

Address:	Commonwealth House 1 - 19 New Oxford Street London WC1A 1NQ		2
Application Number:	2014/4983/P	Officer: Neil McDonald	
Ward:	Holborn & Covent Garden		
Date Received:	01/08/2014		
Proposal: Demolition and reconstruction of 7th and 8th floors, erection of a roof extension at 9th floor level and infill works to provide additional office floorspace (Class B1a) and new roof top plant. Changes of use at ground floor level from retail (A1/A3) to provide a relocated and enlarged office entrance (B1a), retail and food and drink uses (A1/A3) and flexible office and retail use (B1a/A1/A3) with associated alterations to shop fronts, replacement of all existing upper floor windows, recladding of the elevations, works to Dunns Passage and other enabling works for refurbishment of existing offices (B1A).			
Drawing Numbers: Existing (prefix 1975_PL) 00 rev P2, 01 rev P2, 02 rev P2, 03 rev P2, 04 – 13 revs P1, 20 – 27 revs P1; Demolition works (prefix 1975_PL) 31 – 42 revs P1, 50 – 55 revs P1; Proposed (prefix 1975_PL) 61 rev P2, 62 rev P2, 64 – 73 revs P1, 80 – 82 revs P1 83 rev P2, 84 - 85 rev P1, 87 rev P1, 88 rev P2, 89 – 90 rev P1. Supporting documents: Design & Access Statement by Orms dated 9 th Sept 2014, Planning Statement by DP9 dated July 2014, Covering letter by DP9 dated 31.07.14, Historic Building Report by Donald Insall Associates dated July 2014, Transport Assessment by Ove Arup dated July 2014, Construction Traffic Management Plan by Ove Arup dated July 2014, BREEAM Report by RES dated July 2014, Energy Strategy Report by RES dated July 2014, Air Quality Statement by Long & Partners dated July 2014, Environmental Noise and Vibration Report by Sandy Brown dated July 2014, Statement of Community Involvement.			
RECOMMENDATION SUMMARY: Grant planning permission subject to conditions and a section 106 Legal Agreement			

ANALYSIS INFORMATION (GEA)

Land Use Details:			
	Use Class	Use Description	Floorspace (GEA)
Existing	A1 Shop		1,471 m ²
	A4 Drinking establishment		130 m ²
	B1a Business – Office		10,690 m ²
	EXISTING TOTAL		12,291 m²

Proposed	<i>A1 Shop</i> <i>B1/A1/A3 flexible use</i> <i>A3 Restaurants and Cafes</i> <i>B1a Business - Office</i> TOTAL PROPOSED GROSS UPLIFT (GEA)	<i>398 m²</i> <i>56 m²</i> <i>895 m²</i> <i>11,894 m²</i> 13,243 m² 952 m²
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Parking Details:		
	Parking Spaces (General)	Parking Spaces (Disabled)
Existing	0	0
Proposed	0	0

OFFICERS' REPORT

Reason for Referral to Committee: This application is reported to Committee because it proposes minor development of more than 450 sq. mtrs of non-residential floorspace [clause 3(i)].

OFFICERS' REPORT

1. SITE

- 1.1 Commonwealth House is a purpose-built, multi-tenanted office building comprising a double-height ground floor of retail and food and drink uses with 8 office floors above. It occupies a triangular site between High Holborn and New Oxford Street. Its apex is at the junction of the two streets. The three sides of the building are arranged around a central lightwell.
- 1.2 The building, which dates from 1939, serves as a distinctive landmark. This is due to its modernist inspired design, its prominent size and location and curved corner feature commanding the site's apex which is topped with a clock tower. Although not statutorily listed, it is noted as making a positive contribution to the Bloomsbury Conservation Area within which the site is situated.
- 1.3 The site's surroundings are predominantly commercial. The building immediately to the west is the vacant, part 7 / part 11-storey former GPO sorting office. This is separated from Commonwealth House by Dunn's Passage, a formerly public route that was closed some years ago following community safety concerns. Dunn's Passage is currently being considered for reopening as part of the refurbishment proposals of the former GPO site, which have recently been submitted as a formal planning application (ref: 2014/5946/P).
- 1.4 To the north on the opposite side of New Oxford Street is St Georges Court which is another large scale, slightly later 20th Century office building which has recently undergone refurbishment with the creation of three flexible retail or restaurant units (Class A1/A3) at ground floor level (application ref. 2012/1400/P). Lining the southern side of High Holborn opposite the site are 9-storey later-20th century office buildings, while further to the east lies the grade 2 listed former Holborn Town Hall and Library.
- 1.5 The application site is located outside of a designated growth area (policy CS2) but is in close proximity to both the Tottenham Court Road Growth Area (the boundary is immediately to the south of the site encompassing the site at 21-31 New Oxford Street - the sorting office site) and the Holborn Growth Area (175m to the east of the site). The application site is also located outside of a designated central London retail frontage, although High Holborn and Kingsway frontage does include Holborn Tower, which is immediately to the north-east of the application site beyond the junction of High Holborn and Bury Place.
- 1.6 The site is highly accessible by public transport (PTAL 6B 'excellent') being close to three underground stations, namely Holborn to the east, Tottenham Court Road to

the south-west (also including the future Crossrail station) and Covent Garden to the south. Both High Holborn and New Oxford Street are served by a number of bus routes. They are also on the London Cycle Network and experience high levels of commuter cycling.

2. THE PROPOSAL

- 2.1 The proposal is for the refurbishment and extension of the existing office building including an additional (9th) floor of accommodation. The scheme does entail the removal of some existing building fabric which the applicant reasons is necessary in order to extend and renew the building to a more functional and modern standard. A key aim of the proposal is to improve the building's access, internal layout and services and to enhance its environmental performance.
- 2.2 There are currently two main office entrances, one on the New Oxford Street frontage and the other (principal entrance, although smaller of the two) being at the nose of the building. Three cores presently dominate the floor plan with the toilet block wrapping around the internal atrium, which creates a pinchpoint beside the main core, blocking cross floorplate views and connections.
- 2.3 The proposal is to reconfigure the building with a single clearer entrance for all office users, clearing the remaining façade for retail use. A new main reception area would be created at the centre of the building in a covered atrium by partial infilling of the existing central atrium. This would be served by a new 4 x 17 person lift shaft along one side. The other two walls to the remaining atrium would undergo the removal of their brick façade to be replaced by full height glazing to create visual connections across floorplates and maximise daylight.
- 2.4 The two existing cores would be remodelled retaining their stairwells (in reconstructed form) but relocating the lifts to the internal atrium. These cores would be extended outwards to infill the current building recesses from 3rd to 5th floor levels on the New Oxford Street frontage and enable the WCs to be moved to occupy the infilled areas and free-up the space around the atrium. The lifts meanwhile would be relocated to the new lift shaft at the opposite end of the atrium. The third, rear core would be removed in its entirety and a new riser formed.
- 2.5 At ground floor, the retail space would be reorganised with smaller shop units to be provided on the New Oxford Street frontage and larger units facing High Holborn. The existing office mezzanine would be removed to give added head height and presence to the retail spaces below. The basement would be reorganised, with the existing lightwell filled in at this level to provide cycle parking and adjacent shower/changing rooms.
- 2.6 At the rear of the building where it faces Dunn's Passage the existing sunken service yard would be restored to grade to provide ground level servicing. This would be combined with the extending back of the corner retail units and new entrances to form a public seating space in association with the food and drink uses of the development to be used during daytime hours (10am – 4.30pm) when the service facilities are not in use, to support an enhanced pedestrian experience in connection with the possible future reopening of Dunn's Passage.

- 2.7 At upper floor levels, floors 7 and 8 would be rebuilt and new full-height glazing with brick piers to match below, while a new roof extension is proposed to be added providing additional office accommodation at ninth floor level. An enclosed plant zone is proposed on top.
- 2.8 The elevations would be retained essentially in their existing form but with some alterations -the main changes being:
- Extension of the cores from 3rd to 5th floor levels with new glazed hexagonal panels (to match the existing in shape and size) and new horizontal metal louvres with glazed fins.
 - Reconstructed, altered 7th and 8th floors plus the addition of a 9th floor roof extension
 - New office entrance onto New Oxford Street and removal of existing entrances
 - Alterations to shopfronts to create a more consistent retail frontage across the whole. This will include new curved glass bay windows but retaining existing stone piers and canopy with reinstatement of lost canopy to New Oxford Street elevation. The existing windows to the removed office mezzanine above would be replaced with clerestory windows.
 - Renewal of all other windows to the elevations
 - New glazed hexagonal panels to nose, cores and central bay to replace existing (damaged) panels, size and pattern of panels to match the existing
 - Soot wash to all existing brickwork
 - New projecting oriel metal windows to 6th floor infill on High Holborn facade
 - New stone capping, balustrading and flag pole added to clock tower

Revisions

- 2.9 The following revisions were made to the application since its original submission:
- Introduction of an additional retail unit on the High Holborn frontage
 - Removal of the revolving doors for a more accessible solution utilising sliding doors
 - Updated Design and Access Statement to reflect the above and to replace the originally submitted CGI images to illustrate more clearly the style, composition and colour of the proposed green glazed brick tiles.
 - Amended red-line plan to correct an error on the original red line plan which excluded a portion of the site which oversails Dunn's Passage. An updated

certificate B was also submitted confirming the adjoining owner has been served noticed.

3. **RELEVANT HISTORY**

- 3.1 The site history includes various applications granted for roof plant and advertisement consent. Historic planning decisions include the following:
- 3.2 PSX0104290 granted 12/06/2001 - Change of use from Class A3 to building lobby and alterations to existing entry doors and lobby.
- 3.3 PS9705317R2 granted 06/04/1998 - Change of use from offices on ground floor from offices (Class B1) to shop/showroom (Class A1), together with the new full height glazing and entrance doors.
- 3.4 P9600916R1 granted 26/07/1996 - Installation of louvre grilles to ground floor on High Holborn elevation.
- 3.5 9401194 refused 18/11/1994 – Installation of new shopfronts (refused due to inadequate replacement design –it being noted that the Council’s view is that the existing shopfronts should be retained and refurbished.
- 3.6 24096/R approved 25/05/1977 - Alterations to eastern entrance
- 3.7 24/01/1939 – Permission granted for the erection of the present office building on the site of 1-19 New Oxford Street and 145-153 High Holborn.

4. **CONSULTATIONS**

Statutory Consultees

- 4.1 **Transport for London** – No comments received
- 4.2 **London Underground Infrastructure Protection** – have confirmed that the applicant is in communication with LUL engineers regarding the proposals and that they have no comments other than that this communication should continue.

Non-Statutory Consultees

- 4.3 **Bloomsbury Conservation Area Advisory Committee** – No comments received

Local Groups

- 4.4 **Seven Dials Trust/Bloomsbury Association/Covent Garden Community Association** – Joint representations in **support**:

- Impressed with the degree of research into the original architect’s intentions (Lafontaine) and welcome the proposal for green tiles which had been considered by the Crown Estate as too risqué at the time.

- Welcome that the proposal is to retain the existing offices and would hope given the flexible layout that room will be made for smaller office suites
- The additional storey is cleverly dealt with and the shopfront curved windows attractive –overall a reinstatement of an important and unusual example of late Art Moderne in the West End.
- It is essential that proposed public realm improvements in association with this site and others in the area are coordinated to the benefit of residents, businesses and visitors.

Adjoining Occupiers

	Original
<i>Number of Letters Sent</i>	02
<i>Number of responses Received</i>	00
<i>Number in Support</i>	00
<i>Number of Objections</i>	00

4.5 In addition to adjoining occupier notifications a press notice was displayed in the Camden New Journal on 08/08/2014 inviting comments by 04/09/2014 and a total of 7 site notices were erected on 08/08/2014 giving until 29/08/2014 for comments.

4.6 No responses were received.

5. POLICIES

5.1 Set out below are the LDF policies that the proposals have primarily been assessed against. However it should be noted that recommendations are based on assessment of the proposals against the development plan taken as a whole together with other material considerations.

LDF Core Strategy and Development Policies adopted 8th November 2010

CS1 - Distribution of growth

CS2 - Growth areas

CS5 - Managing the impact of growth and development

CS6 – Providing quality homes

CS7 – Promoting Camden's Centres and shops

CS8 - Promoting a successful and inclusive Camden economy

CS9 - Achieving a successful Central London

CS11 - Promoting sustainable and efficient travel

CS13 - Tackling climate change through promoting higher environmental standards

CS14 - Promoting high quality places and conserving our heritage

CS15 - Protecting and improving our parks and open spaces and encouraging biodiversity

CS17 - Making Camden a safer place

CS18 - Dealing with waste and encouraging recycling

CS19 - Delivering and monitoring the Core Strategy

DP1 - Mixed use development

DP10 – Helping and promoting small and independent shops
 DP12 - Managing impact of food and drink uses
 DP13 - Employment premises and sites
 DP16 - Transport implications of development
 DP17 - Walking, cycling and public transport
 DP18 - Parking standards and limiting the availability of car parking
 DP19 - Managing the impact of parking
 DP20 - Movement of goods and materials
 DP21 - Development connecting to the highway network
 DP22 - Promoting sustainable design and construction
 DP23 - Water
 DP24 - Securing high quality design
 DP25 - Conserving Camden's heritage
 DP26 - Managing the impact of development on occupiers and neighbours
 DP28 - Noise and vibration
 DP29 - Improving access
 DP30 - Shopfronts
 DP31 - Provision of, and improvements to, public open space and outdoor sport and recreation facilities.
 DP32 - Air quality and Camden's clear zone

5.2 **Supplementary Planning Policies**

Camden Planning Guidance (CPG)

- CPG 1 Design 2013
- CPG3 Sustainability 2013
- CPG5 Town centres, retail and employment 2013
- CPG 6 Amenity 2011
- CPG 7 Transport 2011
- CPG 8 Planning obligations 2011

5.3 **London Plan July 2011**

5.4 **NPPF 2012**

6. **ASSESSMENT**

6.1 The main issues raised by this proposal are considered to be:

- Land use –namely the principle of office expansion including the Council's policy for seeking a mix of uses including residential
- Retail and food and drink policies
- Design and heritage
- Impacts on neighbouring amenity, including noise
- Transport and the public realm
- Sustainable building practices and energy efficiency
- Other matters

Land use

- 6.2 The proposed development entails the expansion of office use on the site to the amount of 952sqm GEA. This is added to the existing building which already totals 12,300sqm. The floorspace uplift is a net figure arrived at by subtracting the floorspace lost from the removal of the office mezzanine (1,130sqm) and other minor displacements (cumulatively 1,421sqm) from the cumulative gains in floorspace from infilling to the cores and lightwell and the construction of the roof extension (2,373sqm). The net gain is comprised exclusively of office space since there is a minor loss of retail floorspace overall.
- 6.3 Camden's policy supports the provision of new business space in highly accessible areas (policies CS1, CS2 and CS3). In addition this proposal to improve and modernise the existing office floorspace would be very much supported by policies CS8, CS9 and DP13 which are concerned with promoting the economy and providing employment opportunities both in Central London and the borough generally.
- 6.4 However proposals in Central London and defined town centres which involve more than 200sqm increase in floorspace such as this one, also need to be assessed in line with policy DP1 relating to mixed use. Policy DP1 requires that up to 50% of all the additional floorspace in such schemes should be provided as housing. This housing should be provided on site, although where it is clearly impractical to do so the Council may accept a contribution to the mix of uses elsewhere in the area, or exceptionally a payment in lieu.
- 6.5 The housing floorspace requirement generated in this case would be for 476sqm (50% x 952) which would be below the threshold to be considered for affordable housing under policy DP3. Nevertheless it is necessary for the applicant to demonstrate why it has not been possible to provide on-site housing as part of the scheme and in the event this is proven, to make the required level of contribution.
- 6.6 Accordingly, the applicant has provided information to show that on-site housing would not be practical for various reasons. Since a significant proportion of floorspace would be required for core and circulation in relation to the small number of units (approximately four), these would need to be provided on the lower floors (1st and 2nd) of the building where standards of daylight and amenity would be less than satisfactory without unacceptable alterations made to the window openings of this conservation area building. Also, the only possible location for the residential would be at the south-west corner. This would lose the potential for a retail frontage on this corner which would compromise the opportunity to open up and activate Dunn's Passage. This is as well as compromising the office floorplates themselves blocking out views/daylight and creating a pinchpoint with the atrium, which including the necessary lift overrun, would extend up to third floor level. Officers agree with the applicant's conclusion that on-site residential would not be viable for the scheme.
- 6.7 The applicant, IIAA Henderson Real Estate, has also confirmed that they do not own any appropriate and available sites in the area which could be developed for residential use. Therefore it is considered appropriate in this instance for the mixed use contribution to be made as a payment in lieu. The necessary sum in this case is £333,200 in line with the formula set out in CPG8 (i.e. 476 sqm x £700 per sqm)

to be secured by S106. With the payment of such a contribution it is considered that the principle of the proposed office extension is acceptable in land use terms.

Retail and food and drink policies

- 6.8 The site falls in-between but outside of the protected Central London retail frontages of Holborn and Tottenham Court Road. The main policy to be considered therefore is DP10, which aims to support small and independent shops which serve day-to-day needs or contribute to the local character and identity of an area. Policy CS9 is also relevant, which supports growth in shops and other uses generally in central London.
- 6.9 Policy DP10 says that permission involving a net loss of shop floorspace outside of designated centres will only be granted where:
- alternative provision is available within 5-10 minutes walking distance;
 - there is clear evidence that the current use is not viable;
 - and within the Central London Area, the development positively contributes to local character, function, viability and amenity.
- As a guide, the policy considers small units to be those of less than 100sqm, although the character and size of local shops in the area generally will be taken into account when considering the appropriate scale for of new premises in a development.
- 6.10 The proposal results in a minor loss of retail floorspace overall (1,601sqm existing, compared to 1,349 proposed), but this is as a result of the reorganisation of the basement accommodation with more plant and ancillary operational space and less retail storage. At ground floor level the amount of floorspace in retail use (A1 and A3 use) actually increases. The table below compares the existing breakdown of ground floor retail floorspace on the site with that proposed:

		Existing GIA sqm		Proposed GIA sqm
	Retail unit 1	25	Retail unit 1	49
	Retail unit 2	77	Retail unit 2	49
	Retail unit 3	37	Retail unit 5	112
	Retail unit 4	72		
	Retail unit 5	27		
	Retail unit 6	23		
	Retail unit 8	335		
A1 Total		596		210
	Retail unit 7	130		
A4 Total		130		0
			Retail unit 4	254
			Retail unit 6	228
A3 Total		0		482
			Retail unit 3	51
A1/A3/B1 Total		0		51
Retail Total				

(Ground)		726		743
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- 6.11 Presently there are 8 units in total, 6 on the New Oxford Street frontage and the 2 largest units fronting High Holborn. The second largest of these units (unit 7) is occupied by a Class A4 wine bar, while the remainder are in A1 use comprising two café/sandwich bars, acupuncture/massage centre, a wine-merchant's, a bookshop and an office supplies retailer on the New Oxford Street frontage and a furniture showroom occupying the largest unit on High Holborn. The proposal would replace the six small retail units on the New Oxford Street frontage with three units, two of 49sqm being expressly for retail (A1) use and the other of 51sqm being for flexible A1/A3/B1 use. Whilst the length of frontage in retail use would decrease on this elevation, it is considered that the new enlarged office entrance (which would roughly correspond with the location of existing units 3 and 4) would give the office function more presence and the attendant comings and goings would help enliven this street frontage. Furthermore, proposed unit 1 on the western end of this elevation would be constructed with a return façade onto Dunn's Passage to help provide the necessary activity and interest to enable it to be brought back into use as part of the public realm of the area.
- 6.12 The eastern 'apex' of the building, which currently accommodates the main office entrance at ground floor level, would become freed up for new retail/food and drink use and would be incorporated in one of the proposed two larger units on the southern side of the building. Again the frontage adjacent Dunns Passage would be extended back to activate this corner and Dunns Passage itself. Both these larger units (units 4 and 6) are proposed for A3 food and drink use. Unit 4 adjoining Dunn's Passage would be able to capitalise on the daytime seating area to be provided during the hours of 10am – 4.30pm when the servicing facilities are not in use.
- 6.13 The nearest designated frontage is High Holborn east of Commonwealth House where the CPG5 guideline is to control changes of use so that not less than 50% of premises is retained within A1 and not more than 25% is food and drink (Classes A3-A%). Considering the subject site is outside of any protected frontage, it is considered that a greater amount of flexibility could apply, although given that the ground floor of the building is already in retail use, with A1 use making up the clear majority of this floorspace, it is considered necessary in the interests of respecting the existing character and amenity of the area that A3 uses make up no more than 50% of the proposed retail frontage.
- 6.14 A revision adding a retail unit to the High Holborn frontage has therefore been requested by officers during the course of the application. This ensures that A3 food and drink uses will not dominate and will amount to no more than 50% of units.
- 6.15 It is relevant for the purposes of DP10 that none of the existing units could be described as serving a local day-to-day need and in any event there are other shops fulfilling this purpose within the nearby designated frontages. On balance, it is considered that whilst the proportion of retail (Class A1) uses and number of units is proposed to decrease, the combined effect from an improved office entrance, the refurbishment works to the building and the contribution to bringing

back into use Dunns Passage will contribute positively to the local area as well as enabling the existing building to continue its role played in the Central London economy. The proposed changes to the ground floor uses are therefore considered to accord with the objectives of policies DP10 and CS9.

- 6.16 An informative should be added denoting that the flexible use unit, if granted, would give flexibility for use for 10 years from the date of the permission. After 10 years the lawful use would revert to whichever of the uses is taking place at the time within the unit.

Impacts of Class A3 uses

- 6.17 Food and drink uses are assessed in relation to policy DP12 which states that planning conditions will be used to address certain amenity issues. Chapter 4 'Central London Area food, drink and entertainment, specialist and retail uses' of CPG5 provides further guidance about the approach to be taken on such uses within the Central London area.
- 6.18 The proposed A3 uses would permit restaurant or café style operations and would not by definition permit the sale of alcohol, which would be sought under a separate licensing process. If such a licence were granted the A3 use class would only permit the sale of alcoholic drinks in accompaniment to food.
- 6.19 The refurbishment of the building includes a strategy for ventilation of the building including fume extraction from A3 units. Air would be drawn in by dedicated ceiling mounted air handling units and then ducted at basement level via a dedicated riser. The riser and its discharge point is shown on supplementary drawings included with the submitted Air Quality Statement as terminating above the ninth floor roof extension towards the Dunn's Passage side of the building and well above the windows of adjoining premises.
- 6.20 Nevertheless A3 uses do have the potential to impact upon the amenities of an area which can raise conflicts with other uses. Currently there are no adjoining residential uses, although this may change in the future with the emerging proposals for the adjacent former post office site. Conditions should therefore be imposed to control the playing of amplified music and hours of use in line with DP12.
- 6.21 In regard to hours of use, the new A3 uses in the adjacent office refurbishment of St Georges Court are controlled to between 08:00 to 23:30 Monday to Thursdays, 08:00 to 00:00 on Fridays and Saturdays and 08:00 to 22:30 on Sundays and Bank Holidays. It is considered reasonable to extend the same controls to the A3 units of Commonwealth House.

Design and heritage

The existing building

- 6.22 Commonwealth House is identified in the Bloomsbury Conservation Area Character Appraisal as a positive contributor and is described as "a 1930s modernist-inspired landmark building of nine storeys: clad in brown brick and faience, it has simple

streamlined detailing, including a curved corner with horizontal bands of steel windows and a prominent clock tower, which terminates long views along the street from the east. The building has high quality original shopfronts on the New Oxford Street frontage". It forms part of Sub Area 8 (New Oxford Street/High Holborn/Southampton Row) which is characterised by areas of large scale 19th and early 20th Century blocks fronting busy thoroughfares.

- 6.23 Opposite across New Oxford Street is the newly refurbished and reworked St Georges Court, a 20th Century building of a similar scale and form to Commonwealth House and also recognised as a positive contributor. To the south across High Holborn is the listed Holborn Town Hall, a baroque style building dating from 1894.
- 6.24 While Commonwealth House has a flat-iron form and terminates in a rounded, symmetrical apex, the building as a whole is not symmetrically composed. The New Oxford Street elevation was given greater significance and emphasis in terms of its architectural hierarchy, detail, material, and form to that of the High Holborn elevation. In particular the New Oxford Street elevation has two tower elements of its own which extend to a height comparable to that of the clock tower. The three tower elements coupled with the modelling of upper floors provides a rich and interesting skyline to the building.

Proposal

- 6.25 This proposal involves adding some additional volume to the building, notably an additional storey; replacement of some building fabric including the windows and concrete panels; alterations to shop fronts and entrances; creation of a public route along Dunns Passage.
- 6.26 Overall officers consider this to be a well-considered proposal which has captured the spirit of the building and as such is a good example of how historic office stock can be refreshed and made viable (therefore securing the historic asset's future) whilst preserving its character and enhancing its contribution to the townscape. The works do result in the loss of, what can accusatively be seen as, a significant amount of original external fabric. However the replacement fabric is in character, of high quality, and either preserves or enhances the appearance of the building. Officers consider that each element of change is justifiable in its own right, and that in the long term it is often in the interest of large commercial buildings such as this to be refurbished comprehensively in one go, rather than piecemeal over time.

Extensions

- 6.27 The proposal seeks to remove the top two set back storeys of the current building (floors 7 and 8) and replace them with three new storeys. This would result in an increase of one storey of office. On top of this is proposed a new area of setback plant. The existing upper floors are set back to a greater degree on the Holborn side than they are on the New Oxford Street side, resulting in an unbalanced form. The new 7th and 8th floors would be realigned to have comparable setbacks on both sides to those of the current New Oxford Street elevation. The additional 9th floor would be further set back and again located to reinforce symmetry either side of the clock tower. So as not to undermine their presence by the additional floor,

the clock tower and the two core towers would be increased in height so that the skyline silhouette of the proposal will remain predominantly as existing.

- 6.28 The replaced 7th and 8th floors will read much as existing, finished in matching brickwork with stone bands and punched window openings. The 9th floor will also follow the character of solid wall with punched windows, but will be finished in metal.
- 6.29 The proposal also in-fills the areas in front of the towers on the New Oxford Street side. These areas already have two storeys in-filled with projecting bays and the proposal increases this to five storeys. Officers consider that the result enhances the streamlined aesthetic of the building without compromising the visual contribution of the towers.

Shop fronts and entrances

- 6.30 The proposal seeks to replace all the shop fronts with new high quality designs in keeping with the character of the building. The character appraisal makes reference to high quality original shop fronts currently existing on the New Oxford Street side. In contrast the High Holborn side has plate glass modern shop fronts which currently detract from the building.
- 6.31 It is proposed that all the shop units become double height, and that the square windows that sit in the stone plinth above the shops become clearstory windows to the shops. This height of unit is becoming more typical along New Oxford Street, as seen in the recent permission at St Georges Court opposite. Unlike St Georges Court however, officers have resisted substantial intervention into the stone plinth, requiring the developer to retain the existing stone base and existing openings which we see as the principle defining character of the ground floor. However officers do recognise that retaining the modest shop fronts in the double units would look unresolved
- 6.32 In addition officers have taken into account that on closer inspection, of the 8 shop fronts on the New Oxford Street side, two are poor quality modern systems and are considered to detract. Of the six that remain, three are considered to be in a close to original state, with the other three partially altered. Also the proposals relocate the office entrance to the centre of the New Oxford Street elevation, sitting directly under the projecting central bay. This is considered to be a good location in regard to the composition of the façade, but in itself would lead to the loss of one of the original shop fronts.
- 6.33 The proposed windows are high quality in both design and finish, introducing curved glass corners which respond to the curved corners of the bays on the main façade. They would result in a consistent approach to the whole building and coordinate with the clearstory windows in the plinth. Officers consider that as the original shop fronts are limited in number and, because of the wider aesthetic benefits of high quality coordinated shop fronts, their replacement is acceptable.

Changes to external fabric

- 6.34 The building has Crittall style casement windows, which as is typical of steel windows of this period show signs of decay and deformity. In addition windows of this type are poor performers from a sustainability perspective and challenging to upgrade. The proposal replaces them with new double glazed metal framed windows which replicate the horizontal fenestration patterns.
- 6.35 The projecting curved bays on the New Oxford Street facade are clad in concrete panels which the character appraisal incorrectly labels as faience. The mistake is easy to make as faience was a typical material used in the period for such façade treatments. The concrete was also made very smooth and with a polished finish giving the illusion of a glazed faience finish. Research has uncovered that the original architect had intended to use faience, but the cladding was changed at the request of the freeholders (Crown Estate) who found the material too progressive.
- 6.36 Some of the concrete panels have begun to fail and these would need to be repaired or replaced. The panels are uninsulated and replacing them brings about an opportunity to enhance the thermal properties of the building and reduce energy demand. Also the proposal introduces new bay elements raising the difficulty of achieving a good match with the existing concrete. Bearing these issues in mind, while at the same time recognising there is a technical logic to replacing this element together with the glazing as they share an interface, it is considered that the replacement of the concrete panels with faience would be a timely intervention and in line with the architect's original intent. The replacement work is proposed in the same honeycomb pattern as existing. Officers consider this to result in an enhancement to the appearance of the building.
- 6.37 With the proposed new materials, including new brickwork on the upper floors and cleaned/repaired brickwork below, the reworked building would have a very fresh appearance. The bricks are Flettons from the Peterborough area, which when new have a very light pinkish colour, but have since darkened from 80 years of city conditions. It is proposed that the bricks be given a soot-wash finish to re-instate the patina of age which is considered acceptable.

Dunn's Passage

- 6.38 Currently the service route from New Oxford Street to High Holborn slopes down to basement level, making it impractical as a pedestrian route. This proposal raises the route to grade so that there is level access through the site. The shop frontage from both New Oxford Street and High Holborn would be wrapped round to also front and activate the opened up route. Officers anticipate that any new development on the neighbouring site to the west would bring forward similar active frontage. The opening up of this active and characterful route will bring some of the rich back-street character of Covent Garden and Bloomsbury into this otherwise large scale area of city and is considered to be a positive element that adds choice and variety to the street structure.

Inclusive design

- 6.39 Concerns were raised by officers during the course of the application regarding the provision of a revolving door to the main office entrance with separate accessible doors alongside. Such an arrangement is discouraged as it is not considered an

inclusive design. Following a revision to this aspect to provide sliding doors to this entrance it is considered that this entrance is now suitably accessible. All other redesigned and new entrances to the building and the shop units are shown as having level access and the proposal is therefore now considered to comply with policy DP29.

Amenity issues

- 6.40 There are no surrounding residential occupiers and therefore a sunlight and daylight impact assessment of the roof extension has not been carried out. However given that the addition is for one floor only and the relatively wide main routes separating the site from the buildings opposite, the potential for impact is relatively low. It should be noted that the proposed location for a residential element as part of the 21-31 New Oxford Street (former Post Office) site is on the southern corner wrapping around to Dunn's Passage immediately adjacent Commonwealth House. However the relationship of the additional floor to the likely position of any residential window openings would make it unlikely that there would be any prejudicial effect on daylight to this part of the 21-31 New Oxford Street building.
- 6.41 There are roof terraces proposed around the perimeters of the building at 7th-9th floor levels and at the three corners of the 9th floor roof extension. These are not of sufficient size to host large scale events that may be problematical from a noise perspective to any nearby residents. A larger sized terrace is to be located at 9th floor roof level, but this is towards the eastern apex of the building and not near any existing or proposed residential uses. It is therefore not considered necessary to seek to impose any particular restriction on the use of these terrace areas by condition or other means.
- 6.42 There is potential for noise and disturbance to occur from the roof plant area proposed by this application. Consequently the applicant has submitted a full Noise Survey and Plant Noise Assessment, which has been considered by the Council's specialist environmental health team. It is concluded that the Council's noise criteria is likely to be met by the proposed works. However as the full details of the plant are not yet known, it is considered necessary to impose a condition to secure technical specifications of plant in this roof top area along with the Council's standard noise condition to ensure that any noise generated can continue to be monitored and controlled to reasonable limits.

Transport and public realm

- 6.43 The site is located on the strategic road network (High Holborn) in the Central London Area. The site is also located within the Holborn & Covent Garden controlled parking zone (CPZ) which operates 24 hours for residents parking bays and 0830-1830hrs for single yellow lines and pay and display bays. The site benefits from a Public Transport Accessibility Level (PTAL) rating of 6b (excellent) and is within convenient walking distance of Tottenham Court Road and Holborn Underground stations as well as numerous bus services.

Transport impact

- 6.44 The trip generation assessment estimates that the proposed development would generate 37 additional person trips during the AM peak hour when compared with

the existing building. This would have a minor impact on the operation of the local transport network.

- 6.45 The applicant has provided a framework Travel Plan (TP) in support of the planning application. This is acceptable subject to required amendments which can be secured by a Section 106 agreement.

Cycle parking

- 6.46 The proposal does not introduce any car parking to what is already a car-free site. It does however propose a secure and fully enclosed cycle storage area with capacity for 89 cycles at basement level within the building. This exceeds both the minimum requirements of Camden's Development Policies and the London Plan revised early minor alterations of October 2013. The majority of the cycle parking spaces would be provided using Josta 2-tier racks with the remainder (10% of spaces) being Sheffield stands. The main access to the basement is provided via a goods/cycle lift adjacent to Dunn's Passage. Stairs with a wheel channel for cyclists are also proposed at two locations and these would be accessed from New Oxford Street. The proposals also include eight showers, one disabled shower and 98 lockers, providing more than one locker per cyclist which is welcomed.
- 6.47 Detailed plans and specifications of the cycle parking spaces should be secured by condition.

Highway works

- 6.48 Various highway works would need to be undertaken to reinstate and improve the condition of the surfacing following the construction works. The works would also include the tying in of Dunn's Passage into the public highway. The applicant has also suggested some minor amendments to the existing traffic management orders directly adjacent to the site (removal of redundant diplomatic parking space and conversion to single yellow line). This would be considered by the Council as part of any public highway and public realm improvements to be delivered following completion of the proposed scheme. A financial contribution should be secured as a Section 106 planning obligation if planning permission is granted.

Public realm

- 6.49 In order to review the pedestrian network surrounding the proposed development, a Pedestrian Environment Review System (PERS) audit was undertaken on 9 May 2014. This assessment evaluated the quality of the local pedestrian network, identifying the extent to which it meets pedestrian needs. The PERS audit showed that the quality of environment for pedestrians was not pleasant, particularly around the junctions of High Holborn with Museum Street and New Oxford Street with Bloomsbury Way. There are a number of unnecessary obstructions on the footways that could be reduced and more wayfinding maps could be introduced. There are also various cycle facilities in the vicinity of the site including a southbound contra-flow cycle lane on Newton Street, an eastbound segregated cycle track on High Holborn and a northbound segregated contra-flow cycle lane on Bury Place. Some of these cycle facilities are in poor condition and/or are in need of being upgraded.
- 6.50 The Council is currently investigating the reduction or removal of traffic on the Holborn gyratory in order to improve conditions for walking, cycling and bus travel. This area roughly extends west to east from Museum Street to Gray's Inn Road

and includes a number of major roads leading off of it. Discussions are on-going with the Council and various neighbouring land owners regarding financial contributions towards this initiative. It is considered having regard to the scale and nature of the proposal that £50,000 would be justified as a contribution towards pedestrian, cycling and environmental improvements in the area and would be secured by S106. Further monies may be contributed for specified public realm enhancements depending on the outcome of discussions between the applicant, the Council and neighbouring land owners. In the event this is agreed, the relevant works would be costed and added to the highway works contribution as referred to above.

- 6.51 The applicant is committed to the opening up and reactivation of Dunn's Passage. Due to issues with ownership, the proposed works for Dunn's Passage itself need to be developed and progressed in conjunction with the owners of the neighbouring property at 21-31 New Oxford Street. However the proposed service access alongside Dunn's Passage would be accessible to the public outside of servicing times, the detailed arrangements for which are included in a Service Management Plan. The longer term plan is to part remove the boundary wall adjacent to 21-30 New Oxford Street and to provide a raised footpath along Dunn's Passage approximately 50mm above the level of the service yard to create a traditional street appearance. The role of the proposal in facilitating this plan is to be welcomed.

Servicing and construction traffic

- 6.52 The transport assessment submitted in support of the planning application includes a servicing strategy for the site. Deliveries to the office and retail units would be undertaken from two loading bays adjacent to Dunn's Passage and an existing loading bay on New Oxford Street directly adjacent to the site. The main vehicular access to the site for servicing is proposed to be via the road adjacent to Dunn's Passage, giving access to servicing bays on the western façade of the building. The access would be one-way with access via High Holborn and egress onto New Oxford Street. The junctions would take the form of simple crossovers.
- 6.53 The proposed servicing arrangements appear to be acceptable. However, conflicts with road users, particularly cyclists and pedestrians would need to be managed/mitigated. This is a particular issue for servicing from Dunn's Passage which will also serve as a pedestrian route between High Holborn and New Oxford Street during standard working hours. A Delivery and Servicing Management Plan should therefore be secured as a Section 106 planning obligation if planning permission is granted.
- 6.54 A draft construction management plan has been submitted in support of the application which covers the main points as to the proposed works and how they would be undertaken. A more detailed construction traffic management plan would be prepared once a Principal Contractor has been appointed. This should cover additional details in regard to vehicle routes, sustainable travel planning guidance for construction workers, various highways licenses required, pedestrian and cyclist safety and measures to avoid construction vehicle movements during peak times. The site would be required to register with the Considerate Constructors Scheme. We would also expect the CMP to confirm that the proposed works will be undertaken in accordance with the best practice guidelines in TfL's Standard for

Construction Logistics and Cyclist Safety (CLOCS) scheme. The final CMP would be secured under S106.

- 6.55 In summary, the proposals are considered to be acceptable in transport terms subject to the final approved plans and financial contributions as outlined above being appropriately secured via a section 106 agreement.

Sustainability and energy

- 6.56 The LDF policy DP22 and CPG3 require all developments involving an increase of over 500 sqm of commercial floorspace to submit a BREEAM pre-assessment with an expected target rating of 'Very Good', or "Excellent" from 2016. Within the respective categories there is an expectation for obtaining 60% of the un-weighted credits in Energy; 60% in Water and 40% in Materials.
- 6.57 A BREEAM pre-assessment has been prepared by Renewable Environmental Services (RES) which targets an overall rating of 82.9% 'Excellent'. It also indicates that well over the target credits will be achieved in Water and Materials categories, i.e. Water – 100% and Materials 92%, although slightly less in Energy – 56.5%. The lower energy performance is however, to be largely expected in a refurbishment of a historic building in a conservation area, where there is a limited extent to which materials can be replaced to upgrade the thermal performance of the building shell.
- 6.58 It should be noted that the energy performance would reduce significantly if the existing precast panels around the nose and bays of the building were to be repaired and retained instead of the current proposal which is to replace them. Their replacement provides an opportunity for insulating the wall behind these panels prior to installing the replacement cladding.
- 6.59 A separate energy strategy report (also by RES) has been submitted which addresses the 3-stages of the London Plan Energy Hierarchy of Lean, Clean and Green design measures. This sets out the energy consumption and carbon savings for each stage. The proposed enhancements to the building shell, along with its new heating, cooling, ventilation and lighting systems, would reduce carbon emissions by 22.8% over the base. Due to the site's location it is not possible to connect to any local heat network and the modest sized extension to an existing single-use building would not justify combined heat and power or a communal energy system being installed. Therefore there are no additional carbon savings to be gained from the 'be clean' stage of the hierarchy. However, in addressing the third stage (green) of the energy hierarchy an assessment of alternative green energy technologies has been carried out. This has identified that both air source heat pumps and photovoltaics (PVs) are suited to the development and can be used alongside the building's existing heating and cooling systems. The air source heat pumps in particular would be suitable to serve 100% of the space heating and cooling demands of the building. The green measures are calculated to achieve an additional 7.1% reduction in carbon emissions. The two combined are targeted to achieve total carbon emissions of 28.2%.
- 6.60 As this is an existing building, CPG3 section 4 seeks that as a guide, at least 10% of the project cost should be spent on improving the environmental performance of

the building in order to reduce carbon emissions. In view of the extent of measures as outlined above, the development is considered likely to meet this requirement.

- 6.61 Due to the use of roof spaces for amenity terraces, semi-open plant and ETFE roof over the atrium, there are limited opportunities for green or brown roof systems to be installed. However, the submitted plans indicate that installing PV panels on top of the roof plant is a feasible option and the precise detail as to the number and extent of these would be expected to come forward as part of a final energy plan in realising the target carbon savings outlined above.
- 6.62 The BREEAM pre-assessment has assumed the appointment of an ecologist to mitigate any ecological impact and enhance on site biodiversity and further details of this and the range of measures to be employed would also be expected as part of a post construction review.
- 6.63 A post construction BREEAM review, along with an Energy Plan should be secured via S106 to ensure that the targeted measures, as outlined above, are achieved in the final design. On this basis the requirements of policies DP22 and CS13 are considered to have been met by the proposal.

Other matters

- 6.64 Since the increase in floorspace is below 1000sqm the Council's policies do not require any obligations on the development, including financial contributions, to provide construction apprenticeships. However, in view of the size of the existing building to be refurbished, the developer has agreed to enter into an agreement for an employment plan to work with LB Camden Economic Development Team and Kings Cross Construction to ensure local residents are provided with opportunities to apply for jobs throughout the construction period with a target of 20% Camden residents employed. They would also sign up to the Camden local procurement code.

Community infrastructure levy

- 6.65 The development would be liable for the Crossrail CIL charge because it involves the addition of more than 100 square metres. CIL is charged on gross internal area (GIA) which in this case is 1256sqm. It may be noted that the GIA floorspace uplift is significantly higher than the equivalent GEA figure. This is due to the effect of partially infilling the lightwell (already included in the existing GEA figure) to gain additional office, atrium, lift core and circulation space which is added to the GIA total.
- 6.66 Based on the Mayor's CIL charging schedule and the information given on the plans, the charge would be £62,800 (1256sqm x £50). This will be collected by Camden after the scheme is implemented and could be subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index. An informative will be attached advising the applicant of this charge.

7. CONCLUSION

- 7.1 The proposed development would enable an increase in the quality of the existing Class B1 floorspace and the attractiveness of the space to future tenants increasing its chances of continued occupation into the longer term. The addition of a further storey of accommodation together with the other alterations and additions would result in a modest expansion of the employment floorspace on the site which accords with Camden's growth policies, and has at the same time been assimilated with minimal impact on the skyline or the building's existing character. Furthermore, the reorganisation of the building entrance and ground floor retail spaces and the related changes to the shop fronts will enhance the building's relationship with the adjacent streets. In combination with the move to bring the servicing up to street level to re-activate the currently closed-off Dunn's Passage, it is considered that the proposals will bring about a real improvement to the public realm and be seen to make a positive contribution to the character and appearance of the Bloomsbury Conservation Area overall.
- 7.2 Planning conditions will seek to ensure that nearby amenity is maintained and an array of measures will also be secured via the S106 Legal Agreement, including significant highways works and sustainability benefits.
- 7.3 Members are referred to the note from the Legal Division at the start of the Agenda.
- 7.4 Planning Permission is recommended subject to a S106 Legal Agreement.

8. LEGAL COMMENTS

- 8.1 Members are referred to the note from the Legal Division at the start of the Agenda.

9. RECOMMENDATION: Grant planning permission with conditions and subject to a S106 legal agreement with the following heads of terms:

- 1) Financial contribution of £333,200 towards provision of new housing in the area**
- 2) Contributions for highway works (amount tbc)**
- 3) Business Travel Plan (including contribution of £5,902 to cover the costs of monitoring and review over a 5 year period) also to include TRICS after study and provide TfL and Camden with the results on completion of the development**
- 4) Service Management Plan**
- 5) Construction Management Plan**
- 6) A financial contribution of £50,000 towards Pedestrian, Cycling and Environmental improvements in the vicinity of the site**
- 7) Sustainability Plan**
- 8) Energy Strategy**
- 9) Plan for local construction jobs and procurement entailing:**
 - i. working to a target of 20% local recruitment.**
 - ii. the applicant advertise all construction vacancies and work placement opportunities exclusively with the Kings Cross Construction Skills Centre for a period of 1 week before marketing more widely.**

- iii. the applicant provide a specified number (to be agreed) of construction work placement opportunities of not less than 2 weeks each, to be undertaken over the course of the development, to be recruited through the Council's Kings Cross Construction Skills Centre.**
- iv. the applicant to sign up to the Camden Local Procurement Code, as per section 8.19 of CPG8**
- v. the applicant provide a local employment, skills and local supply plan setting out their plan for delivering the above requirements in advance of commencing on site**

Condition(s) and Reason(s):

- 1 The development hereby permitted must be begun not later than the end of three years from the date of this permission.

Reason: In order to comply with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).

- 2 The development hereby permitted shall be carried out in accordance with the following approved plans:

Existing (prefix 1975_PL) 00 rev P2, 01 rev P2, 02 rev P2, 03 rev P2, 04 - 13 revs P1, 20 - 27 revs P1; Demolition works (prefix 1975_PL) 31 - 42 revs P1, 50 - 55 revs P1; Proposed (prefix 1975_PL) 61 rev P2, 62 rev P2, 64 - 73 revs P1, 80 - 82 revs P1 83 rev P2, 84 - 85 rev P1, 87 rev P1, 88 rev P2, 89 - 90 rev P1.

Supporting documents: Design & Access Statement by Orms dated 9th Sept 2014, Planning Statement by DP9 dated July 2014, Covering letter by DP9 dated 31.07.14, Historic Building Report by Donald Insall dated July 2014, Transport Statement by Ove Arup dated July 2014, Construction Traffic Management Plan dated July 2014, BREEAM Report by RES dated July 2014, Energy Strategy Report by RES dated July 2014, Air Quality Statement by Long & Partners dated July 2014, Environmental Noise and Vibration Report by Sandy Brown dated July 2014, Statement of Community Involvement.

Reason: For the avoidance of doubt and in the interest of proper planning.

- 3 All new external work shall be carried out in materials that resemble, as closely as possible, in colour and texture those of the existing building, unless otherwise specified in the approved application.

Reason: To safeguard the appearance of the premises and the character of the immediate area in accordance with the requirements of policy CS14 of the London Borough of Camden Local Development Framework Core Strategy and policies DP24 and DP25 of the London Borough of Camden Local Development Framework Development Policies.

- 4 Detailed drawings, or samples of materials as appropriate, in respect of the following, shall be submitted to and approved in writing by the local planning authority before the relevant part of the work is begun:

- a) Details including sections at 1:10 of all windows (including jambs, head and cill), ventilation grills, external doors and gates;
- b) Plan, elevation and section drawings, including fascia, cornice, pilasters and glazing panels of the new shopfronts at a scale of 1:10;
- c) Manufacturer's specification details of all facing materials (to be submitted to the Local Planning Authority) and samples of those materials (to be provided on site).

The relevant part of the works shall be carried out in accordance with the details thus approved and all approved samples shall be retained on site during the course of the works.

Reason: To safeguard the appearance of the premises and the character of the immediate area in accordance with the requirements of policy CS14 of the London Borough of Camden Local Development Framework Core Strategy and policies DP24 and DP25 of the London Borough of Camden Local Development Framework Development Policies.

- 5 The Class A3 use hereby permitted shall not be carried out outside the following times: 08:00 to 23:30 Monday to Thursdays, 08:00 to 00:00 on Fridays and Saturdays and 08:00 to 22:30 on Sundays and Bank Holidays.

Reason: To safeguard the amenities of the adjoining premises and the area generally in accordance with the requirements of policies CS5 and CS7 of the London Borough of Camden Local Development Framework Core Strategy and policy DP26, DP28 and DP12 of the London Borough of Camden Local Development Framework Development Policies.

- 6 No music shall be played on the premises in such a way as to be audible within any adjoining premises or on the adjoining highway.

Reason: To safeguard the amenities of the adjoining premises and the area generally in accordance with the requirements of policies CS5 and CS7 of the London Borough of Camden Local Development Framework Core Strategy and policy DP26 and DP12 of the London Borough of Camden Local Development Framework Development Policies.

- 7 Technical specification details of the mechanical plant to be installed within the roof top plant area as shown on the approved floor plans, together with an accompanying acoustic report, shall be submitted to and approved by the local planning authority prior to installation of this plant. The plant shall not be operated other than in complete accordance with such measures as may be approved.

Reason: To safeguard the amenities of occupiers of the proposed use, adjoining premises and the area generally in accordance with the requirements of policy CS5 of

the London Borough of Camden Local Development Framework Core Strategy and policies DP26 and DP28 of the London Borough of Camden Local Development Framework Development Policies.

- 8 Noise levels at a point 1 metre external to sensitive facades shall be at least 5dB(A) less than the existing background measurement (LA90), expressed in dB(A) when all plant/equipment (or any part of it) is in operation unless the plant/equipment hereby permitted will have a noise that has a distinguishable, discrete continuous note (whine, hiss, screech, hum) and/or if there are distinct impulses (bangs, clicks, clatters, thumps), then the noise levels from that piece of plant/equipment at any sensitive façade shall be at least 10dB(A) below the LA90, expressed in dB(A).

Reason: To safeguard the amenities of the adjoining premises and the area generally in accordance with the requirements of policy CS5 of the London Borough of Camden Local Development Framework Core Strategy and policies DP26 and DP28 of the London Borough of Camden Local Development Framework Development Policies.

- 9 Before the development commences, detailed plans and specifications of the 89 secure and covered cycle parking spaces in the area shown on the approved basement floor plan shall be submitted to and approved by the local planning authority. The approved facility shall thereafter be provided in its entirety prior to the first occupation of the development, and permanently retained thereafter.

Reason: To ensure the development provides adequate cycle parking facilities in accordance with the requirements of policy CS11 of the London Borough of Camden Local Development Framework Core Strategy and policy DP17 of the London Borough of Camden Local Development Framework Development Policies.

Informative(s):

- 1 Your proposals may be subject to control under the Building Regulations and/or the London Buildings Acts which cover aspects including fire and emergency escape, access and facilities for people with disabilities and sound insulation between dwellings. You are advised to consult the Council's Building Control Service, Camden Town Hall, Argyle Street WC1H 8EQ, (tel: 020-7974 6941).
- 2 Noise from demolition and construction works is subject to control under the Control of Pollution Act 1974. You must carry out any building works that can be heard at the boundary of the site only between 08.00 and 18.00 hours Monday to Friday and 08.00 to 13.00 on Saturday and not at all on Sundays and Public Holidays. You are advised to consult the Council's Compliance and Enforcement team [Regulatory Services], Camden Town Hall, Argyle Street, WC1H 8EQ (Tel. No. 020 7974 4444 or on the website <http://www.camden.gov.uk/ccm/content/contacts/council-contacts/environment/contact-the-environmental-health-team.en> or seek prior approval under Section 61 of the Act if you anticipate any difficulty in carrying out construction other than within the hours stated above.
- 3 The Mayor of London introduced a Community Infrastructure Levy (CIL) to help

pay for Crossrail on 1st April 2012. Any permission granted after this time which adds more than 100sqm of new floorspace or a new dwelling will need to pay this CIL. It will be collected by Camden on behalf of the Mayor of London. Camden will be sending out liability notices setting out how much CIL will need to be paid if an affected planning application is implemented and who will be liable.

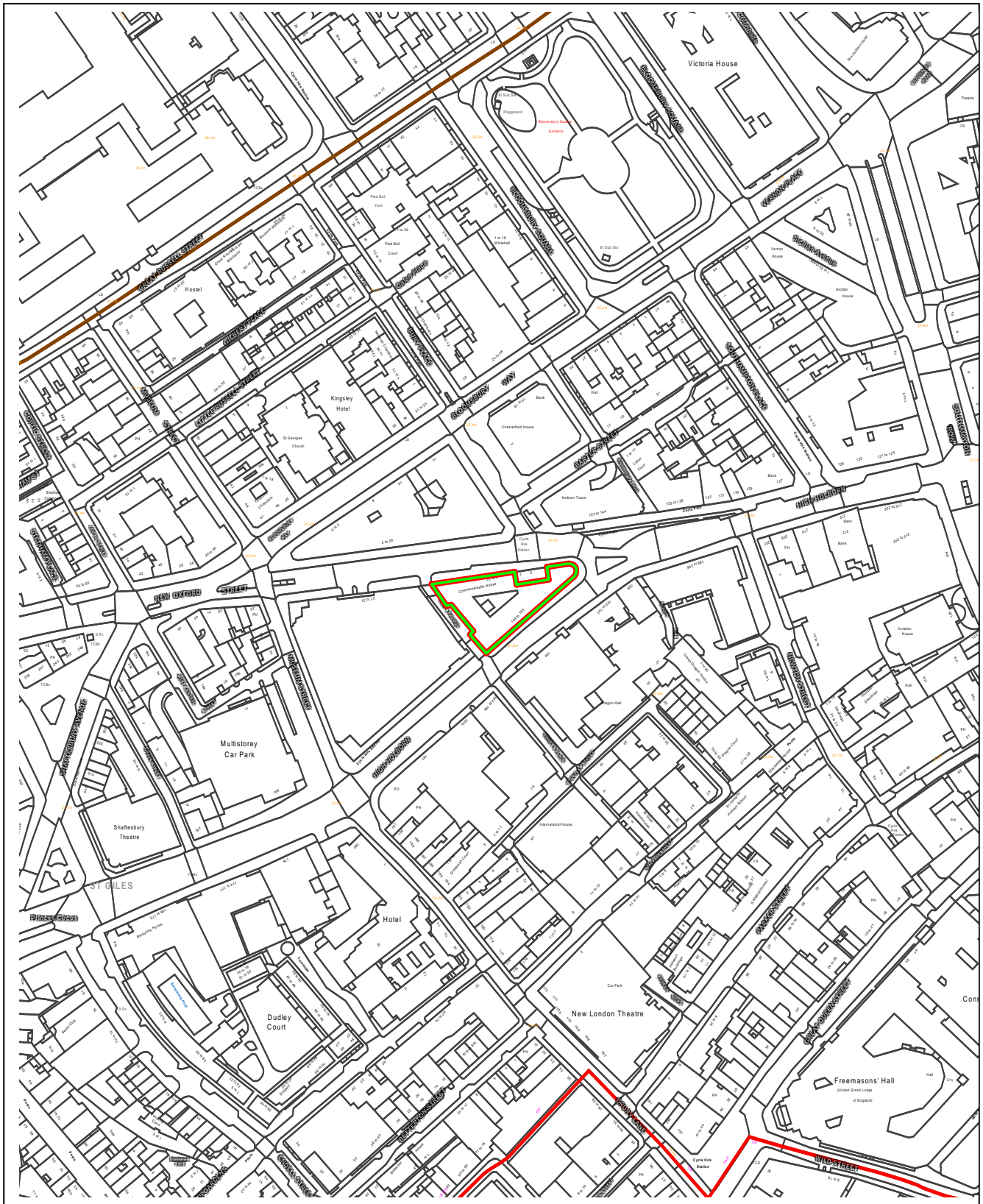
The proposed charge in Camden will be £50 per sqm on all uses except affordable housing, education, healthcare, and development by charities for their charitable purposes. You will be expected to advise us when planning permissions are implemented. Please use the forms at the link below to advise who will be paying the CIL and when the development is to commence. You can also access forms to allow you to provide us with more information which can be taken into account in your CIL calculation and to apply for relief from CIL.

<http://www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil>

We will then issue a CIL demand notice setting out what monies needs to be paid when and how to pay. Based on the Mayor's CIL charging schedule and the information given on the plans, the charge would be £62,800 (1256sqm x £50). Failure to notify Camden of the commencement of development will result in a surcharge of £2500 or 20% being added to the CIL payment. Other surcharges may also apply for failure to assume liability and late payment. Payments will also be subject to indexation in line with the construction costs index.

Please send CIL related documents or correspondence to CIL@Camden.gov.uk

- 4 Your attention is drawn to the need for compliance with the requirements of the Environmental Health regulations, Compliance and Enforcement team, [Regulatory Services] Camden Town Hall, Argyle Street, WC1H 8EQ, (tel: 020 7974 4444) particularly in respect of arrangements for ventilation and the extraction of cooking fumes and smells.
- 5 This permission is granted without prejudice to the necessity of obtaining consent under the Town and Country Planning (Control of Advertisements) (England) Regulations 2007. Application forms may be obtained from the Council's website, www.camden.gov.uk/planning or the Camden Contact Centre on Tel: 020 7974 4444 or email env.devcon@camden.gov.uk.
- 6 You are advised that if implemented, the alternative use permission for the A1/B1/A3 unit hereby approved gives flexibility of use for 10 years from the date of this permission. After 10 years the lawful use would revert to whichever of the uses is taking place at the time.
- 7 Your attention is drawn to the fact that there is a separate legal agreement with the Council which relates to the development for which this permission is granted. Information/drawings relating to the discharge of matters covered by the Heads of Terms of the legal agreement should be marked for the attention of the Planning Obligations Officer, Sites Team, Camden Town Hall, Argyle Street, WC1H 8EQ.



Application No: 2014/4983/P

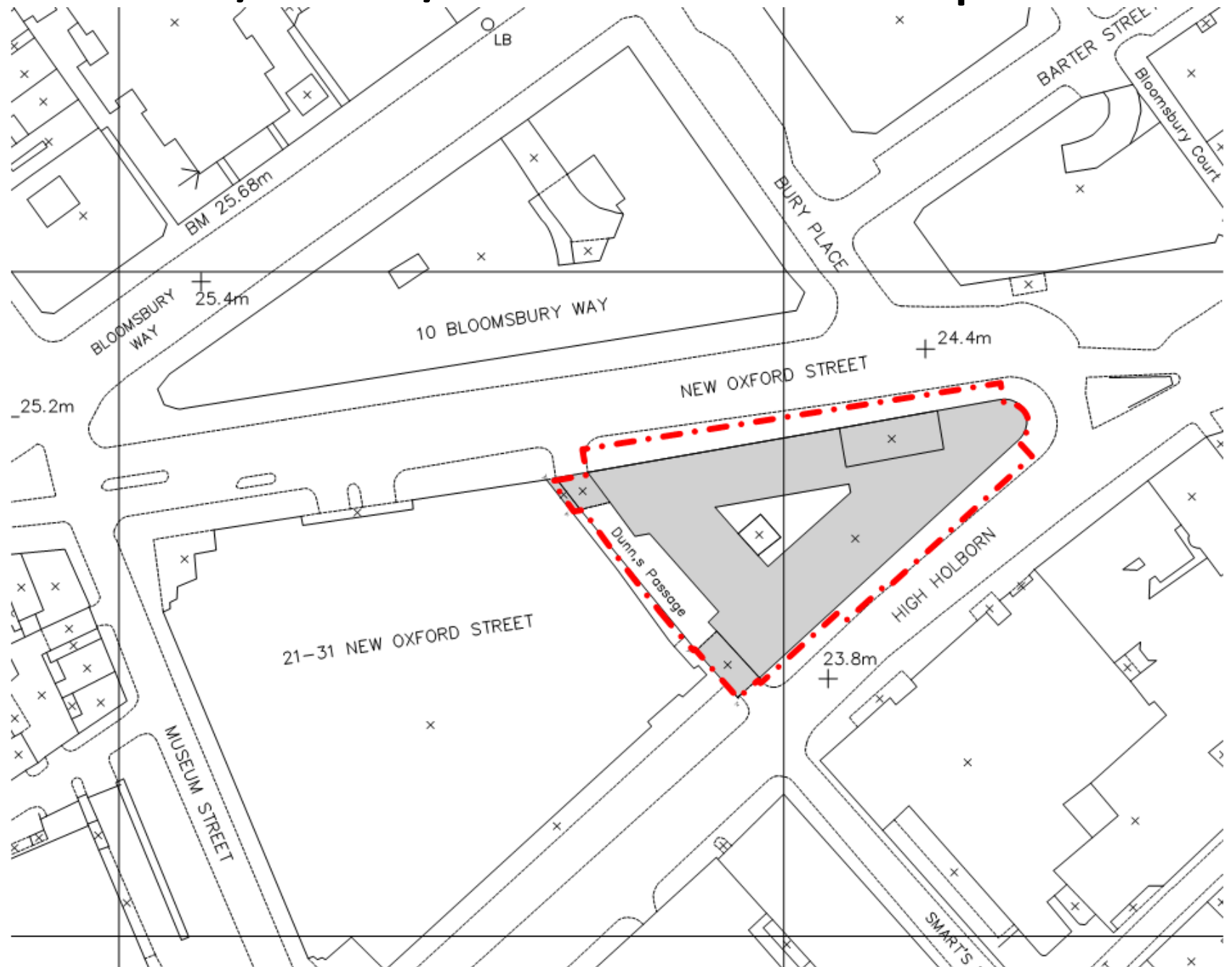
**Commonwealth House
1 - 19 New Oxford Street
London, WC1A 1NQ**

**Scale:
1:2520
Date:
26-Sep-14**



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2014/4983/P - Site location plan



Existing – eastern apex



Existing – north elevation

Commonwealth House



New Oxford Street (looking west)

Existing – south elevation

Commonwealth House

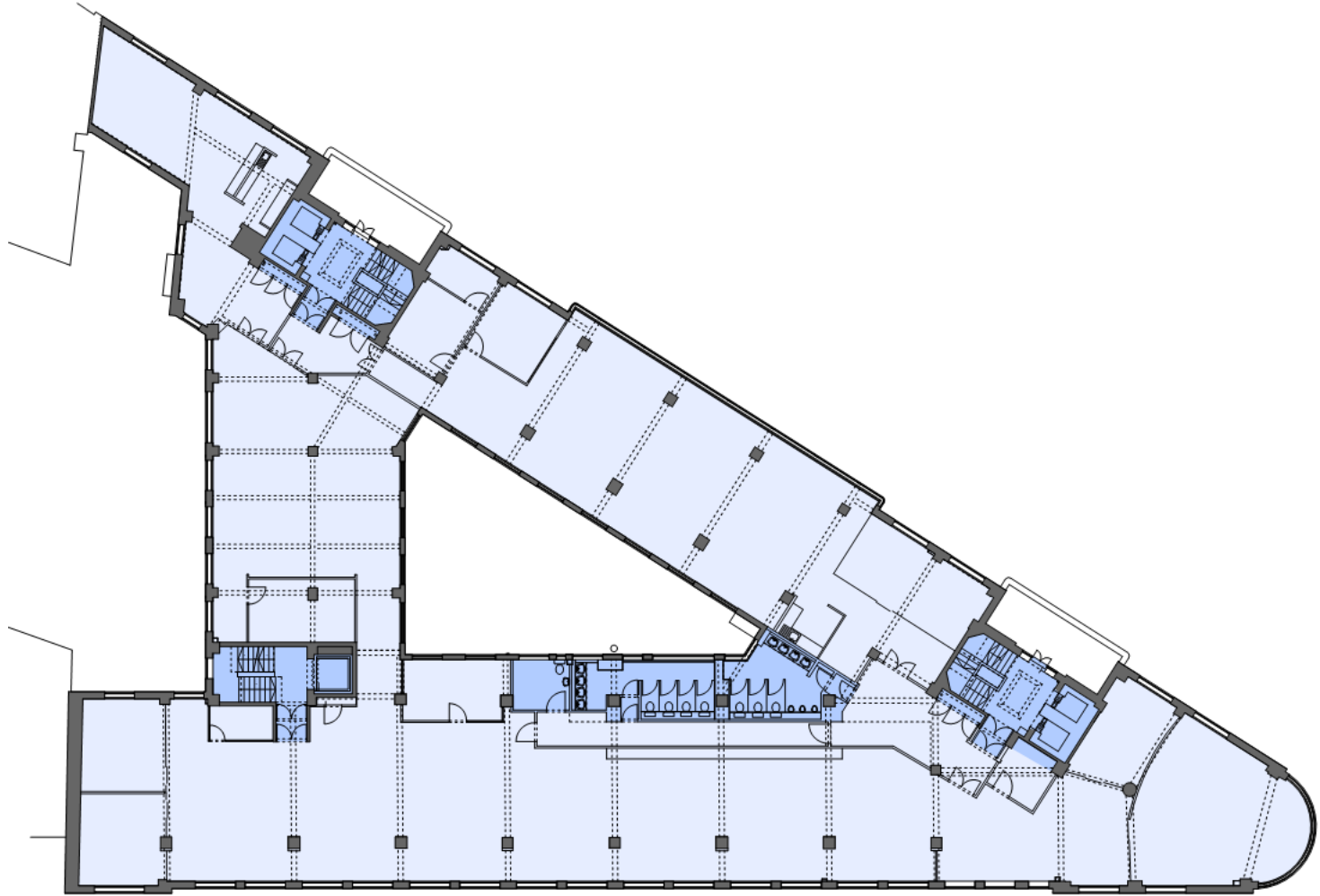


High Holborn (looking north)

Existing ground floor showing original/non-original shopfronts



Existing typical upper floor plan showing existing cores and toilet block



Existing shop fronts



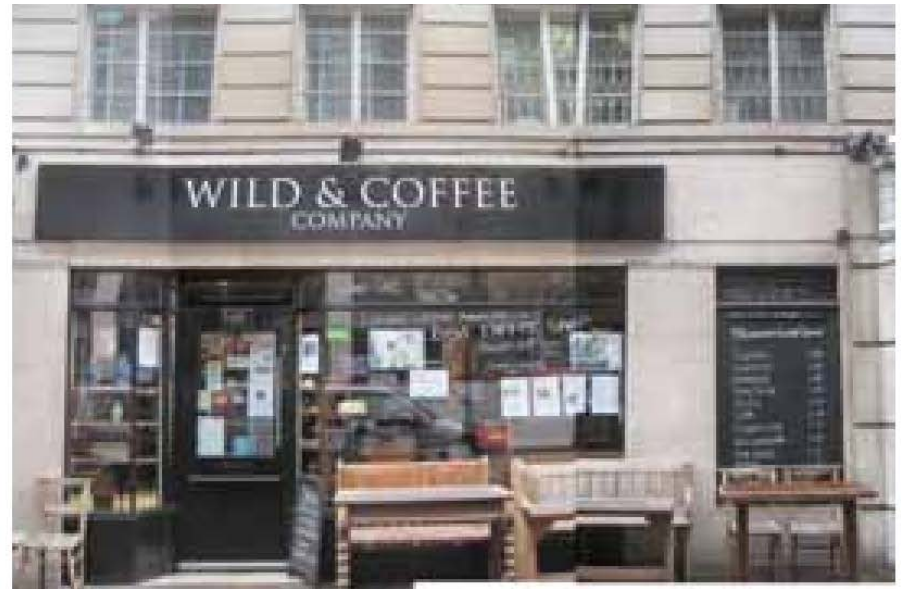
New Oxford Street (altered)



High Holborn (replaced)



New Oxford Street (replaced)

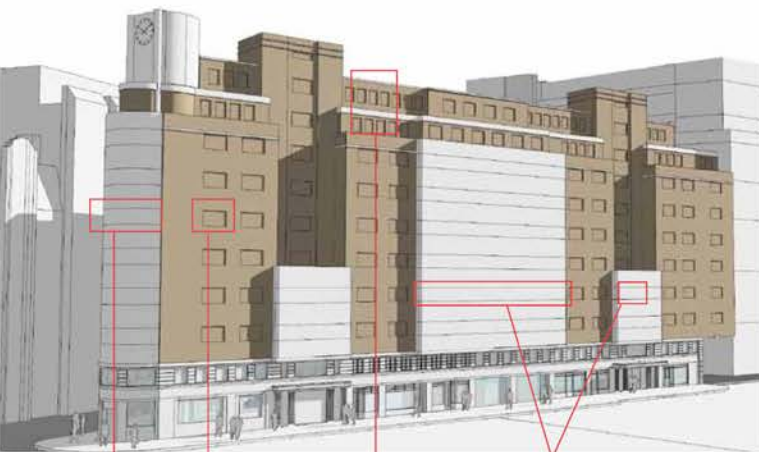


New Oxford Street (original)

Existing windows study

Windows

There are 4 window key types on the existing building with slight variations of each. The windows are in poor condition with rust damage. Many have bowed beyond repair and have been subsequently screwed shut. There is secondary glazing to the inside face.



Type C

Type A

Type D

Type B

Steel windows with horizontal glazing bars, outward opening pane either side of fixed central window. Secondary glazing to internal face.



Type A - 'punch' windows

Steel windows with horizontal glazing bars, outward opening pane either side of fixed central window. Curved ends, Secondary glazing to internal face.



Type B - strip windows (New Oxford Street bays)

Facetted steel windows (these are not original) with horizontal glazing bars, outward opening pane either side of fixed central window. Facetted,



Type C - strip windows to 'nose' (facetted)

Tall steel windows, no glazing bars, intermittent top fanlight with numerous cutouts for extract fans etc.



Type D - upper floor tall windows

Proposed ground floor and access strategy

The following layout is a summary of the key access and egress points to the building. All access / egress points from pavement level into the building at ground floor level will be level access.

Further details can be found in the Transport report by Arup which is an appendix to this report.

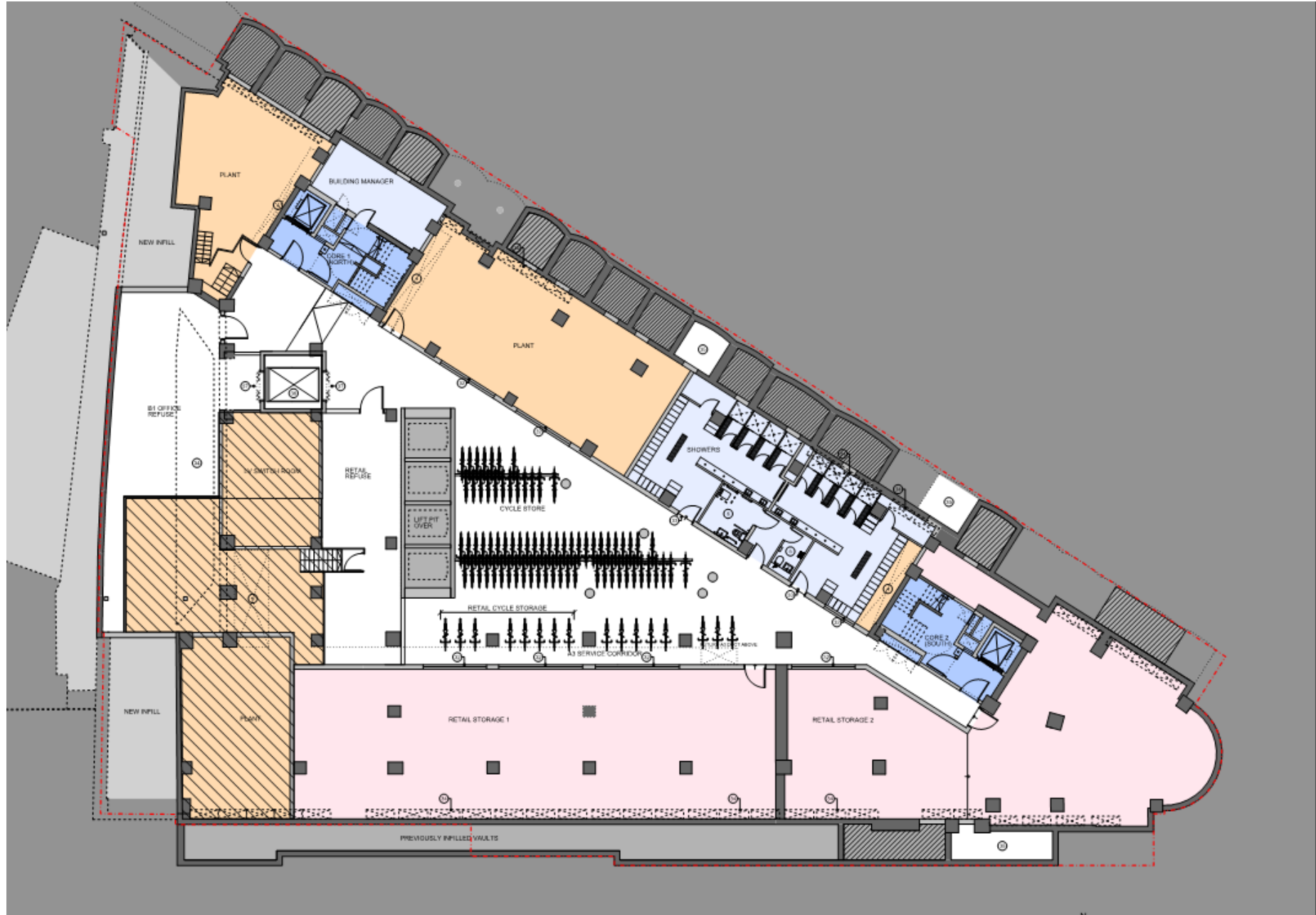
- 1 4 x 17p lift serving G-9 *
- 2 fire fighting lift serving B-9**
- 3 fire fighting stair serving B-9**
- 4 cycle / refuse / goods lift serving B-G

* end lift serves roof terrace
** south core serves B-8th only

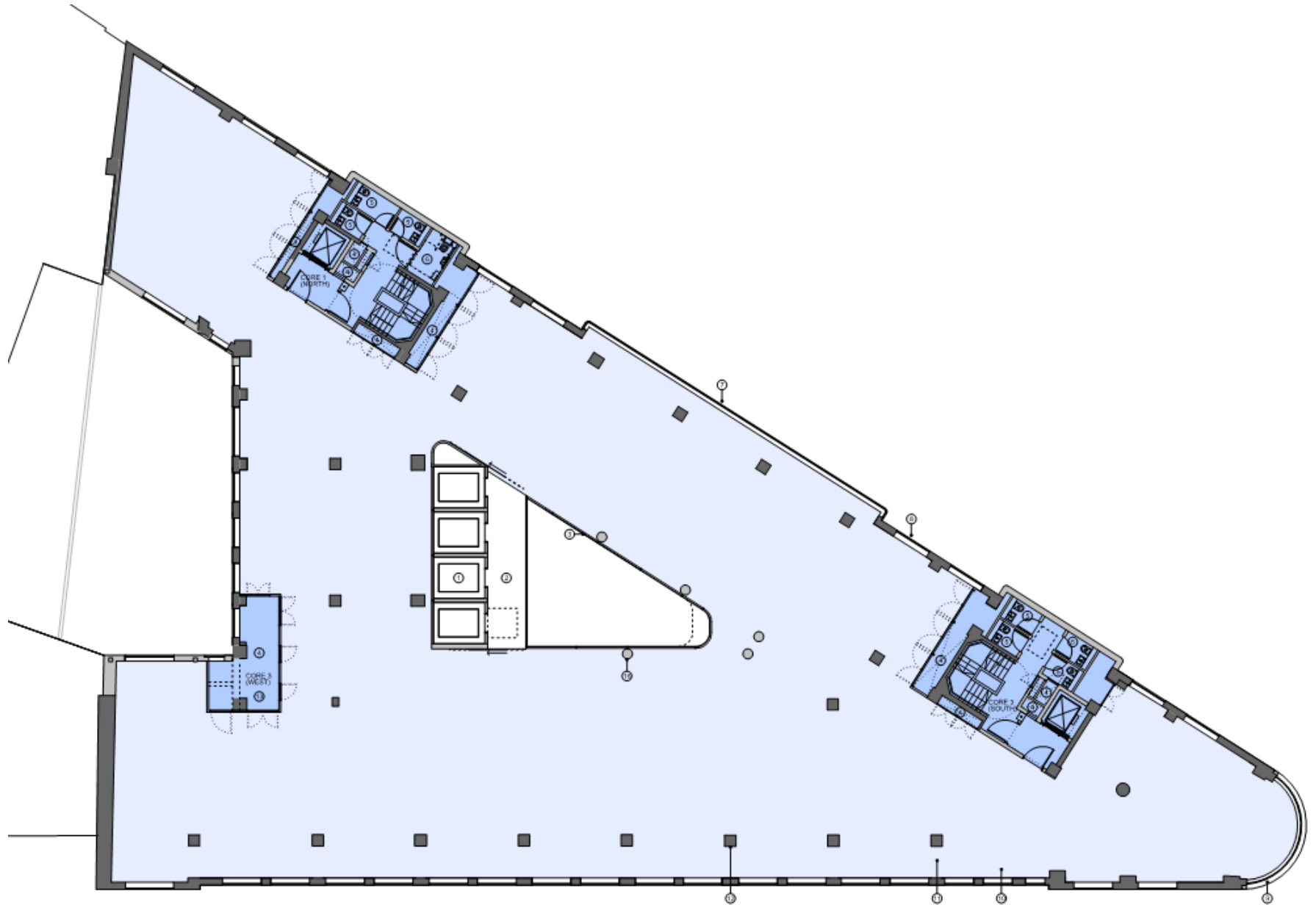
-  Level Office pedestrian entrance & egress from pavement
-  Level Retail pedestrian access from pavement
-  Level Cycle access & egress, level access from pavement (level access to basement via dedicated lift or stepped access via stair with wheel rail)
-  Vehicular loading access (refer to Arup Transport report)
-  Loading bay
-  Fire escape from cores
-  Refuse Access - refuse to be brought to grade for collection from basement via dedicated lift



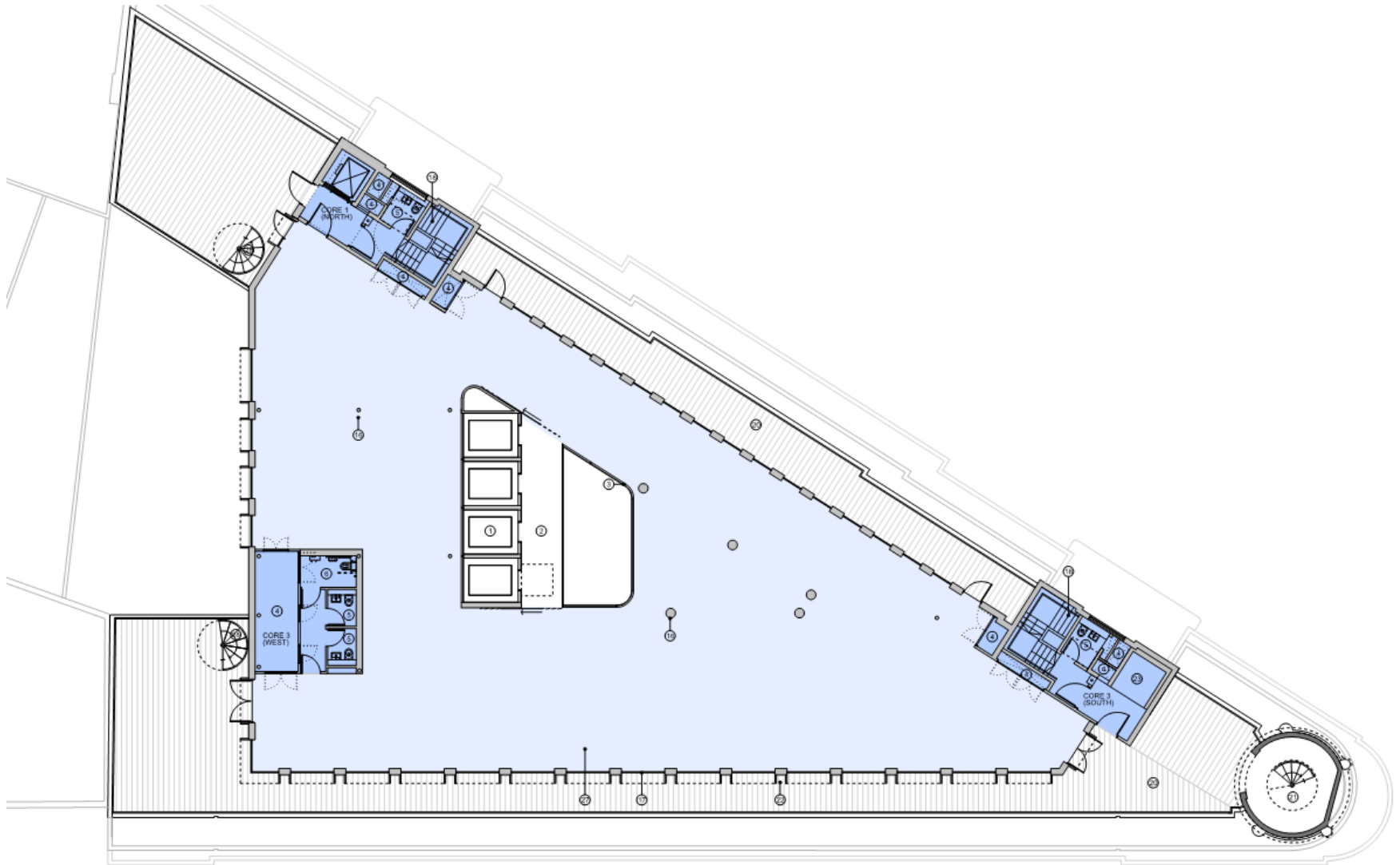
Proposed basement plan



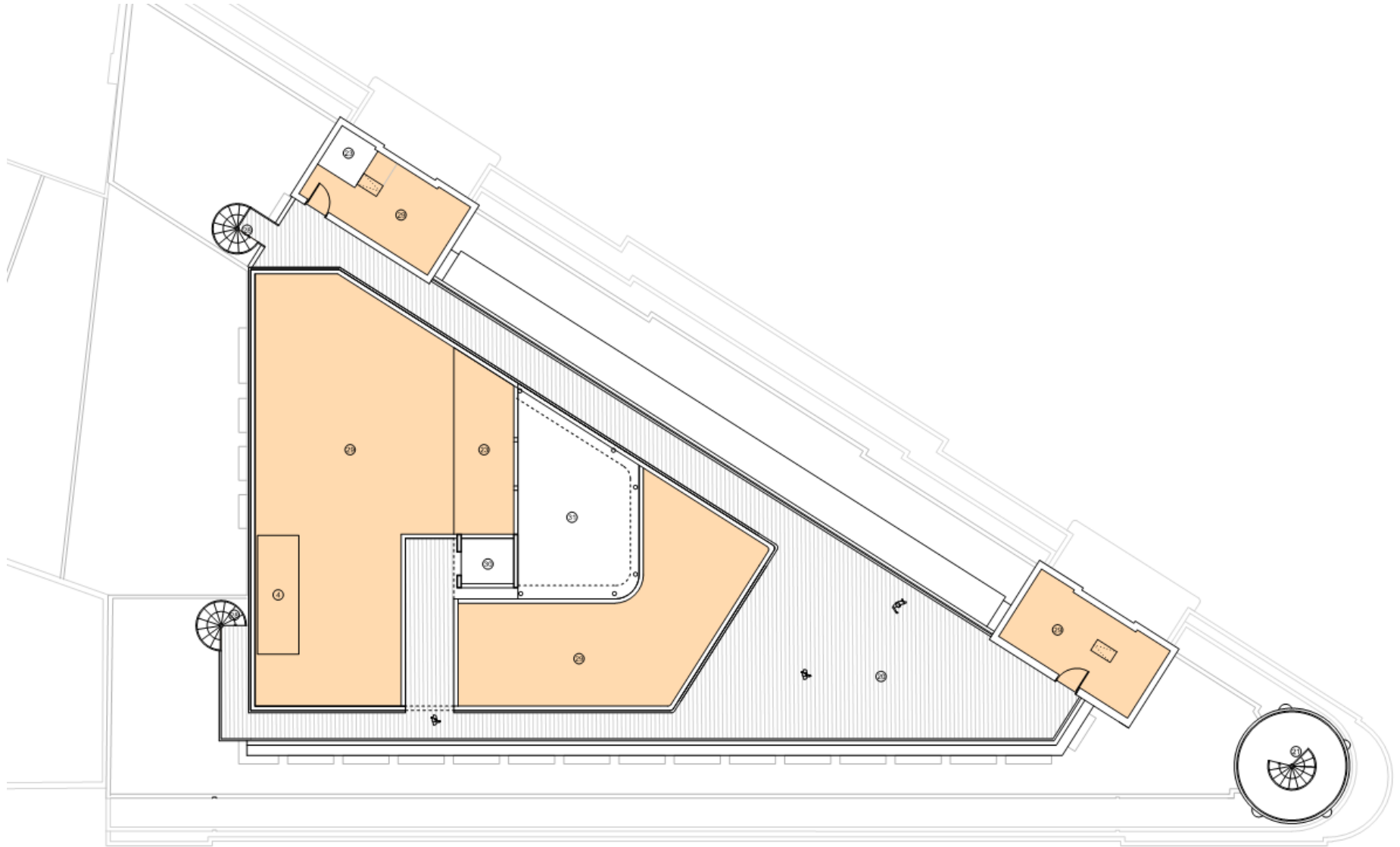
Proposed typical upper floor



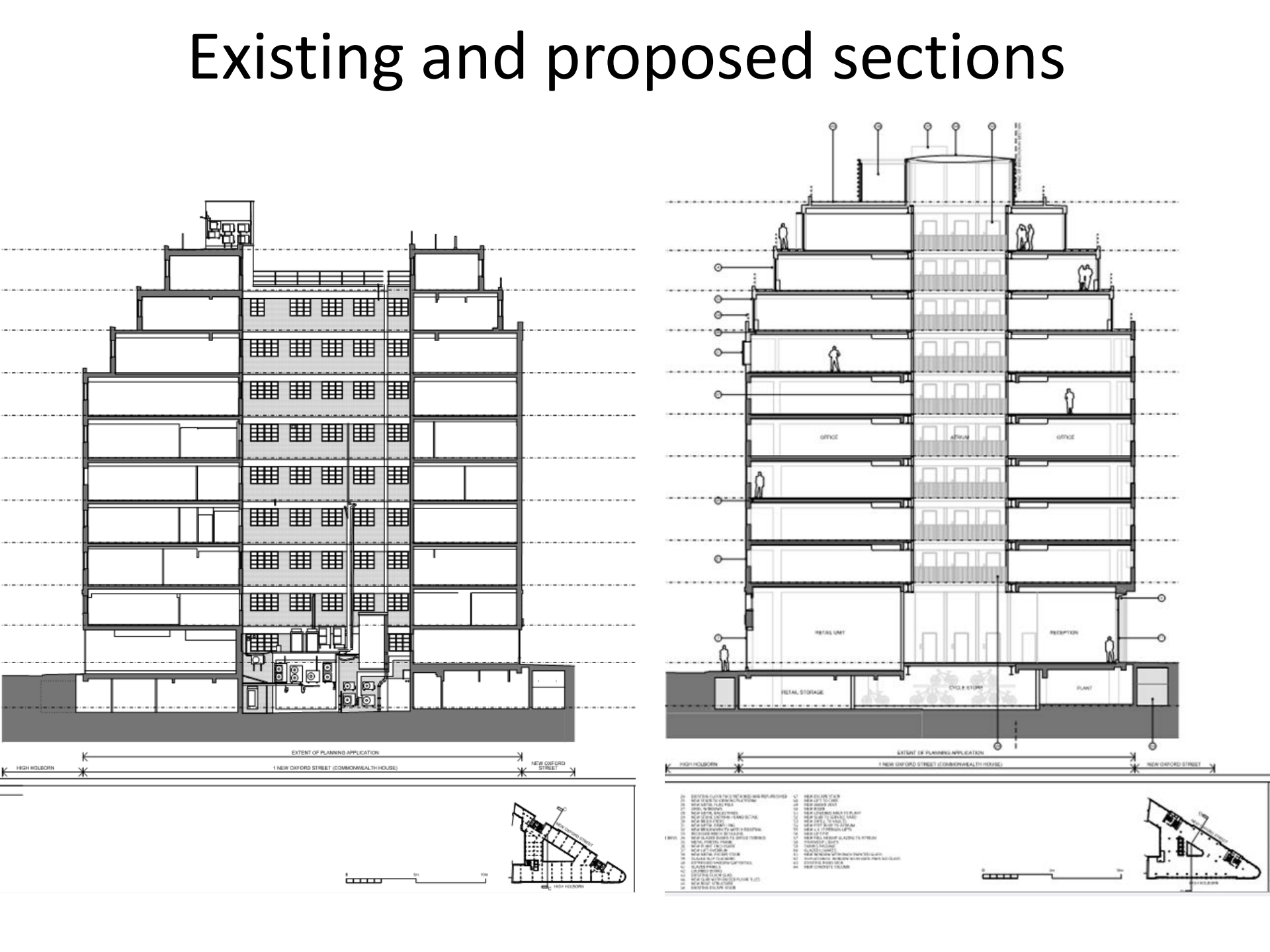
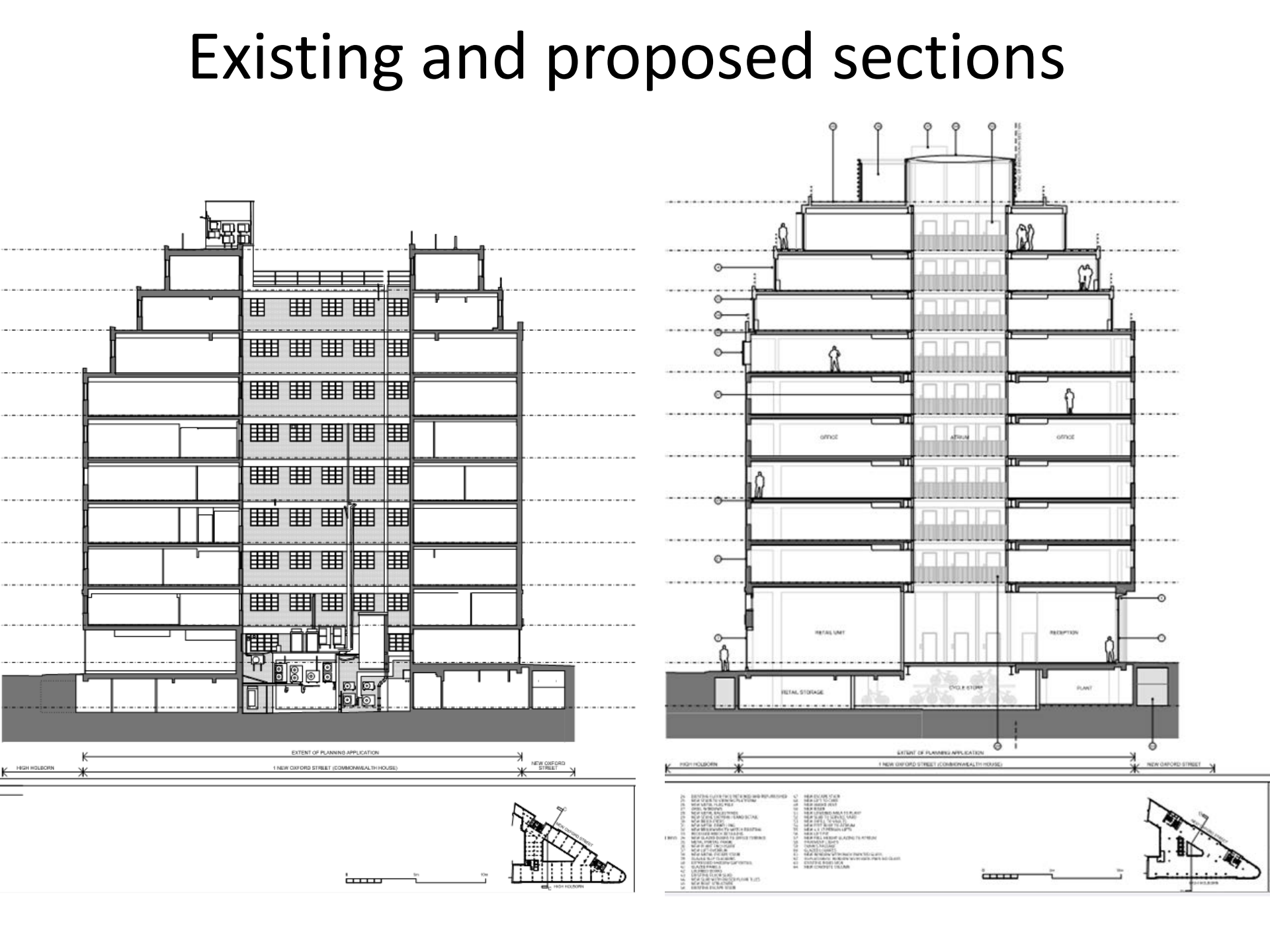
Proposed roof extension (ninth floor)



Proposed roof plan

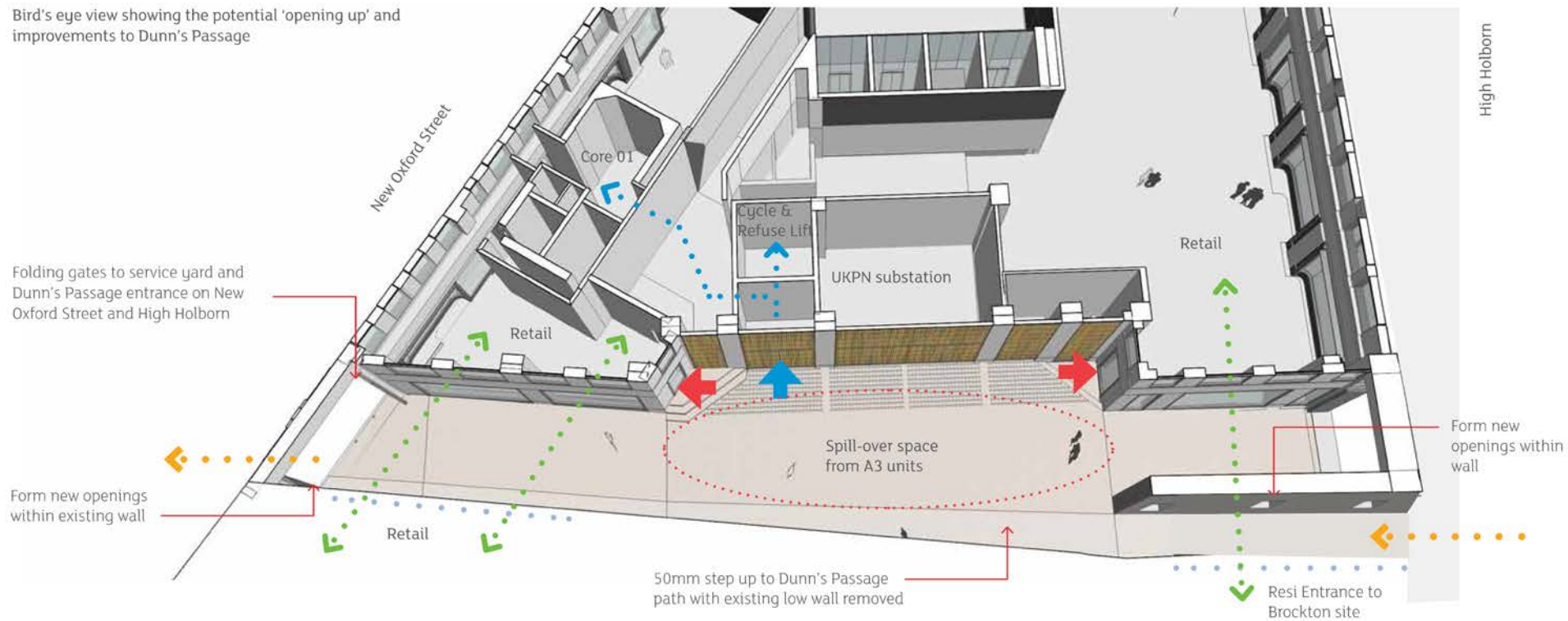


Existing and proposed sections



Dunns Passage detail –to become a shared service / A3 spill-over space (service access only from 1630 – 2200 hrs)

Bird's eye view showing the potential 'opening up' and improvements to Dunn's Passage



Key view 1 – High Holborn (long view)

The following pages set out the x 4 key views agreed with the Camden Planners. Note, for visual clarity, trees have been removed from views 2 and 4 of the proposed scheme.



Key view 2 – High Holborn (short view)



Key view 3 – from Bury Place



Key view 4 - New Oxford Street (looking east)



Proposed southern elevation – High Holborn

View east from High Holborn

New brick parapet over oriel windows, soot washed to match existing bricks.

New projecting oriel metal windows to 6th floor infill

Dark soot wash to all existing brickwork

All existing windows replaced with new windows, fenestration details in keeping with existing windows



Design detail – typical core bay

New glazed hexagonal panels to replace existing panels (damaged), insulated behind to improve the building's thermal envelope. Size and pattern of panels to match existing hexagonal precast panels

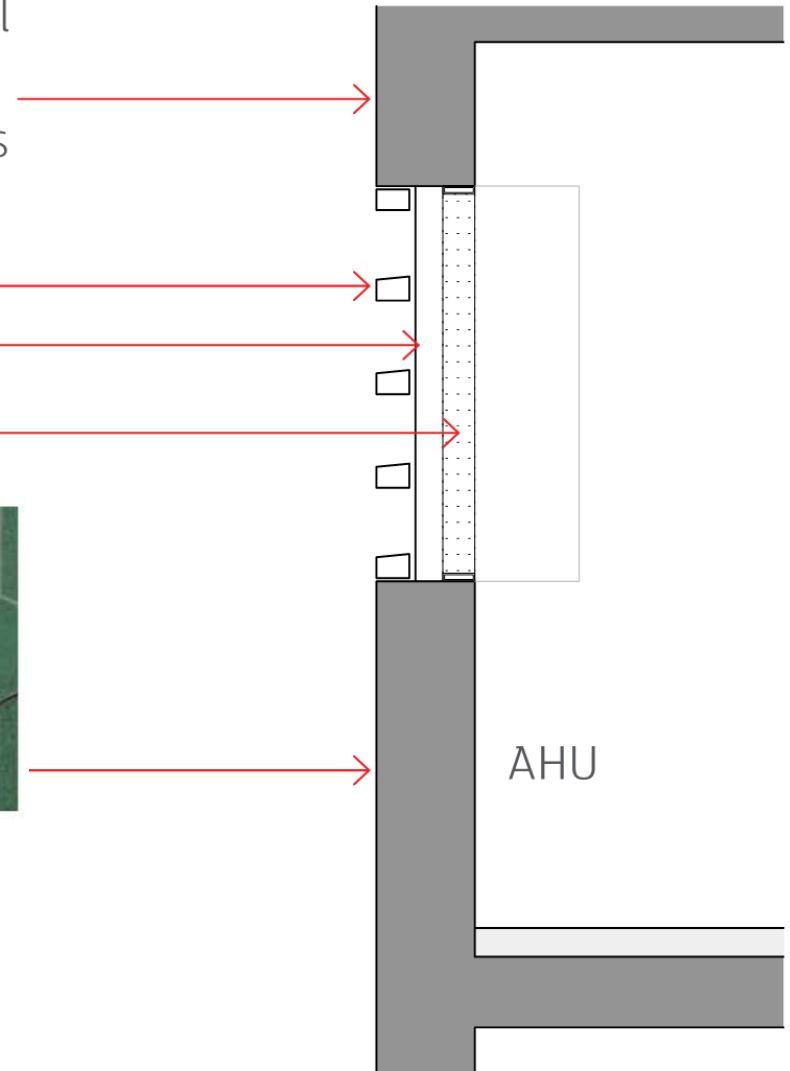
New glazed horizontal fins

Perforated screen mesh

Air intake louvres



Sample image of glazed green hexagon panel. The size and shape of the proposed replacement glazed hexagon panels are to match the existing pre-cast panels. The glazed green tiles have been chosen to reflect the original Architect's intended material.



3-D model showing top of building

Acoustic louvres with glazed faience horizontal fins to plant room, in keeping with the glazed fin detail to lower floor cores

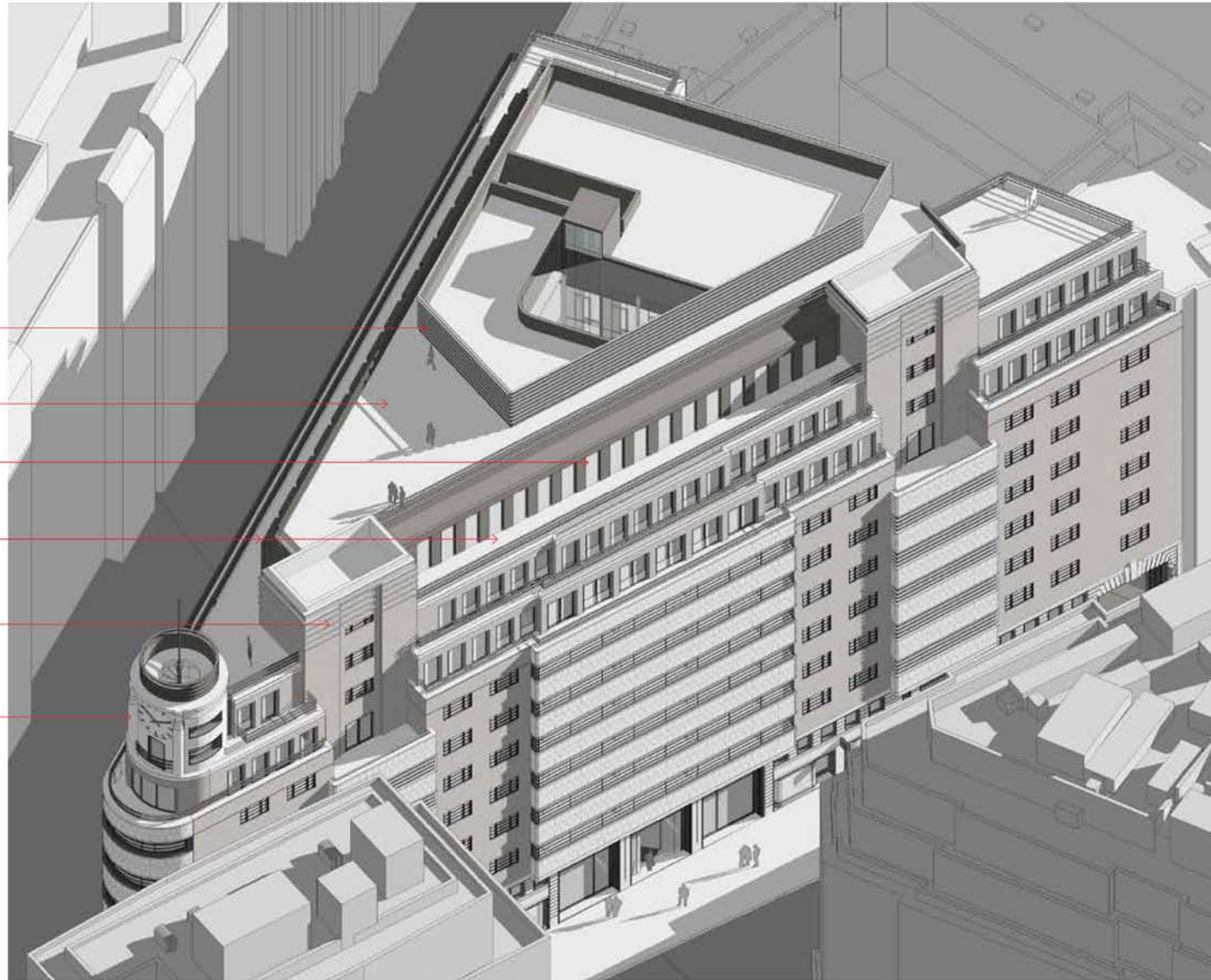
Terrace

9th floor facade : metal clad piers with glazed infill informed by architecture of 7th and 8th

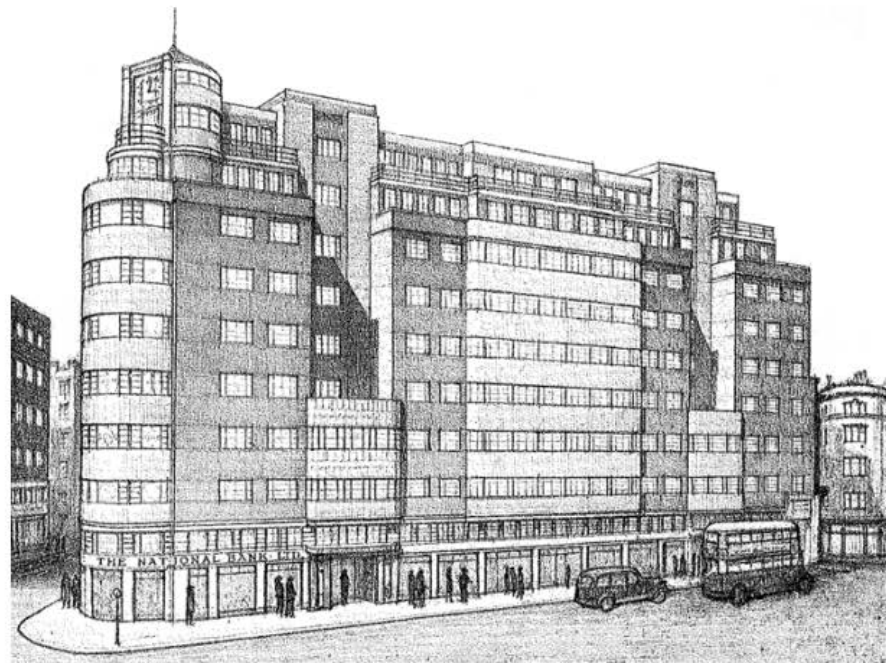
Terrace

Retain and increase height of brick towers to improve squat proportions, 'upscaling' whilst retaining the stepped profile of the existing roof.

Retain and increase height of clock tower to improve squat proportions, New Portland stone overcladding.



Comparison of 1939 original design with current proposed



Commonwealth House 1939



Commonwealth House - Proposed scheme