

Traffic Management London Ltd

Weatherill House, New South Quarter, 23 Whitestone Way, Croydon, CR0 4WF

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MRPP

Martin Robeson Planning Practice

28 Maresfield Gardens

Construction Traffic Management Plan



DIRECTORS: A. ELLIOTT, R. SHERGOLD SECRETARY: K. LAY

REGISTERED OFFICE: 50 MEDWAY DRIVE, FOREST ROW, EAST SUSSEX, RH18 5NT

REGISTERED IN THE UK NO: 5578543



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28 Maresfield Gardens – Construction Traffic Management Plan.

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Introduction.

This Construction Traffic Management Plan (CTMP) has been produced in support of a planning application at 28 Maresfield Gardens, NW3. The planning application is for the excavation of a basement and construction of a lower ground floor extension to the rear; improvements to the roof and fenestration including the addition of a dormer window; improvements to elevational treatment and hard and soft landscaping works.

Camden Council requires, as part of its validation requirements, the submission of details relating to control over construction traffic, and traffic management relating to congestion and safety on the highway.

The objective of this document is, therefore, to identify the key construction-related activities arising from the development, and to provide the strategy for managing logistical operations associated with them, so that they minimise their impacts upon the local road network, in terms of congestion and highway safety. When there are to be impacts arising from the development, then we will provide the necessary mitigation measures.

This CTMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Cyclist Safety](#) (CLOCS) scheme) and [Camden's Minimum Requirements for Building Construction](#) (CMRBC).

The approved contents of this CTMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CTMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CTMP does not prejudice or override the need to obtain any separate consents or approvals such as traffic orders, parking suspensions or hoarding licences.



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Location.

The development is located on a wide avenue. The detached property is located on a large plot with large front and rear gardens. The site has a large forecourt parking area with a hardstanding area that is 26m in length. This arrangement is particularly well suited to a construction project of this scale enabling all construction operations to be contained within the site and minimising the impact on the street and the surrounding area.

In order to best facilitate operations to be contained within the site, we will need to temporarily widen the existing vehicles access to better accommodate all types of vehicles. The existing access does not conform to current design standards with regard to minimum width. Therefore a temporary crossover licence will be applied for.

The site is located on a residential street with facilities such as schools and a church. Minimising the impact on the local residents and facility users is of paramount importance. The CTMP comprises a series of management measures to limit or mitigate the impact of construction works upon the amenity of local residents and road users.

Alongside standard construction management methods, the CTMP addresses a series of potential construction impacts relevant to this particular development project and this particular site and context:

- Managing all types of construction vehicle movements to and from the site
- Vehicle entry and exit routes to the site.
- Avoiding queuing of construction vehicles in the vicinity.
- Ensuring the free flow of vehicles along Maresfield Gardens.
- Limiting impacts on residents' on-street parking spaces and minimising the number of parking suspensions required.
- Limiting impacts on the operation of local schools and church
- How construction vehicles will travel to and from the site.

Conclusion

As described above, construction works at 28 Maresfield Gardens will be carried out such that the impact on the local residents and the wider transport network will be minimised.



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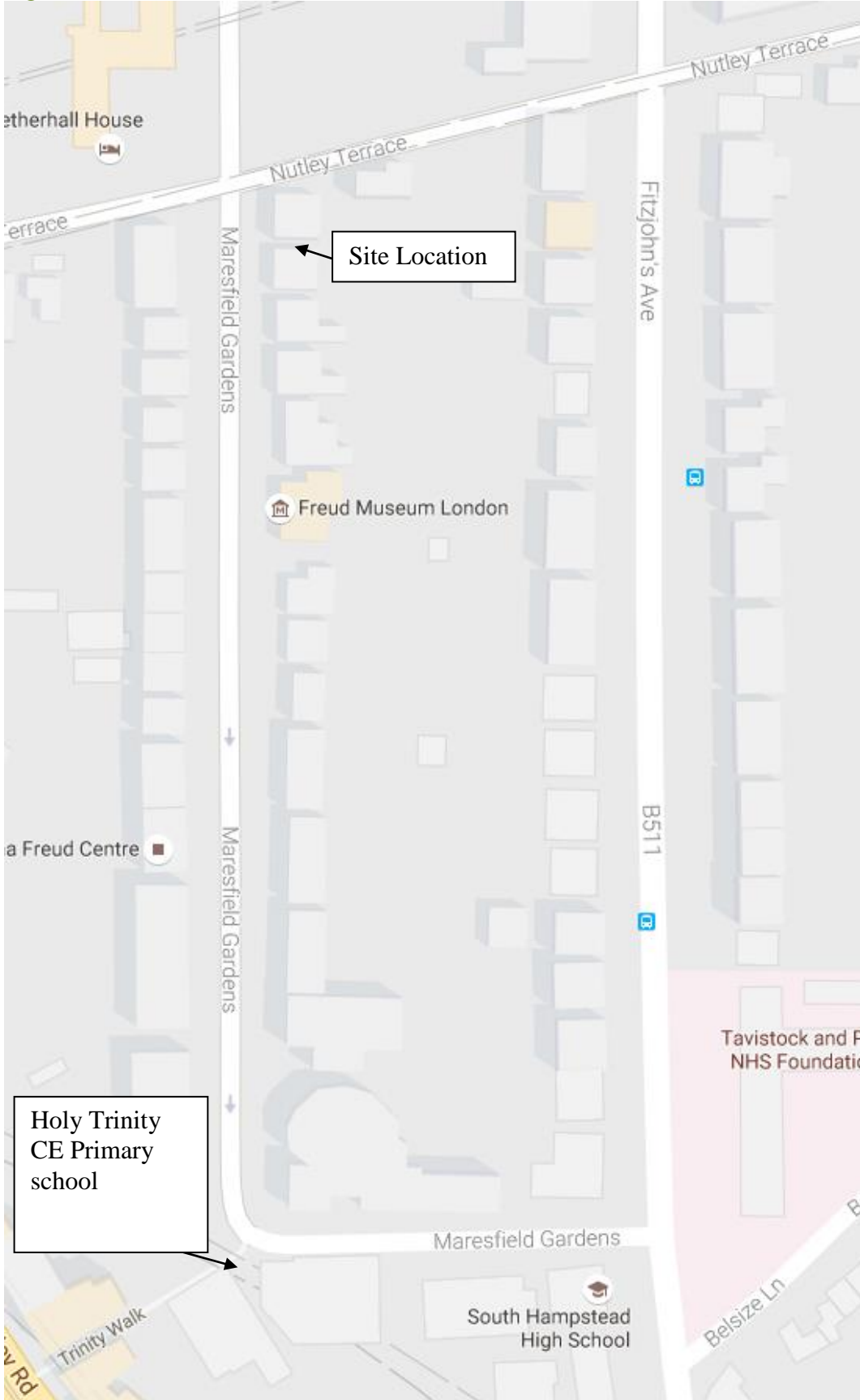
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Fig 1. Site location



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3 Aims & Purposes.

Aims and purposes

The primary aim of this draft CTMP is to minimise the impact of construction on the surrounding community, specifically the residents of Maresfield Gardens, both for construction on site and the transport arrangements for servicing. Camden requires that the CTMP establishes control over construction traffic and includes measures to safeguard highway safety and minimise congestion.

It is envisaged that this draft CTMP will be conditioned as part of any forthcoming planning consent. The CTMP will remain a live document for the construction phase and will be reviewed and updated by the construction team when required throughout the duration of the construction works.

This document will form the basis of agreeing the construction logistics arrangements with Camden Council, as appropriate. The logistics will be dependent on the suppliers, working methodology and programme being co-ordinated by the principal contractor.

This CTMP is for planning purposes only. Some of the detail provided is provisional and will be subject to revision when the contractor has been appointed for the works. The document will be revised and updated to include relevant information as and when details are known and to address any issues that may have been identified through consultation with the Council and local residents as the project progresses.

The measures for the safe delivery or collection to and from site include:

- A) Reducing road vehicle trips, especially in peak periods, leading to less Congestion, including with regard to cumulative effect.
- B) Suppliers will be preferred according to their FORS safety status.
- C) Maintaining traffic flows at all times.
- D) Maintaining pedestrian movements at all times.
- E) The main contractor will be a member of the Considerate Constructors Scheme, and work within the relevant Code of Construction Practice.
- F) Continued liaison with residents & businesses throughout life of development.
- G) To follow current standards for CLOCS.



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The owner of this document will be the Construction Site Manager who is responsible for its day to day implementation. The Project Manager will also be responsible for overseeing the whole development.

Reviews will be conducted by the site manager on a regular basis to ensure the plan is followed correctly.

The Construction site manager is responsible for the communication of this plan once adopted by the programme, to all staff impacted, in accordance with the relevant Document Control procedure.

The Construction site manager will be responsible for ongoing liaison with the local residents association.

4 Consultation.

4.1 Local people understand the local context, and can provide constructive and valuable advice on how best to carry out this development. Consultation is a valuable and proven method for ensuring that developments are undertaken in the best way possible.

Therefore, once a contractor has been commissioned, they will then commence the stakeholder consultation process, using this CTMP as part of the communication package. This CTMP is a live document, and, should there be any changes to the proposed methods resulting from the consultation process, then any changes must be agreed with the local planning and highways authority.

This CTMP has been designed to minimise all impacts on the local community and road network. Therefore we do not envisage any significant changes to the plan being required.

During construction, the contractor will be required to provide regular updates to the local residents, especially those that are situated nearest to the site, and, who could be affected by any parking suspensions etc.

The contractor will provide a single point of contact to the local community for a consistent communications process. Should there be any issues or concerns raised, the contractor will provide a response within 5 days.



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5 Vehicle Routing and Times.

5.1 All construction vehicles required at the site will arrive via the TLRN network on the A41 Finchley Road. They will head northbound up B511 Fitzjohns Avenue, turn left into Nutley Terrace, and finally a right turn into Maresfield Gardens.

To depart site, all vehicles will drive southbound on Maresfield Gardens, turn right onto College Crescent returning to TLRN A41.

All construction vehicles departing site will turn right when leaving Maresfield Gardens as this is the location of the cycle gate.

Drawing No TM-MRPP-28 Maresfield Gardens-001

Please note that all drawings are provided in a separate PDF document. This is due to the formatted size of the drawings (A3).

5.2 All construction vehicles will be on restricted hours due to the close proximity of the nearby schools : South Hampstead High School, and, Holy Trinity CE Primary School.

The restricted hours for all construction vehicles will be:

Monday to Friday 0930 – 1500

Saturday 0930 - 1600

The Construction Site Manager will be responsible for advance booking and coordinating all deliveries and collections, as well as specifying the type of vehicles accessing the road, and arranging the necessary licenses.

The CTMP will be issued to all sub-contractors. Contractors will be issued with a project route map to distribute to their delivery drivers and suppliers. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable, construction vehicles shall not proceed to the site. As this development is very small, the number of daily construction vehicle movements is de minimus.



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6 Site Access.

6.1 All vehicles will access the site from Maresfield Gardens travelling southbound only. All construction vehicles will drive directly into site. Qualified LANTRA (or similar) banksman will be in attendance for all vehicle movements to aid the driver in manoeuvring safely and aid any residents/ road users in passing safely if required. Priority will always be given to members of the public where possible. Further details on the management of pedestrians and cyclists are outlined in a separate section of the report.

It is an important strategy to ensure that operations relating to construction vehicles are undertaken on site as much as practicable to ensure public safety. We have identified that it is possible to ensure that all construction vehicles can enter the site and undertake their operations within the existing curtilage of the property. However, in order to achieve this outcome, we will be required to make some minor changes to the existing vehicles access and parking arrangements.

6.2 The following picture shows the existing vehicles access. We do not know when this was installed, however it can be asserted that is prior to 1988 as it does not conform to current design standards with regard to minimum widths.



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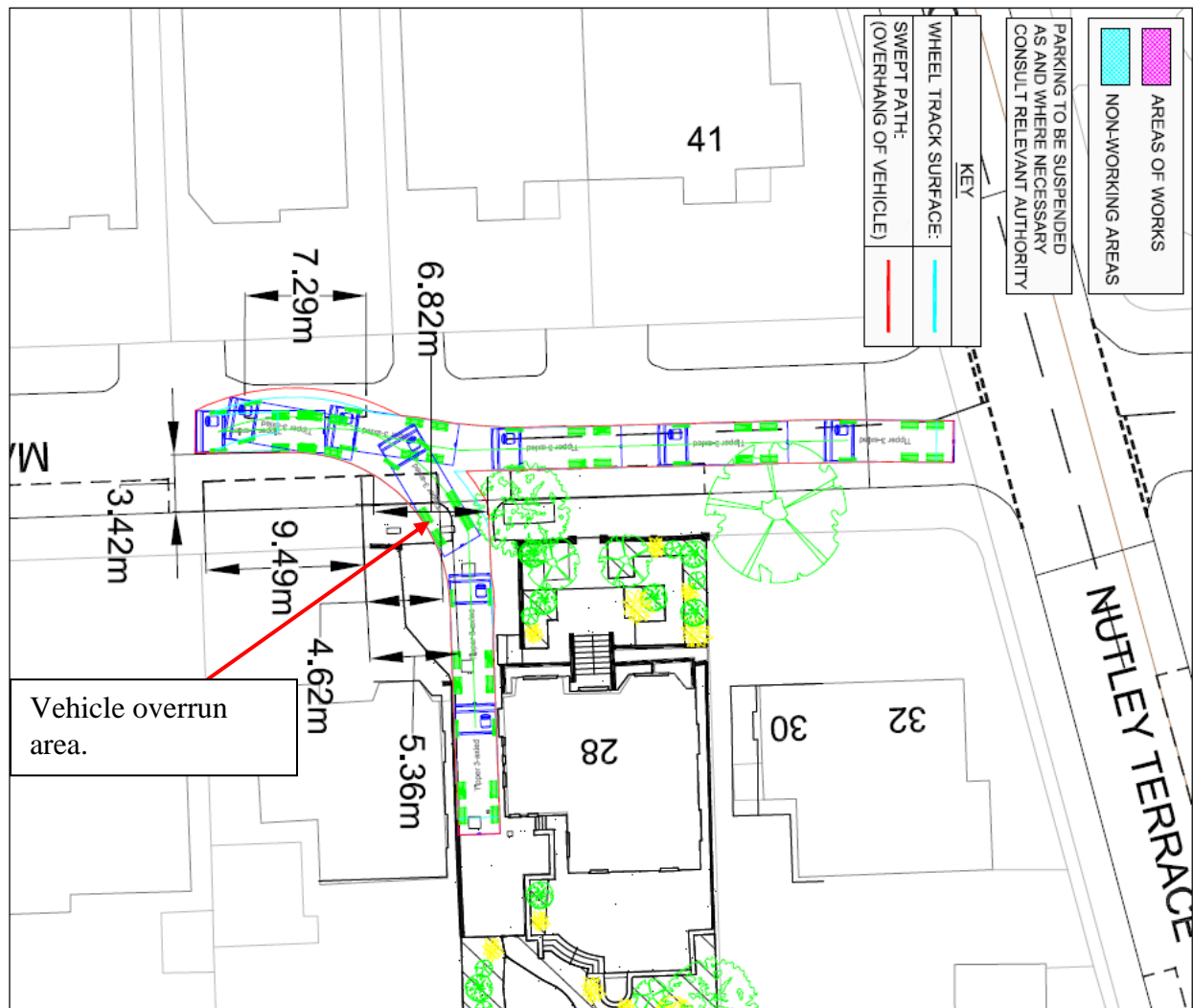
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6.3 The following graphic is a snapshot for the vehicle swept path of a concrete lorry reversing into site. As you will see from the plan, the vehicle will overrun the footway on the right side of property.



To ensure that this development achieves the desired outcome to remove all construction vehicles off Maresfield Gardens, thereby minimising its impacts upon the local residents, cyclists and pedestrians, we propose that the client is permitted to apply for a temporary extension to their existing vehicles access. This licence will allow the contractor to widen the existing vehicle access by approximately 4.4m as agreed with the highways authority. The contractor will then be required to undertake works to strengthen and modify the footway to protect it, and any utility assets against any damage from construction vehicles. All works will be undertaken



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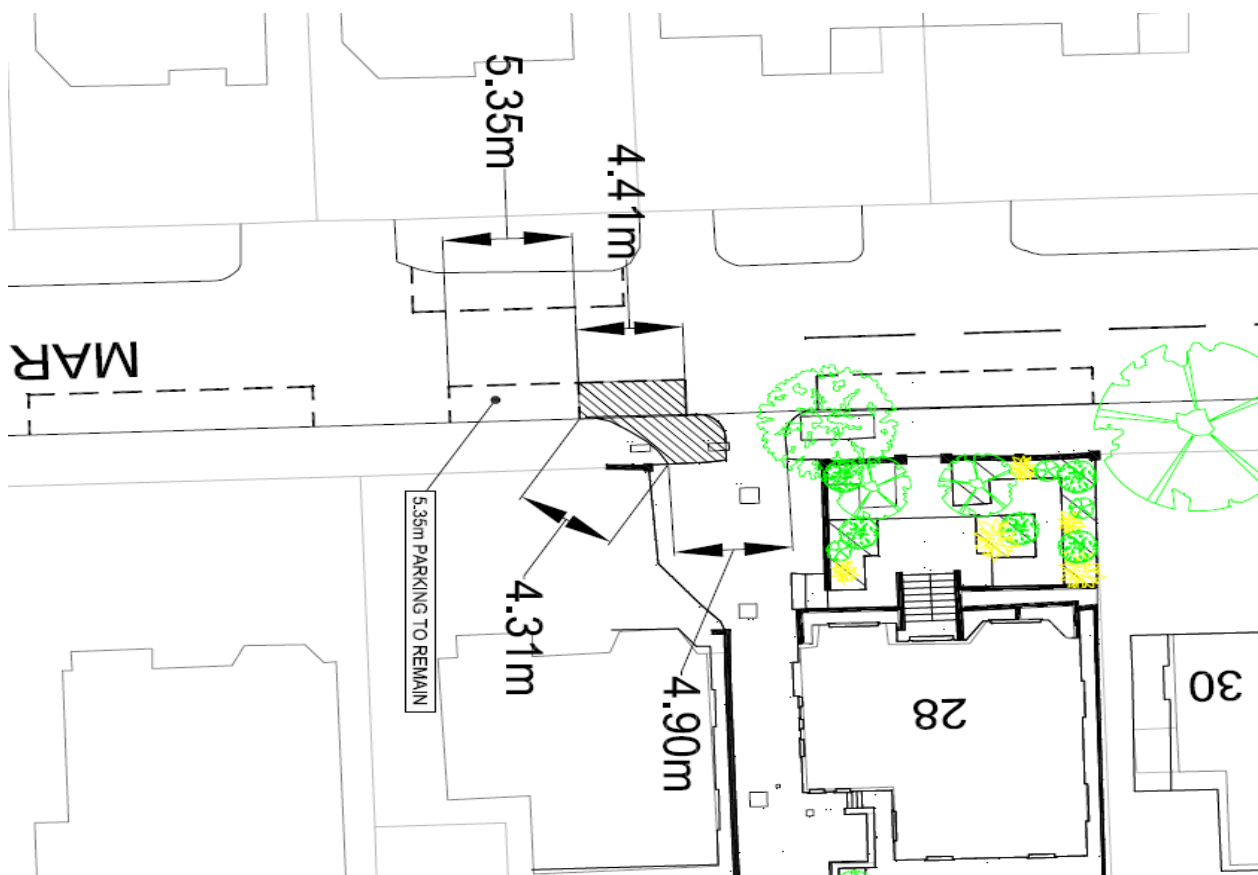
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in accordance to local authority specifications and approvals. The client/contractor will be required to gain all relevant permissions. At the end of the project the access arrangement will be returned to its original layout.

6.4 The proposed widening of the vehicle access will also impact the on street parking facilities. We will be required to reduce the length of the existing parking bay to a single car length as agreed with the local traffic authority. These alterations will be temporary, and at the end of the project the parking facilities will be returned to the original layout. The changes that we require are very minor, and do not adversely affect the overall parking capacity of the street.

In order to change the existing parking bay arrangement, a Traffic Order will be required. This process requires approximately 8 – 12 weeks, and includes local residents consultation.

The following graphic is a snapshot of the proposed road layout.



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This proposal to make the very minor amendment to the parking bay improves the strategy and impact of the development. It will also significantly reduce the requirement for any other parking bay suspensions. The full detailed drawing is provided separately, and would need to be agreed with the local authority highways team.

Drawing No TM-MRPP-28 Maresfield Gardens-002

6.5 Trees – There is an existing highway tree close to the left side of the vehicle access. This tree has undergone pruning in recent times, and does not overhang the property to any extent that would restrict any vehicles reversing into the site.

As all vehicles will be reversing in from the right side of the vehicle access, and will be under the guidance of a banksman, there is no risk to the tree being struck.

7 Key Site Construction Vehicle Activities.

7.1 There are a number are key activities associated with the development of the site that will have specific logistical operations that require further detail to identify how these operations will be managed. These items are:-

7.2 Scaffolding – There will be a requirement for a scaffolding structure to access the roof of the property.

The scaffolding vehicle will be required to visit the site two times to erect and dismantle the structure.

The vehicle will undertake it operations from within the site. The vehicle will be reversed into site under the guidance of a qualified banksman. We will require the suspension of the parking bay outside No 26 to facilitate this action, but as stated we will only require the suspensions twice to accommodate this vehicle.

7.3 Concrete Pour – There will be a requirement for concrete pours for the basement slab and underpinning.

The concrete vehicle will be required to visit site approximately 4 – 6 times.

The vehicle will undertake it operations from within the site. The vehicle will be reversed into site under the guidance of a qualified banksman.



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We will require a parking suspension outside No 26 to complete this activity.

Drawing No Drawing No VSP-MRPP-28 Maresfield Gardens-002

7.4 Muck Away – The proposed method for the removal of all excavated material is a skip swap method.

The skip vehicle will be required to visit site approximately 2 times per week during the basement excavation phase.

The vehicle will undertake its operations from within the site. The vehicle will be reversed into site under the guidance of a qualified banksman. We will require the suspension of the parking bay outside No 26 to facilitate the vehicle off the highway.

Drawing No Drawing No VSP-MRPP-28 Maresfield Gardens-001

7.5 Deliveries of materials – There are two types of delivery vehicles required for the development, LGV & HGV.

The LGV vehicles will be required to visit site approximately 2 times per week throughout the build.

The vehicle will undertake its operations from within the site. The vehicle will be reversed into site under the guidance of a qualified banksman. We do not require any parking suspension to complete this activity as the vehicle access has been temporarily widened to accommodate this vehicle.

The rigid hiab vehicles will be required to visit site approximately 1 per month.

The vehicle will undertake its operations from within the site. The vehicle will be reversed into site under the guidance of a qualified banksman. We will require the suspension of the parking bay outside No 26 once per month to facilitate the vehicle off the highway.

The swept path is the same as the rigid skip lorry.



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In total there is a requirement for approximately 40 parking bay suspension events to facilitate vehicle movements into site. The frequency of these events are around 2 per week. Therefore we propose the strategy to suspend the parking bay outside No 26, twice per week on a fixed day rota. We propose Tuesday & Thursday only. This will allow the residents to fully plan around the works, and will require the contractor to properly plan and schedule his activities. This strategy minimises the impact for on street parking demands. The final agreement for the parking bay suspension will be undertaken between the contractor and the local authority, in consultation with the local residents.

The widening of the existing vehicles access has allowed us to significantly reduce the amount of parking bay suspensions as all LGV deliveries will be able to gain access direct into site, but, more importantly it allows this development to get all of its construction vehicles off the highway, thereby removing all risks associated with any loading and unloading across the public highway.

Removing all construction vehicles off the public highway should be encouraged at all times to meet the outcomes of Camden's local strategy to reduce the impacts from development works.

8 Vehicle movement planning and monitoring.

8.1 a. The control system for the arrival and departure of all vehicles to prevent congestion around the worksite and its access routes will be a paper booking form produced by excel. All contractors/deliveries and collections will be entered on to this manually input to an excel spreadsheet by the site manager. The number of vehicles will however be low, it is estimated that we only require an average of 1 - 2 deliveries per day, which is a very low impact and manageable with the space available. No vehicle will be allowed access without this prior booking being in place. This list will be updated every day.

b. The above will give the traffic marshal/banksman a means of ensuring only vehicles notified in advance will be allowed to enter the site.

c. The banksman will be responsible for ensuring that any vehicle allowed onto site is checked for its FORS status. Any vehicle not complying with the required minimum Bronze level standard will not be allowed to unload, and will then be instructed to leave site.



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d. All HGV vehicles will be restricted to fixed days deliveries twice per week.

9 Pedestrians.

9.1 This section details the provision for pedestrian management and safety.

a. All existing footways will be maintained.

b. Competent Lantra qualified Banksman and Traffic Marshalls will supervise all construction traffic entering and exiting the site to ensure safe movement of vehicles. Priority will be given to pedestrian to pass the site entrance, before a vehicle is reversed into site, or allowed to exit.

d. No pedestrian diversions are required.

e. The public highway in the vicinity of the site entrance will be kept clean and free of detritus materials.

f. No loading or unloading operations will occur across the public highway.

g. All construction vehicles will have a minimum Bronze level FOR's status.

10 Cycling.

10.1 This section details the provision for cycling safety.

a. Competent Lantra qualified Banksman and Traffic Marshalls will supervise all construction traffic entering and exiting the site to ensure safe movement of vehicles. The banksman / traffic marshall shall use a Stop/Go board to temporarily halt any cyclist whilst a construction vehicle is reversing into site.

b. All HGV's will make right turns out of Maresfield Gardens back onto the main road network only to avoid the risk of blind spots for drivers.

d. Suppliers will be chosen with preference to those who have acquired registration with the Freight Operators Recognition Scheme 'FORS'. A minimum standard of Bronze will be required.

e. Suppliers will be chosen with preference to their safety records.



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f. The contractor will provide the method for adhering to the CLOCS scheme. The CLOCS standard point 3.4.7 (Supply Chain Compliance) will be fully adhered to at all times.

g. The contractor will undertake the driver licence checks.

Pedestrian and cyclist safety will be a priority of the contractor. The contractor will provide a method for adhering to the CLOCS scheme. All delivery and collection vehicles must follow TFL's Safer Lorry Scheme recommendations and drivers should undertake cycle safety awareness courses. In addition, delivery vehicles and sub-contractors who have registered with the FORS bronze level (minimum) scheme will be employed.

11 Parking Suspensions and Highway Licenses.

11.1 This section details the requirements for Traffic Order Regulations.

a. A TRO will be required to amend the existing length of the parking bay outside No 26 Maresfield Road. The parking bay will be shortened by approximately 4.4m for the duration of the build. This will facilitate all construction vehicles to access the site, making this a safer operation. At the end of the project, the road will be returned to its previous layout. The client/contractor will liaise with the relevant council officers for this approval process.

b. Parking bay suspensions will be required approximately once per month. These bays will only be suspended during the hours of 0930 – 1500, and will be returned to residents use after. A strategy will be agreed with the residents and local authority to suspend the single bay on a fixed rota basis, this will allow both the residents and the contractor to plan their activities. The contractor will make the necessary applications to suspend the bays. These suspension will facilitate all construction vehicles to access the site.

c. Temporary vehicle crossover. We will require a licence to temporarily extend the width of the existing vehicle crossover. This will facilitate all construction vehicles to access the site. The contractor will be responsible for ensuring that the highway works are undertaken in accordance with local authority specifications and approvals. At the end of the build, the access will be restored to its original layout.

Drawing No TM-MRPP-28 Maresfield Gardens-002



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12 Committed Developments.

12.1 There is another development currently in planning consultation for No 9 Maresfield Gardens.

The programme will need to be confirmed by the Contractor. The CTMP will be reviewed weekly and the Project Manager will be responsible for liaising and co-ordinating traffic routing with any other developments in the area as necessary. The Contractors/Project Managers from nearby construction sites will have weekly scheduling meetings, where they will coordinate any vehicle movement to/from the sites to avoid any unnecessary disruption. During the meetings, Contractors will be expected to discuss the feasibility of sharing deliveries from the same supplier to further reduce the impact on the local highway network and local residents.

12.2 Collaboration, as these sites are adjacent to each other, and, could potentially be in progress at the same time, there may be several opportunities to collaborate which may reduce the impact from construction vehicles, and traffic management. The opportunities are:-

- a. Deliveries – We may be able to collaborate with shared deliveries & partial loads to reduce the amount of vehicles required to site.
- b. Empty vehicles – There may be an opportunity for an empty vehicle once unloaded from visiting one site, to then take away items from the other, thereby reducing the amount of vehicles required to the sites.

Our site manager will maintain good communications with the development to ensure best practise and ongoing opportunities.



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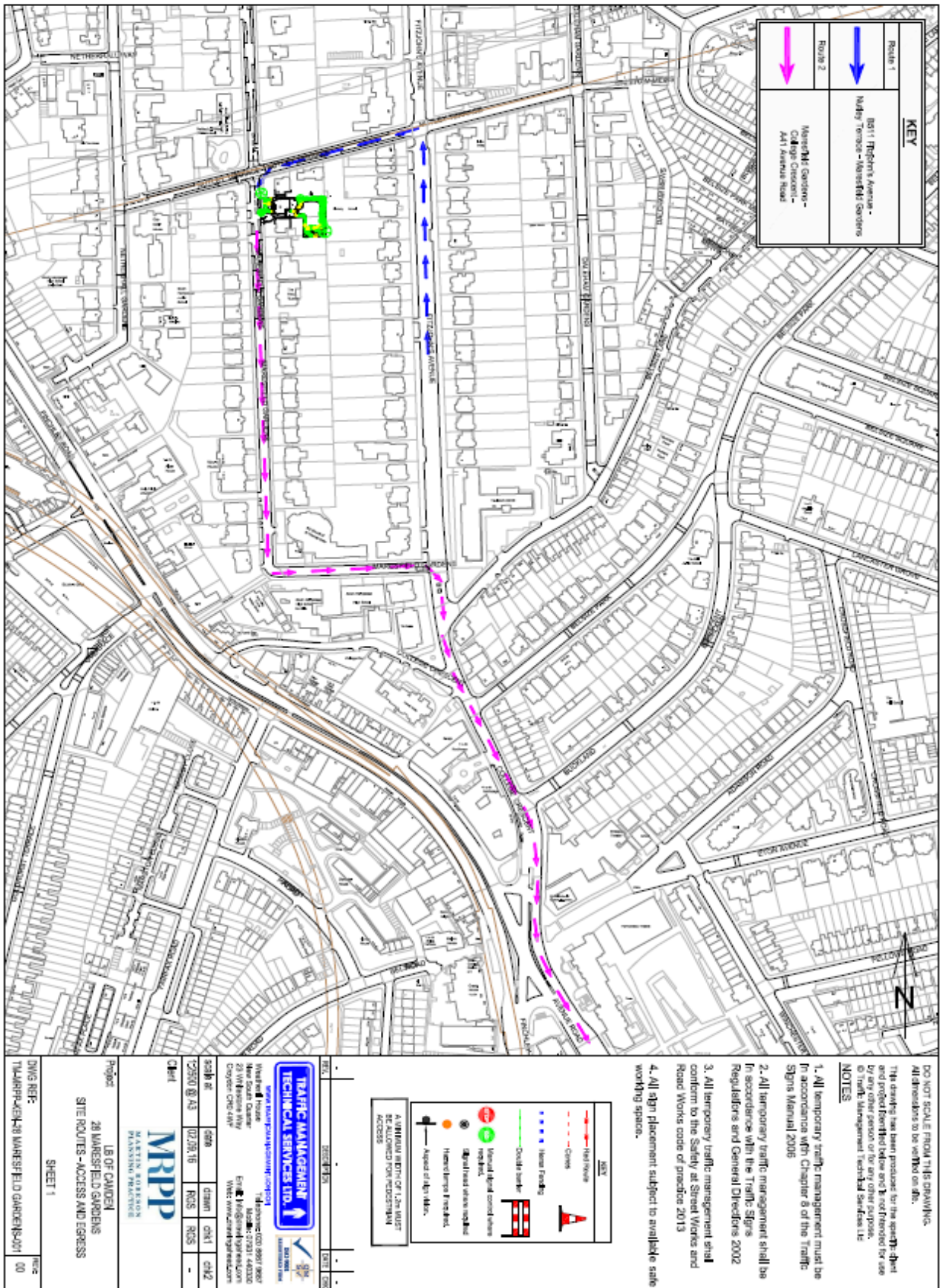
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Appendix – Technical Drawings.



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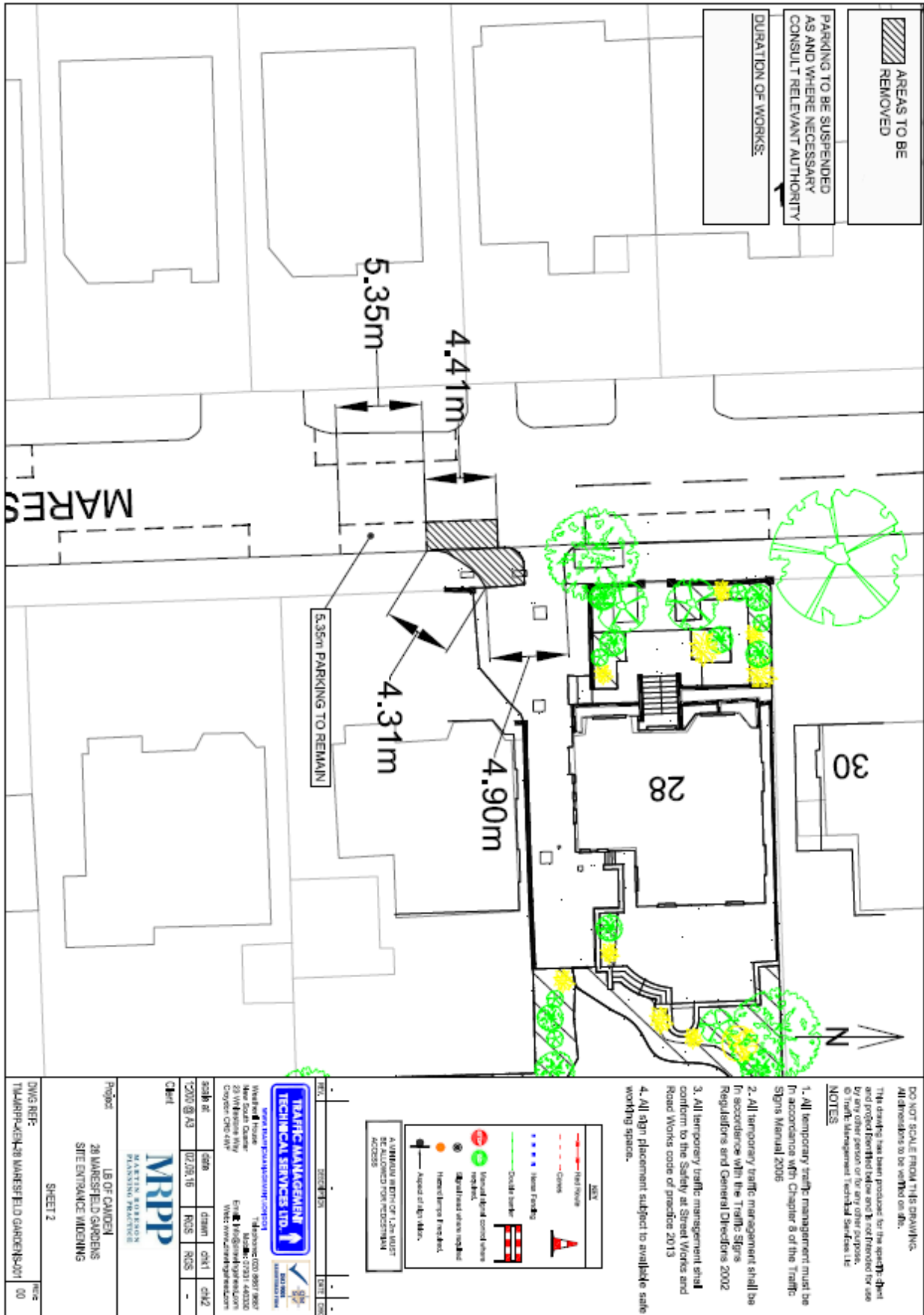


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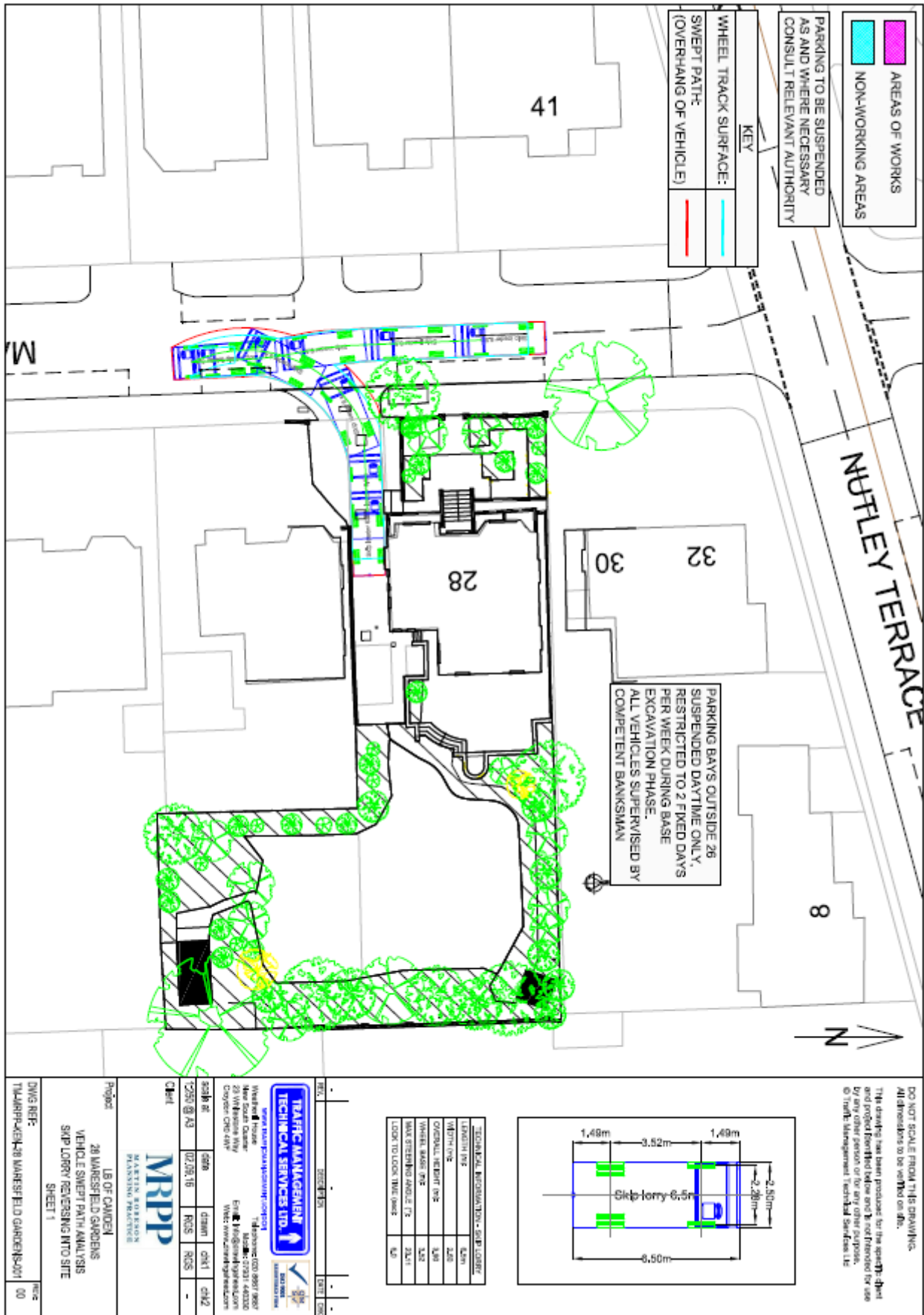


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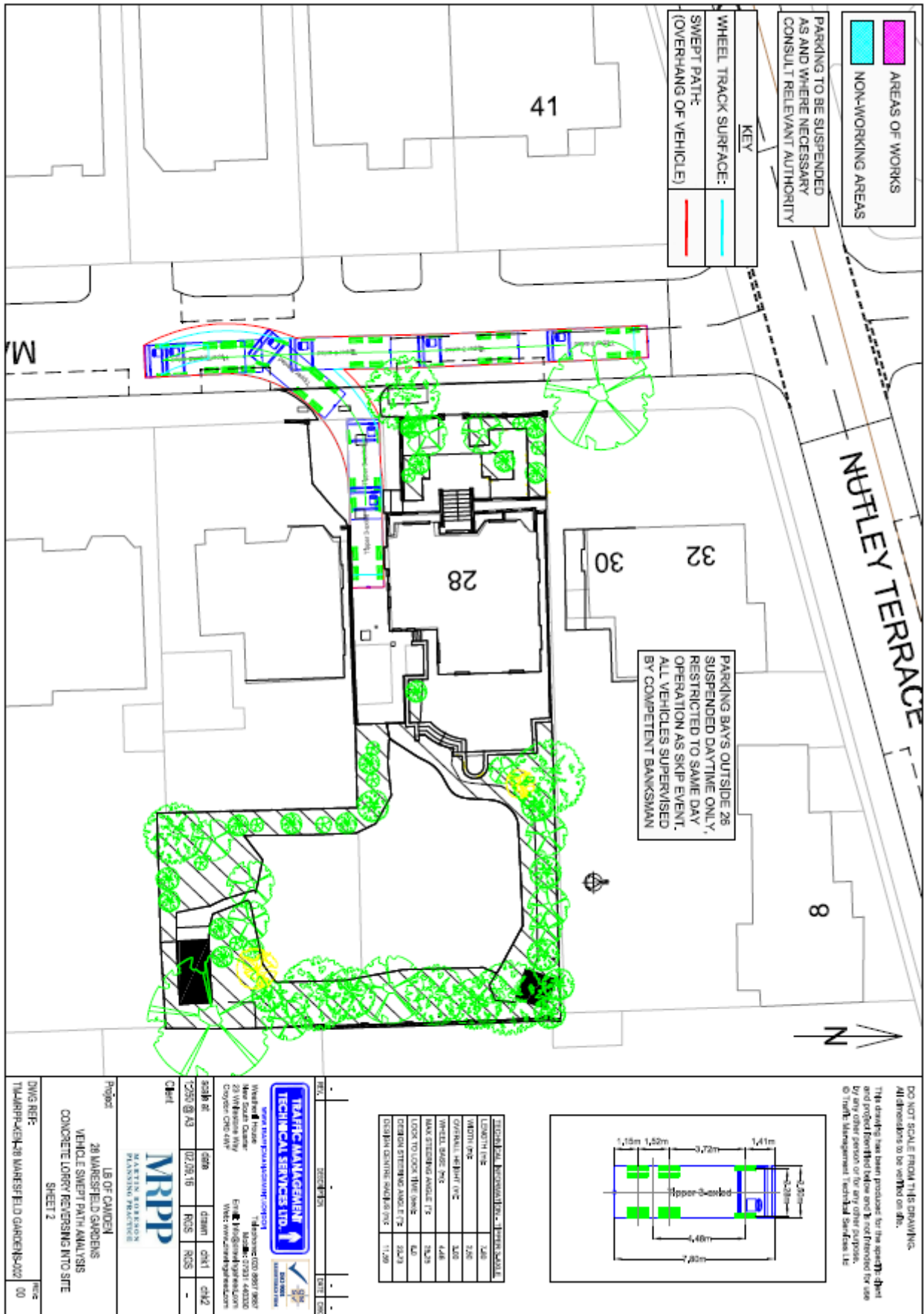


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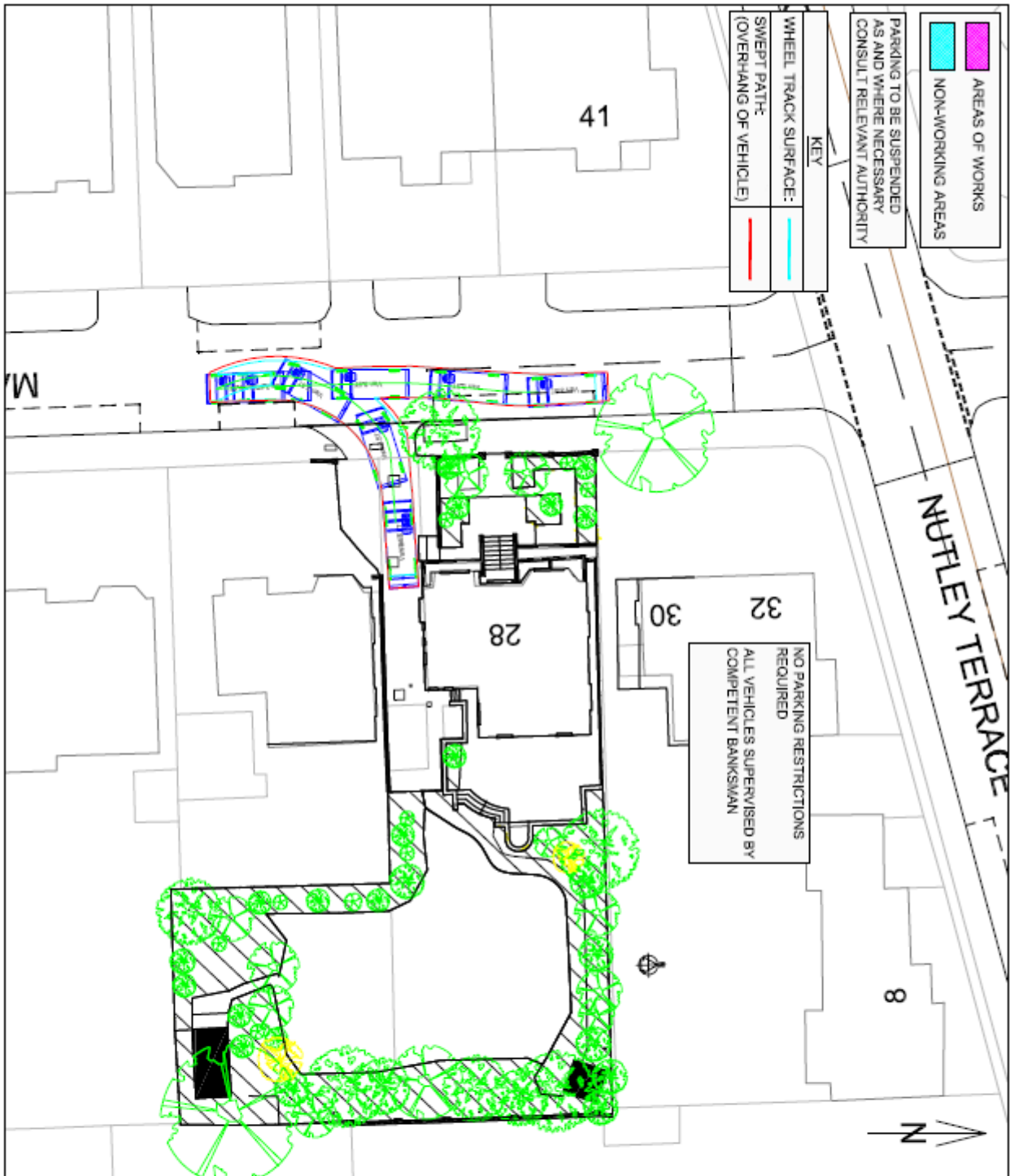


Traffic Management London Ltd

Weatherill House, New South Quarter, 23 Whitestone Way, Croydon, CR0 4WF

Tel: 0208 667 9687 Fax: 0871 918 8748

Web: www.trafficmanagement.london



DO NOT SCALE FROM THIS DRAWING.
All dimensions to be worked on site.
The drawing has been produced for the specific plot and project identified below and is not intended for use by any other person or for any other purpose.
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DESCRIPTION	UNIT	QTY
1. SPECIAL PAINT (WHEEL TRACKS, SWEEP PATHS)	SQM	240
2. WHEEL TRACK SURFACE	SQM	240
3. SWEEP PATH SURFACE	SQM	240
4. WHEEL TRACK SURFACE (OVERHANG OF VEHICLE)	SQM	240
5. SWEEP PATH SURFACE (OVERHANG OF VEHICLE)	SQM	240
6. SPECIAL STITCHING (WHEEL TRACKS)	SQM	240
7. SPECIAL STITCHING (SWEEP PATHS)	SQM	240

TRAFFIC MANAGEMENT TECHNICAL SERVICES LTD.
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 Website: www.trafficmanagement.london

MRPP
 MARTIN ROBERTSON
 PLANNING PRACTICE

Client: 12501 AJ 02/09/16 RGS CH1 CH2

Project: LS OF GARDENS
 28 WARESFIELD GARDENS
 VEHICLE SWEEP PATH ANALYSIS
 LONG WHEELBASE VAN
 SHEET 3

DWG REF: TLA001P-KEN-28 WARESFIELD GARDENS-000
 DATE: 00



DIRECTORS: A. ELLIOTT, R. SHERGOLD SECRETARY: K. LAY
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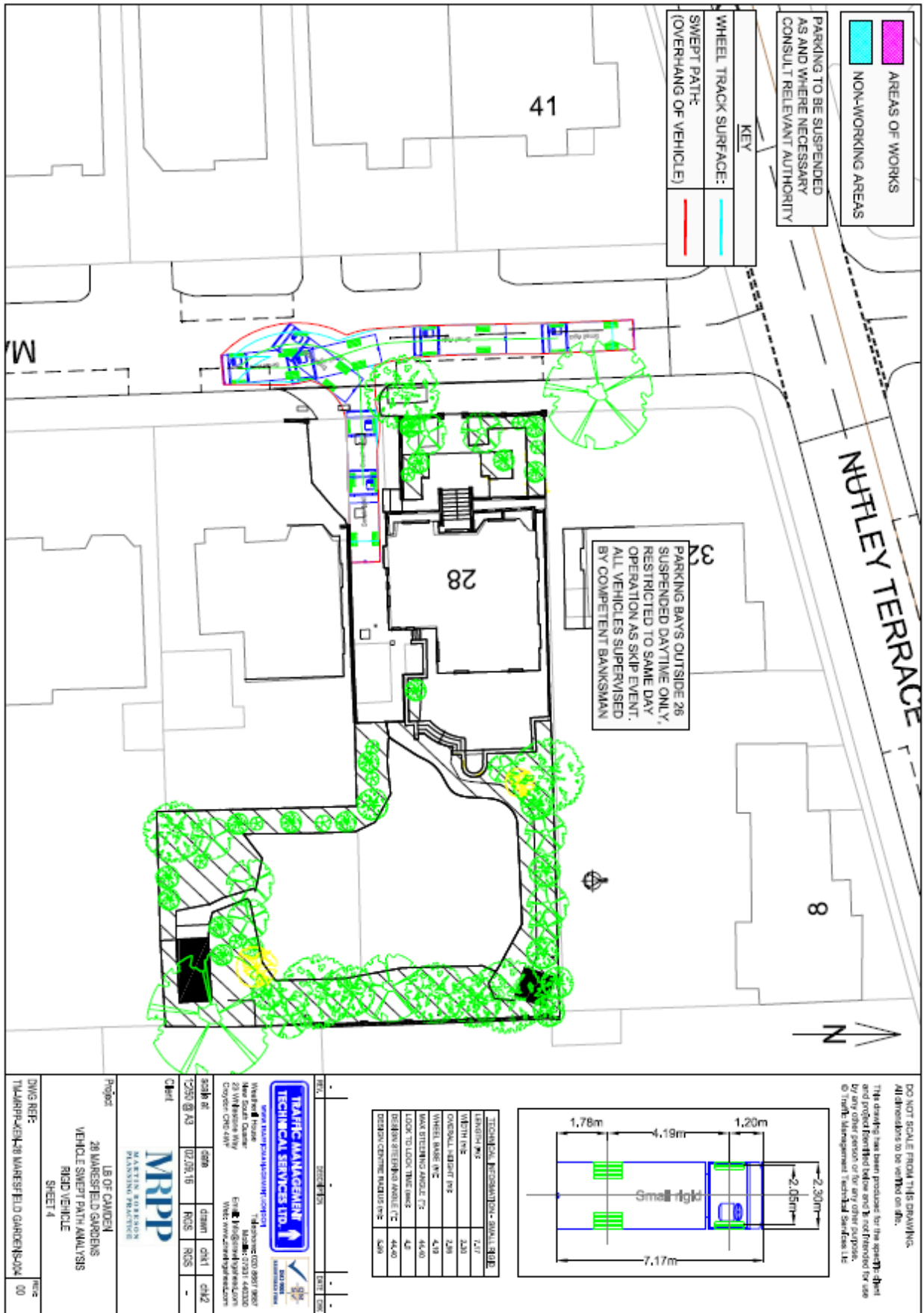


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