

CAMDEN WHARF
LONDON, NW1

PRE-PLANNING
SUBMISSION

4 MAY 2016

REVISION: A

PREPARED FOR

Castlehaven Row Ltd

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Photograph of the existing building from Chalk Farm Road on Hampstead bridge

1.0 INTRODUCTION

1.1 PROJECT BRIEF

There is a significant demand for high-quality A3 restaurant space and B1 space in Camden. The Camden Wharf building has a large unsightly roof plant area which can be relocated and the rooftop extended to provide for this need and to improve the building local views. The existing second and third floor can also be extended to provide additional office area at these levels.

The private road to the west side of the building, which is currently used for parking and loading/ unloading, has the potential to be fully pedestrianised and linked to the canal walk.

The current loading bay/ service yard can be used to create new ancillary spaces and a new lobby with a lift to connect to the 4th floor. A new shopfront will also be introduced in place of the existing bifolding gate to attract visitors and to help connect the front and the back of the existing building.

In summary, the design proposal includes:

1. Extensions to the 2nd and 3rd floors as B1 and ancillary use
2. Extension to the 4th floor as dual B1/ A3 use
3. New lift core to serve the restaurant and connect the ground floor with the 4th floor extension linking it to the new lobby/ reception area
4. Existing services relocated and new services designed into the scheme
5. Re-planning of current loading bay/ service yard to be partly used for required ancillary spaces (including cycle storage, showers and lockers facilities, waste storage, and new lobby) generated from additional floor space and uses
6. New shopfront to enhance the new restaurant/ office entrance at ground floor
7. Public realm reinstatement and private road enhancement
8. Enhanced bio-diversity and sustainability accreditation including a BREEAM Excellent for the new space

1.0 INTRODUCTION

1.2 LOCATION

The Camden Wharf building overlooks at Regent's Canal and Camden Lock Market and it is surrounded by mixed use buildings, the Holiday Inn Hotel and a Grade II listed building on the canal side.

The central location of the building and its proximity to Camden Town underground station, several bus routes on Camden High Street and Chalk Farm Road, and the Camden Lock Market gives this building great accessibility and therefore the opportunity to accommodate high quality tenants and retailers.

2.0 CONSERVATION AND EXISTING LAND USE

2.1 CAMDEN CONSERVATION AREAS

The building is not listed but it is located within the Regent's Canal conservation area as highlighted on the conservation area map opposite. The Grade II listed Starbucks Lock Keepers Cottage sits in front of the Camden Wharf building on the canal side.

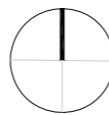


Site Location Plan



Regent's Canal Conservation Area

--- Site Boundary



Regent's Canal

2.0 CONSERVATION AND EXISTING LAND USE

2.2 EXISTING LAND USE - GROUND FLOOR



KEY:

- Town Centre Boundary
- A1 Retail
- A2 Financial Services
- A3 Restaurants and Cafés
- A1 Drinking Establishments
- A5 Hot food / takeaway
- B1 Business
- C1 Hotel
- C2 Residential Institutions
- C3 Residential
- D1 Institutions
- D2 Leisure
- Site Under Application

3.0 SITE CONTEXT AND EXISTING BUILDING

3.1 EXISTING BUILDING

The Camden Wharf building is a concreted frame building with rendered elevations. Its original planning permission was granted in 1999 and the building was built in the early 2000s. Please refer to the Planning History Schedule by Gerald Eve appended to this document for further details about the planning history of the building.

The building currently accommodates a combination of retail and restaurant units, with the upper floors providing office space. The existing retail and restaurant units are accessed from the canal side and the offices are entered from Jamestown Road.

The deliveries come from the private road to the west side of the building through the external corridor to the rear of the units at ground floor. The ground floor designated services yard is used for storage as shown on the photos in this page and has the potential to accommodate new ancillary spaces for the tenants and a new shopfront in place of the existing bifolding gate.

The one storey plant enclosure at the top can be relocated within the building extensions and the services distributed more efficiently in order to rationalise the space and create a new extension at 4th floor.



View from Chalk Farm Road



View of existing service yard entrance



View from Jamestown Road



View from Private Road



View from Camden High Street



Views from roof



Views of GF external corridor



Internal service yard views



3.0 SITE CONTEXT AND EXISTING BUILDING

3.2 LOCAL REGENERATION

Recent years have seen a significant amount of regeneration and new development in the local area. Camden Town is an ever changing context.

Along the Regent's Canal and the opposite Camden Lock Market, Mace are currently delivering the first phase of Hawley Wharf development for Market Tech Holdings. The scheme will see the delivery of over 190 new private and affordable homes, a new single entry primary school and nursery, retail and market spaces, and employment space.

In 2015 there was a resolution to grant proposals to sensitively refurbish and improve the famous Camden Lock Market. A scheme has been formulated to improve the existing historic buildings, to provide vibrant new market spaces and to improve the access and routes through the market.

Behind the iconic Gilgamesh bar & restaurant, near Camden Lock Market and Stables Market, office buildings have recently been refurbished to accommodate co-working spaces for small companies.

The Roundhouse Theatre next to Chalk Farm Tube Station is now a heavyweight venue hosting renowned events such as The Electric Proms.

Other redevelopments along Chalk Farm Road and Haverstock Hill include residential buildings with residential apartments above ground floor retail and restaurant units as well as the Haverstock Hill School with its distinctive metal mesh façade. The London regional scheme is also being developed next to the Holiday Inn hotel on Jamestown Road.



Hawley Wharf



Camden Lock Market Refurbishment



Stables Market Refurbishment



Building C, Co-working, Camden Lock Market



The Roundhouse Refurbishment



Haverstock Hill School



The Henson Building, Camden Town



Belmont Street warehouse conversion and roof extension



Princess Park



4.0 DESIGN DEVELOPMENT

4.1 PROJECT ASPIRATIONS

The Camden Wharf building offers an important opportunity for securing high quality restaurant space in Camden.

The proposal provides the following:

1. Prime A3 accommodation on the 4th floor for a new restaurant
2. Relocate the current plant enclosures and visible main plant and boilers flues out of site
3. Improved external elevation with an active shopfront on the private road
4. Improved public links with prioritised pedestrian use of the private road and a link to the canal pathway
5. Provision of additional office accommodation
6. Removed of existing heavy balustrade and replacement with all glass panels
7. Bio-diverse site enhancements with a green meadow roof
8. Improved building sustainability with the additional areas

4.0 DESIGN DEVELOPMENT

4.2 CONCEPT EXAMPLES

As part of the feasibility study, the team reviewed other projects that could inform the proposals. These projects have provided useful examples of roof top restaurant extensions in different locations in central London and how these have responded to their local context and surroundings through design.



Aqua Nueva, Regent Street



Forest Restaurant and Bar, Selfridges, Oxford Street



Madison, St Paul

4.0 DESIGN DEVELOPMENT

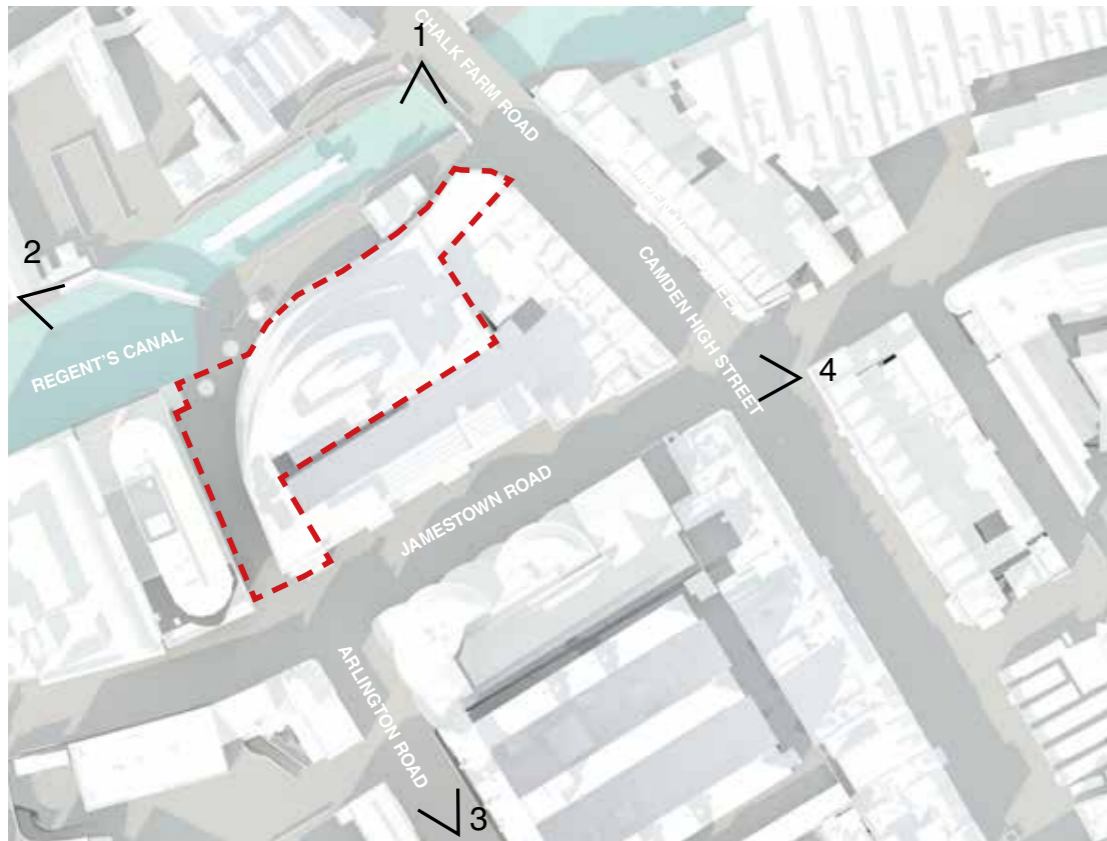
4.3 MASSING MODEL STUDY - KEY VIEWS

A massing exercise has been undertaken to establish any effects of the proposal on four key views.

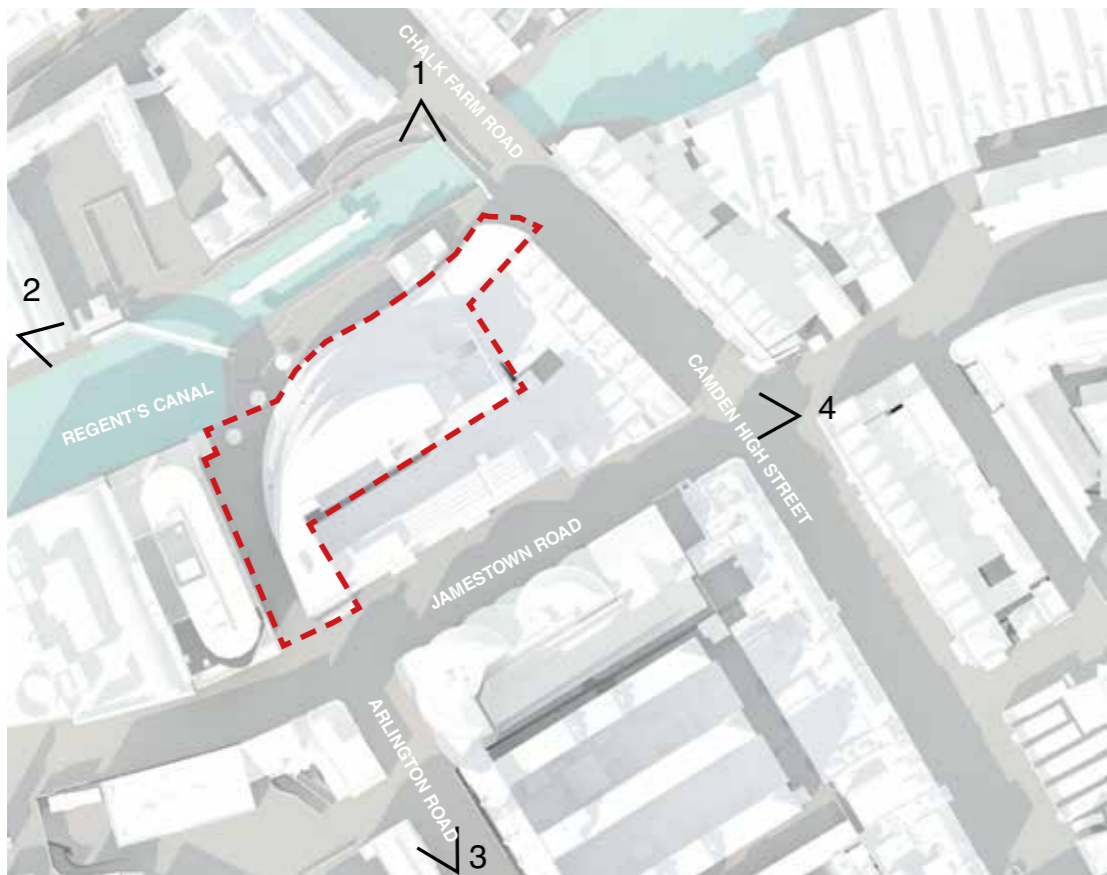
The proposed extensions have been collaged into photographs to create a more realistic representation of the intervention.

The views shown over the next pages are taken from the points highlighted on the key plans to the left.

As shown in these views the intervention is in keeping with the proportions of the existing building and it rationalises the services arrangement so that they are not visible from the key views.



Key views on existing site plan



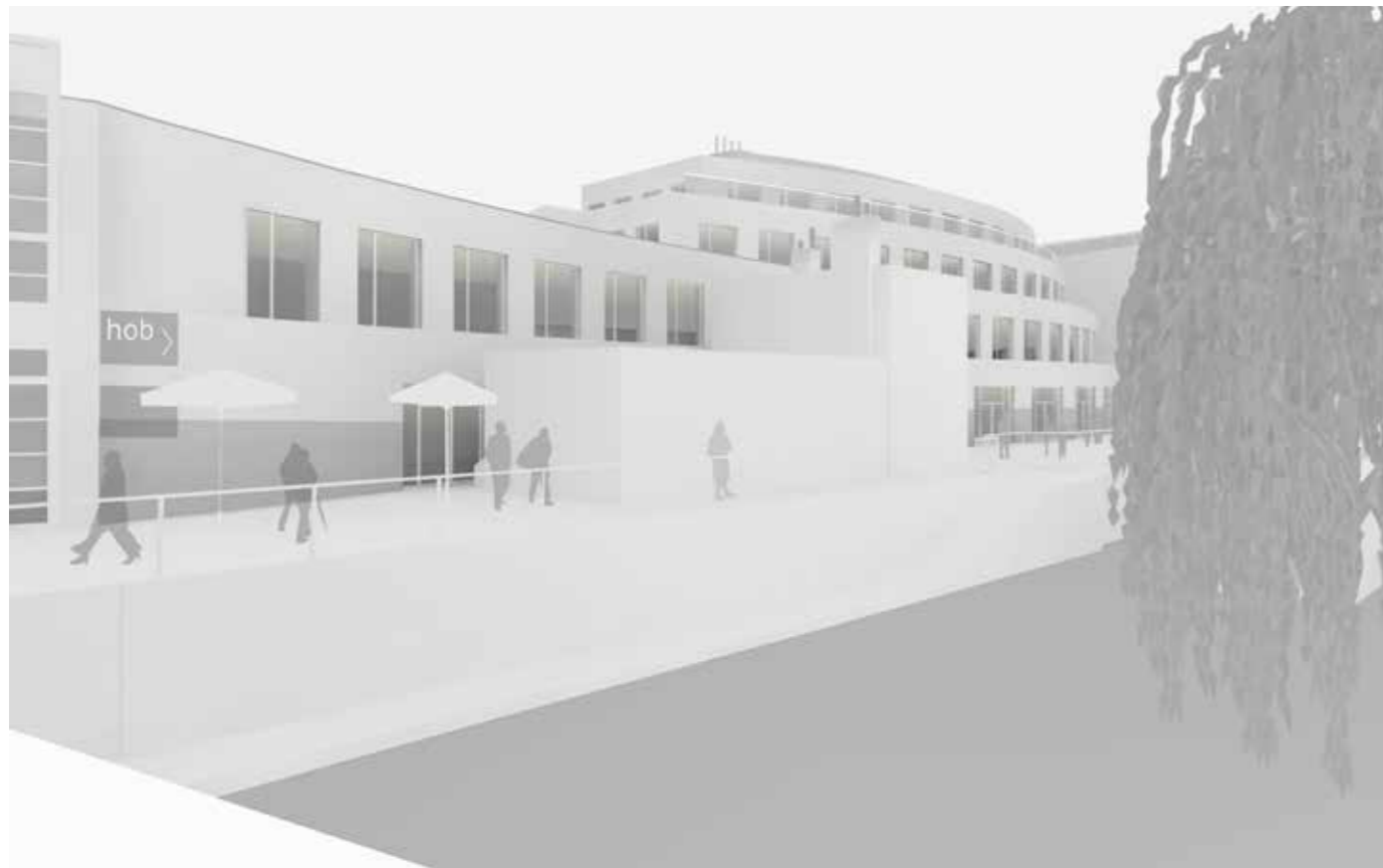
Key views on proposed site plan

4.0 DESIGN DEVELOPMENT

4.3 MASSING MODEL STUDY - VIEW 1 FROM CHALK FARM ROAD ON BRIDGE

This view is taken from the bridge on Chalk Farm Road towards the Regent's Canal and overlooks at the northern side of the building and the existing listed cafe.

The existing building plant area and flues are visible from this location and the new extension and re-arranged services will improve on this by making the view more consistent but still proportionate to its surroundings and the existing building and listed cafe to the canal side.



1. Existing View from Chalk Farm Road on Hampstead bridge



1. Existing Photo from Chalk Farm Road on Hampstead bridge



1. Proposed View from Chalk Farm Road on Hampstead bridge



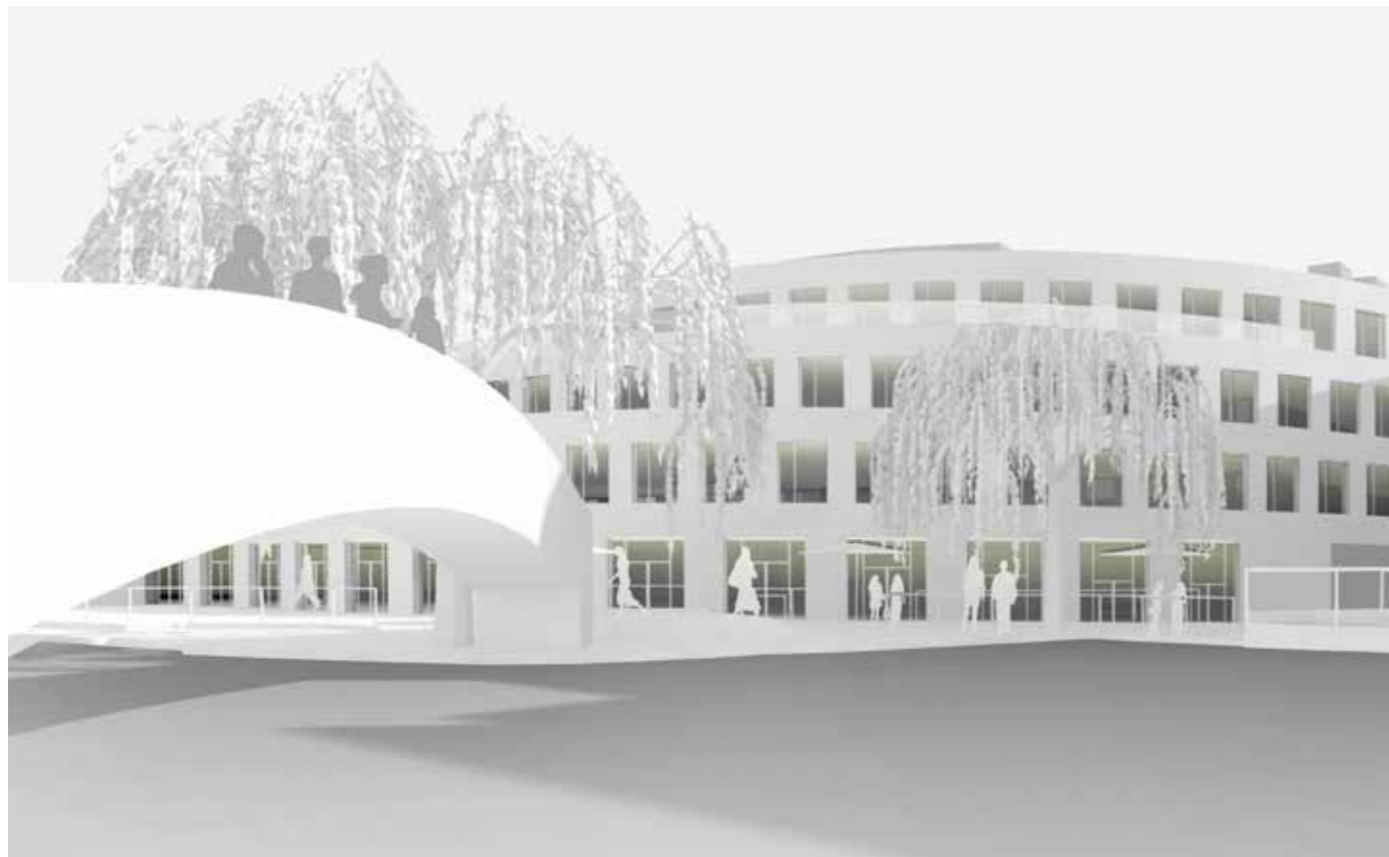
1. Proposed Photo collage from Chalk Farm Road on Hampstead bridge

4.0 DESIGN DEVELOPMENT

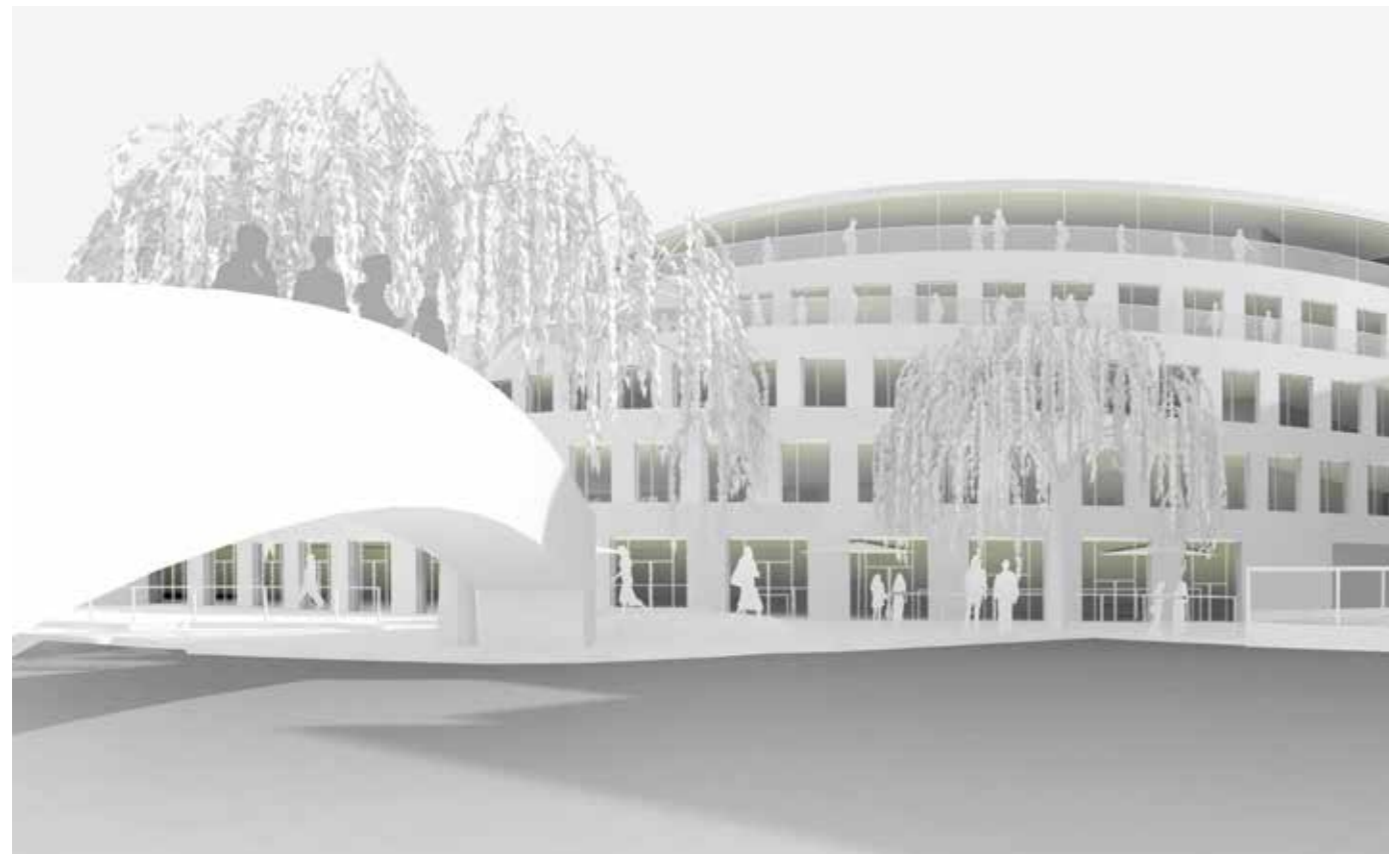
4.3 MASSING MODEL STUDY - VIEW 2 FROM CAMDEN LOCK MARKET

This view is taken from the canal walk on the Camden Lock Market side. It looks towards the front facade of the building and the listed pedestrian bridge that connects the two sides of Regent's Canal.

The existing building plant area is visible from this location as well as from view 1. The new extension and re-arranged services will improve on this by relocating all plant so that it is no longer visible.



2. Existing View from Camden Lock Market



2. Proposed View from Camden Lock Market



2. Existing Photo from Camden Lock Market



2. Proposed Photo collage from Camden Lock Market

4.0 DESIGN DEVELOPMENT

4.3 MASSING MODEL STUDY - VIEW 3 FROM ARLINGTON ROAD

This view is taken from Arlington Road and looks at the front of the building where the entrance for the existing offices is located. The existing buildings on this road show a good deal of variety in the style, height and material used.

The view shows the proposed green wall on the top floor new plant and demonstrates that the 4th floor extension is lower than the floors below on this side of the building.



3. Existing View from Arlington Road



3. Existing Photo from Arlington Road



3. Proposed View from Arlington Road



3. Proposed Photo collage from Arlington Road

4.0 DESIGN DEVELOPMENT

4.3 MASSING MODEL STUDY - VIEW 4 FROM CAMDEN HIGH STREET

This view is taken from the junction between Camden High Street, Jamestown Road and Hawley Crescent and looks west towards Jamestown Road. The foreground is dominated by the road, pavement, terraces, new built and street furniture. It also shows a fair amount of character on Camden High Street. The existing buildings display variety in style, height and materials used.

Only a small part of the subject building can be seen from this location and the existing plant and services dominate this corner. The proposed view and photo-montage aim to demonstrate that the new intervention will rationalise the south east corner of the building and will incorporate the services into the extension so that they won't be as visible.



4. Existing View from Camden High Street



4. Proposed View from Camden High Street



4. Existing Photo from Camden High Street

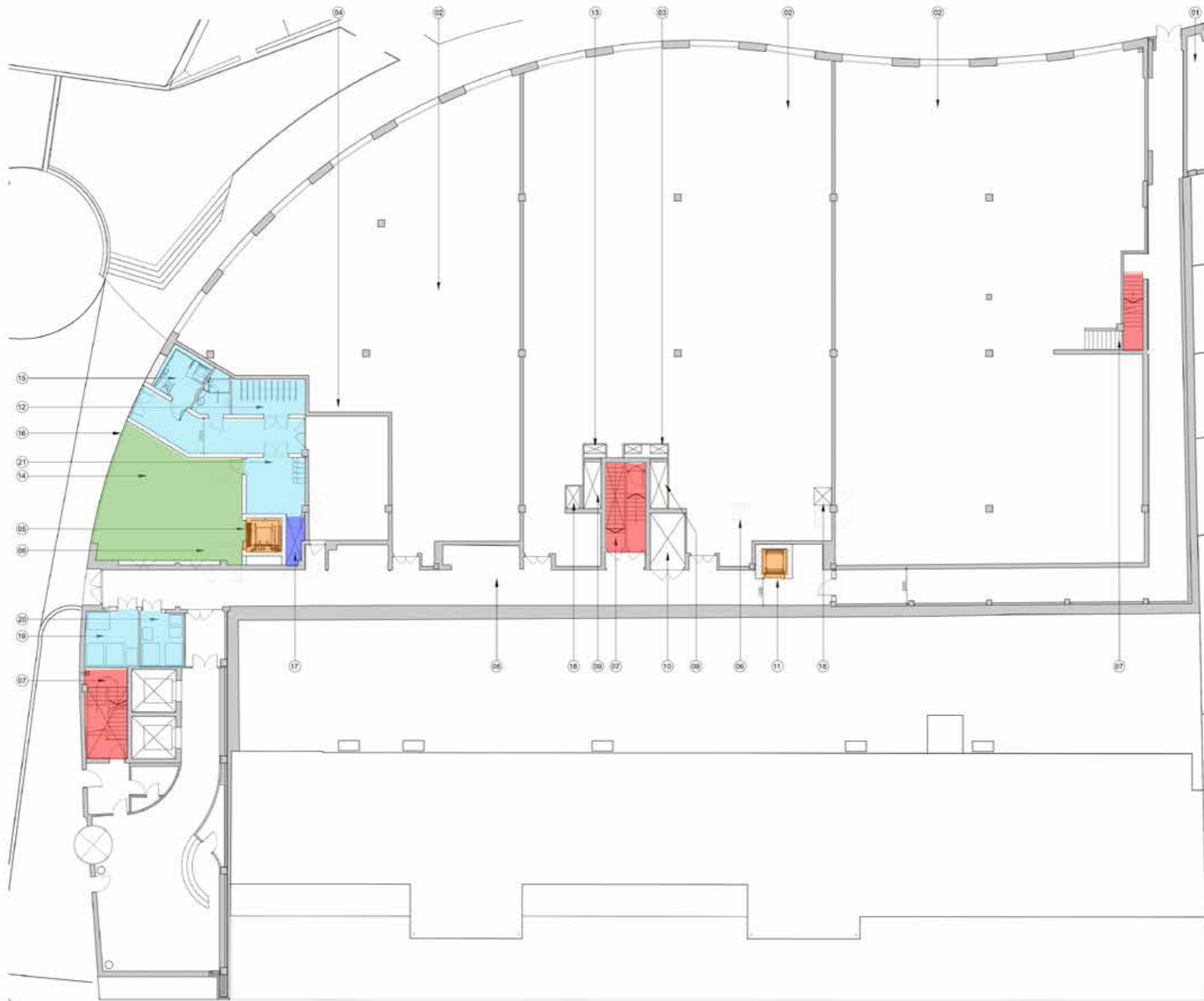


4. Proposed Photo collage from Camden High Street

4.0 DESIGN DEVELOPMENT

4.4 GROUND FLOOR

A new entrance including new shopfront and lobby will be created within the existing service yard at ground floor. Ancillary spaces for the new tenants will also be created at this level. A passenger lift and a services/goods lift will be introduced at the rear of the building to connect with the top floor.



Notes:

1. A1 existing retail unit
2. A3 existing restaurant unit
3. Electrical riser
4. LEB substation
5. New 13 person passenger lift (car size 1400mm x 1600mm)
6. Assumed existing pile structure
7. Existing escape stair
8. Existing external service corridor
9. Ventilation riser
10. Office supply AHU intake
11. New service/goods lift (car size 1000mm x 1550mm)
12. Bike store
13. LTHW/CHW riser
14. New A3 lobby
15. New showers, lockers and bike facilities
16. New shopfront
17. New riser
18. Smoke vent
19. Bin store: recycled waste
20. Bin store: general waste
21. Locker room and cleaner's cupboard

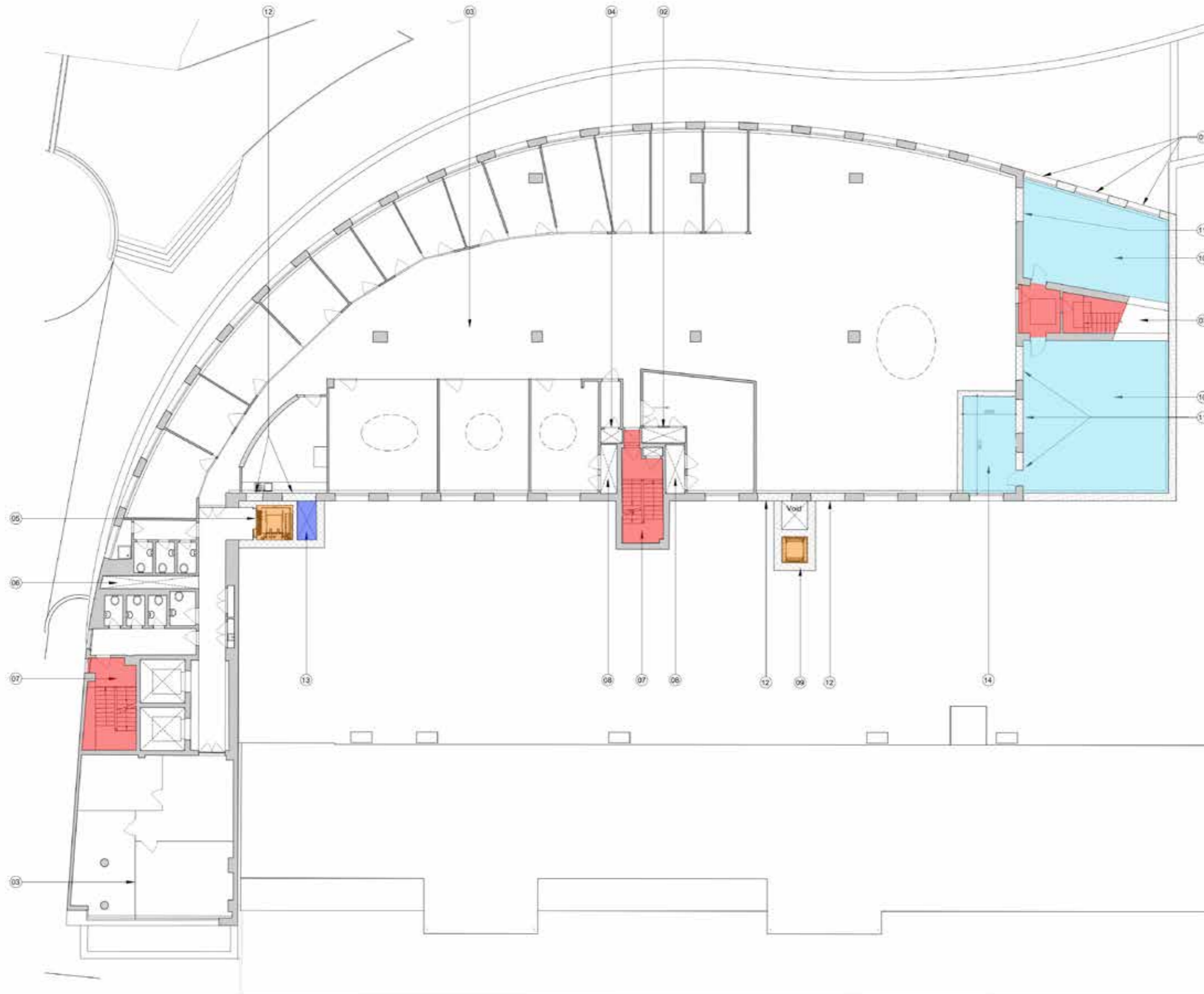
- | | |
|---|----------------------|
|  | New lift |
|  | Escape route |
|  | New riser |
|  | New ancillary |
|  | A3/ B1 lobby |
|  | Existing wall/column |
|  | New wall/column |



4.0 DESIGN DEVELOPMENT

4.5 SECOND FLOOR AND THIRD FLOOR EXTENSIONS

The second floor will be extended and will incorporate most of the plant area which is currently installed at roof level. The existing escaping staircase to the north side of the building will be retained and the plant areas will be accessible from either side of the stair lobby.



Notes:

1. New window to match existing
2. Electrical riser
3. Existing B1 unit
4. LTHW/CHW riser
5. New 13 person passenger lift (car size 1400mm x 1600mm)
6. Multi service riser
7. Existing escape stair
8. Ventilation riser
9. New service/goods lift (car size 1000mm x 1550mm)
10. New external plant area (GIA: 114SQM)
11. Existing windows blocked and internal office partitions removed
12. Existing windows blocked
13. New riser
14. Internal plant area (GIA: 16.5 SQM)

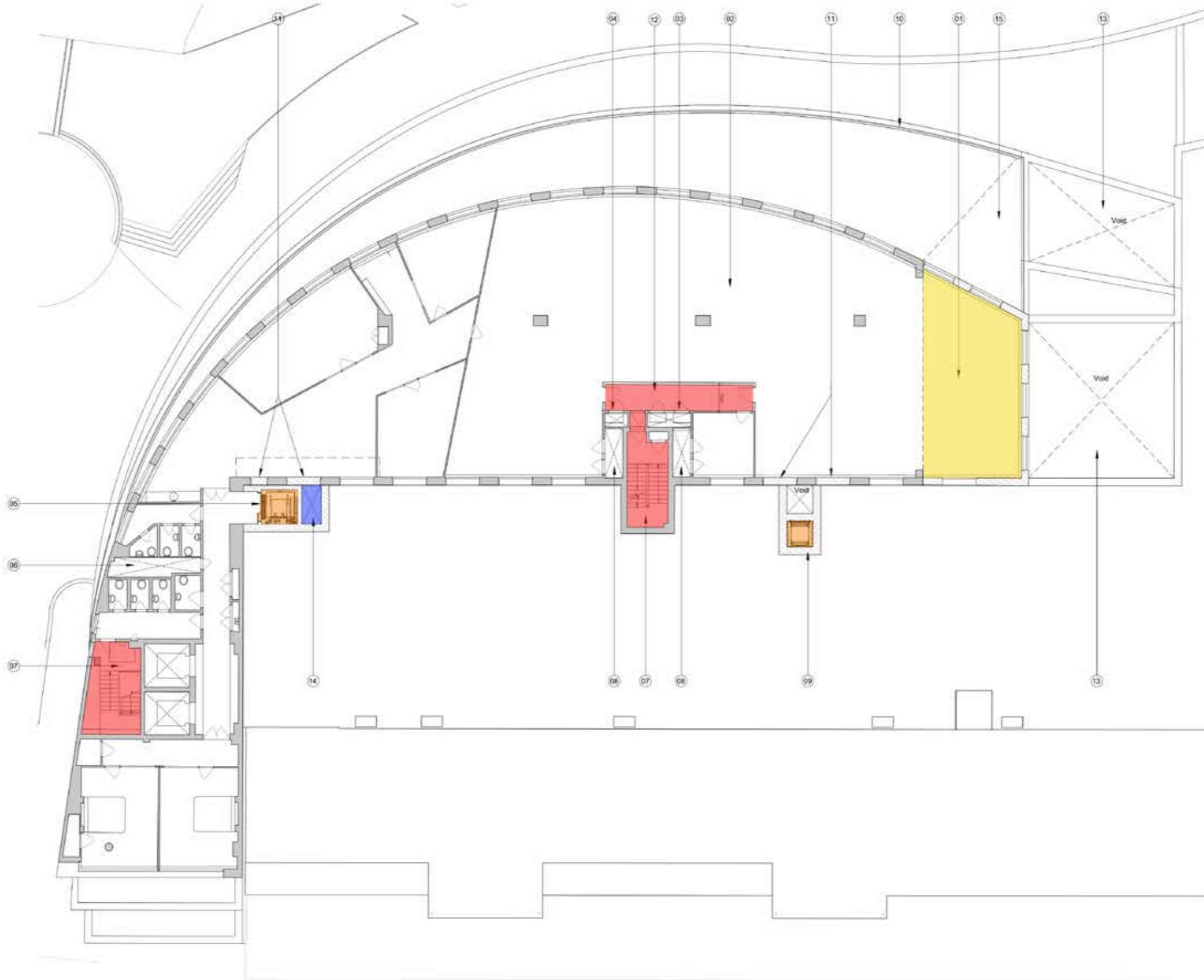
- New lift
- Escape route
- New riser
- New ancillary
- Existing wall/column
- New wall/column



4.0 DESIGN DEVELOPMENT

4.5 SECOND FLOOR AND THIRD FLOOR EXTENSIONS

The third floor will be extended to create additional office space. The terrace at this level will also be increased and a new fully glazed balustrade installed in place of the existing one.



Notes:

1. Additional B1 (NIA/GIA: 58.2SQM)
2. Existing B1 unit
3. Electrical riser
4. LTHW/CHW riser
5. New 13 person passenger lift (car size 1400mm x 1600mm)
6. Multi service riser
7. Existing escape stair
8. Ventilation riser
9. New service/goods lift (car size 1000mm x 1550mm)
10. New glass balustrade with stainless steel channels
11. Existing windows blocked
12. New fire protected corridor
13. Void; plant below
14. New riser
15. Existing terrace extended

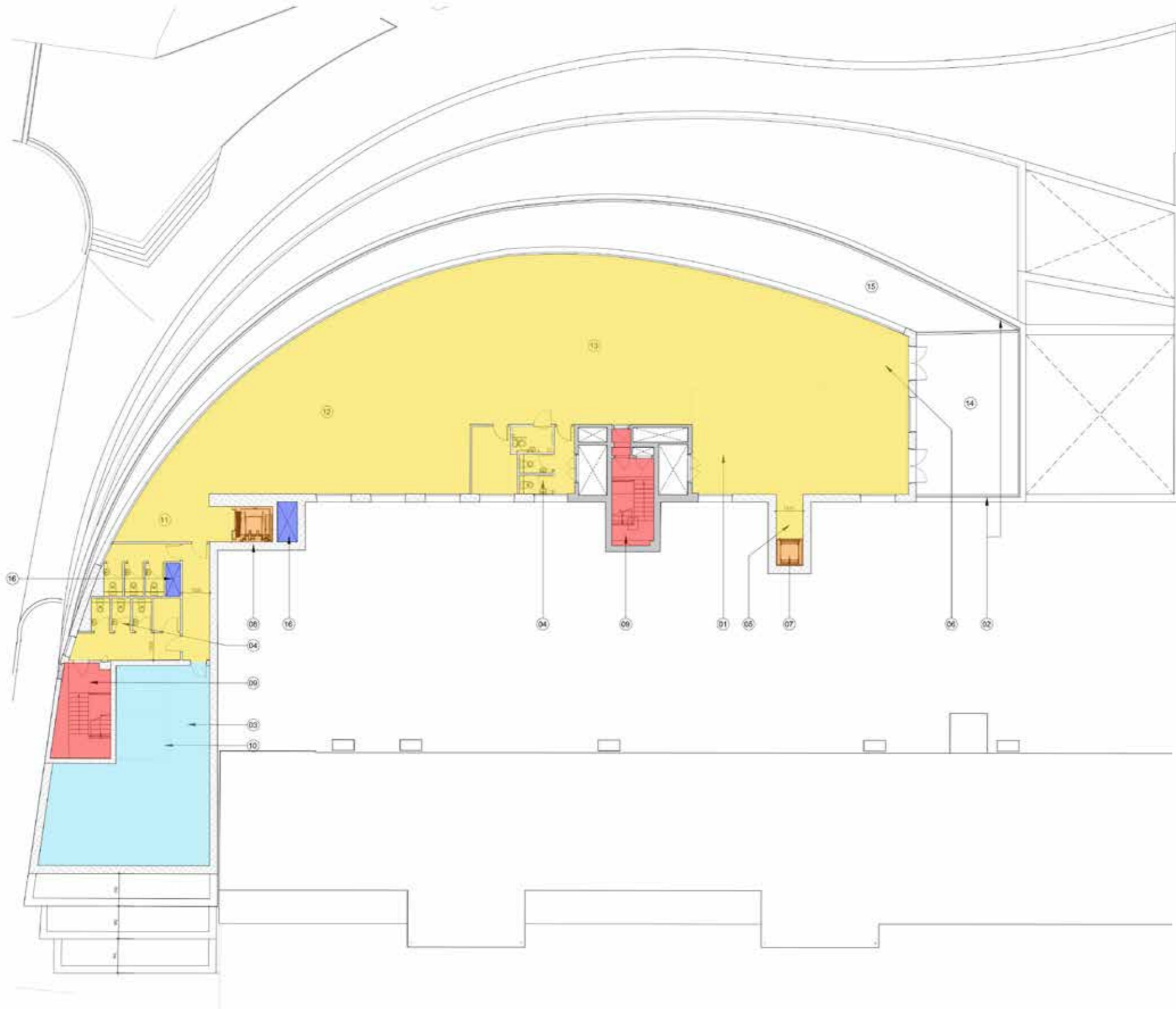
- Additional B1 area
- New lift
- Escape route
- New riser
- Existing wall/column
- New wall/column



4.0 DESIGN DEVELOPMENT

4.6 FOURTH FLOOR EXTENSION

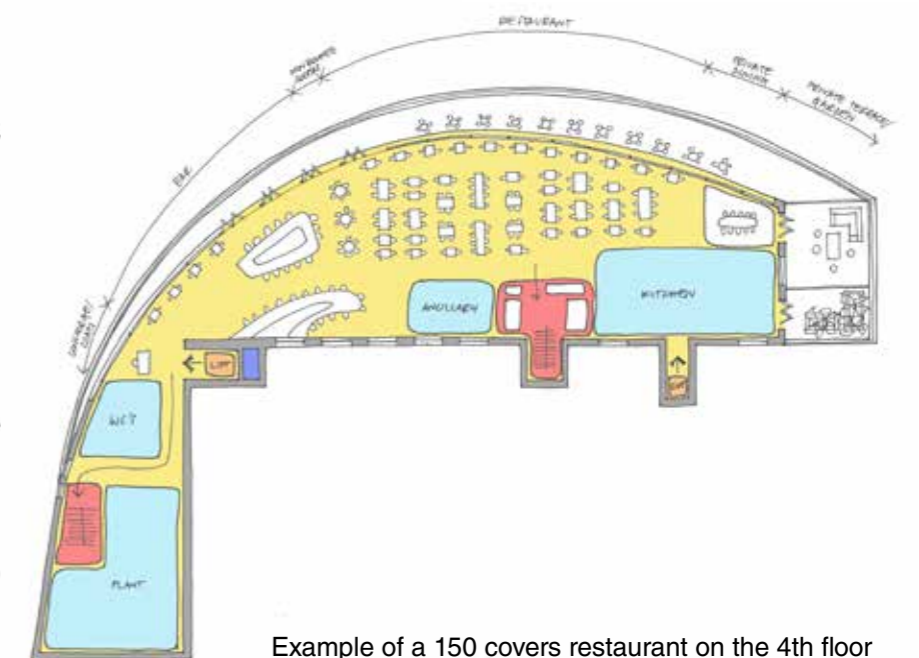
The large roof plant area will be partially relocated at 2F and at this floor and the rooftop extended to provide a new dual A3/ B1 space. The plans on this page show a flexible base built and an example for the restaurant layout to describe how this floor can be arranged. New private and public terraces for the restaurant and ancillary spaces will also be introduced. A new passenger lift and a services/ goods lift will connect this level to the ground floor and the existing escape stairs will be reused and extended. The new plant will be installed on this level and integrated with the one storey roof extension.



Notes:

1. Kitchen area	Additional A3/ B1 area
2. Glass balustrades with stainless steel channels	New lift
3. Plant	Escape route
4. WC's area	New riser
5. Service lobby area	New ancillary
6. Private dining area	Existing wall/column
7. New service/goods lift (car size 1000mm x 1150mm)	New wall/column
8. New 13 person passenger lift (car size 1400mm x 1600mm)	
9. Escape stair and existing risers	
10. Lift over-run	
11. Concierge/ welcome/ coat area	
12. Bar area	
13. Restaurant area	
14. Private terrace/ green roof	
15. Public terrace	
16. New riser	

0 2 5 10m



Example of a 150 covers restaurant on the 4th floor

4.0 DESIGN DEVELOPMENT

4.7 PUBLIC REALM/ SERVICING IMPROVEMENT



1



2



3



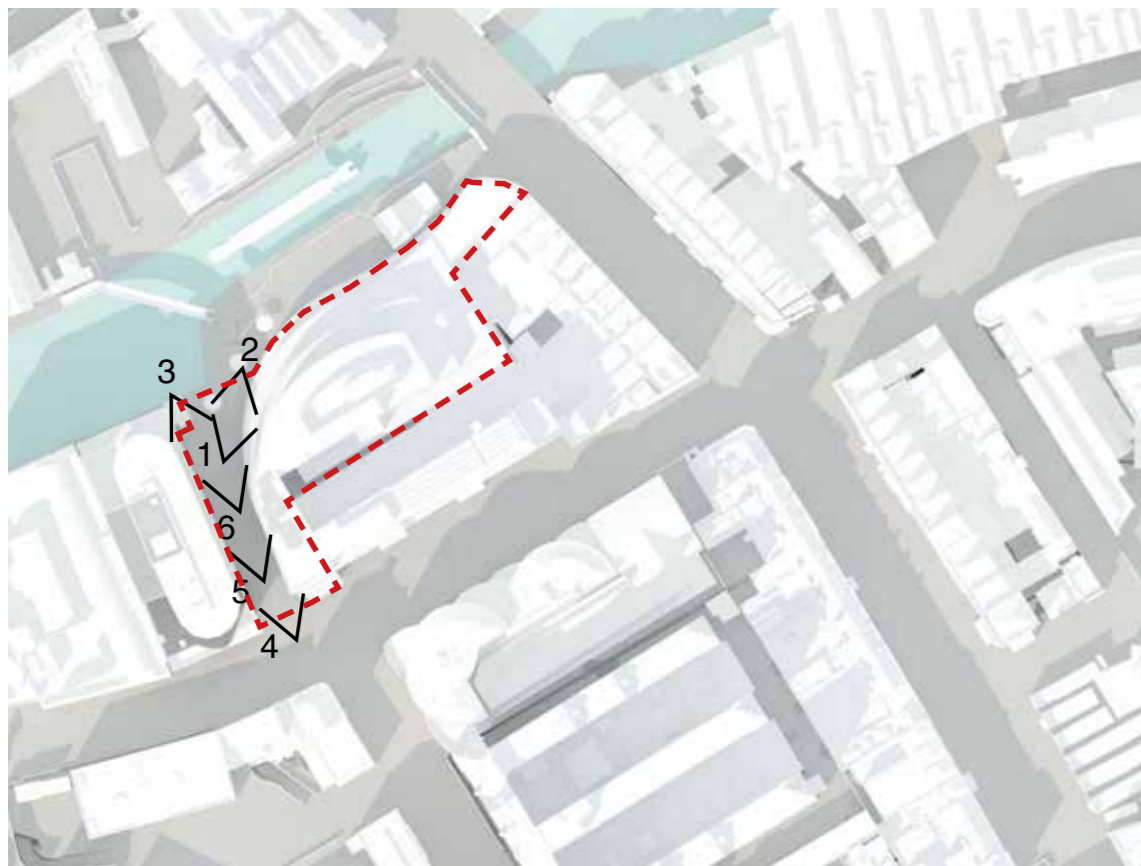
4



5



6



The public realm and transport strategy aims to improve the accessibility and permeability of the building from both the canal side and Jamestown Road. As shown in the photos on this page, the private road to the west side of the building is currently used for loading/ unloading deliveries and car parking. It is poorly connected to the canal walk and is not used by pedestrians.

The transport strategy is based on the existing operational approach and the majority of the trips are expected to be made using sustainable forms of transport.

Cycle Parking

Cycle parking is currently provided within the ground floor servicing area. There is no visitor parking provided in the immediate vicinity of the building. Additional cycle parking will be provided to serve the extension to the building in accordance with the most recent London Plan standards and to meet the higher requirements between B1 and A3 uses. Based on the current proposals the redevelopment would need to provide six long-stay bike spaces and fifteen short stay bike spaces. When identifying the location and design of cycle parking, consideration will be given to Camden Planning Guidance 7: Transport. It is envisaged that long-stay cycle parking requirements will be met using a cycle stacking system and short-stay spaces provided using either 'Camden' or 'Sheffield' type bicycle stands. Visitor cycle parking will be located outside the building.

Supporting facilities for cyclists (i.e. showers, lockers and changing rooms) will be provided. As there is no specific guidance on the quantities of these facilities, standards have been applied, which is one locker per cycle space and one shower per 10 spaces.

Car Parking

In accordance with Policy DP18 (Parking standards and limiting the availability of car parking) in Camden's Development Policies document, the development will be car free and no motorcycle parking is to be provided.

Waste Storage

Camden Wharf has an existing loading bay on the ground floor, accessed via a private road that connects to Jamestown Road. This loading area is not currently utilised with servicing predominantly taking place on the private access road and Jamestown Road. Given the lack of usage of the on-site loading area it is proposed that loading/unloading continues to take place as existing outside the building. The servicing strategy will be informed by surveys of existing activity which are to be undertaken in May 2016.

Waste storage is currently provided at ground floor level for existing tenants and the current waste storage and collection arrangements for these tenants will remain as existing. Waste generation for the additional floor space are calculated using criteria set out in BS5906:2005. The upper floor is to be flexible B1/A3 use. A3 restaurant use would generate more waste than B1 use so the assessment assumes that the upper floor will become a restaurant as a worst case scenario. This ensures that there is sufficient space to meet the likely demands of a restaurant occupier.

Transport/ Travel Plan

A Transport Statement will summarise the transport proposals and the likely implications on the local transport network. The Transport Statement will be submitted as part of the planning application.

A Travel Plan Statement will be developed for the proposed B1/A3 flexible space to be provided on the upper floor. A Travel Plan Statement is applicable to smaller developments that fall below the thresholds for a Full Travel Plan (2500sqm for B1 use and 750sqm for A3 use). Camden Wharf is in an accessible location and no car parking is to be provided so it is anticipated that employees and visitors will travel to the site sustainably. The Travel Plan Statement will however outline a set of measures and initiatives aimed at encouraging the use of sustainable modes of travel.

4.0 DESIGN DEVELOPMENT

4.7 PUBLIC REALM/ SERVICING IMPROVEMENT - EXISTING



The private road to the west side of the building is currently used for loading/unloading deliveries and car parking. It is poorly connected to the canal walk and it is not used by pedestrians as much as it should.

4.0 DESIGN DEVELOPMENT

4.7 PUBLIC REALM/ SERVICING IMPROVEMENT - PROPOSED









The Camden Wharf project will enhance the public realm by increasing the permeability of the existing private road to the west side of the building. Deliveries will still come to the building via this route but the proposal includes for pedestrian footfall only and will link this road with the canal walk by rearranging the pavement and providing a new entrance for the restaurant or offices at 4th floor.

The existing bin store at ground floor will be expanded by to provide sufficient waste storage space for the new extensions. Ancillary spaces will also be provided and will include a secure bike storage, lockers, and showers in the area of the existing loading bay. Short-stay bike spaces will be provided outside the building and a loading bay will be delineated by the new paving arrangement.

Notes:

1. No. 8 Sheffield/ Camden bike upstands
2. New A3/ B1 entrance
3. Loading bay
4. Granite slabs
5. Paving tiles
6. Proposed Waste Storage

-  New A3/ B1 entrance
-  Vehicular traffic
-  Pedestrian footfall
-  New granite slabs
-  New paving tiles
-  Site Boundary



4.0 DESIGN DEVELOPMENT

4.8 NEW A3 ENTRANCE



Existing View



Proposed View

4.0 DESIGN DEVELOPMENT

4.9 LAND USE

	Existing Areas		Proposed Areas		Uplift	
	sq m	sq ft	sq m	sq ft	sq m	sq ft
Basement Floor						
GEA	381	4,104	391	4,213	10	109
GIA	33	360	40	426	6	67
NIA	0	0	0	0	0	0
Ground Floor						
GEA	2,096	22,561	2,098	22,587	2	26
GIA	1,941	20,894	1,943	20,913	2	19
NIA	1,365	14,694	1,365	14,694	0	0
First Floor						
GEA	1,511	16,260	1,540	16,572	29	312
GIA	1,421	15,296	1,448	15,588	27	293
NIA	1,252	13,476	1,252	13,476	0	0
Second Floor						
GEA	1,140	12,271	1,290	13,881	150	1,610
GIA	1,059	11,395	1,205	12,975	147	1,580
NIA	887	9,551	887	9,551	0	0
Third Floor						
GEA	803	8,644	893	9,608	90	963
GIA	737	7,930	820	8,823	83	893
NIA	584	6,285	1,202	12,937	618	6,652
Fourth Floor						
GEA	183	1,974	681	7,333	498	7,333
GIA	174	1,877	531	5,716	357	5,716
NIA	0	0	411	4,426	411	4,426
Total GEA	6,114	65,815	6,893	74,194	779	10,354
Total GIA	5,365	57,750	5,987	64,441	622	8,568
Total NIA	4,088	44,006	5,118	55,084	1,029	11,078



Allotments on roof



Green roof and green wall



PV panels and green roof combined

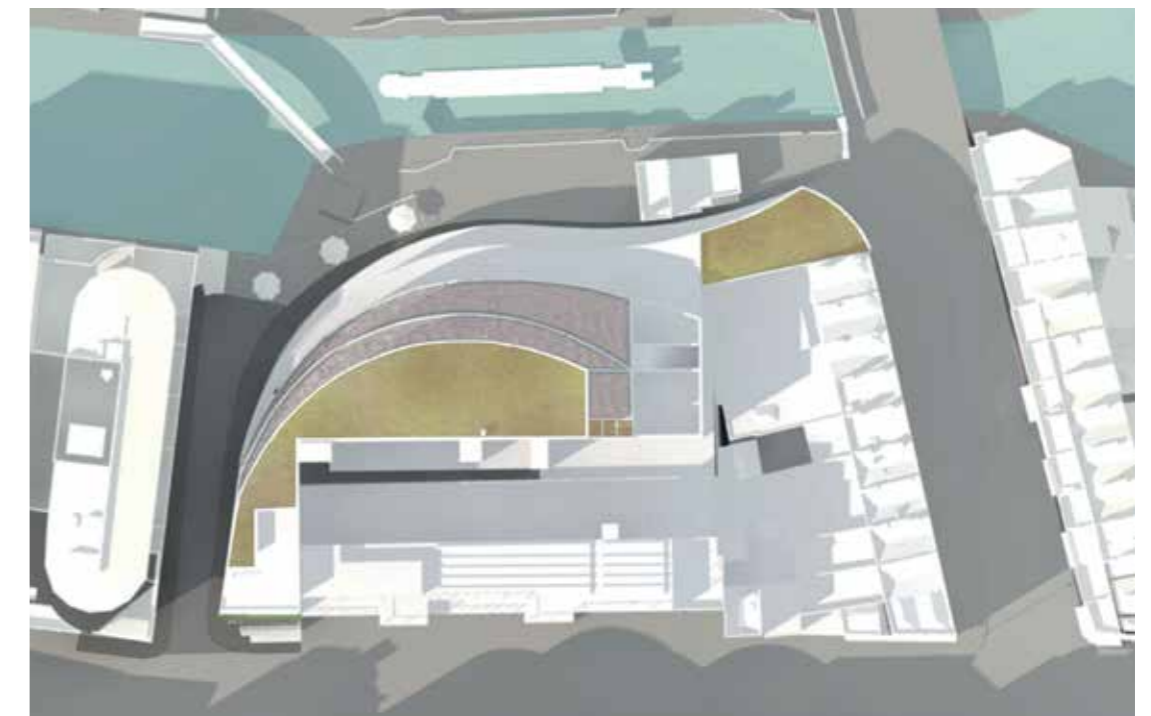
4.0 DESIGN DEVELOPMENT

4.10 SUSTAINABILITY STRATEGY

The proposed restaurant extension will be assessed against BREEAM New Construction 2014, with a target of Excellent. This target requires the project to achieve an overall score of 70%. The design team intends to exceed this score through incorporation of sustainability measures into the design to minimise the energy, water, waste and materials impact of the project. A green roof combined with renewable technologies can be incorporated as part of the 4th extension. Allotments are planned as part of the top floor extension as well as a green wall at the rear of the new plant area.

For an Excellent rating the project must also achieve a small number of mandatory performance criteria. The proposed waste storage facilities will meet the mandatory criteria for Operational Waste. During design development an energy model will be utilised to demonstrate improvements in energy efficiency exceeding the Building Regulations Part L baseline.

An Ecology survey has been completed by the project's Ecologist, which will be used to guide measures to enhance biodiversity on site.



Roof plan

4.0 DESIGN DEVELOPMENT

4.11 SERVICES, STRUCTURE AND MIXED USE

Services

The plant currently at roof level will be relocated within the new extensions in order to create the 4th floor restaurant extension. In addition, some of the plant on the shelf at Level 1 will be reconfigured to make room for the new lifts to the restaurant at the rear of the building.

Plant space for the proposed 4th floor restaurant will be provided at 4th floor while the existing plant will be redistributed at 2nd and 4th floor.

The sketches on this page describe the major existing and proposed services locations and illustrates both where the existing services are going to be relocated and where the new services are going to be installed.

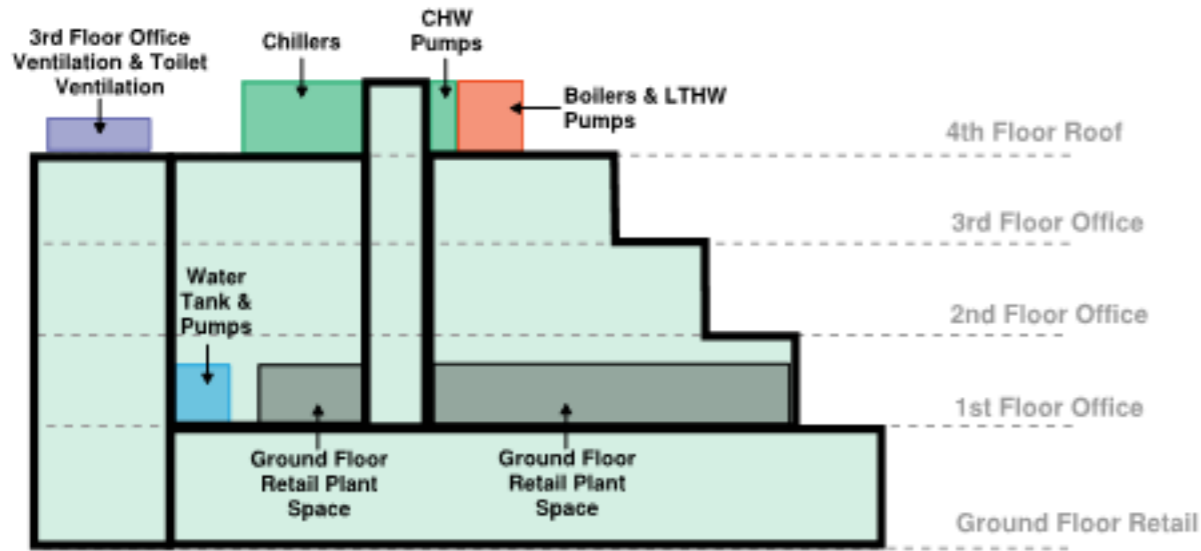
Structure

The structural proposals are as follows:

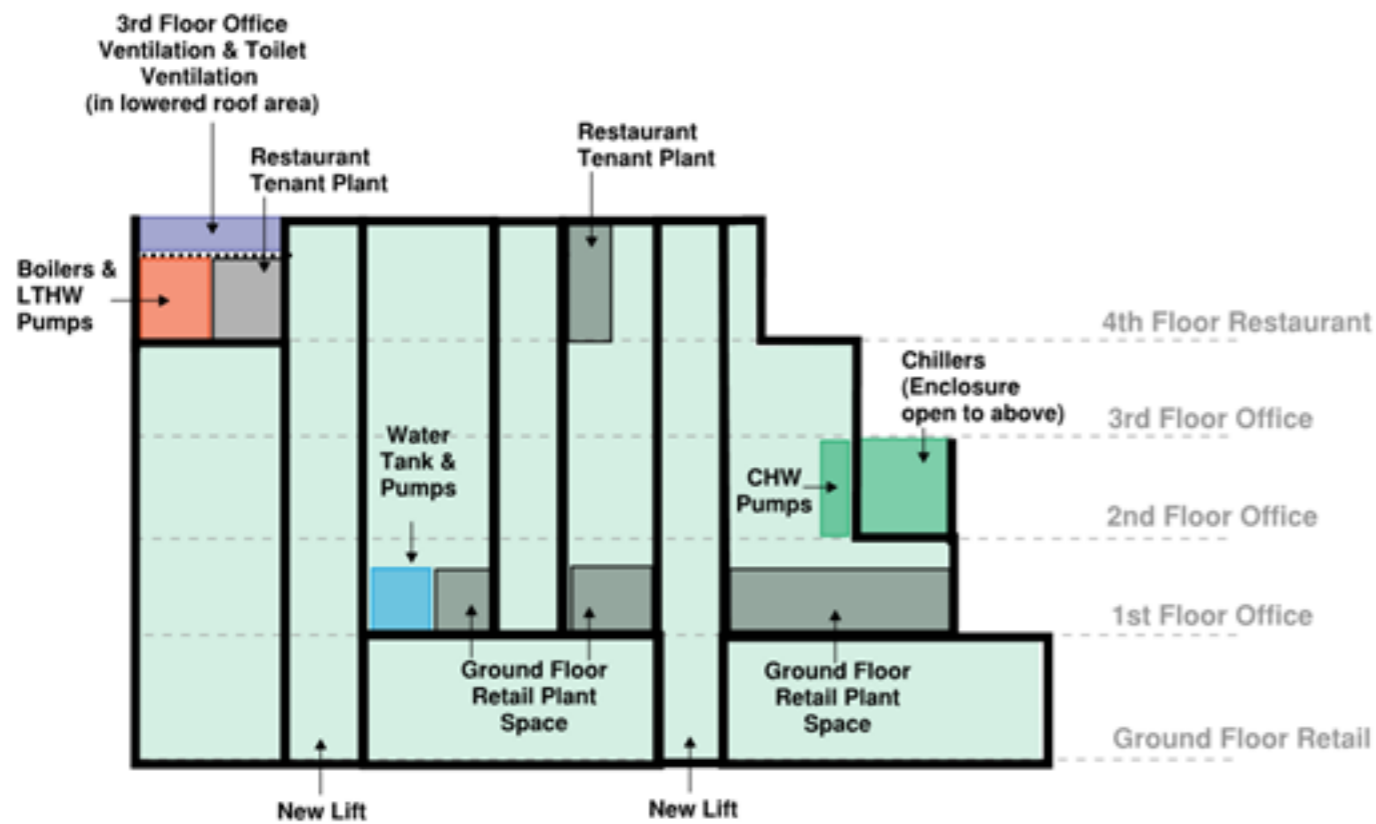
- New roof construction over additional accommodation, including vertical support. Generally this will be a lightweight construction, but may follow the original building's concrete frame in certain locations
- Potentially strengthening of existing structure to carry additional loads from added areas and new load paths
- New lift shafts and pits; the pits to be excavated through the existing ground floor slab between pile caps
- Miscellaneous builders work in relation to the revisions to building services
- The existing frame will largely be retained and demolition will therefore be limited to local removal of façade and finishes, creation of new openings, and achievement of sound interfaces between new and existing structure.

Mixed Use

The proposal generates a residential requirement. The residential floor space will be provided on another site owned by the Applicant. A planning application in respect of the residential is envisaged to be made at the same time as this application.



Existing services provision




Proposed services provision

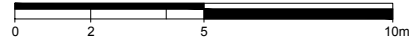
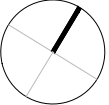
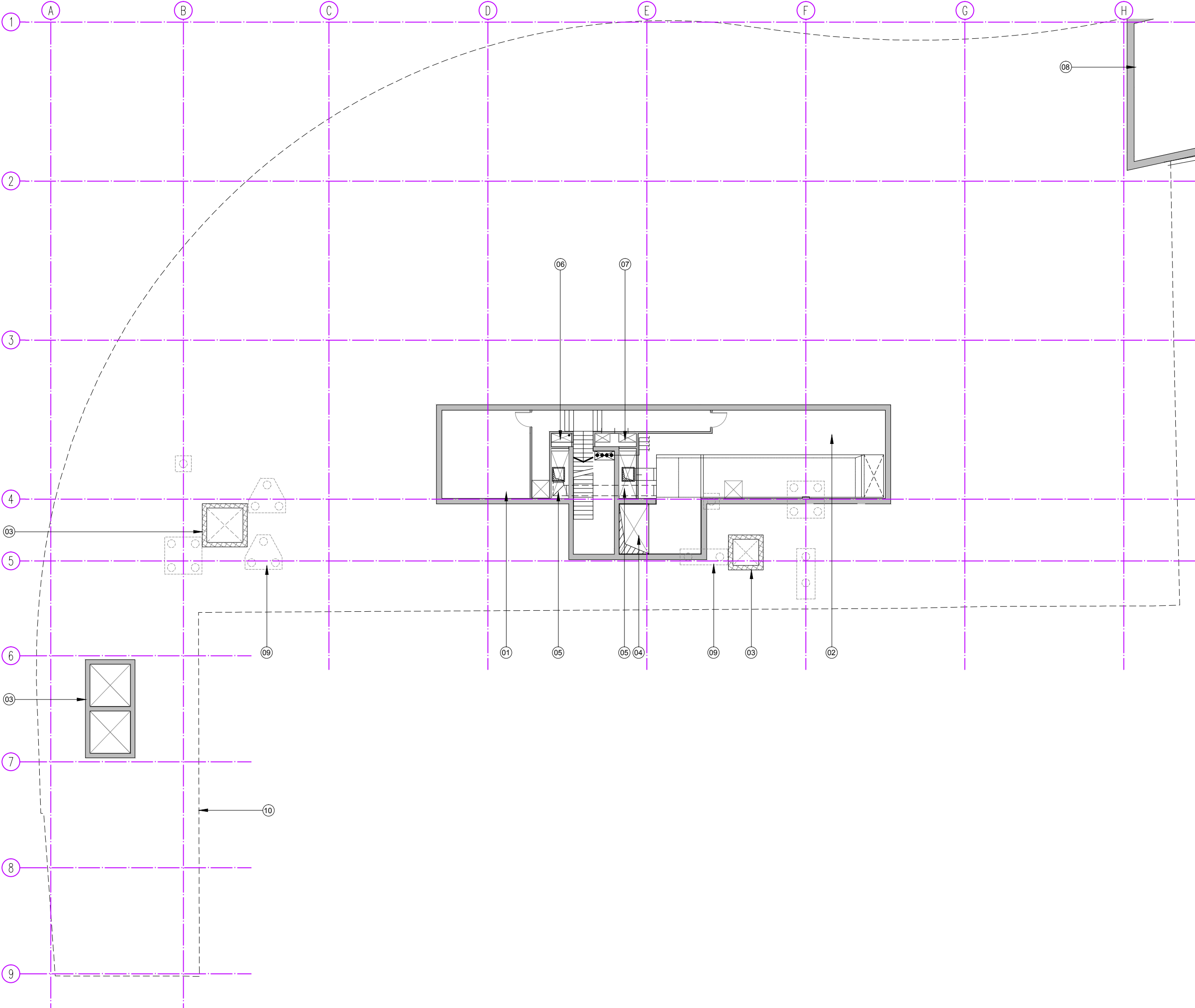
APPENDICES

BARR GAZETAS

Notes:

- 1. Electrical plant room
- 2. Supply AHU plant room
- 3. Lift pit
- 4. Air intake
- 5. Ventilation riser
- 6. LTHW/CHW riser
- 7. Electrical risers
- 8. A1 existing retail unit
- 9. Assumed existing pile structure
- 10. Line of building above

-  Existing wall/column
-  New wall/column



For Information

Rev.	Description	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

Project
Camden Wharf

Title
Proposed Basement Floor Plan

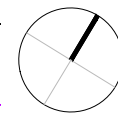
Project No.	Drawing No.	Rev.
1524	20.200	B
Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	04.04.16

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Notes:

1. A1 existing retail unit
2. A3 existing restaurant unit
3. Electrical riser
4. LEB substation
5. New 13 person passenger lift (car size 1400mm x 1600mm)
6. Assumed existing pile structure
7. Existing escape stair
8. Existing external service corridor
9. Ventilation riser
10. Office supply AHU intake
11. New service/goods lift (car size 1000mm x 1550mm)
12. Bike store
13. LTHW/CHW riser
14. New A3 lobby
15. New showers, lockers and bike facilities
16. New shopfront
17. New riser
18. Smoke vent
19. Bin store: recycled waste
20. Bin store: general waste
21. Locker room and cleaner's cupboard

-  Existing wall/column
-  New wall/column



For Information

Rev.	Description	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

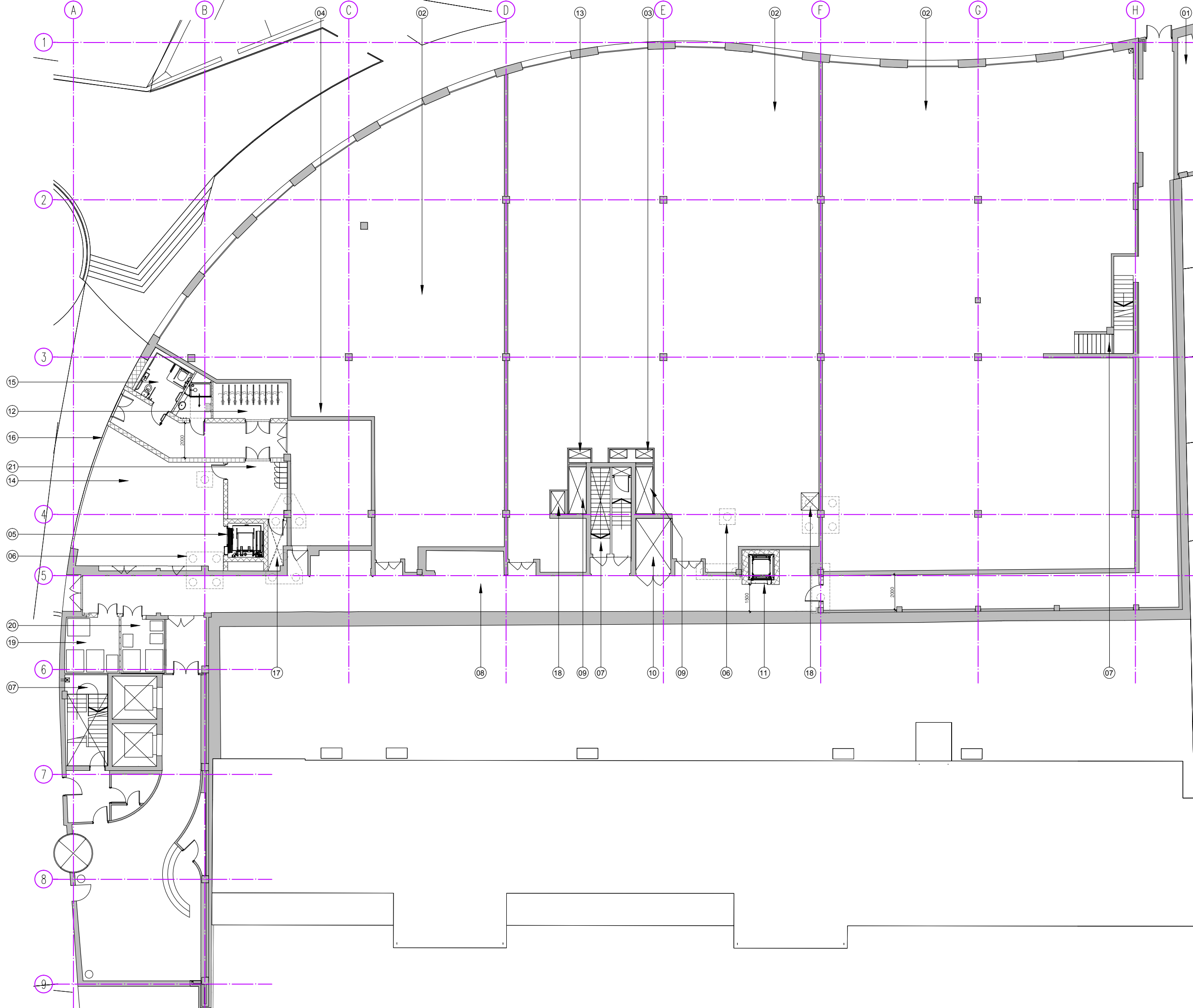
Client
Castlehaven Row Ltd

Project
Camden Wharf

Title
Proposed Ground Floor Plan



Project No.	Drawing No.	Rev.
1524	20.201	B
Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	04.04.16

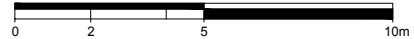
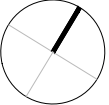
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Notes:

1. Existing escape stair
2. Existing B1 unit
3. New green roof
4. Electrical riser
5. LTHW/CHW riser
6. Existing windows blocked
7. New 13 person passenger lift (car size 1400mm x 1600mm)
8. Existing multi service riser
9. Water tank in new location on existing decking
10. Ventilation riser
11. AHU intake
12. New service lift (car size 1000mm x 1150mm)
13. Existing ground floor retail plant space
14. New riser

-  Existing wall/column
-  New wall/column



For Information

Rev.	Description	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

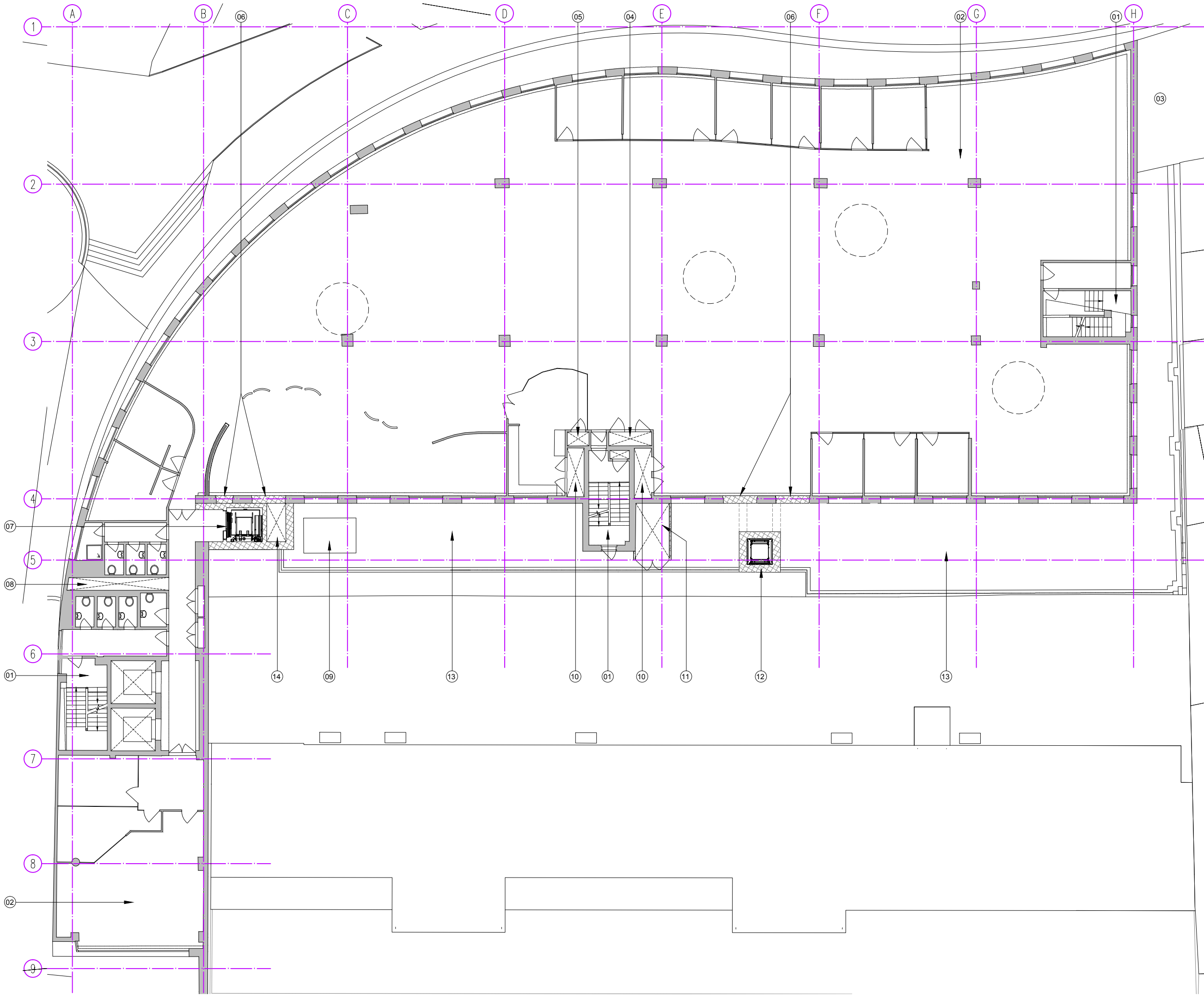
Project
Camden Wharf

Title
Proposed First Floor Plan

Project No.	Drawing No.	Rev.
1524	20.202	B

Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	08.04.16

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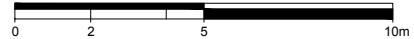
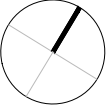


BARR GAZETAS

Notes:

1. New window to match existing
2. Electrical riser
3. Existing B1 unit
4. LTHW/CHW riser
5. New 13 person passenger lift (car size 1400mm x 1600mm)
6. Multi service riser
7. Existing escape stair
8. Ventilation riser
9. New service/goods lift (car size 1000mm x 1550mm)
10. New external plant area (GIA: 114.5SQM)
11. Existing windows blocked and internal office partitions removed
12. Existing windows blocked
13. New riser
14. Internal plant area (GIA: 16.5 SQM)

-  Existing wall/column
-  New wall/column



For Information

Rev.	Description.	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

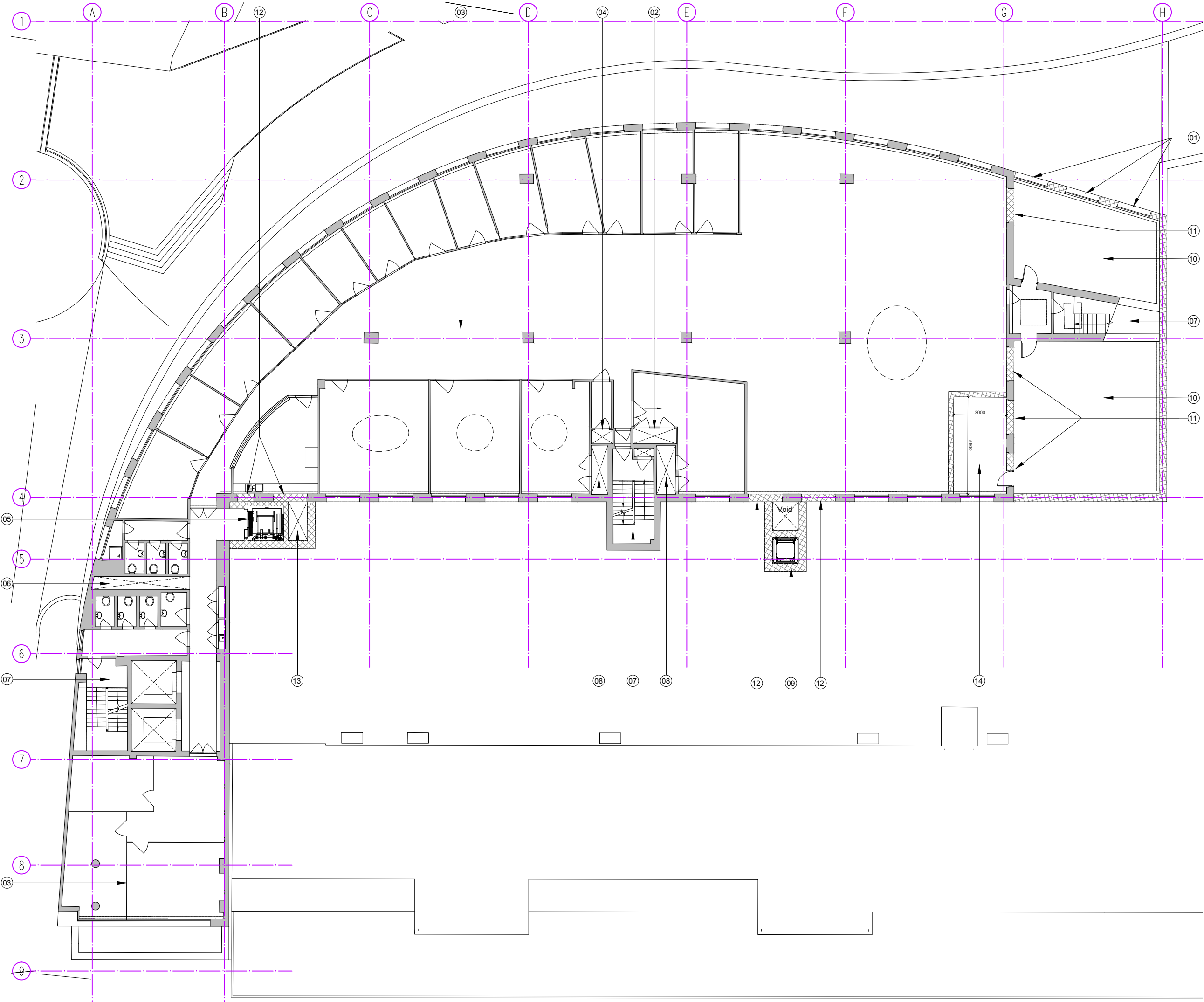
Project
Camden Wharf

Title
Proposed Second Floor Plan

Project No.	Drawing No.	Rev.
1524	20.203	B

Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	08.04.16


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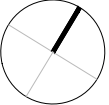
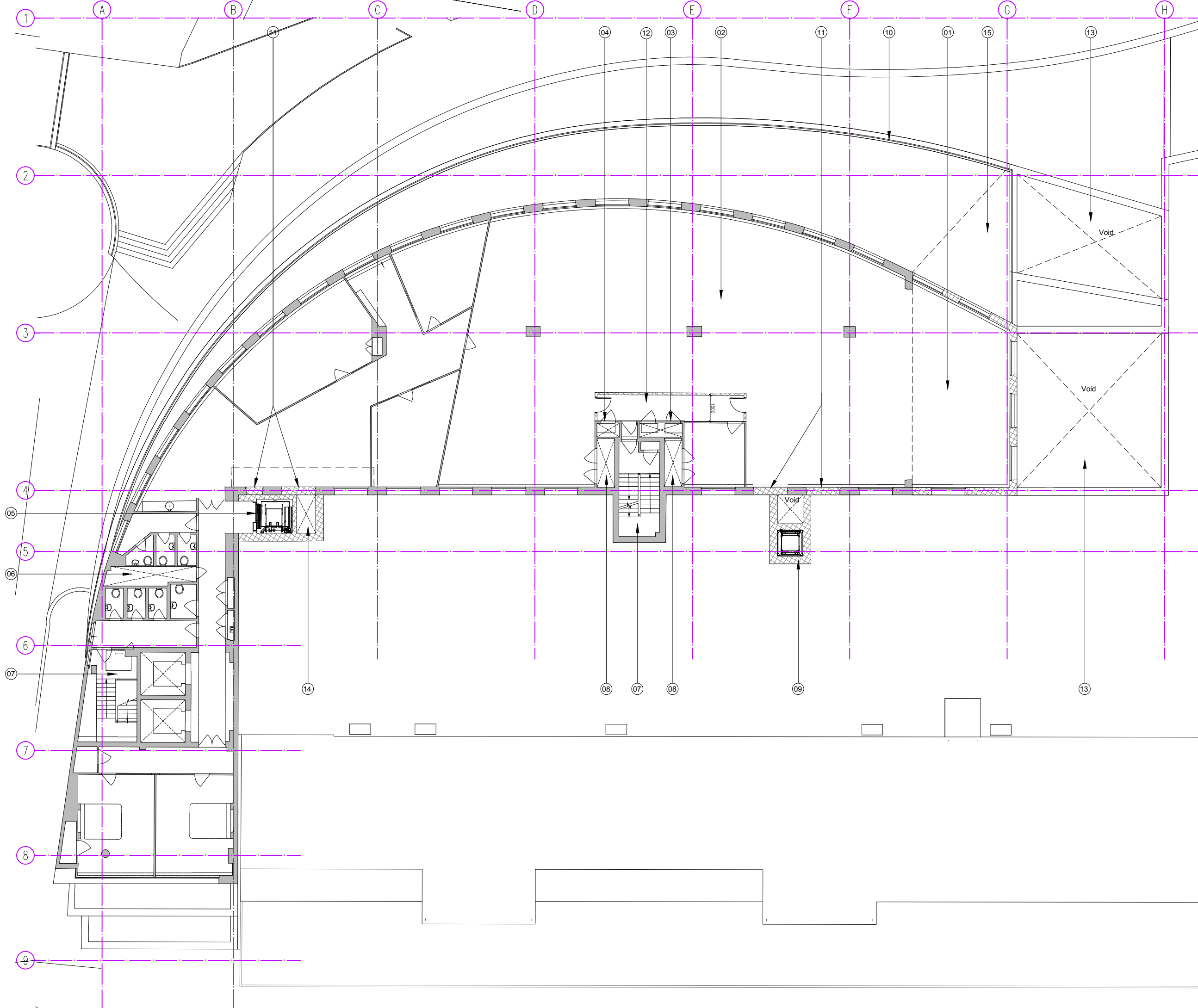


BARR GAZETAS

Notes:

- 1. Additional B1 (NIA/GIA: 58.2SQM)
- 2. Existing B1 unit
- 3. Electrical riser
- 4. LTHW/CHW riser
- 5. New 13 person passenger lift (car size 1400mm x 1600mm)
- 6. Multi service riser
- 7. Existing escape stair
- 8. Ventilation riser
- 9. New service/goods lift (car size 1000mm x 1550mm)
- 10. New glass balustrade with stainless steel channels
- 11. Existing windows blocked
- 12. New fire protected corridor
- 13. Void; plant below
- 14. New riser
- 15. Existing terrace extended

-  Existing wall/column
-  New wall/column



For Information

Rev.	Description.	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

Project
Camden Wharf

Title
Proposed Third Floor Plan

Project No.	Drawing No.	Rev.
1524	20.204	B

Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	08.04.16

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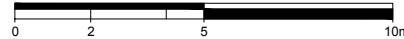
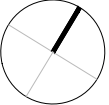
BARR GAZETAS

Notes:

- 1. Kitchen area
- 2. Glass balustrades with stainless steel channels
- 3. Plant
- 4. WC's area
- 5. Service lobby area
- 6. Private dining area
- 7. New service/goods lift (car size 1000mm x 1150mm)
- 8. New 13 person passenger lift (car size 1400mm x 1600mm)
- 9. Escape stair and existing risers
- 10. Lift over-run
- 11. Concierge/ welcome/ coat area
- 12. Bar area
- 13. Restaurant area
- 14. Private terrace/ green roof
- 15. Public terrace
- 16. New riser

Existing wall/column
 New wall/column

Restaurant GIA: 531SQM



For Information

Rev.	Description	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

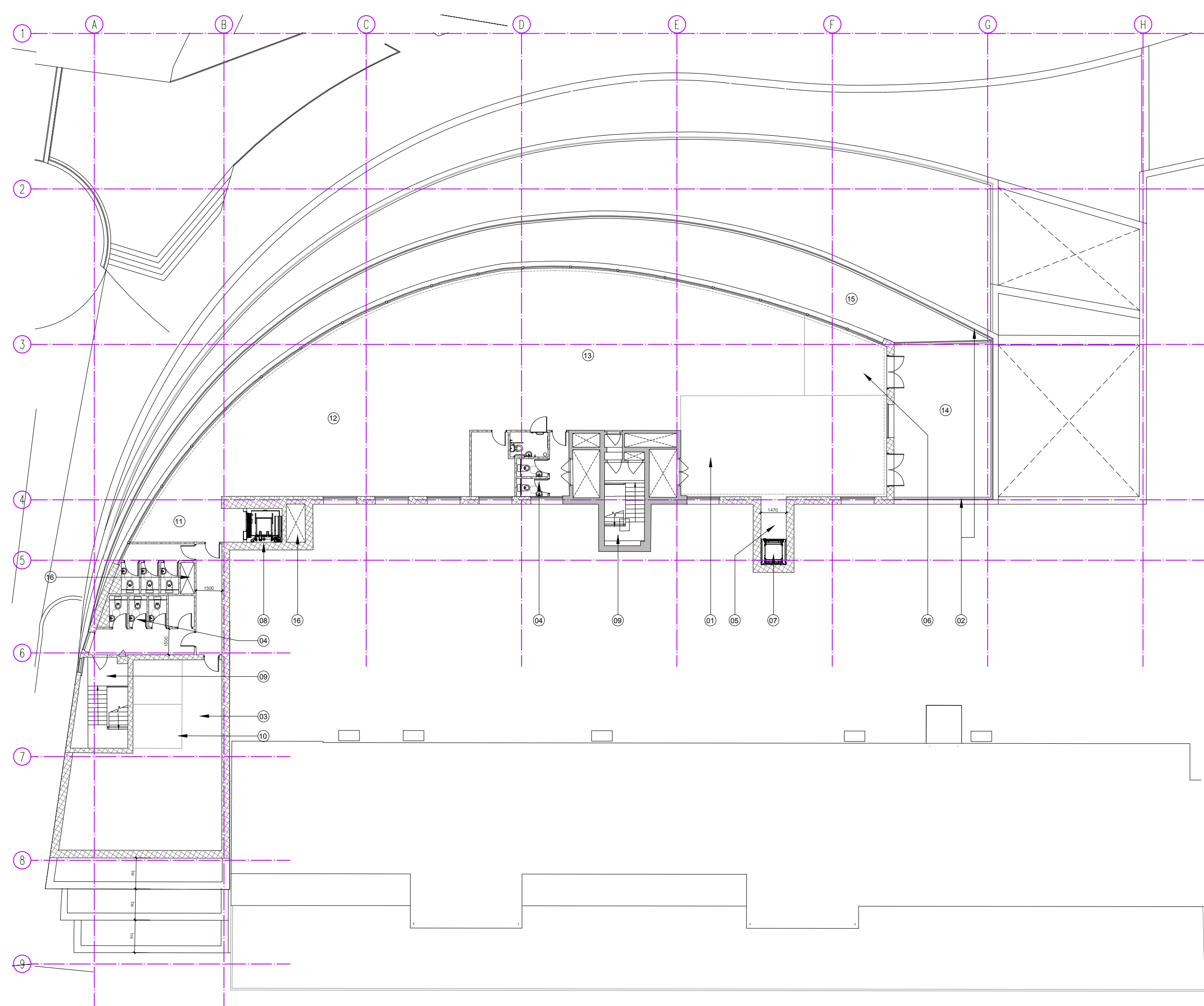
Project
Camden Wharf

Title
Proposed Fourth Floor Plan

Project No.	Drawing No.	Rev.
1524	20.205	B

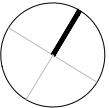
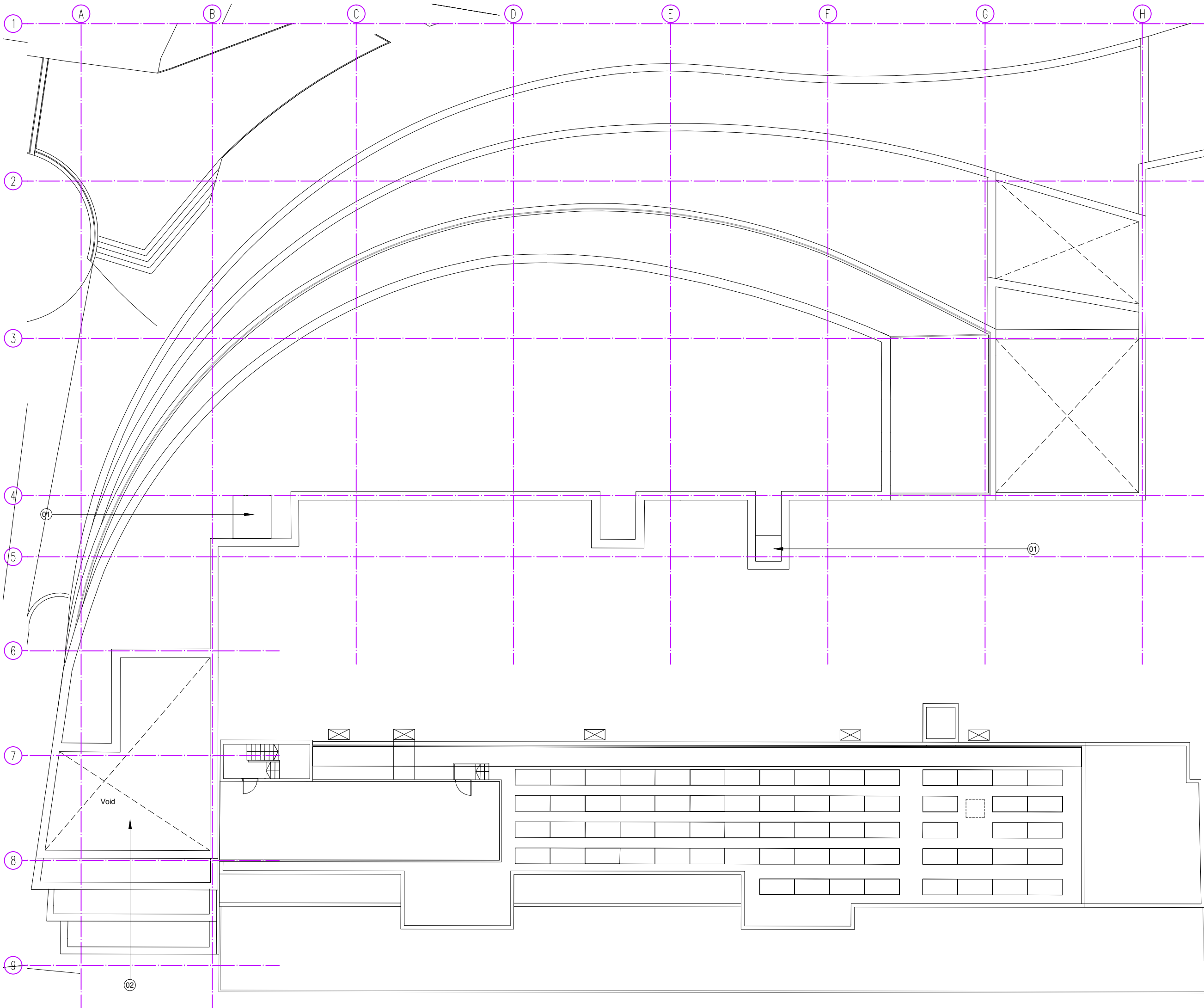
Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	04.04.16

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Notes:

- 1. Lift over-run
- 2. Void; plant below



For Information

Rev.	Description	By	Date
B	For Information	GV	25.04.16
A	For Information	HS	13.04.16

Client
Castlehaven Row Ltd

Project
Camden Wharf

Title
Proposed Roof Floor Plan

Project No.	Drawing No.	Rev.
1524	20.206	B

Scale	1:100 @ A1	1:200 @ A3
Drawn	Checked	Date
HS	GV	04.04.16

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Camden Wharf Planning History Schedule



GERALDEVE

Date Received	Reference No	Type of Application	Description	Decision	Date of Decision
10-09-2013	2013/4950/A	ADV	Display of 2 x externally illuminated fascia signs, replacement of 1 x externally illuminated mounted sign, 2 x non illuminated amenity boards, 1 x externally illuminated halo sign and 2 x externally illuminated projecting signs to the ground floor front and side elevations of the existing pub (Class A4).	Grant	07-11-2013
18-07-2005	2005/2795/P	PP	Erection of a standby power generator to the rear flat roof at first floor level of office (class B1).	Grant	26-08-2005
18-11-2002	PEX0200991	CND	Submission of details of roof plant; lighting; hard and soft landscaping; access and facilities for disabled people; and bicycle storage pursuant to conditions 02, 05, 06, 09, 10 and 14 of the planning permission dated 15th March 2002 for a hotel on the site. As shown by: Environmental Noise Assessment Report October 2002 (Acoustics Plus Ltd); WD1-002(B) rev 10; WD1-002(A) rev 10; WD1-513(B) rev 2; WD1-031(A) rev 1; lighting specs labelled Ex4, Ex 5; bicycle stand specs labelled Condition 14	Grant	21-03-2003
26-04-2002	PEX0200348	PP	The removal of condition 17 of the planning permission dated 15/3/99 (PE9700702R3) which requires all plant approved in connection with the A3 uses to be operated only during the restaurant hours. As shown by: HDL1 & noise impact assessment	Grant	30-09-2002
26-04-2002	PEX0200347	PP	The variation of condition 13 of the planning permission	Grant	30-09-2002

Camden Wharf Planning History Schedule



GERALDEVE

			dated 15/3/99 (PE9700702R3) to read "Any music played within the premises shall be maintained at such a level so as to ensure that it does not cause a statutory noise nuisance to local residences or business premises". As shown by: HDL1 & noise assessment		
26-04-2002	PEX0200346	PP	The variation of condition 9 of permission granted on 19th March 1999 (ref.no. 9700702R3) to extend the hours of use between the following times - 0800 hrs to 2400 hrs (excluding drinking and clearing up).	Withdrawn	20-09-2002
21-01-2002	PEX0100706	PP	Proposed elevations of the approved ground-floor restaurant/bar (Class A3) unit, and guard rail around the external seating area. As shown on drawing numbers: Ground floor plan, AS01-02, AD11, AD12 (in respect of the external rail only), AV02 Rev D, ML, AL01 Rev B, 3610/M/01B	Grant	18-03-2002
03-07-2000	PE9900702	PP	Variation of the planning permission dated 15.3.1999 (Registered No PE9700702R3) for the redevelopment of the eastern part of the site by the erection of a four-storey building for mixed A1, A3 and B1 and the retention of the lock-keepers cottage as a Canal Information Centre with ancillary retail/cafe comprising alterations to the elevations, fenestration, shopfront, balustrading, terraces, roof plant and the omission of the approved canopy over the pedestrian area as well as the discharging of part of condition 1 of the said planning permission relating to approval of details of balustrades, shopfronts, the canopy attached to the commercial building and any structure related to it. As shown on	Grant	03-08-2001

Camden Wharf Planning History Schedule



GERALDEVE

			drawing numbers: 100C, 101G, 102H, 103G, 104G, 105G, 106F, 107G, 109G, 110H, 111H, 112H, 205G, 300F, 301F, 302F, 500A, 501A, 504B, 505B, 560A, 561A		
04-11-1999	PE9900877	PP	Variations to the planning permission dated 15th March 1999 Registration No. PE9700713R3 for the redevelopment of the western part of the site by the erection of a five storey hotel involving increasing the height and area of the roof level plant enclosure and alterations to the north and south elevations, as shown on drawing numbers: 1097:101C, 102D, 103D, 104D, 105D, 106D, 107D, 108D, 109D, 110E, 111E, 112E, 113C; L100/14, L101/P14, L102/P11, L103/P9, L104/P9, L105/P12, L106/P4 and L107/P3.	Refuse	20-12-1999
15-10-1999	PE9900827	PP	Variation of condition 09 of the planning permission dated 15.3.1999 (Registered No PE9700702R3) for the redevelopment of the eastern part of the site by the erection of a four- storey building for mixed A1, A3 and B1 uses to permit opening times of 8.00 to 00.45 hours on Mondays to Saturdays and 8.00 to 00.15 hours on Sundays, as shown on drawing numbers: Site plan (extract from 100B)	Grant	13-01-2000
03-11-1998	PE9700713R3	PP	Redevelopment of the western part of the site by the erection of a five-storey hotel, as shown on drawing numbers: 100B, 101C, 102D, 103D, 104D, 105D, 106D, 107D, 108D, 109D, 110E, 111E, 112E and 113C.	Grant	15-03-1999

Camden Wharf Planning History Schedule



GERALDEVE

<p>03-11-1998</p>	<p>PE9700702R3</p>	<p>PP</p>	<p>Redevelopment of the eastern part of the site by the erection of a four-storey building to accommodate class A3 (food and drink) on the ground floor, class A1 (retail) on part of the ground and first floors, class B1 (business purposes) on the upper floors. Use of the former lock-keeper's cottage as a canal information centre, as shown on drawing numbers: 100B, 101C, 102D, 103D, 104D, 105D, 106D, 107D, 108D, 109D, 110E, 111E, 112E, 113C,</p>	<p>Grant</p>	<p>15-03-1999</p>
<p>01-10-1998</p>	<p>PE9700713R2</p>	<p>PP</p>	<p>4 APPLICATIONS HAVE BEEN SUBMITTED - 2 PLANNING APPLICATIONS, 1 LISTED BUILDING APPLICATION AND 1 CONSERVATION AREA APPLICATION: PE 9700702R2 - Redevelopment of the eastern part of the site including: Demolition of all buildings except 289 Camden High Street (lock keeper's cottage); erection of a four storey building to accommodate office floorspace over ground floor restaurant accommodation on the canal frontage and ground and first floor retail space on the Camden High Street frontage, and the refurbishment and alteration of 289 Camden High street for use as a canal information centre. PE9700713R2 - Redevelopment of western part of site, including demolition of all existing buildings and the erection of a building to accommodate a 132 bedroom hotel. LE9700703 - 289 Camden High Street. - Demolition of adjoining brick boundary walls, external stair and garden walls. Introduction of new window and/or door openings on north east, south east and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor</p>	<p>Withdrawn</p>	<p>03-10-1998</p>

Camden Wharf Planning History Schedule



GERALDEVE

			to form roof terrace, new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. CE9700704 - Demolition of all buildings within a Conservation Area. (ADDITIONAL PLANS SUBMITTED).		
01-10-1998	PE9700702R2	PP	<p>4 APPLICATIONS HAVE BEEN SUBMITTED - 2 PLANNING APPLICATIONS, 1 LISTED BUILDING APPLICATION AND 1 CONSERVATION AREA APPLICATION: PE 9700702R2 - Redevelopment of the eastern part of the site including: Demolition of all buildings except 289 Camden High Street (lock keeper's cottage); erection of a four storey building to accommodate office floorspace over ground floor restaurant accommodation on the canal frontage and ground and first floor retail space on the Camden High Street frontage, and the refurbishment and alteration of 289 Camden High street for use as a canal information centre. PE9700713R2 - Redevelopment of western part of site, including demolition of all existing buildings and the erection of a building to accommodate a 132 bedroom hotel. LE9700703 - 289 Camden High Street. - Demolition of adjoining brick boundary walls, external stair and garden walls. Introduction of new window and/or door openings on north east, south east and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor to form roof terrace, new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. CE9700704 - Demolition of all buildings within a Conservation Area. (ADDITIONAL PLANS</p>	Withdrawn	03-11-1998

Camden Wharf Planning History Schedule



GERALDEVE

			SUBMITTED).		
29-07-1998	LE9700703R1	LBC	Internal and external works of alteration, partial demolition, in connection with use as an information centre. (Revised plans submitted).	Withdrawn	27-01-1998
09-07-1998	PE9700713R1	PP	4 APPLICATIONS HAVE BEEN SUBMITTED - 2 PLANNING APPLICATIONS, 1 LISTED BUILDING APPLICATION AND 1 CONSERVATION AREA APPLICATION: PE 9700702R1 - Redevelopment of the eastern part of the site including: Demolition of all buildings except 289 Camden High Street (lock keeper's cottage); erection of a four storey building to accommodate office floorspace over ground floor restaurant accommodation on the canal frontage and ground and first floor retail space on the Camden High Street frontage, and the refurbishment and alteration of 289 Camden High street for use as a canal information centre. PE9700713R1 - Redevelopment of western part of site, including demolition of all existing buildings and the erection of a building to accommodate a 132 bedroom hotel. LE9700703 - 289 Camden High Street. - Demolition of adjoining brick boundary walls, external stair and garden walls. Introduction of new window and/or door openings on north east, south east and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor to form roof terrace, new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. CE9700704 - Demolition of all buildings within a	Withdrawn	

Camden Wharf Planning History Schedule



GERALDEVE

			Conservation Area. (PLANS SUBMITTED)		
09-07-1998	PE9700702R1	PP	<p>4 APPLICATIONS HAVE BEEN SUBMITTED - 2 PLANNING APPLICATIONS, 1 LISTED BUILDING APPLICATION AND 1 CONSERVATION AREA APPLICATION: PE 9700702R1 - Redevelopment of the eastern part of the site including: Demolition of all buildings except 289 Camden High Street (lock keeper's cottage); erection of a four storey building to accommodate office floorspace over ground floor restaurant accommodation on the canal frontage and ground and first floor retail space on the Camden High Street frontage, and the refurbishment and alteration of 289 Camden High street for use as a canal information centre. PE9700713R1 - Redevelopment of western part of site, including demolition of all existing buildings and the erection of a building to accommodate a 132 bedroom hotel. LE9700703 - 289 Camden High Street. - Demolition of adjoining brick boundary walls, external stair and garden walls. Introduction of new window and/or door openings on north east, south east and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor to form roof terrace, new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. CE9700704 - Demolition of all buildings within a Conservation Area. (PLANS SUBMITTED)</p>	Withdrawn	01-10-1998
29-09-1997	LE9700703	LBC	Demolition of adjoining brick boundary walls and external stair and garden walls. Use as a cafe; introduction of new window and/or door openings on	Withdrawn	28-07-1998

			north east, south east, and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor to form roof terrace, new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. (Plans submitted)		
16-09-1997	PE9700702	PP	<p>4 APPLICATIONS HAVE BEEN SUBMITTED - 2 PLANNING APPLICATIONS, 1 LISTED BUILDING APPLICATION AND 1 CONSERVATION AREA APPLICATION: PE 9700702 - Redevelopment of the eastern part of the site including: Demolition of all buildings except 289 Camden High Street; erection of a building to accommodate office floorspace and catering school over ground floor and basement restaurant accommodation on the canalfrontage and restaurant accommodation over ground floor retail floorspace on the Camden High Street frontage and refurbishment and alteration of 289 Camden High street (The Lock-keepers cottage) for use as a cafe: Relocation of the recycling facilities and tourist information centre; with ancillary loading facilities. PE9700713 - Redevelopment of western part of site, including demolition of all existing buildings and erection of a building to accommodate a 155 bedroom hotel. LE9700703 - 289 Camden High Street. - Demolition of adjoining brick boundary walls, external stair and garden walls. Use as a cafe; introduction of new window and/or door openings on north east, south east and south west elevations, internal partitions at ground floor, new circular stair, steel and glass balustrade at first floor to form roof terrace,</p>	Withdrawn	08-07-1998

Camden Wharf Planning History Schedule



GERALDEVE

			new rooflight, use of chimney stack for ventilation ducting, new paved terrace at ground floor front. CE9700704 - Demolition of all buildings within a Conservation Area. (PLANS SUBMITTED)		
10-09-1997	PE9700713	PP	Redevelopment of western part of site including demolition of all existing buildings and erection of a building to accommodate a 155 bedroom hotel. (Plans submitted).	Withdrawn	08-07-1998
05-09-1997	CE9700704	CAC	Demolition of the existing buildings in connection with the redevelopment of the site, as shown on drawing number: 099	Grant	15-03-1999
15-06-1990	9003293	OP	Redevelopment of the sites: SUFFOLK WHARF - Office retail & canal related space. MAINTENANCE DEPOT NORTH - Office. MAINTENANCE DEPOT SOUTH - Housing & Community space. STONEYARD -OPTION 1: Workshop/training & Community space STONEYARD - OPTION 2: Recycling Workshop/training community space. *(Plans submitted)	Withdrawn	08-05-2003
26-10-1964	TP19920/27634	PP	The erection of a single-storey building for use as a refuse vehicle garage at Suffolk Wharf, Jamestown Road, St. Pancras	Grant	17-12-1964
06-08-1941	TP19920/2543	PP	Special and Temporary Buildings and Structures. Suffolk Wharf, Camden High Street, St. Pancras.	Grant	08-10-1941