

Construction Management Plan

London Borough of Camden

147 Kentish Town Road, NW1 8PB

Please list all iterations here:

4th Oct '16	No.1	Hugh Kelly
19th Oct'16	No.2	Hugh Kelly
28th Oct'16	No.3	Hugh Kelly

Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: 147 Kentish Town Road, London, NW1 8PB

Planning ref: 2014/5900/P - Original Planning application. APP/X5210/W/15/3004348 - Appeal Documents

Type of CMP – Condition discharge / Section 106 planning obligation / Major sites framework

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Hugh Kelly – Kelly Contractors UK Ltd

Address: Unit 54c Thames Industrial Park, Princess Margret Road, East Tilbury, RM18 8RH

Email: hugh@kelly-contractors.co.uk

Phone: 01375840586

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Kieran Flaherty

Address: Unit 54c Thames Industrial Park, Princess Margret Road, East Tilbury, RM18 8RH

Email: kieran@kelly-contractors.co.uk

Phone: 07557301509

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of [Community Investment Programme \(CIP\)](#), please provide contact details of the Camden officer responsible.

Name: Kieran Flaherty

Address: Unit 54c Thames Industrial Park, Princess Margret Road, East Tilbury, RM18 8RH

Email: kieran@kelly-contractors.co.uk

Phone: 07557301509

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Hugh Kelly

Address: Unit 54c Thames Industrial Park, Princess Margret Road, East Tilbury, RM18 8RH

Email: hugh@kelly-contractors.co.uk

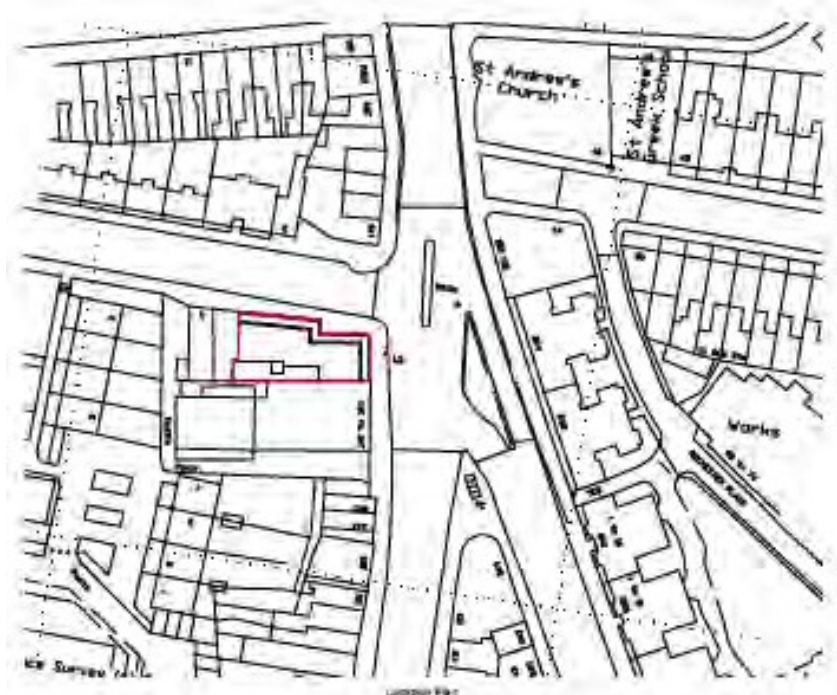
Phone: 07767227097

Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

The project is located at 147 Kentish Town Road and is accessed from Castle Road via Kentish Town Road. The site is in a mix of commercial businesses along Kentish Town Road and a prominent residential area, it is bordered by Kentish Town Road to the front and Castle street to the side, the other two boundaries are the next door business and to the rear car park to adjoining business premises.

The construction work at 147 Kentish Town Road involves the demolition of the internal structure to the old building whilst retaining the street facades. The basement and ground floor will be developed into commercial use areas whilst the upper two floors will be residential accommodation per the details of the planning drawings. The basement will be extended to underneath the existing garden area formed with reinforced concrete retaining walls.



7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

The works involve formation of new offices and apartments within the existing structure at the front and rear of the property. The works involve the erection of a steel structure consisting of basement and ground floor offices and 8 no. apartment dwellings.

8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

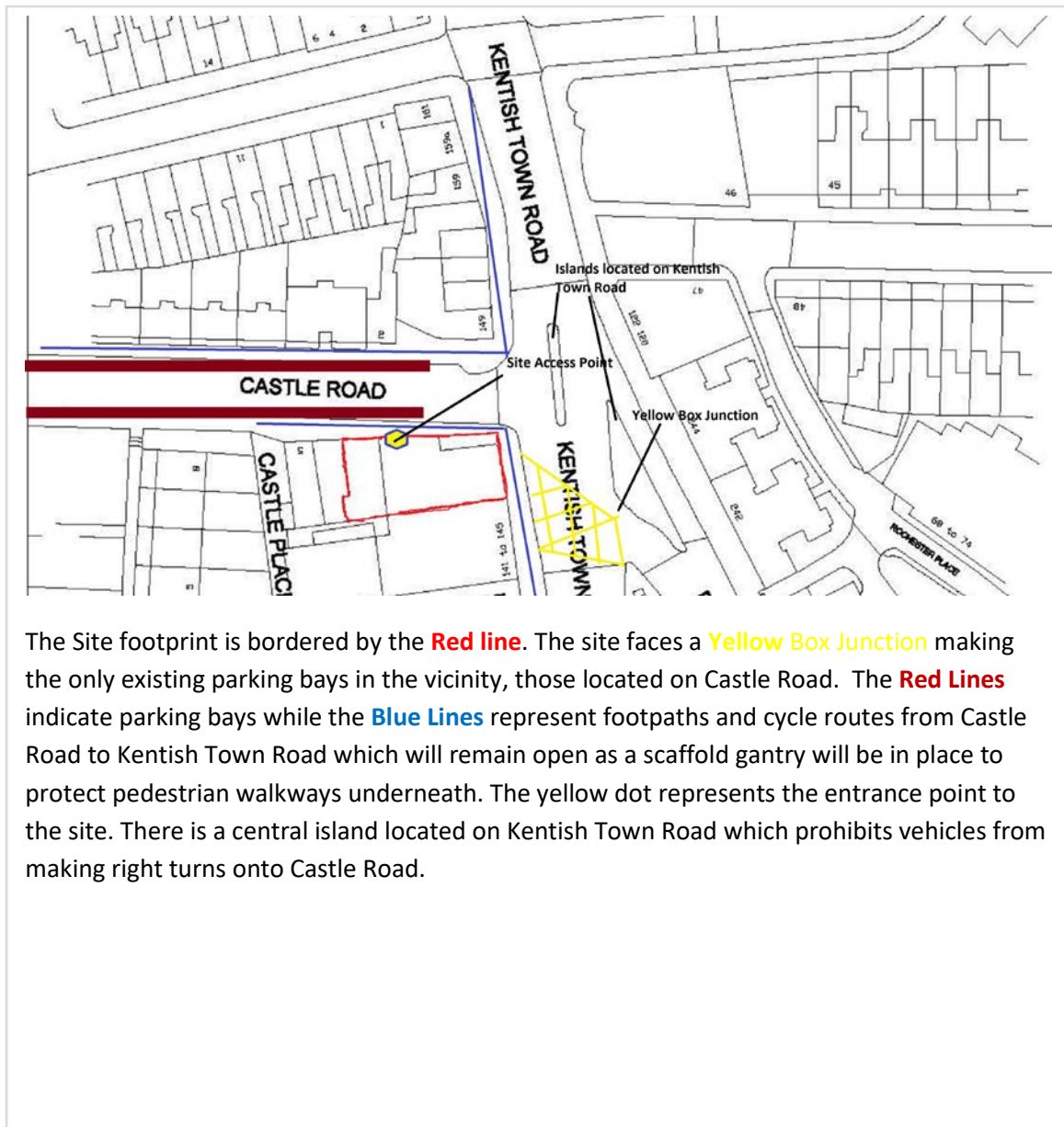
Nearest Potential Receptors likely to be affected by the Activities on Site

There is a commercial shop called Cash Converters 141-145 Kentish Town Road located next door to 147 Kentish Town. To the rear of the site along Castle Road is a block of houses (Nr. 3 & 5 Castle Road) with two vacant retail units on the ground floor level with a block of houses/apartments opposite. Levertson & Sons LTD, a funeral home is located opposite to the site on the corner of Kentish Town Road/Castle Road while directly across from 147 Kentish Town Road are multiple blocks of flats.

Noise generated by the construction process will be considered and its impact on neighbouring properties mitigated. Suitable mitigation measures to be used include:

- Standard construction hours.
- The use of quieter alternative methods or mechanical plant, where reasonably practical.
- Locating plant, equipment, site offices, storage areas and worksites away from neighbouring properties where reasonably practical.
- Machines and equipment, in intermittent use will be shut down or throttled down to a minimum when not in use;
- The use of site hoardings or portable acoustic enclosures/screens where practical.
- Maintaining and operating all vehicles, plant and equipment such that extraneous noise from mechanical vibration, creaking and squeaking is kept to a minimum.
- All temporary site lighting will be faced into the site, and not directed towards any neighbouring properties.
- During works the main air pollution emissions are the dust generated when building materials are broken up and the fumes from machinery. Kelly Contractors will use high pressure hoses to saturate all bulk materials with water during the process and whilst loading the waste materials for disposal. Machinery exhaust emissions will be kept as low as is practical by using well maintained vehicles and machinery at all times.
- Hoarding will be erected around the site. Along with reducing the visual impact and providing protection for the construction workers and public, this will also act as a barrier for dust and dirt originating from within the site.
- A monarflex sheeting will enclose the scaffolding which will mitigate against dust and noise.
- All HGV's removing spoil from the site will be fully sheeted to minimise the risk of any mud over spilling onto the highway. The excavation is being loaded directly from conveyors into an enclosed skip on the road and then taken out of the skip via a grab lorry. The roadways will be kept clean at all times.
- Kelly Contractors will ensure that the area around the site including the public highway is regularly and adequately swept to prevent any accumulation of dust and dirt.

9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.



The Site footprint is bordered by the **Red line**. The site faces a **Yellow Box Junction** making the only existing parking bays in the vicinity, those located on Castle Road. The **Red Lines** indicate parking bays while the **Blue Lines** represent footpaths and cycle routes from Castle Road to Kentish Town Road which will remain open as a scaffold gantry will be in place to protect pedestrian walkways underneath. The yellow dot represents the entrance point to the site. There is a central island located on Kentish Town Road which prohibits vehicles from making right turns onto Castle Road.

10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal).

Please see attached the programme for the works in Appendix A.

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

Kelly Contractors working hours will be

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

Thames water, UKPN, BT – Kelly Contractors intend to discuss installation dates with the utilities suppliers, agree trenching details with them and coordinate installation dates. UKPN have already stated the main power supply coming into the site might be sufficient but tests to the power supply still need to be done. Confirmation on the pathway of main power supplies still need investigation and confirmation on route. After this information has been received a full drawing will be issued as addendum to this CMP.

13. Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

A public meeting was held at the Congregational Church Hall in Kelly Street on Thursday 20th October at 7pm. Kelly Contractor's 2 Company Directors were in attendance in addition to the appointed architect on the Scheme. At this meeting the site manager and architect discussed the contents of the CMP with residents, who expressed satisfaction with proposals being brought forward.

There was a general concern amongst residents in regards to the noise and dust pollution of the works to be carried out. Kelly Contractors has assured residents that the necessary implementations will be made to mitigate Dust and noise pollution while monitoring such adequately which is laid out in this CMP. A long period of the meeting was therefore spent outlining these mitigation methods and pointing out to residents the noise/dust pollution expected.

Once the main build works commence a Contact Board will be displayed prominently outside the site office at 3 Castle Road. We will ensure that any concerns that the residents may have will be resolved. At the meeting we made it clear to the residents that we were available at all times. They expressed gratitude that they are constantly being updated on the site progress. Another local residents meeting will be arranged once more after works on site begin to give residents an opportunity to discuss any of their concerns.

Please see attached flyer which is currently posted up outside the site in Appendix C.

14. Construction Working Group

Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works, the way in which the contact details of the person responsible for community liaison will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

Kelly Contractors will provide a detailed newsletter 14 days prior to construction commencement on site. We will also provide a newsletter every 8 weeks after the first newsletter has been produced. We will also attend meetings with the residents and business associations, as appropriate.

15. Schemes

Please provide details of any schemes such as the 'Considerate Constructors Scheme', such details should form part of the consultation and be notified to the Council. Contractors will

also be required to follow the "[Guide for Contractors Working in Camden](#)" also referred to as "[Camden's Considerate Contractors Manual](#)".

Kelly Contractors have registered the project with Considerate Construction Scheme, CLOCS, and Guide for contractors working in Camden.

The site's Considerate Constructors Scheme Site Reference is 55877.

The works will be carried out in accordance with the Considerate Constructors Scheme and in such a way to minimize the impact on the local environment and amenities.

16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

We have reviewed the traffic route and are not aware of any other known developments occurring within our construction phase.

17. Name of Principal contractor:

Kelly Contractors UK Ltd

Unit 54c Thames Industrial Park, Princess Margaret Road, East Tilbury, RM18 8RH

Email: hugh@kelly-contractors.co.uk

18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our [CLOCS Overview document](#) and [Q18 example response](#)).

Contracts

Kelly Contractors UK LTD will only enter a contractual agreement with FORS bronze accredited operators or equivalent. Hauliers companies/Operators will be required to submit along with quotes, a copy of their FORS Bronze accreditation for consideration. All appointed operators must prepare a written assurance that they will comply with the following:

- ✓ Hauliers companies/Operators must operate on routes approved by London Borough of Camden.
- ✓ Hauliers companies/Operators must have additional safety equipment on vehicles over 3.5t.
- ✓ Drivers must receive additional training on Safe Urban Driving, E-Learning, Van Smart, Cycle Awareness, and Vehicle Safety Equipment. They are required to present certificates as proof that they have completed training.
- ✓ Drivers will be required to present their driver's license to Kelly Contractors prior to deliveries for driver's license check.
- ✓ Operators are required to report, investigate and analyse collisions.

Desktop Check

Kelly Contractors UK LTD will perform checks against the FORS database of trained drivers and accredited companies along with requesting certificates as our method of due diligence. This will be carried out per risk scale based to ensure we are in compliance with CLOCS managing supplier compliance guide.

Site

Kelly Contractors will clearly label access and egress points to the site to assist operators/employees with commuting to and from the site. A banksman will be present to control the movement of vehicles entering and exiting the site and will also be given the responsibility of operating a delivery book system. This Delivery book system requires the operators to provide their FORS ID number and reason for entering the site before being booked onto the site.

Offloading and unloading will be done on site as much as possible and the site manager will ensure this process is done in compliance with the CMP.

19. Please confirm that you as the client/developer and your principal contractor have read and understood the [CLOCS Standard](#) and included it in your contracts. Please sign-up to join the [CLOCS Community](#) to receive up to date information on the standard by expressing an interest online.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers:

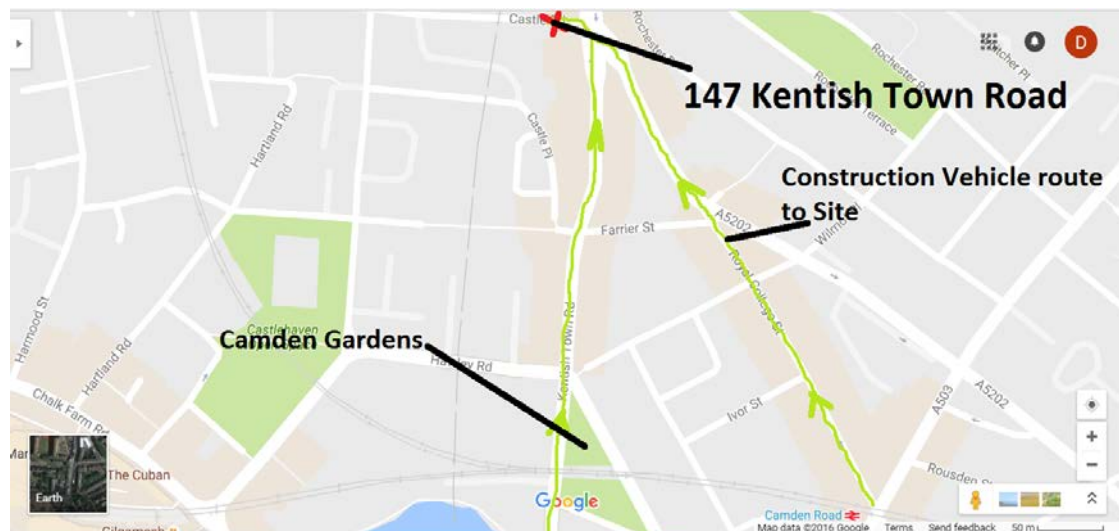
We can confirm that we have included the requirements to abide by the CLOCS Standard in our contracts to our contractors and suppliers.

20. Traffic routing: *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

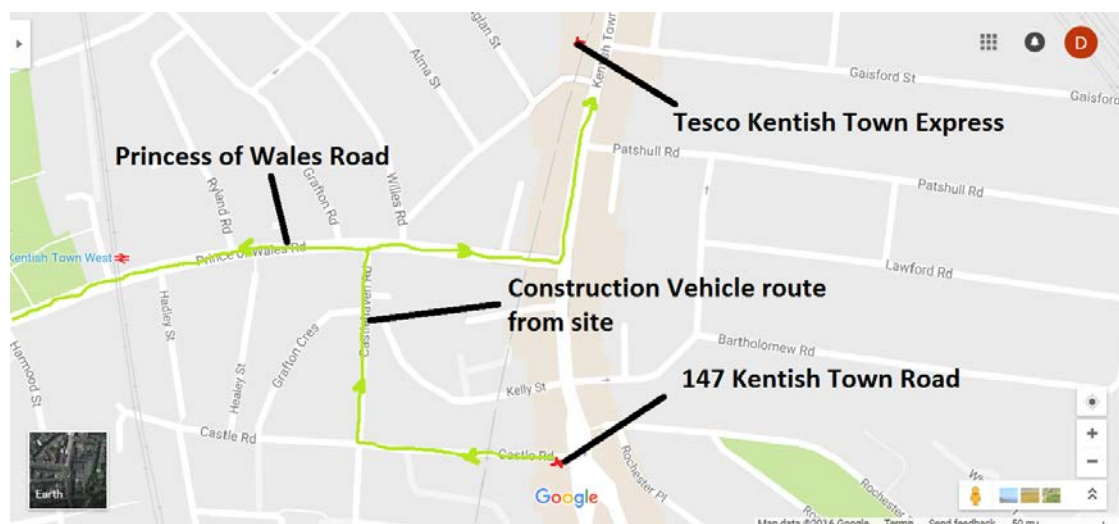
Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of links to the [Transport for London Road Network](#) (TLRN).



Above is a map illustrating the proposed route construction vehicles will take to site. Vehicles will need to approach the site from the south which is Kentish Town Road coming from Camden gardens where they are then required to turn onto Castle Road to access the site.



The above map illustrates the suggested routes for construction vehicles to take once leaving the site. It's suggested that they travel northbound by exiting Castle road where they take the right onto CastleHaven road. Two routes are outlined above; they can follow the road towards Tesco Kentish Town Express or make the left from CastleHaven Road onto Princess of Wales Road.

b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

On a weekly basis the Logistics Manager will evaluate details of the daily profile of deliveries proposed for the upcoming week.

Hauliers will be required to contact the site on a daily basis and indicate their delivery schedule for the following day. The proposed deliveries will be checked against the weekly delivery schedule. This will be overseen by the Logistics Manager to ensure deliveries are controlled and vehicles are not waiting on local Roads, thereby ensuring that there is always space at the site to accommodate the necessary plant and deliveries.

Sufficient time will be given between deliveries to allow for any delays as a result of the delivery vehicle getting stuck in traffic or the loading/unloading taking longer than expected and to avoid any vehicles waiting on the surrounding highway network.

Use of the agreed vehicle routes shall be included as a contractual requirement of the Sub-Contractors and will be communicated to all individuals associated with the works. It is envisioned that this information will be communicated in the form of a leaflet or email and will include information with regard to times of operation, delivery routes, the call up procedure and delivery slot information. Visitors to site will be made aware of local transport trains, buses and the main route if driving. We will endeavour to stress that they use public transport to prevent any added traffic to the local area.

21. Control of site traffic, particularly at peak hours: *"Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries"* (P20, 3.4.6)

Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the [Guide for Contractors Working in Camden](#)).

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work,

including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.

Kelly Contractors are aware that the site is located in one of Camden Councils town centre areas. Kentish Town Road is a busy cycle and pedestrian route and one of the busiest traffic corridors in the borough. Various schools are located nearby. Construction Vehicle movements therefore will be scheduled to take place between 0930 and 1500 hours on Monday to Friday and between 0800 and 1300 hours on Saturdays.

Typical sizes of Vehicles and approximate frequency and times of day they will need to access the site.

Stage 1 Basement Excavation - There will be a maximum of 5 lorry movements per day. Lorries will enter the site and remove the fill via a grab lorry. Once the excavation works are complete to the front if the property works will commence on the basement works to the rear. A enclosed conveyor will be put in place with a 16 yard will be place on two parking bays suspended along Castle Road in front of the property. Earth will go into the skip via a conveyor and be taken away via a grab lorry. A banksman will be on hand at all times.

Stage 2 Steel Erection - there will be a maximum of 6 visits in total. It is planned that the road will be closed along Castle Road for the steel erection.

Stage 2 and 3 Concrete Pours - 4 concrete lorries per day, pumping of concrete will be done from the front entrance of the site, we will be able to hold the pump and concrete vehicles within the hoarding line loading bay.

Stage 4 Deliveries for residential

and office fit outs.

- Debris/ rubble/ waste 16 yard skips
- Muck away Lorry 9m x 2.5m (Dwell time 30min per load)
- Concrete Lorry 9m x 2.5m (Dwell time 20min per delivery)
- Mobile crane 12.300m x 2.430m Dwell time 8hours)
- Delivery vehicle type 1 5m x 2.15m (Dwell time 20min to 1hour)
- Delivery vehicle type 2 7m x 2.15m (Dwell time 20min to 1hour)
- Delivery vehicle type 3 10m x 2.500m (Dwell time 20min to 1hour)
- Delivery vehicle type 4 14.154m x 2.520(Dwell time 20min to 30min)

We have reviewed all deliveries and will maintain a clear path down Castle Road, with the exception of the date of the aforementioned proposed road closure for the steel erection.

We have reviewed the traffic route and are not aware of any other known developments occurring within our construction phase.

b. Please provide details of other developments in the local area or on the route.

We have reviewed the traffic route and are not aware of any other known developments occurring within our construction phase.

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

On a weekly basis the Logistics Manager will evaluate details of the daily profile of deliveries proposed for the upcoming week.

Hauliers will be required to contact the site on a daily basis and indicate their delivery schedule for the following day. The proposed deliveries will be checked against the weekly delivery schedule. This will be overseen by the Logistics Manager to ensure deliveries are controlled and vehicles are not waiting on local Roads, thereby ensuring that there is always space at the site to accommodate the necessary plant and deliveries.

Sufficient time will be given between deliveries to allow for any delays as a result of the delivery vehicle getting stuck in traffic or the loading/unloading taking longer than expected and to avoid any vehicles waiting on the surrounding highway network.

Use of the agreed vehicle routes shall be included as a contractual requirement of the Sub- Contractors and will be communicated to all individuals associated with the works. It is envisioned that this information will be communicated in the form of a leaflet or email and will include information with regard to times of operation, delivery routes, the call up procedure and delivery slot information. Visitors to site will be made aware of local transport trains, buses and the main route if driving. We will endeavor to stress that they use public transport to prevent any added traffic to the local area.

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

N/A

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

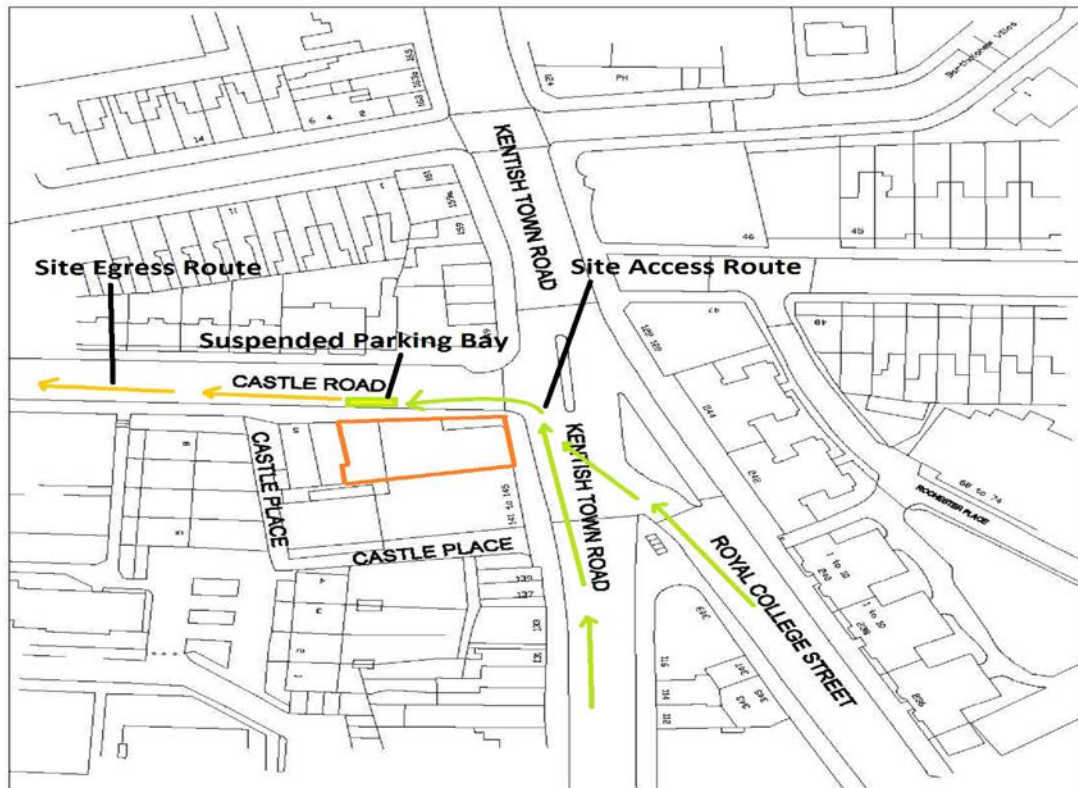
N/A

22. Site access and egress: *“Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.”* (P18, 3.4.3)

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and other traffic when vehicles are entering and leaving site, particularly if reversing.

a. Please detail the proposed access and egress routes to and from the site

Green lines indicate the proposed routes to access the site.
Orange lines indicate the proposed routes to exit the site



b. Please describe how the access and egress arrangements for construction vehicles will be managed.

The site is located in one of Camden Councils town centre areas. Kentish Town Road is a busy cycle and pedestrian route and of the busiest traffic corridors in the borough. Various schools are located nearby.

Construction vehicle movements will be scheduled to avoid peak periods. Construction vehicle movements will therefore need to be rescheduled to take place between 0930 and 1500 hours on Monday to Friday and between 0800 and 1300 hours on Saturdays. All construction vehicle movements and all loading/unloading activity in the general vicinity of the site will be supervised by a suitably competent banksman or traffic marshal.

In conjunction with the above we will follow the following policy set out below:

On a weekly basis the Logistics Manager will evaluate details of the daily profile of deliveries proposed for the upcoming week. Hauliers will be required to contact the site on a daily basis and indicate their delivery schedule for the following day. The proposed deliveries will be checked against the weekly delivery schedule. This will be overseen by the Logistics Manager to ensure deliveries are controlled and vehicles are not waiting on local Roads, thereby ensuring that there is always space at the site to accommodate the necessary plant and deliveries.

Sufficient time will be given between deliveries to allow for any delays as a result of the delivery vehicle getting stuck in traffic or the loading/unloading taking longer than expected and to avoid any vehicles waiting on the surrounding highway network.

Use of the agreed vehicle routes shall be included as a contractual requirement of the Sub-Contractors and will be communicated to all individuals associated with the works. It is envisioned that this information will be communicated in the form of a leaflet or email and will include information with regard to times of operation, delivery routes, the call up procedure and delivery slot information. Visitors to site will be made aware of local transport trains, buses and the main route if driving. We will endeavour to stress that they use public transport to prevent any added traffic to the local area.

c. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).

Hauliers/Operators will be required to turn onto Castle Road where the parking bays beside the site will be suspended. There are no tight manoeuvres on vehicle routes to and from the site. Vehicles will not be required to turn around on Castle Road. Vehicles will pull into the suspended parking bays beside the site along Castle Road. All construction vehicle movements and all loading/unloading activity in the general vicinity of the site will be supervised by a suitably competent banksman or traffic marshal. Entry and exit routes will be via either Castle Road or Kentish Town Road.

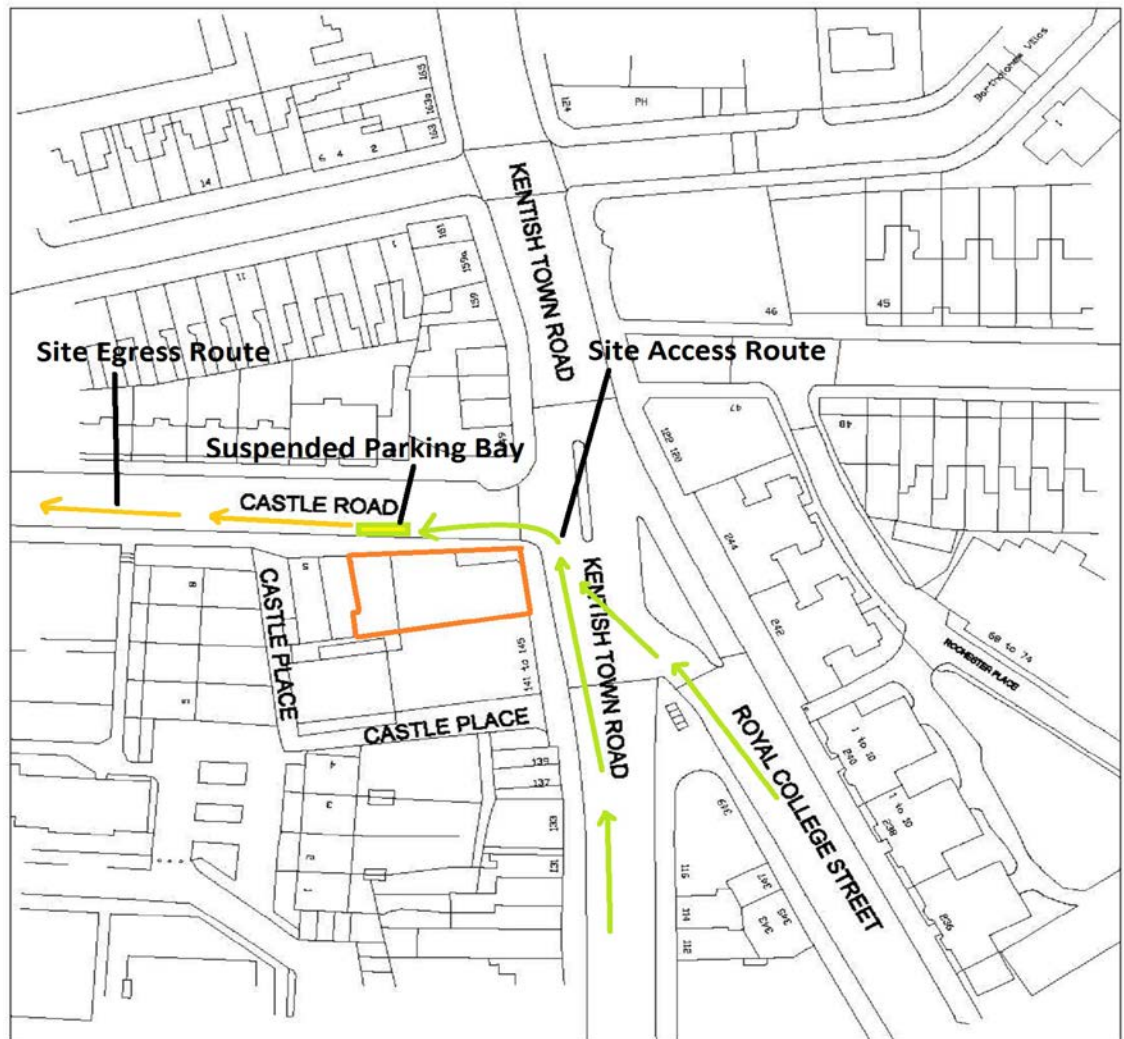
d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled.

- All HGV's removing spoil from the site will be fully sheeted to minimise the risk of any mud over spilling onto the highway. A wheel-washing facility will be provided, as required, for the duration of the construction works to ensure the levels of soil on roadways near the site are minimised. The wheel-washing facilities will be in the form of a hose down point located adjacent to the entrance. The excavation is being loaded directly from conveyors into the skip which will be grabbed into the lorry. So the wheel washing requirement is minimised, any overspill will be washed off the Road surface.
- Kelly Contractors will ensure that the area around the site including the public highway is regularly and adequately swept to prevent any accumulation of dust and dirt.

23. Vehicle loading and unloading: *"Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable."* (P19, 3.4.4)

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required.



This diagram shows access onto site and loading and unloading areas. The parking bays adjacent to the site will be suspended parking and will be used to for unloading and loading during the construction life as explained in the diagram above.

A banksman will be overseeing all traffic, along with a logistics manager on all deliveries. This should not affect pedestrian and cyclist safety. Castle Road is a side street which has relatively low levels of pedestrian traffic. We are also segregating pedestrians from site traffic. We have diverted the footpath to allow access to Castle Road. Kelly Contractors will install all safety signs on hoarding and on foot paths to show clear and safe access routes to site.

Access onto site will be monitored through swipe cards which will allow only approved site personnel access. The access route to divert pedestrian to the footpaths/pavement will all be pre-agreed with Camden and meet the traffic act code of practice. We have taken into account the existing drop-down curbs to allow access for wheelchair users, individuals with walking impairment, young children, prams, blind and partially sighted people. We are maintaining existing routes along Castle Road.

24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. **Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.**

Information regarding parking suspensions can be found [here](#).

We propose to get the two bays outside the entrance of 147 Kentish Town Road along Castle Road suspended for the works on site. Kelly Contractors will apply for the required amount of Parking Bays 17 days in advance of any required suspension as stated within Camden suspension parking requirement. It may be more effective to apply for the parking bays to be suspended via a temporary traffic restriction application. A minimum of 2 bays will be required throughout the project. General unloading will take place from a suspended bay in front of the project gates and Banksman will ensure that members of the public are kept away by use of barriers if required. Refer to diagram in Appendix F which note proposed bays to be suspended.

25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

- a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).

Construction of temporary vehicular accesses will not be required. Construction Vehicles will not be required to reverse into the site.

b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

N/A

26. Diversions

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

N/A

27. VRU and pedestrian diversions, scaffolding and hoarding

Pedestrians and/or cyclist safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc.

A secure hoarding will generally be required at the site boundary with a lockable access.

a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.



- Existing Scaffolding to full height of building enclosed in monarflex sheeting
- 2.4m High ply Hoarding painted in Camden approved colours

The site area and the contractor's compound will be enclosed with a 2.4m high metal or wooden hoarding. This will be adapted as necessary, and will be painted as agreed with the LBC. Hoarding panels would be maintained and kept clean for the duration of the project. Vehicle deliveries to the site will be from Castle Road. Deliveries will be controlled by our banksman who will traffic manage delivery vehicles and ensure that the carriageway is not blocked during unloading operations.

The banksman will be overseeing all traffic, along with a logistics manager on all deliveries. This should not affect pedestrian and cyclist safety. Castle Road is a side street which has relatively low levels of pedestrian traffic. We are also segregating pedestrians from site traffic. Kelly Contractors will install all safety signs on hoarding and on footpaths to show clear and safe access routes to site. Access on to site will be monitored through swipe cards which will allow only approved site personnel access.

N.B. Please see appendix D for picture of the existing hoarding and scaffolding on site. The existing Hoarding will be repaired and painted in Camden approved colours prior to commencement of works on site. We will be using the existing Scaffolding that is in place currently per the picture in Appendix D and the appropriate scaffolding and hoarding license will be in place prior to commencing works on site.

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

A scaffold will be placed along the front of 147 Kentish Town Road similar to the scaffold that existed there previously. This scaffold will extend around the corner along Castle road once the building works commence on site. Kelly contractors will plywood out the base of the scaffold and paint it. Safety signage will be on the scaffold. The scaffold will be inspected weekly.

The following types of highway licence will be required to facilitate the works

- Skip Licence
- Hoarding Licence
- Scaffolding Licence

28. Please list all [noisy operations](#) and the construction method used, and provide details of the times that each of these are due to be carried out.

Works will be restricted to 9am-6pm Monday to Friday and 9am-1pm every Saturday.

List of Noisy Works

- **Excavation Works** – using motorized mechanical diggers.

Construction Times – Monday to Friday – November 14th 2016 – 15th March 2017. The mechanical diggers will be operating within Camden Councils working hours.

- **Concreting** – concrete will be poured using motorized line pumps.

Construction Times: Monday to Friday – November 14th 2016 – 15th March 2017. Line pumps will be used approx. 2 times per week for a period of 3 hours each. The line pumps will be operating within Camden Councils working hours.

- **Steel Erection & Metal Deck Installation** – steel will be erected using a mobile crane. The steel will be fixed in place by using powered hand tools.

Construction Times: The Times Steel erection will commence in December 2016 and will run through till 8th April 2017. The steel will be erected within Camden Councils working hours.

The site specific Method Statements will include general site procedures for reducing noise, detailing best practice that will be followed during the construction works all in line with the recommendations of BS5228.

The site manager will monitor noise generally throughout the day to ensure we don't cause unnecessary nuisance, and this would include shouting, bad language, radios and out of hour's deliveries.

Prior to starting any works which would create excessive noise, site management will notify neighbours as to what we plan to do and for how long. The basic precautions that will be taken to minimise noise generated on the site include using silenced plant, segregation and exclusion through screening if operations are excessive, avoidance of leaving equipment running whilst not immediately engaged in operation.

In the event that a complaint or concern is raised, an immediate review will be completed by the site to remove the problem wherever possible or minimise the issue. The interested parties will also be notified. This would be recorded in a file kept on site which would be available to view at any time. In the event that there is still a problem after action was taken to reduce noise, the complaint will be reviewed and discussions held with the third party to understand the problem further and evaluate whether the particular problem can be rectified or at least improved. Communication will be maintained in conjunction with the interested authorities.

Kelly Contractors employs Safety, Health and Environment Advisers who are trained and experienced in the use of noise monitoring equipment. We retain our own noise monitoring equipment and regularly carry out monitoring checks during the course of construction, to ensure noise levels adjacent to the works are within specified limits.

29. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

A survey of background noise levels has been carried out by Clarke Saunders. **This survey can be found attached. A construction acoustic report, also prepared by Clarke Saunders, can be found attached in Appendix B.**

Contractors note: High ambient levels = a 10 hr LAeq >65 and Low ambient levels = a 10 LAeq < 65

30. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

A summary of worst case noise emissions for each discreet phase of works described have been predicted on the basis of the proposed equipment, relevant noise data provided in BS5228 and the advice of the building contractor with regard to number of items in use and likely operational 'on times'. This is summarised in the table below

Please note that the 'on-time' represents the actual time that the noise source is likely to be present during a typical two-hour period, allowing for intermittent operation, downtime during setting-up, equipment relocation and breaks.

We have used Annex F of BS 5228-1: 2009 as a minimum to predict the plant noise levels, to estimate the likely daily (LAeq, 10hr) at the nearest sensitive premises.

Source	BS5228 Reference	m	No.in Use	"On time"	10hr LAeq dB(A)
Concrete Saws	Hand Held pneumatic breaker	10	2	15%	83
Hoists	Caged Material Hoist	10	1	15%	68
Hand Breakers, Cutters drills and small tools	Cutter	10	5	20%	79
Excavator breaker	Tracker Excavator	10	1	25%	77
Piling Rig	Large rotary bored piling rig	10	1	20%	83
Wheel Washer	Water Pump	10	1	25%	65
Heavy Goods Vehicle	Lorry	10	2	15%	80

Vibration levels would be expected to be at their highest during piling works. Sheet piling planned in the development may induce some vibration but this is not expected to present a problem to the local residents. However should these or any other activity be deemed likely to, or actually found to cause disruption, then the following monitoring procedure can be implemented:

- A two-stage peak particle velocity (p.p.v.) criterion has been adopted, taking into account current guidance on the control of vibration on demolition, construction and open sites
- On site recording will be completed by the relevant Contractor and their recording findings will be taken and issued to the Construction Manager to ensure adherence with specified levels below,
- 5mm/s p.p.v. 'soft' limit; when exceeded, the contractor should temporarily halt works. Works should only be resumed after consultation with the acoustic engineers and the client.
- 10mm/s p.p.v. 'hard' limit; when exceeded, the contractor should stop work and make a thorough structural examination of the adjacent properties and consult with the design team before re commencing works.

31. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

Below, we have given some examples of methods to reduce, as far as possible, noise and vibration created by construction work. Kelly Contractors will choose machinery that has the quietest noise output available for the activity you are carrying out. Machinery and vehicles will be fitted with effective silencers wherever available, and kept in good working order. Equipment will be operated so it produces as little noise as possible. Equipment will be shut down when it is not in use. Machinery will be based as far away from noise-sensitive properties as reasonably possible. We will use barriers and enclosures if any activities are likely to be noisy at sensitive premises. The scaffolding around the perimeter of the site along Castle Road and Kentish Town Road will be covered in monarflex scaffolding.

Timber Hoardings to reduce noise breakout from activities will also enclose the site. Gates and access points will not face onto any especially sensitive buildings such as residential property, hospitals, schools and businesses. Gates and access points along Castle Road will be kept open for as little time as possible. The timber hoarding will be free of any significant holes or gaps which will aid in resisting sound transfer.

Fixed items of construction equipment will be electrically powered rather than diesel or petrol driven. If this is not possible, we will provide other protection against noise such as baffles, covers or enclosures e.g monarflex to scaffolding along Kentish Town Road and Timber Hoarding along Castle Road. Where practicable, acoustic blankets shall be used around noisy plant.

Everyone on site will be advised to reduce noise as far as possible both to protect themselves and the community. We will not allow anti-social behaviour such as shouting, using radios and swearing.

Actions to be taken where noise exceeds the predicted levels

In the event that the limits have been exceeded the operation will be modified and the noise and/or vibration rechecked from that operation to verify that the corrective action has been effective. These actions may include reducing the operating hours, resting the equipment, changing the method of working or temporary barriers. If in the event a review it is considered that there is no practical method of reducing the noise levels from a particular operation it will be carried out at particular times agreed by all parties.

For regenerated structure borne noise where required Kelly Contractors will incorporate 2hr on/off respite periods to reduce impact to nearby sensitive receptors. Noisy works shall occur between 9am to noon and 2pm to 5.30pm and 9am to 1pm on Saturdays.

32. Please provide evidence that staff have been trained on BS 5228:2009

Our operatives are trained to employ appropriate techniques to keep site noise to a minimum, and will be effectively supervised to ensure best working practice in respect of noise reduction is followed. All employees will be advised regularly of the following as part of their training:

- a) The proper use and maintenance of tools and equipment.
- b) The positioning of machinery on site to reduce the emission of noise to the neighbourhood and to site personnel.
- c) The avoidance of unnecessary noise when carrying out manual operations and when operating plant and equipment.
- d) The protection of persons against noise.
- e) The operation of sound measuring equipment (selected personnel).

Special attention will be given to the use and maintenance of sound reduction equipment fitted to power tools and machines. Persons issued with ear protection equipment will be instructed on its use, care and maintenance.

Education programmes will be provided which draw attention to harmful effects of noise and make it clear that there are several ways in which employees can help themselves to protect their hearing for example:

- By using and maintaining measures adopted for noise control.
- By reporting defective noise control equipment to their superiors.
- By not damaging or misusing ear protectors provided and by immediately reporting damage to or loss of such items to their superiors.

A programme of monitoring will be implemented to ensure that condition limits are not exceeded and that all recommendations are met.

Kelly Contractors management staff will help by recognising the need for employees to make proper use of equipment so that noise emission will be minimised.

33. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

The principle construction activities that will generate dust are typically demolition, excavation, foundations and external works.

The materials disturbed by excavation activities are inert materials (principally crushed concrete and clay/gravel fill) and therefore the dust generated during their removal and transportation does not represent a hazard to either people or the environment. We will also add shielding to cutting equipment. The site will be enclosed by a scaffold which will be protected by monarflex sheeting.

When activities are being carried out that risk generating large volumes of airborne dust, Kelly Contractors will employ dust suppression measures. This will normally take the form of damping down and dust screens. Good site management will be strictly enforced to ensure work areas are kept clean and tidy at all times to prevent the migration of dust throughout the site.

We will erect a full site boundary, keeping away from sensitive receptors, and there will be a fully trained Manager on site throughout the construction period. We will be using water as dust suppressant where applicable and muck-away trucks will be covered to prevent wind effects on contents.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Kelly Contractors will have a hose and pressure washer at the main entrance to prevent any dirt/dust leaving the site. We will employ a road sweeper on a day-to-day basis, as required to maintain a clean road surface. The main time where the roads will need to be cleaned within the project will be when ground works commence i.e. removal of soil /clay etc. We will monitor this carefully.

The footways in the general vicinity of the site will be kept in a clean and tidy condition at all times (i.e any significant spillages to be cleaned up as they occur).

35. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

Arrangements for monitoring Noise, Dust and Vibration

Kelly Contractors will aim to ensure that noise levels within adjacent buildings do not exceed 70db (LAeq, 10hr). 73db (LAeq,10hr) is identified as the first action level which will effectively aid in keeping daily noise levels below 70 db (LAeq, 10hr).

In order to monitor the noise level Kelly Contractors will:

- Install two semi-permanent Class 1 sound level meters at appropriate site boundary locations, continuously monitoring a range of noise metrics, including LMax, LMin, LAeq, LA90, at 15 minute intervals.
- Make provision for alerts via SMS or email when levels breach specified noise levels allowing site staff to undertake immediate investigation and take remedial action where necessary.
- Make provision for weekly/monthly reports to the Council on request, detailing daily noise emissions, and listing and discussing of any noise level triggers by text alert and action taken.
- Provide a site map of NML's

Our scheme for mitigation will be robust enough to achieve the aforesaid noise limit. We will include for respite periods, during intrusive work.

Vibration & Dust

Electronic monitors are already in place on site at the request of London Underground (Tubelines) and Camden Council for the current demolition works on site. Vibration levels are sent directly to a monitoring company and we are made aware if any of the limits are breached. These systems are already in place for the current demolition works on site.

We will make provision for weekly/monthly reports to the Council on request, detailing dust and vibration levels, and listing and discussing of any vibration and dust levels triggered by text alert and action taken.

36. Please confirm that a Risk Assessment has been undertaken at planning application stage in line with the GLA policy. [The Control of Dust and Emissions During Demolition and Construction 2104 \(SPG\)](#), that the risk level that has been identified, and that the appropriate measures within the GLA mitigation measures checklist have been applied. Please attach the risk assessment and mitigation checklist as an appendix.

An Air Quality Assessment has been undertaken and has focused on the impact of construction dust and emissions. This assessment has been prepared taking into account all relevant local and national guidance and regulations.

The risk levels of Dust Soiling and PM₁₀ effects have both been assessed and identified.

37. Please confirm that all of the GLA's 'highly recommended' measures from the [SPG](#) document relative to the level of risk identified in question 36 have been addressed by completing the [GLA mitigation measures checklist](#).

This is not a high risk site. As it is low risk, Air quality on dust in line with SPG was completed

- 38. If the site is a 'High Risk Site', 4 real time dust monitors will be required. If the site is a 'Medium Risk Site', 2 real time dust monitors will be required. The risk assessment must take account of proximity to sensitive receptors (e.g. schools, care homes etc), as detailed in the [SPG](#). Please confirm the location, number and specification of the monitors in line with the SPG and confirm that these will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

39. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

The buildings have recently become vacant therefore no site inspections have been carried out to date. A specialist contractor will be appointed to carry out a site inspection and remove rodents if they are found on site prevent them from moving to other properties around the area. Other initiatives we will implement are as follows:

- No waste on site
- No eating or drinking on site other than canteen area
- Capping of drains
- Traps installed

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey was carried out per the attached asbestos survey Ref: S-04614. The removal of asbestos and demolition has already been carried out under the previous Construction Management Plan.

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

Kelly Contractors will provide a smoking area away from the main gate to ensure limited health risks to local residents. Interaction can take place with non-construction personnel. Site personnel will not be permitted to loiter outside the main gate.

Within Kelly Contractors' Health and safety plan we state 'No personnel shall indulge in fighting, horseplay, tomfoolery or practical jokes including wolf whistling etc.'

We will work on a red card system, therefore any personal found to be acting within a manner we deem unacceptable, will be removed from site and consequently barred from working on any Kelly site within the UK.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are

applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

From 1st September 2015

(i) Major Development Sites – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

(ii) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

From 1st September 2020

(iii) Any development site - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

(iv) Any development site within the Central Activity Zone - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (11/16 - 02/18)
- b) Is the development within the CAZ? (Y/N): N
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N): Y
- d) Please provide evidence to demonstrate that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered: All relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered.
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection: The inventory of NRMM will be kept on site and all machinery will be regularly serviced and service logs kept on site for inspection.
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required: All records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required.

Note: Kelly Contractors shall conform that site lighting should be positioned and directed so as not to intrude unnecessarily on adjacent buildings and land uses. It should not cause distraction or confusion to passing drivers on adjoining highways.

Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Please notify that council when you intend to start work on site. Please also notify the council when works are approximately 3 months from completion.

Signed: *Hugh Kelly*

Date: 19th October 2016

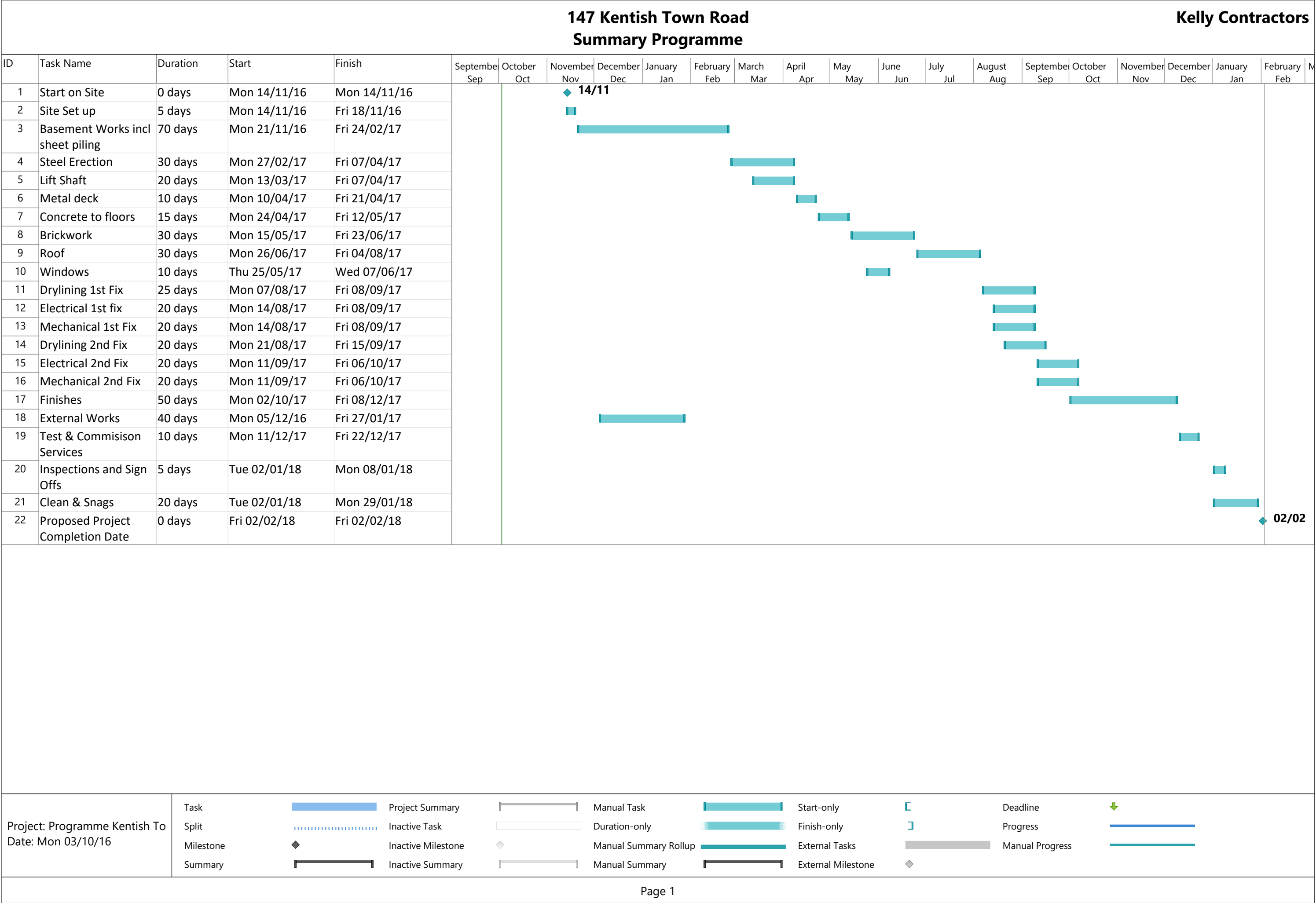
Print Name: Hugh Kelly

Position: Director

Please submit to: planningobligations@camden.gov.uk

APPENDIX A

SUMMARY PROGRAMME



APPENDIX B

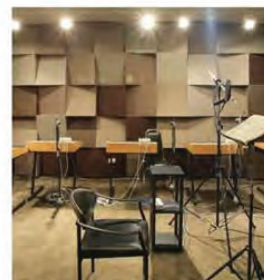
CLARKE SAUNDERS

**NOISE IMPACT ASSESSMENT INCLUDING SURVEY OF BACKGROUND NOISE
LEVEL**



REPORT AS8692.160128.ENS

147 KENTISH TOWN ROAD, LONDON



ENVIRONMENTAL NOISE SURVEY



Prepared: 15 July 2016



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1.0	INTRODUCTION	1
2.0	SURVEY PROCEDURE & EQUIPMENT	1
3.0	RESULTS	1
4.0	DISCUSSION	1
5.0	CONCLUSION	2

List of Attachments

AS8692/SP1	Indicative Site Plan
AS8692/TH1-TH2	Environmental Noise Time Histories
Appendix A	Acoustic Terminology

1.0 INTRODUCTION

Clarke Saunders Associates has been commissioned by Roman Pardon Architects to undertake an environmental noise survey in order to measure the prevailing background noise climate at the site.

2.0 SURVEY PROCEDURE & EQUIPMENT

A survey of the existing background noise levels was undertaken at second floor level of the existing building at the location shown in site plan AS8692/SP1. Measurements of consecutive 5-minute L_{Aeq} , L_{Amax} , L_{A10} and L_{A90} sound pressure levels were taken between 13:55 hours on Tuesday 26th January and 13:40 hours on Wednesday 27th January 2016.

The following equipment was used during the course of the survey:

- Rion data logging sound level meter type NA28;
- Rion sound level calibrator type NC-74.

The calibration of the sound level meter was verified before and after use. No significant calibration drift was detected.

The weather during the survey was dry with light winds, which made the conditions suitable for the measurement of environmental noise.

Measurements were made generally in accordance with ISO 1996-2:2007 *Acoustics - Description, measurement and assessment of environmental noise – Part 2: Determination of environmental noise levels*.

Please refer to Appendix A for details of the acoustic terminology used throughout this report.

3.0 RESULTS

Figures AS8692/TH1-TH2 show the L_{Aeq} , L_{Amax} , L_{A10} and L_{A90} sound pressure levels as time histories at the measurement position.

4.0 DISCUSSION

The background noise climate at the property is determined by road traffic noise in the surrounding streets.

Measured minimum background noise levels are shown in Table 4.1 below.

Monitoring period	Minimum $L_{A90,5mins}$
07:00 - 23:00 hours	53 dB 21:35-21:40, 26/01/2016
23:00 - 07:00 hours	51 dB 03:50-03:55, 27/01/2016
24 hours	51 dB

Table 4.1 - Minimum measured background noise levels

[dB ref. 20µPa]

5.0 CONCLUSION

An environmental noise survey has been undertaken at 147 Kentish Town Road, London by Clarke Saunders Associates between Tuesday 26th January and 13:40 hours on Wednesday 27th January 2016, in order to establish the existing background noise climate.

pp.

Ben Alexander AMIOA

CLARKE SAUNDERS ASSOCIATES

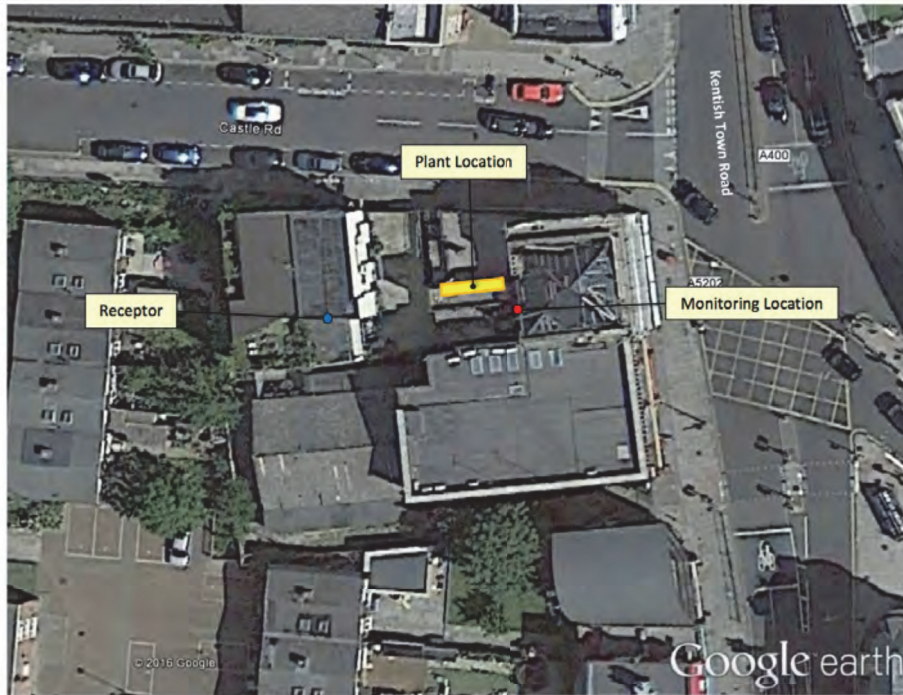
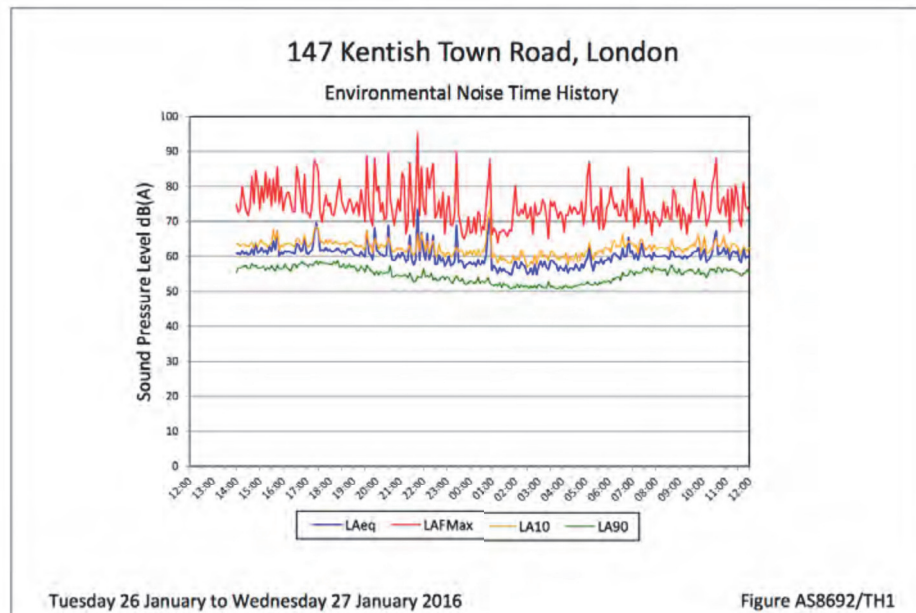
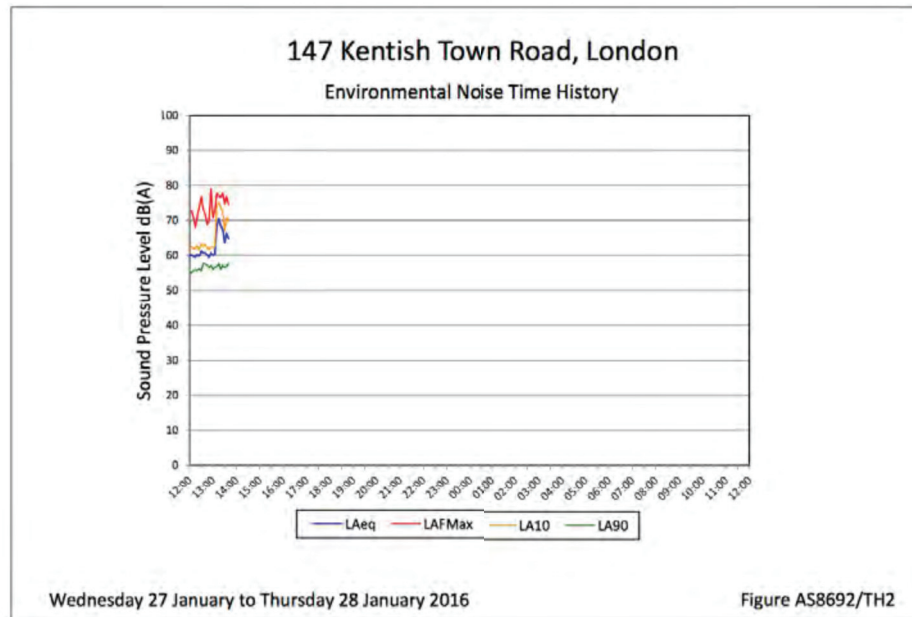


Figure AS8692/SP1

clarke saunders | acoustics

clarke saunders | acoustics





APPENDIX A

ACOUSTIC TERMINOLOGY & HUMAN RESPONSE TO BROADBAND SOUND

1.1 Acoustic Terminology

The human impact of sounds is dependent upon many complex interrelated factors such as 'loudness', its frequency (or pitch) and variation in level. In order to have some objective measure of the annoyance, scales have been derived to allow for these subjective factors.

Sound	Vibrations propagating through a medium (air, water, etc.) that are detectable by the auditory system.
Noise	Sound that is unwanted by or disturbing to the perceiver.
Frequency	The rate per second of vibration constituting a wave, measured in Hertz (Hz), where 1Hz = 1 vibration cycle per second. The human hearing can generally detect sound having frequencies in the range 20Hz to 20kHz. Frequency corresponds to the perception of 'pitch', with low frequencies producing low 'notes' and higher frequencies producing high 'notes'.
dB(A):	Human hearing is more susceptible to mid-frequency sounds than those at high and low frequencies. To take account of this in measurements and predictions, the 'A' weighting scale is used so that the level of sound corresponds roughly to the level as it is typically discerned by humans. The measured or calculated 'A' weighted sound level is designated as dB(A) or L_A .
L_{eq}:	A notional steady sound level which, over a stated period of time, would contain the same amount of acoustical energy as the actual, fluctuating sound measured over that period (e.g. 8 hour, 1 hour, etc). The concept of L_{eq} (equivalent continuous sound level) has primarily been used in assessing noise from industry, although its use is becoming more widespread in defining many other types of sounds, such as from amplified music and environmental sources such as aircraft and construction. Because L_{eq} is effectively a summation of a number of events, it does not in itself limit the magnitude of any individual event, and this is frequently used in conjunction with an absolute sound limit.
L_{10} & L_{90}:	Statistical L_n indices are used to describe the level and the degree of fluctuation of non-steady sound. The term refers to the level exceeded for n% of the time. Hence, L_{10} is the level exceeded for 10% of the time and as such can be regarded as a typical maximum level. Similarly, L_{90} is the typical minimum level and is often used to describe background noise. It is common practice to use the L_{10} index to describe noise from traffic as, being a high average, it takes into account the increased annoyance that results from the non-steady nature of traffic flow.

1.2 Octave Band Frequencies

In order to determine the way in which the energy of sound is distributed across the frequency range, the International Standards Organisation has agreed on "preferred" bands of frequency for sound measurement and analysis. The widest and most commonly used band for frequency measurement and analysis is the Octave Band. In these bands, the upper frequency limit is twice the lower frequency limit, with the band being described by its "centre frequency" which is the average (geometric mean) of the upper and lower limits, e.g. 250 Hz octave band extends from 176 Hz to 353 Hz. The most commonly used octave bands are:

Octave Band Centre Frequency Hz	63	125	250	500	1000	2000	4000	8000
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1.3 Human Perception of Broadband Noise

Because of the logarithmic nature of the decibel scale, it should be borne in mind that sound levels in dB(A) do not have a simple linear relationship. For example, 100dB(A) sound level is not twice as loud as 50dB(A). It has been found experimentally that changes in the average level of fluctuating sound, such as from

APPENDIX A

ACOUSTIC TERMINOLOGY & HUMAN RESPONSE TO BROADBAND SOUND

traffic, need to be of the order of 3dB before becoming definitely perceptible to the human ear. Data from other experiments have indicated that a change in sound level of 10dB is perceived by the average listener as a doubling or halving of loudness. Using this information, a guide to the subjective interpretation of changes in environmental sound level can be given.

INTERPRETATION

Change in Sound Level dB	Subjective Impression	Human Response
0 to 2	Imperceptible change in loudness	Marginal
3 to 5	Perceptible change in loudness	Noticeable
6 to 10	Up to a doubling or halving of loudness	Significant
11 to 15	More than a doubling or halving of loudness	Substantial
16 to 20	Up to a quadrupling or quartering of loudness	Substantial
21 or more	More than a quadrupling or quartering of loudness	Very Substantial

APPENDIX C

DISCREET PEST CONTROL JOB RECEIPTS


Notes - Internal use only

Job: 1 2 3 5 7 Treatment Report: 0 1

Contract: 2 9 / 6

Cancel Form ☐

✓



Treatment Report: 0 1 Date: 2 2 0 6 1 6

Visit Type:	Materials Used:	Qty	Units
Initial Treatment <input checked="" type="checkbox"/>	PERMISTON BAIT STATIONS	4	
Routine <input type="checkbox"/>	NATURAL FRESH BAIT	320	gm
Follow Up <input type="checkbox"/>			
Call Out <input type="checkbox"/>			

Job: ☐ Cont: ☐ 1 2 3 5 7

Customer Name and Address
147 KENTISH TOWN ROAD
LONDON

Post Code
NW1 6PB

Pest Activity Found: Y ☐ N ☒

☐ Cockroach ☐ Mice ☐ Rats ☐ Bed Bugs ☐ Flies ☐ Moths ☐ Ants ☐ Wasps

Location:

Infestation Level: H ☐ M ☐ L ☐

Bait Boxes/Traps Checked ☒

Bait Boxes Replenished ☐ IT

Insect Monitors Checked ☐

Housekeeping Standards: Ex ☐ G ☐ A ☒ P ☐

Pest Risks	
Stock Damage <input type="checkbox"/>	Legal Action <input type="checkbox"/>
Building Damage <input type="checkbox"/>	Safety/Welfare <input type="checkbox"/>
Contamination <input type="checkbox"/>	Reputation <input type="checkbox"/>
Disease <input type="checkbox"/>	Other <input type="checkbox"/>

Fly Control Unit

Good Working Order ☐ Serviced ☐

Next Treatment W/C: 7 DAYS

Extra Visit Required ☐ Job Completed ☐

Report:

AN INSPECTION WAS CARRIED OUT TO CHECK FOR RAT ACTIVITY.

NO EVIDENCE OF ANY CURRENT PROBLEMS WERE FOUND.

BAITS WERE Laid FOR MONITORING PURPOSES

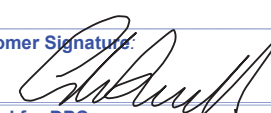
Recommendations:


FOLLOW UP IN 7 DAYS (8.7.16)

Continued on additional report: ☐

Advice for customer discussed: Yes ☐ No ☐

Customer Name (Please Print):

Customer Signature: 

Signed for DPC: S. I. L. 

ADVICE TO CUSTOMERS:

During their treatment visit our technicians will:

- Place baits in protected situations and other pesticide materials in appropriate situations and will make these known to you:
- Search for and remove rat and mouse bodies.

If spilt/exposed baits are found between normal visits, please telephone your Discreet Pest Control office for prompt service.

The following interim actions may also be take before collection:

For spilt/exposed bait: Wear protective gloves. Place the bait in a plastic bag and keep safely locked away.

For rat or mouse bodies: Wear protective gloves. Either dispose of by incineration or deep burying, or place safely away from other animals and children.

If pesticide baits are accidentally handled, wash hands immediately with soap and water. For any accidental pesticide ingestion or poisoning, seek appropriate medical or veterinary treatment. Telephone Discreet Pest Control on **0845 678 0181** for further advice and for bait replacement.

Notes - Internal use only

Job: 1 9 3 5 7

Treatment Report: 02

6/7

Cancel Form

Contract:



Treatment Report:

02

Date

060716

Visit Type:

Initial Treatment ☐
Routine ☐
Follow Up ☒
Call Out ☐

Materials Used:

RATINOR FLESH BAITS
80gm TRAYS.
(Bromadiolone)

Qty

Units

2660 gm

Job: ☒ Cont: ☐

1 9 3 5 7

Customer Name and Address

147 KENNEL TOWN ROAD
LONDON

Post Code

NW1 8PB

Pest Activity Found: Y ☒ N ☐

Cockroach ☐ Mice ☒ Rats ☐ Bed Bugs ☐ Flies ☐ Moths ☐ Ants ☐ Wasps ☐

Location:

GROUND FLOOR

Infestation Level: H ☐ M ☒ L ☐

Bait Boxes/Traps Checked ☒

Bait Boxes Replenished ☒

Insect Monitors Checked ☐

Housekeeping Standards: Ex ☐ G ☐ A ☐ P ☒

Pest Risks

Stock Damage ☐ Legal Action ☐
Building Damage ☐ Safety/Welfare ☒
Contamination ☐ Reputation ☐
Disease ☐ Other ☐

Fly Control Unit

Good Working Order ☐ Serviced ☐

Next Treatment W/C: 13/7/16

Extra Visit Required ☐ Job Completed ☐

Report:

SOME RAT ACTIVITY
WAS FOUND IN TWO
LOCATIONS INTERNALLY
AND EXTERNALLY ADJACENT
TO THE PREMISE ENTRANCE.
THE TAKEN BAIT WAS
REPLACED.

Recommendations:

A FOLLOW UP VISIT TO
BE CARRIED OUT ON 13/7/16

Continued on additional report: ☐

Advice for customer discussed: Yes ☐ No ☒

Customer Name (Please Print):

Customer Signature:

Signed for DPC:

S. Hunt

ADVICE TO CUSTOMERS:

During their treatment visit our technicians will:

- Place baits in protected situations and other pesticide materials in appropriate situations and will make these known to you:

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If pesticide baits are accidentally handled, wash hands immediately with soap and water. For any accidental pesticide ingestion or poisoning, seek appropriate medical or veterinary treatment. Telephone Discreet Pest Control on 0845 678 0181 for further advice and for bait replacement.

Notes - Internal use only

Job: 1 9 3 5 7 Treatment Report: 03
Contract: 3/7

Cancel Form ☐



Treatment Report: 03 Date 1 3 0 7 1 6

Visit Type:
Initial Treatment ☐
Routine ☐
Follow Up ☒
Call Out ☐

Materials Used:	Qty	Units
.		

Job: ☐ Cont: ☒ 1 9 3 5 7

Customer Name and Address
167 KENWICK TOWN ROAD
LONDON

Post Code NW1 8PB

Pest Activity Found: Y ☒ N ☐
☐ Cockroach ☐ Mice ☐ Rats ☐ Bed Bugs ☐ Flies ☐ Moths ☐ Ants ☐ Wasps

Location: DOOR AREA

Infestation Level: H ☐ M ☐ L ☒

Bait Boxes/Traps Checked ☒
Bait Boxes Replenished ☐
Insect Monitors Checked ☐

Housekeeping Standards: Ex ☐ G ☐ A ☒ P ☐

Pest Risks
Stock Damage ☐ Legal Action ☐
Building Damage ☐ Safety/Welfare ☐
Contamination ☒ Reputation ☐
Disease ☐ Other ☐

Fly Control Unit
Good Working Order ☐ Serviced ☐

Next Treatment W/C: 20/7/16

Extra Visit Required ☐ Job Completed ☐

Report:
A FOLLOW UP VISIT WAS
CARRIED OUT
NO LIVE PESTS WERE FOUND
INTERIALLY.
A MINOR BAIT TAKE WAS
FOUND OUTSIDE THE ENTRY
DOOR
SUFFICIENT BAIT IN PLACE

Recommendations:
FURTHER VISIT ON
20/7/16

Continued on additional report: ☐

Advice for customer discussed: Yes ☐ No ☒

Customer Name (Please Print):

Customer Signature:

Signed for DPC:
S. Khan

ADVICE TO CUSTOMERS:

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Notes - Internal use only

Completed

Job: 1 2 3 5 7 Treatment Report: 20/7 04

Contract:

Cancel Form



Treatment Report:

04

Date

20 07 16

Visit Type:

Initial Treatment ☐
 Routine ☐
 Follow Up ☒
 Call Out ☐

Materials Used:

Qty

Units

Job: ☒ Cont: ☐

1 9 2 1 7

Customer Name and Address

167 Kew Road
 London

Post Code

W 4 1 8 P 3

Pest Activity Found: Y ☐ N ☒

Cockroach ☐ Mice ☐ Rats ☐ Bed Bugs ☐ Flies ☐ Moths ☐ Ants ☐ Wasps ☐

Location:

Infestation Level: H ☐ M ☐ L ☐

Bait Boxes/Traps Checked ☒

Bait Boxes Replenished ☐

Insect Monitors Checked ☐

Housekeeping Standards: Ex ☐ G ☐ A ☒ P ☐

Pest Risks

Stock Damage ☐ Legal Action ☐
 Building Damage ☐ Safety/Welfare ☐
 Contamination ☐ Reputation ☐
 Disease ☐ Other ☐

Fly Control Unit

Good Working Order ☐ Serviced ☐

Next Treatment W/C: Complete

Extra Visit Required ☐ Job Completed ☒

Report:

NO FURTHER RAT
 ACTIVITY FOUND.
 NO SUGGESTIONS WERE
 REPORTED

Recommendations:

Complete

Continued on additional report: ☐

Advice for customer discussed: Yes ☐ No ☐

Customer Name (Please Print):

Customer Signature:

Signed for DPC:

S. Hewitt

ADVICE TO CUSTOMERS:

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APPENDIX C

Flyer re Public Meeting at the Congregational Church Kentish
Town

PUBLIC MEETING

at

Kentish Town Congregational Church
Thursday 20th October 7pm

To discuss the construction work at

THE CASTLE 147 KENTISH TOWN ROAD

You are invited to attend a meeting to discuss various aspects of the construction works at 147 Kentish Town Road and how the contractor will take steps to mitigate the impact of the works.

We would also like to get your views on our proposals for the liaison with the local community in the coming months.

We look forward to meeting you on the 20th.

PARDON CHAMBERS

ARCHITECTS



APPENDIX D

Existing Scaffold & Hoarding on Site

Existing Scaffolding to be Retained and Hoarding to be repaired and re painted in LBC approved colours and extended along Castle Road to enclose the site boundary

