in case of fire.

The new spiral staircase is located in the north-west corner, next to the west entrance and within end bay, positioned between the existing cast iron primary beams and structural elements [fig. 10].

Staircase structure is to be self-supporting from central column and constructed in steel with an aged effect black finish to resemble wrought iron. Handrails and treads are proposed to be cast black wrought iron. It will require the removal of two vaulted arch bays and 1no wrought iron secondary I-section beam (refer to architects and structural engineer's drawings for details).

The detail design is contemporary and such that the stairs will relate to the architectural and tectonic language being implemented by other works across the wider site.

Summary

The proposals considered together with the other previously consented works to the building will comply with the recognised objectives of the building's sustainable future.

The proposals aim to create a modern, versatile space that preserves the historic character of the building while also meeting the demands of the twenty-first century hospitality business.



Figure 9 Long Stable ground floor showing area opposite the existing lift.

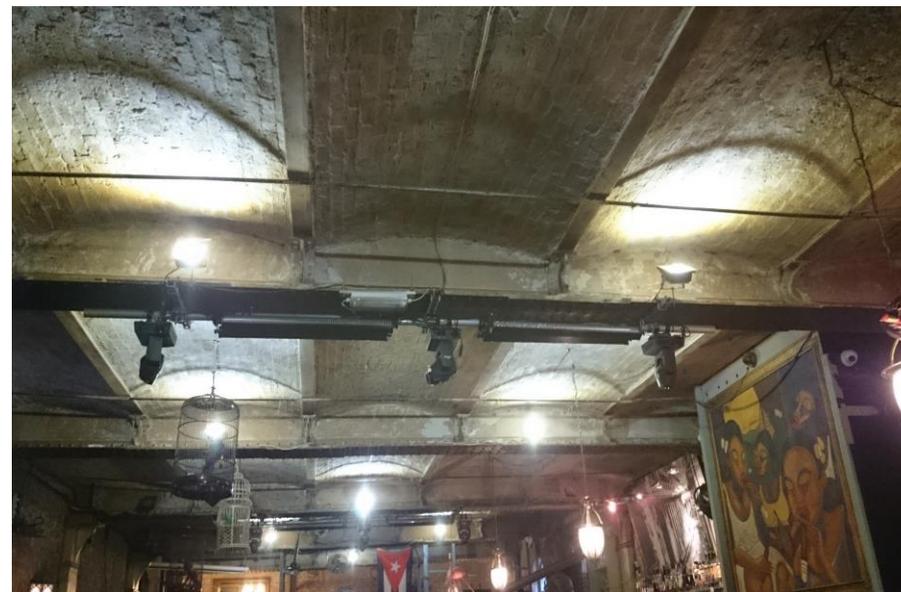


Figure 10 Long Stable ground floor showing typical structural bay.



Figure 11 Long Stable ground-floor partition walls to be removed.



Figure 12 Long Stable first-floor partition walls to be removed.



Figure 13 First-floor partition wall to be removed with insertion of new kitchen on the left.



Figure 14 Long Stable first floor showing bar to be replaced with kitchen area, retaining view of the exposed roof.



Figure 15 Long Stable ground-floor bar area to be extended.

5 IMPACT ASSESSMENT

Overall, the proposed works will have minor impact on the historic fabric, but they will also bring considerable public benefit. Very little original fabric will be removed, and generally, the proposals are passive in terms of physical impact on the building. The historic character of the building is maintained, leaving the brick walls, trusses on the first floor and arches on the ground floor exposed.

Layout:

The piecemeal arrangement of modern partitions will be removed, giving a better sense of the building's historic character. The new kitchen area on the first floor will be reduced in size and the existing view of the exposed roof structure will be retained.

Additional partitions are minor and do not detract from the building's historic character.

Replacements:

The new entrance doors are considered to be in keeping with the historic industrial character of the building and the site. Replacement of the timber flooring will provide a more aesthetically pleasing finish. Removal of the existing floor at ground floor level also will allow for the repair works to the original cobbled floor underneath.

Repairs:

Repairs to the original fabric will have a beneficial impact on the building and enhance the historic character.

New Fittings:

Combined with the internal reconfigurations and refit, which have a negligible impact on the building as they relate to contemporary finishes only, the proposed works will create a more sustainable use for the building while preserving its historical significance.

Lighting:

The current lighting scheme is an amalgamation of different light fittings of varying styles. The proposed lighting is a more holistic scheme that seeks to create a stronger visual connection through style and materials (in the bar and restaurant areas). Impact to the historic fabric is negligible, as the fittings will all be on a track/cable trays or fitted within existing, non-original suspended ceilings.

Services:

The extraction has been split into two smaller ducts, rather than one large, in order to minimise the impact on the historic fabric.

Two proposed voids (450x450mm each) will require minimal removal of the historic fabric and are supported by the structural engineer's drawings and statement. The ventilation system in the proposed 'food preparation' area is an operational requirement that will facilitate an improved use of the building, securing its longevity. At first floor level, the ducts will be located above the existing trusses and have minimal visual impact. Other proposals such as the fire safety equipment will have negligible impact, as they are mostly located within new fabric. The optimised and long term use of the space mitigates the minimal loss of fabric.

Staircase:

The staircase proposals have been discussed with the Conservation Officer [CO] (Catherine Bond) and feedback received in an email dated 12 January 2017, where the CO agreed to a proposal for the new internal staircase in principle, *'provided that the design is discreet and involves minimum disruption to historic fabric'*.

Alternative (linear plan form) options have been considered for the proposed staircase, one dog-leg staircase and one a straight flight along the north wall. However, these options are considered to be unfeasible due to two major constraints: the exposed structural beam and the existing electricity box in the north-west corner.

Regarding the structural constraint, both options would require part removal of the primary steel beam (refer to drawing in Appendix 3), which would have significant structural implications.

The relocation of the existing electricity box to provide more space for a dog leg design was considered, but, as this box provides for the entire Triangle site, its relocation would require considerable intervention and further alterations to existing fabric.

However, even if it would be practicable to remove the electricity

box the dog-legged (or indeed the straight flight) staircase would still not have enough clearance to reach the first floor level without the part removal of the primary beam.

The spiral staircase is therefore proposed as the only viable and practical option that will not require relocation of the electricity box (that serves the whole triangle site) or removal of the primary beam and that will have the least impact on the existing building fabric.

It will penetrate through two of the brick structural arches and require the removal of limited areas of the existing brick and timber upper floor. This will enable more efficient circulation and significantly improve fire safety by an additional means of escape.

Summary

It is considered that the proposals collectively constitute 'less than substantial harm' to the heritage asset and that these impacts will be outweighed by the public and heritage benefits of the proposals, particularly in regard to the repair works and improved fire safety and the overall resultant improvements to the building's interior character and appearance; all thus having a highly positive impact overall.

There will, therefore, be public benefit accruing from the proposal not only in the greatly improved functionality and use of available internal space within the building but also in the provision of a sustainable solution for future market needs and expectations.

6 NPPF CONSIDERATIONS

All policies in the NPPF adopted 27th March 2012 constitute the government's view of what sustainable development means in practice. The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions.

"Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy." (NPPF Paragraph 7).

This statement deals principally with Section 12 of the NPPF, "Conserving and enhancing the historic environment", however Heritage considerations and issues are prevalent throughout the framework.

Paragraph 6 of the NPPF states: "The purpose of the planning system is to contribute to the achievement of sustainable development. The policies in paragraphs 18 to 219, taken as a whole, constitute the Government's view of what sustainable development in England means in practice for the planning system." The entire Framework constitutes a definition of "sustainable development" with no one part assuming greater weight than any other.

NPPF Paragraph 128 states that "In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contributions made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on

that significance".

Furthermore, **NPPF Paragraph 129** states that 'Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this assessment into account when considering the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposal'.

As recommended in Paragraphs 128 and 129, an assessment of the significance of the heritage asset has been provided earlier in this report.

This report aims at fulfilling the National Policy requirement for provision of proportionate information which will enable the assessment of the likely impacts of proposed development on the special historic and architectural interest of the Provender Store by the Local Planning Authority. The significance assessment was based on historic research and consultation of relevant historic records and was examined according to the criteria set out in English Heritage's 'Conservation Principles'.

The research and assessments that have been carried out are believed to be "sufficient to understand the potential impact of the proposal on the significance of the heritage asset."

Paragraph 131 states: 'In determining applications, local planning authorities should take account of:

- The desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- The positive contribution that conservation of heritage

assets can make to sustainable communities including their economic vitality; and

- The desire of new development making a positive contribution to local character and distinctiveness’.

‘Conservation’ is defined in the NPPF only for heritage policy as: ‘the process of maintaining and managing change to a heritage asset in a way that sustains and, where appropriate, enhances its significance’.

Paragraph 134 states: ‘Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including its optimum viable use’.

As set out in Section 5 of this report, it is contended the proposed works to the Long Stable will enhance the listed building significance and that of the stables complex at large and provide public and heritage benefit. It is considered there is ‘less than substantial harm’ involved in the installation of the stairs, the new kitchen area, the removal of modern internal partitions and the internal refit on both floors.

The established market use of the former stables is not challenged by the proposals; the works will improve not only the general appearance of the building but its use and circulation thus providing a sustainable solution.

The public benefit the proposal provides is in preserving and improving not only the listed building but also the already established "optimum viable use" for the site, and therefore, the enhancement of the wider historic environment in general.

The proposal will aid in safeguarding the continued use and long term preservation of the building. It is therefore concluded that the significance of the heritage asset will be preserved and enhanced

in compliance with the NPPF requirements.

7 LOCAL PLANNING POLICY

Camden Local Development Framework, Camden Core Strategy, 2010 -2025, Adopted Version, November 2010.

The Camden Core Strategy Policy CS14 – ‘Promoting high quality places and conserving our heritage’, sets out the requirements to safeguard Camden’s heritage. The overall strategy is to sustainably manage growth in Camden in a way that conserves and enhances the heritage and valued places that give the borough its unique character.

Camden Planning Guidance provides advice and information on how the Local Authority applies its planning policies. The guidance is consistent with the Core Strategy and the Development Policies, and forms a Supplementary Planning Document (SPD) which is an additional “material consideration” in planning decisions.

CPG 1- ‘Design’; deals with heritage issues in Section 3. This section sets out further guidance on Core Strategy Policy CS14- ‘Promoting high quality places and conserving our heritage’, and Development Policy DP25- ‘Conserving Camden’s Heritage’.

Paragraph 3.22 refers to the statutory requirement, when assessing applications for listed building consent, to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

It is considered that the proposal not only preserves but enhances the character of the historic environment through the improvement

of circulation and use of available space throughout the Long Stable.

Paragraph 3.23 expands on the desirability to retain original or historic features and to carry out repairs in matching materials. The proposals should seek to respond to the special historic and architectural constraints of the listed building, rather than significantly change them.

Proposals that will necessitate removal of original fabric have been limited to areas of the building that minimises potential impact and preserves primary structural elements and features of significance. It is considered that the proposals will preserve the overall character and appearance of the building and its surroundings with a sympathetic contemporary intervention.

Development Policy DP25 – ‘Conserving Camden’s Heritage’.

The works are considered to preserve and enhance the essential character and appearance of the Long Stable and the surrounding conservation area.

8 CONCLUSION

The Long Stable building is a listed Grade II building that is included in the Heritage at Risk Register. The building is open to the public and in a continuous state of flux. The building has been subject to change and in order to remain relevant and sustainable will continue to change and evolve. The proposed scheme is driven by a need to not only conserve and enhance the building, but also to ensure a sustainable solution that safeguards the current and future use of the market.

The proposed works will allow an internal layout that will facilitate the optimum viable use of the building, improving the circulation

pattern throughout the building. The historic character and architectural importance of the heritage asset will be preserved and enhanced safeguarding its long term conservation. It will also improve its internal appearance and ease of use, encouraging value and responsible care for the building.

This proposal complies with policy at the heart of the NPPF in respect of sustaining and enhancing not just the building itself but the significance of the Regent’s Canal Conservation Area and the heritage assets within and in the vicinity of the site.

The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions. Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy (NPPF Para 7).

For the above reasons, it is considered that the proposal would be acceptable in the context of the setting of the heritage assets in the immediate surroundings and the conservation area. It is therefore concluded that the proposed works satisfy the relevant clauses of the NPPF as detailed above and is consistent with the spirit of local policies and national conservation principles.

APPENDIX I LISTED BUILDING DESCRIPTION

List entry Summary

List entry Number: 1258101

Grade: II

Four blocks of industrial stabling, now workshops and warehousing. c1855-1870, with later Victorian additions. For the London and North-Western Railway Company's Camden Goods Yard. Stock brick, with hipped slate roofs, some stone lintels. Some iron columns internally, but floors and roofs generally of timber. EXTERIOR: mostly of 2 storeys.

Northernmost block (A) abutting on Chalk Farm Road, c1855, with upper storey of c1895. Long curved front road, mostly of 2 storeys with eaves cornice but western end of one and a half storeys, somewhat altered, with chimney on roof. Round-headed half windows for stabling on ground storey, segment-headed industrial windows in upper storey (eastern end only). Elevation towards yard irregular. Eastern portion has cantilevered open balcony at first-floor level retaining some concrete horse troughs and connected by bridge to Block B and separately to ramp on Block C. Ceilings of ground storey have jack-arch iron and brick construction. INTERIOR of upper storey has separate compartments and paving for horses but no stalls.

Block B immediately to north of North London railway line. 3 storeys. Ground storey c1868, originally provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A.

Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A. Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper storey 1881; horse ramp on north side of block c1895, connected with balcony on Block A. Round-headed windows on ground storey, segment-headed industrial windows above on both north and south sides. South side formerly had another horse ramp, of 1881, and covered bridge connecting with Block B, demolished in 1980s. Ground storey has iron and brick jack-arch construction and iron stanchions against walls stamped 'Norton and Son Darlaston'.

Block D at right-angles and to west of Blocks B and C. 2 storeys. Ground storey c1868, upper storey c1881. Main elevation faces eastwards, with return northwards. Round-headed half-windows for stabling at ground level, some segment-headed sash windows above. Tall brick chimneys. INTERIOR with original timber benching, one timber partition and some harness hooks. Said to have been formerly the Tack Room for the stabling. Formerly connected by a bridge at south end to Block B. Included as a rare example of substantial industrial stabling and a major surviving portion of the former Camden Goods Yard. Forms a group with the 'Horse Hospital' to north-west (qv) and with further remnants of stabling and warehouses west of Block D (qv). A tunnel (now blocked) south of the North London line connects the complex with further LNWR buildings and the Regent's Canal south of the North London Line.

Listing NGR: TQ2862684201

APPENDIX II PLANNING APPLICATION SUMMARY

Development Description	Date Granted	Date Registered	Planning ref.	LBC ref.
Replacement of external stair, link and canopy to Long Stable Building.	10.08.2016	07.07.16	2016/3673/P	2016/3779/L
Works to Cuban Building including installation of mechanical and electrical services, refurbishment of roof, replacement of public toilets and installation of 4x CCTV units to east and west elevations.	06.11.2015	15.10.2016	2015/4958/P	2015/5826/L
Refurbishment of Long Stables building staircase and Chalk Farm building walkway.	06.11.2015	15.10.2015	2015/4960/P	2015/5825/L
Replacement windows and associated works.	08.12.2015	28.09.2015	2015/3775/P	2015/5483/L
Alterations in connection with replacement doors and associated works.	08.12.2015	28.09.2015	2015/3777/P	2015/5484/L

