

Construction Management Plan  
9 Maresfield Gardens, London NW3 5SJ

Prepared by: Edward Trollope MEng  
Reviewed by: Emma Bailey BE MSc CEng MICE

Date: July 2016

Reference: 25297

**Revisions**

Rev	Date	By	Notes
1	July 2016	ET	Draft issue for Planning

Appendices

Appendix A: Construction Management Plan

Appendix B: Route to Site Drawings

Appendix C: Tracking Drawings

## Introduction

Price and Myers have been commissioned to compile a Construction Management Plan (CMP) for 9 Maresfield Gardens in support of a planning application for the proposed extension and alteration of this property.

## Aims and purposes

The primary aim of this draft CMP is to minimise the impact of construction on the surrounding community, specifically the residents of Maresfield Gardens, both for construction on site and the transport arrangements for servicing.

It is envisaged that this draft CMP will be conditioned as part of any forthcoming planning consent. The CMP will remain a live document for the construction phase and will be reviewed and updated by the construction team when required throughout the duration of the construction works.

This document will form the basis of agreeing the construction logistics arrangements with Camden Council, as appropriate. The logistics will be dependent on the suppliers, working methodology and programme being co-ordinated by the principal contractor.

This draft CMP is for planning purposes only. Some of the detail provided is provisional and will be subject to revision when the contractor has been appointed for the works. The document will be revised and updated to include relevant information as and when details are known and to address any issues that may have been identified through consultation with the Council and local residents as the project progresses.

## Structure and Contents

This document will act to give the reader an overview of the principles on which the CMP is based.

Appendix A presents the actual CMP, in Camden Council's template, and gives a greater level of detail than what is described here.

Appendix B shows the proposed route of construction/delivery traffic to site, with Appendix C presenting tracking drawings for construction vehicles.

Supporting drawings can be found in Appendices B and C.

## Proposed works

The proposed works at 9 Maresfield Gardens comprise of internal alterations and rear extensions with terraces/balconies. The development includes the lateral extension of an existing semi-basement level and the formation of front and side light-wells.

The proposed works are limited to the basement and ground floor. No alterations are proposed to the front elevation (above basement level) or to the roof.

## Site

The development is located on a wide avenue. The detached property is located on a large plot with large front and rear gardens. The site has a forecourt parking area with two vehicular accesses and a central pedestrian access. This arrangement is particularly well suited to a construction project of this scale enabling many construction operations to be contained within the site and minimising the impact on the street and the surrounding area.

## Impacts

The site is located on a residential street with facilities such as schools and a church. Minimising the impact on the local residents and facility users is of paramount importance. The CMP comprises a series of management measures to limit or mitigate the impact of construction works upon the amenity of local residents and road users.

Alongside standard construction management methods, the CMP addresses a series of potential construction impacts relevant to this particular development project and this particular site and context:

- Managing all types of construction vehicle movements to and from the site
- Vehicle entry and exit routes to the site.
- Avoiding queuing of construction vehicles in the vicinity.
- Ensuring the free flow of vehicles along Maresfield gardens.
- Limiting impacts on residents' on-street parking spaces and minimising the number of parking suspensions required.
- Limiting impacts on the operation of local schools and church
- How construction vehicles will travel to and from the site.

It is proposed that construction/delivery vehicles will access the road in front of site via Maresfield Gardens. A 'wait and load' methodology will be employed, meaning skips and grab lorries will be brought to site, filled and then immediately removed.

## Conclusion

As described above, construction works at 9 Maresfield Gardens will be carried out such that the impact on the local residents and the wider transport network will be minimised.

Appendix A: Construction Management Plan

# Construction Management Plan

pro forma v2.1

# Contents

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# Revisions & additional material

Please list all iterations here:

Date	Version	Produced by
06/16	DRAFT	Edward Trollope (for Price and Myers)

## Additional sheets

Please note – the review process will be quicker if these are submitted as Word documents or searchable PDFs.

Date	Version	Produced by



# Introduction

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any **cumulative impacts of other nearby construction sites**, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) [6: Amenity](#) and [CPG](#) [8: Planning Obligations](#)).

This CMP follows the best practice guidelines as described in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Cyclist Safety \(CLOCS\)](#) scheme) and [Camden's Minimum Requirements for Building Construction \(CMRBC\)](#).

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The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as for road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)."

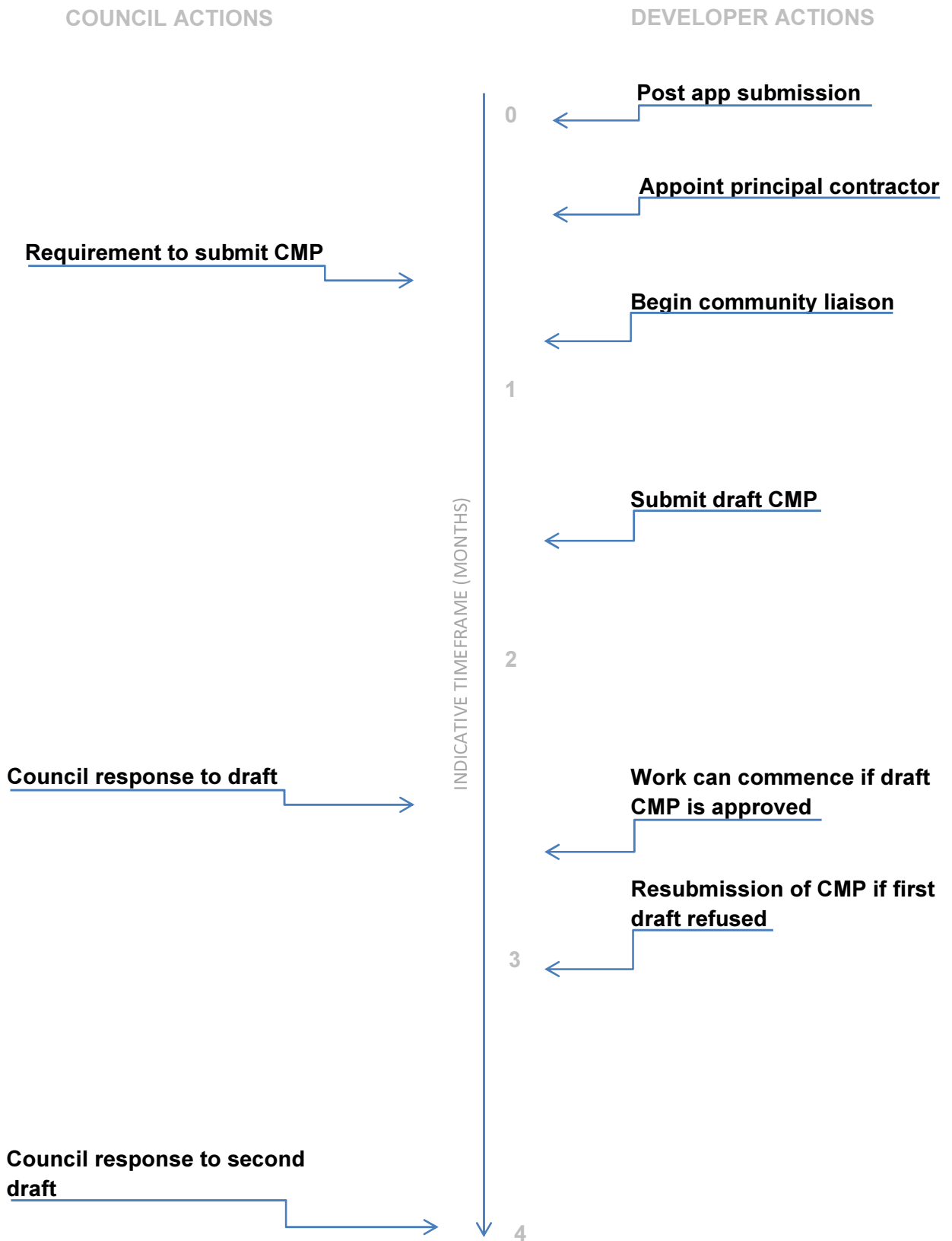
Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. **It is preferable if this document, and all additional documents, are completed electronically and submitted as Word files to allow comments to be easily documented. These should be clearly referenced/linked to from the CMP.**

Please notify that council when you intend to start work on site. Please also notify the council when works are approximately **3 months from completion**.

(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction, etc.)

Revisions to this document may take place periodically.

# Timeframe



# Contact

1. Please provide the full postal address of the site and the planning reference relating to the construction works.

Address: 9 Maresfield Gardens, Camden, NW3 5SJ

Planning ref: Not yet submitted

Type of CMP: For Section 106 planning application

2. Please provide contact details for the person responsible for submitting the CMP.

Name: Achikam Surkis

Address: AS Studio Ltd, 2 Magdalen Mews, London NW3 5HB

Email: chiki@as-studio.co.uk

Phone: 02077941625

3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

The responsibility of the site will be under the site manager, however, until the contractor is appointed please see details in question 2.

4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3. In the case of [Community Investment Programme \(CIP\)](#), please provide contact details of the Camden officer responsible.

Until the contractor is appointed please see details in question 2.

5. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Until the contractor is appointed please see details in question 2.

# Site

6. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

The site is located near the south west side of Maresfield Gardens and comprises a fully detached three storey brick property which includes five residential units. The property has a large rear garden with the site sloping away downwards from the front elevation towards the road. The property is within the Fitzjohn Netherhall conservation area and is considered a positive contributor to the area, but is not a listed building. The site has three points of access through the front garden area and a large rear garden which could be used for construction compound/storage.

7. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings etc).

The renovation and internal alteration of the existing house, with a rear extension and a terrace/balcony. The development includes front and side light-wells, and a lateral extension of an existing semi-basement level. The development is located on a wide two-way street. There is unrestricted access/egress to/from Nutley Terrace and Fitzjohns Avenue, but there is a one-way exit to the south east. The area is residential with two schools located to the south of the site. These items will be addressed in this CMP.

8. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting etc.).

The nearest potential receptors are Holy Trinity CE Primary School, South Hampstead High School, St Thomas More Church and the residential dwellings adjacent to the site. The procedures detailed in this CMP will seek to mitigate the impact (including dust, noise, vibration, lighting etc.) on these local buildings. The receptors will be made fully aware of construction activities. Figure 1 below shows highlights the nearby receptors.



**Figure 1: Local Receptors to Development**

The site's construction team will deal with any queries and provide immediate response to any issues raised. The site will be screened where appropriate; all hoarding will be maintained to a high standard throughout the progress of the project.

9. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.

Please refer to drawing 25297-700 in Appendix C for a plan showing the local highway network layout and off site characteristics. Maresfield Gardens is considered a cycle route and all delivery and collection vehicles must follow TFL's Safer Lorry Scheme recommendations and drivers should undertake cycle safety awareness courses.

10. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal).

Site setup, soft strip and enabling works	1 weeks
Demolition & Reclamation	1 week
Basement excavation	12 weeks
Underpinning, piling and basement construction	10 weeks
Superstructure amendments	4 weeks
Envelope	4 weeks
Internal finishes	3 weeks
Landscaping (overlapping with internals)	4 weeks
Commissioning final fit out	3 weeks
<b>Total Construction Period</b>	<b>42 weeks (TBC)</b>

11. Please confirm the standard working hours for the site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays



The standard working hours for the site will be 8:00am to 6:00pm on Monday to Friday, 8:00am to 1:00pm on Saturdays and there will be no working on Sundays or Public Holidays. Due to the proximity of Holy Trinity CE Primary School and South Hampstead High School (both of which are on the proposed departure route) to the development, the proposed departure route, deliveries and collection times will be restricted to of 9:30am to 3:00pm during term time and 9:30am to 4:30pm outside term time (for Monday to Friday). No abnormal loads or exceptions to these times are expected at this time.

12. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

A strategy for coordinating the connecting services will be developed and implemented.

# Community Liaison

**A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft. This consultation must relate to construction impacts, and should take place following the grant of planning permission in the lead up to the submission of the CMP. A consultation process specifically relating to construction impacts must take place regardless of any prior consultations relating to planning matters. This consultation must include all of those individuals that stand to be affected by the proposed construction works. These individuals should be provided with a copy of the draft CMP, or a link to an online document. They should be given adequate time with which to respond to the draft CMP, and any subsequent amended drafts. Contact details which include a phone number and email address of the site manager should also be provided.**

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the wellbeing of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. **The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off.** This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

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## Cumulative impact

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements should consider establishing contact with other sites in the vicinity in order to manage traffic routing and volumes. Developers in the Tottenham Court Road area have done this to great effect.

**The Council can advise on this if necessary.**

### 13. Consultation

The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents **prior to submission of the first draft CMP**.

Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. Details of meetings including minutes, lists of attendees etc. must be included.

In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason should be given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.

Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.

Contact with neighbouring landowners, residents, businesses, local representatives and the emergency services will be made at the earliest opportunity and maintained throughout the project, informing them of the construction process. At the time of writing the neighbours had not been contacted regarding the proposed development, however, the property owner intends to discuss the proposals with the neighbours during the planning process.

The site's construction team will deal with any queries and provide immediate response to any issues raised.

### 14. Construction Working Group

Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works, the way in which the contact details of the person responsible for community liaison will be advertised to the local community, and how the community will be updated on the upcoming works i.e. in the form of a newsletter/letter drop, or weekly drop in sessions for residents.

The contractor will ensure that a staffed telephone enquiry line is maintained at all times when site works are in progress to deal with enquiries from the local community. The telephone number (and any changes to it) will be publicised widely in the local community. A newsletter to nearby neighbours will be prepared and sent out prior to construction and during crucial stages along the course of the works.

Should complaints be received concerning works/activities, then any works/activities pertaining to the complaint will cease until further agreement/working methodology to work is negotiated.

It is proposed that a Construction Working Group will be formed by the Construction Project Manager to ensure that residents are aware of how the construction works are progressing and provide them with the opportunity to raise any issues that may arise as they occur.

Representatives of the local Residents Association will be invited to join the Construction Working Group as well as any other parties affected by the works and Council Officers. This will include residents of neighbouring properties, and properties that border the construction vehicle route.

The contractor will liaise with the schools and St. Thomas More Church to co-ordinate vehicle movements with events held by these receptors.

## 15. Schemes

Please provide details of any schemes such as the 'Considerate Constructors Scheme', such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the "[Guide for Contractors Working in Camden](#)" also referred to as "[Camden's Considerate Contractors Manual](#)".

The contractor will adhere to the 'Guide for Contractors Working in Camden' and the project will be registered with the 'Considerate Constructors Scheme'. All delivery and collection vehicles must follow TFL's Safer Lorry Scheme recommendations and drivers should undertake cycle safety awareness courses. In addition, delivery vehicles and sub-contractors who can demonstrate that they meet the bronze level Fleet Operator Recognition Scheme (FORS) membership standards will be employed.

## 16. Neighbouring sites

Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.

No.2 Maresfield Gardens appears to have ongoing construction works. However, this information is taken from the Camden planning records website (with no completion date stated). The programme will need to be confirmed by the Contractor. The CMP will be reviewed weekly and the Project Manager will be responsible for liaising and co-ordinating traffic routing with any other developments in the area as necessary. The Contractors/Project Managers from nearby construction sites will have weekly scheduling meetings, where they will coordinate any vehicle movement to/from the sites to avoid any unnecessary disruption. During the meetings, Contractors will be expected to discuss the feasibility of sharing deliveries from the same supplier to further reduce the impact on the local highway network and local residents.

# Transport

**This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.**

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the [CLOCS Standard](#).

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by the council to ensure compliance. Please refer to the CLOCS Standard when completing this section. Guidance material which details CLOCS requirements can be accessed [here](#), details of the monitoring process are available [here](#).

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.

**Please refer to the CLOCS Overview and Monitoring Overview documents referenced above which give a breakdown of requirements.**

## CLOCS Considerations

### 17. Name of Principal contractor:

Until the contractor is appointed please see details in question 2.

### 18. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our CLOCS Overview document in the appendix and CLOCS Standard point 3.4.7).

The contractor will provide the method for adhering to the CLOCS scheme. The CLOCS standard point 3.4.7 (Supply Chain Compliance) will be fully adhered to at all times.

Qualified LANTRA (or similar) banksman will be in attendance for all vehicle movements to aid the driver in manoeuvring safely and aid any residents/ road users in passing safely if required. Priority will always be given to members of the public where possible.

The operators shall only use vehicle routes agreed with the London Borough of Camden and be accredited to bronze level FORS or equivalent. If required, additional safety equipment will be fitted to vehicles. All delivery and collection vehicles must follow TFL's Safer Lorry Scheme recommendations and drivers should undertake cycle safety awareness courses. The contractor will perform driver licence checks, record, investigate and analyse any collisions and ensure that they have written to their supply chain informing them of the need to comply with the above requirements.

### 19. Please confirm that you as the client/developer and your principal contractor have read and understood the [CLOCS Standard](#) and included it in your contracts. Please sign-up to join the [CLOCS Community](#) to receive up to date information on the standard by expressing an interest online.

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers:

The contractor will confirm once appointed.

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.



## Site Traffic

Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.

**20. Traffic routing:** *“Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.”* (P19, 3.4.5)

Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (i.e. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.

Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.

a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of links to the [Transport for London Road Network](#) (TLRN).

Appendix B contains figures which show the public highway network in the vicinity of the site including link to the road network.

Considering the existing site constraints, the only feasible access point for all construction vehicles is via the front of the property on Maresfield Gardens. Considering the location of the site and the wider road network, it is assumed most construction vehicles will be approaching the site from the north where the A41 connects to the North Circular Road and the M1, rather than the south (central London). The primary access and egress route to the site has therefore been set up as shown in Appendix B.

Mourne House and No 2 Maresfield Gardens existing egress routes will be clear from any construction vehicles at all times. Vehicles coming from the north will have clear access as currently available, and vehicles from the south will also have clear traffic flow due to suspended parking bays adjacent to St. Thomas More Church (this is shown on drawing series 25297-700 in Appendix C). The section of Maresfield Gardens where the property is located has an exit only at the southern end, therefore, vehicles will enter from the north via Fitzjohns Avenue/ Nutley Terrace.

b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.

The CMP will be issued to all sub-contractors. Contractors will be issued with a project route map to distribute to their delivery drivers and suppliers. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable, construction vehicles shall not proceed to the site.

Qualified LANTRA (or similar) banksman will be in attendance for all vehicle movements to aid the driver in manoeuvring safely and aid any residents/ road users in passing safely if required. Priority will always be given to members of the public where possible. A banksman will be present on the corner of Maresfield Gardens by the schools to alert pedestrians and cyclists to the presence of any construction vehicles. The driver will remain in the vehicle at all times so it can move immediately if necessary. It is intended to programme the works so that concrete deliveries will be carried out during the morning period and waste away during the afternoon, that way if there are any delays at the concrete plant they will not go beyond the daily delivery cut-off time of 3:00pm.

Pedestrian and cyclist safety will be a priority of the contractor. The contractor will provide a method for adhering to the CLOCS scheme. All delivery and collection vehicles must follow TFL's Safer Lorry Scheme recommendations and drivers should undertake cycle safety awareness courses. In addition, delivery vehicles and sub-contractors who have registered with the FORS bronze level scheme will be employed.

All contractors and sub-contractors will be made aware of the location of Holy Trinity CE Primary School and South Hampstead High School relative to the development. The proposed egress route, deliveries and collection times will be restricted to of 9:30am to 3:00pm during term time and 9:30am to 4:30pm outside term time (for Monday to Friday).

**21. Control of site traffic, particularly at peak hours:** *“Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)*

Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the [Guide for Contractors Working in Camden](#)).

A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.

a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.

Although the preferred method of delivery would be to park construction vehicles on site, there is insufficient space to turn and exit the site safely. Therefore, the site is a pedestrian site only. Vehicles will not enter the construction site but remain on Maresfield Gardens for a short period of time to aid 'wait and load' deliveries and collections. The contractor will liaise with the schools and St. Thomas More Church to co-ordinate deliveries outside of opening and closing times of the school and occasions when high volumes of traffic are expected at the church.

Vehicles include:

- Muck away/grab lorry and skip lorry vehicle: 4 wheels, 20tonne, (L) 7.8m x (W) 2.6m
- Concrete delivery vehicle: 6 wheels, 26 tonne, (L) 7.8m x (W) 2.6m (Heaviest vehicle to attend site)
- Truck mounted concrete pump: 4 wheels, (L) 5.24m x (W) 1.90m
- Ballast and loose materials 4 wheel, 17 tonne, (L) 7.8m x (W) 2.6m
- General Building materials: 4 wheel HIAB flat-bed or 7.5 tonne rigid vehicle, (L) 7m x (W) 2.25m
- Sundry materials: 4 wheel, 3 tonne (van/flat-bed), (L) 4m x (W) 2m
- Heavy good vehicle: 6 wheels, 20tonne, (L) 10m x (W) 2.5m (Largest vehicle to attend site)
- Scaffolding delivery vehicle: 8 wheels, 26tonne, (L) 9m x (W) 2.5m

The estimated dwell time for spoil removal lorries is 20-30 minutes, material lorries is 10-20 minutes and 20-30 minutes for concrete lorries, during this period qualified (LANTRA or similar) banksmen would be present at all times. Only one delivery would occur at a time. No queuing will be permitted.

During the various phases of work we would expect:

- Site strip out and demolition:
  - 4no. skip lorry (“wait and load”) changes per week,
  - 1no. flat-bed lorry per day,
  - 1no. scaffolding delivery vehicle per day for 3 days,
- Excavation
  - Maximum 2no. muck away/grab lorry per day,
- Structure:
  - 2no. skip lorry (“wait and load”) changes per week,
  - 3no. ready mix concrete deliveries per week during concrete pouring with 1no. truck mounted concrete pump,
  - 1no. general building materials delivery per day,
  - 1no. heavy goods vehicles per fortnight (maximum)
- Fit out and Landscaping:
  - 2no skip lorry (“wait and load”) changes/week,
  - 1no flat-bed lorry per day.
- Various ballast/loose material and sundry materials vehicles would deliver throughout the works on small vehicles. It is expected that these deliveries will occur once a week.

It is intended to programme the works so that concrete deliveries will be carried out during the morning period and waste away during the afternoon, that way if there are any delays at the concrete plant they will not go beyond the daily delivery cut-off time of 3:00pm. A weekly schedule for all deliveries will be produced.

To minimise the likelihood of congestion during construction periods, strict monitoring and control of construction vehicles entering and egressing Maresfield Gardens will be implemented. Maximum vehicle sizes and maximum dwell times for each construction vehicle type will be revised as necessary by the contractor. All construction deliveries will be pre-booked and pre-arranged as part of the efficient operation and construction work.

The use of a booking system and having delivery times agreed with contractors means that vehicles are not forced to wait prior to site delivery. No waiting of delivery vehicles will be permitted on Maresfield Gardens or the surrounding highways. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable, construction vehicles shall not proceed to the site.

b. Please provide details of other developments in the local area or on the route.

As detailed in question 16, No.2 Maresfield Gardens appears to have ongoing construction works. Please refer to question 16 for further details. There are not currently any other construction sites within the immediate area of the proposed development site. The CMP will be reviewed weekly and the Project Manager will be responsible for liaising and coordinating traffic routing with any other developments in the area as necessary. The contractors/project managers from construction sites will have weekly scheduling meetings, where they will coordinate any vehicle movement to and from the sites to avoid any unnecessary disruption. During the meetings, Contractors will be expected to discuss the feasibility of sharing deliveries from the same supplier to further reduce the impact on the local highway network and local residents.

The temporary pedestrian diversion utilises the existing dropped access outside No. 2 Maresfield Gardens. The Contractors from both sites will liaise to ensure this is clear when the temporary pedestrian diversion is being used.

c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.

The site is a pedestrian site only. Vehicles will not enter the site itself but remain on Maresfield Gardens for a short period of time to aid 'wait and load' deliveries and collections. All deliveries will be pre-arranged and the contractor will be aware of all deliveries from sub-contractors through weekly coordination meetings. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable construction vehicles shall not proceed to the site.

d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for any vehicle/driver compliance checks. Please refer to question 24 if any parking bay suspensions will be required for the holding area.

It is not anticipated there will be any off-site holding areas required for the development, with all work contained within the site hoardings.

e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of construction material consolidation centres).

Accurate design information, material specifications and drawings will be produced at the detailed design stage, specifying all the design information, building components, building layouts and elevations. This will enable the supply chain to precisely provide materials needed on site. The correct use of the information during ordering process will reduce the risks of wastage and reworking. The contractor will try where possible to use local suppliers which can supply multiple materials to site, to reduce the amount of construction vehicles attending site.

Sub-contractors will be encouraged to deliver materials on a timely basis, this limits the amount of materials stored on site. To achieve timely deliveries (e.g. 'just in time') efficiently, accurate progress reporting of the project's programme will occur on a regular basis. The material delivery schedule will be in line with the materials required on-site.

**22. Site access and egress:** *"Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles."* (P18, 3.4.3)

Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and other traffic when vehicles are entering and leaving site, particularly if reversing.

a. Please detail the proposed access and egress routes to and from the site

The site is a pedestrian site only and vehicles will not enter the site itself but remain on Maresfield Gardens for a short period of time to aid 'wait and load' deliveries and collections. Please refer to drawing series 25297-700 in Appendix C for drawings showing vehicle access to the site for deliveries and collections. Reserving of construction vehicles will not be permitted. Banksman will be in position to direct the construction vehicles to the temporary pull in area and will ensure safe pedestrian passage is maintained at all times.

b. Please describe how the access and egress arrangements for construction vehicles will be managed.

Please see drawing series 25297-700 in Appendix C for drawings showing the temporary parking suspension for each activity. The length of road directly adjacent to 9 Maresfield Gardens has a single yellow line. The road directly outside the property will be used for short period of time during collections and deliveries throughout the works. The current clear width of road between parking bays is 3m, and when construction vehicles are temporarily parked outside the property the clear width will be less than 3m. Therefore, it is proposed the parking bay (12m long) on the opposite side of the road will be temporarily suspended for a short period of time during collections and deliveries throughout the works, in order to provide a minimum of 3m clear roadway (as existing). This suspension is the minimum necessary to carry out the development.

Qualified (LANTRA or similar) banksmen will be in position during the transfer of materials across the footway to ensure that safe pedestrian passage is maintained at all times and priority will be given to members of the public crossing the footway. A banksmen will be present on the corner of Maresfield Gardens by the schools to alert pedestrians and cyclists to the presence of any construction vehicles.

The existing clear footway width will be maintained at all times. The pavement will be temporarily suspended during this grab removal process in the interest of public safety. Details of this can be found in answer to question 27a.

c. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).

Please refer to drawing series 25297-700 in Appendix C for drawings showing the swept paths for construction vehicles attending site and site boundary arrangements.

d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled.

The site is a pedestrian site only and vehicles will not enter the site. The pavement and public highway will be cleaned after every spoil removal. Tarpaulin will be laid on the pavement when the grab lorry removes spoil and when concrete is being pumped onto site to reduce the spillage/leaking on the pavement and speed up the cleaning and re-opening of the footway. Please refer to Question 23 and 27a for details of temporary suspended footway. No development dirt shall be evident on the highway at the end of any working day



**23. Vehicle loading and unloading:** *“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.” (P19, 3.4.4)*

If this is not possible, Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and motor traffic in the street when vehicles are being loaded or unloaded.

Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that loading/unloading is carried out safely. Please outline in question 24 if any parking bay suspensions will be required.

Please refer to drawing series 25297-700 in Appendix C for drawings showing site characteristics. The site is a pedestrian site only. Vehicles will not enter the site itself but remain on Maresfield Gardens for a short period of time to aid ‘wait and load’ deliveries and collections. Please refer to drawing series 25297-700 in Appendix C for drawings showing the loading and unloading arrangements for construction vehicles attending site and site boundary arrangements.

During basement excavation, muck-away grab lorries will use the ‘wait and load’ methodology for spoil removal. The spoil will be moved to and piled in an area within the site boundary from which a grab lorry can remove the spoil (whilst avoiding the nearby trees). This will be covered to prevent dust and debris blowing around and off the site. The use of a grab lorry for spoil removal reduces the number of construction vehicles to site.

The pavement will be temporarily suspended during this grab removal process in the interest of public safety. Details of this can be found in answer to question 27a. The pavement and public highway will be cleaned after every spoil removal. Tarpaulin will be laid on the pavement when the grab lorry removes spoil, to reduce the spillage on the pavement and speed up the cleaning and re-opening of the footway.

For other waste removal, a ‘wait and load skip’ methodology will be employed, meaning skips will be temporarily brought to site, subsequently loaded and immediately taken away from site. It is proposed to store all waste materials in bags within the site boundary before transfer to a temporary skip. Colour coded bins will be made available and signposted to reduce the risk of cross contamination, and covered to prevent dust and debris blowing around and off the site.

Please refer to question 24 for temporary parking bay suspensions. There will be no waiting of construction vehicles permitted near the site. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable, construction vehicles will be instructed not to proceed to the site. Surrounding properties will not suffer obstructed access during the works. Mourne House (11 Maresfield Gardens) has an egress route adjacent to the property, which will be kept clear at all times. Tree roots will be protected against mechanical impacts and damage upon tree trunks by the installation of hoarding around trees. All relevant information in regards to tree protection methodology can be found in the Arboricultural Report (ha/aiams1/9maresfieldgdns, 24<sup>th</sup> June 2016). A copy of this report will be retained on site for the duration of the build process. Overhanging tree branches in the vicinity of the site are not deemed low enough to be obstructed or damaged by construction vehicles. The existing tree (titled T2 in the Arboricultural Report) will be subject to general tree improvement pruning. Furthermore, grab lorries will be parked with the grab arm to the southern end (this is shown on drawing series 25297-703 in Appendix C) in front of the site to avoid damage to tree branches on site.

Qualified LANTRA (or similar) banksman will be in attendance for all vehicle movements to aid the driver in manoeuvring safely and aid any residents/ road users in passing safely if required. Priority will always be given to members of the public where possible. A banksman will be present on the corner of Maresfield Gardens by the schools to alert pedestrians and cyclists to the presence of any construction vehicles. The driver will remain in the vehicle at all times so it can move immediately if necessary. Appropriate signage and barriers conformant with the Traffic Signs Manual and NRSWA requirements will be implemented.

## Highway interventions

Please note that Temporary Traffic Orders (TTOs) and hoarding/scaffolding licenses may be applied for prior to CMP submission but won't be granted until the CMP is signed-off.

### 24. Parking bay suspensions and temporary traffic orders

Please note, parking bay suspensions should only be requested where absolutely necessary. Parking bay suspensions are permitted for a maximum of 6 months, requirement of exclusive access to a bay for longer than 6 months you will be required to obtain [Temporary Traffic Order \(TTO\)](#) for which there is a separate cost.

Please provide details of any proposed parking bay suspensions and TTO's which would be required to facilitate construction. **Building materials and equipment must not cause obstructions on the highway as per your Considerate Contractors obligations unless the requisite permissions are secured.**

Information regarding parking suspensions can be found [here](#).

Please see drawing series 25297-700 in Appendix C for drawings showing the temporary parking suspension. The length of road directly adjacent to 9 Maresfield Gardens has a single yellow line. The road directly outside the property will be used for short period of time during collections and deliveries throughout the works. The current clear width of road between parking bays is 3m, and when construction vehicles are temporarily parked outside the property the clear width will be less than 3m. Therefore, it is proposed the parking bay (12m long) on the opposite side of the road will be temporarily suspended for a short period of time (during collections and deliveries) throughout the works. This will aid manoeuvring of vehicles, and will provide a 4.2m clear roadway which will be kept clear at all times (this is shown on drawing series 25297-700 in Appendix C). This suspension is the minimum necessary to carry out the development.

There will be no waiting of construction vehicles permitted near the site. Suppliers shall call the site a minimum of 20 minutes before their vehicle arrives at site, to confirm that the loading area is available. If the loading area is unavailable construction vehicles will be instructed not to proceed to the site. Qualified LANTRA (or similar) banksman will be in attendance for all vehicle movements to aid the driver in manoeuvring safely and aid any residents/ road users in passing safely if required. Surrounding properties will not suffer obstructed access during the works.

The existing footway width and a clear roadway width of 3m for vehicular passage will be maintained at all times. Parking spaces will be made available for resident parking outside of the delivery times and temporary suspensions will only be during the proposed delivery times, leaving kerbside parking available every day before 9:30am and after 4:30pm Monday to Friday, after 1:00am on Saturdays and all day Sundays.

Please see Appendix C for drawings showing the temporary parking suspensions for each activity. These are summarised in the table on the next page.

Alternatives to private car transport will be considered by the construction team and efforts will be made to communicate the advantages and encourage all site personnel and visitors to use public transport. The site is located approximately 140m east of Finchley Road Tube Station. Multiple busses (13, 82, 113, 187, 268, C11, D, N13, N113) service Finchley Road, with the closest bus stop (FJ) approximately 90m south west of the site.

Please see the table below summarising the temporary parking suspensions for each activity.

Vehicle	Activity	Reason for activity	Length of temporary suspension	Duration of temporary suspension	Average frequency of temporary suspension
Skip lorry	Temporary loading of site waste (using 'wait and load')	To remove site waste	12m	20 to 30 minutes	1 per day
Grab lorry	Temporary loading of spoil (using 'wait and load')	To remove spoil from basement excavation	12m	20 to 30 minutes	2 per day during basement excavation
Concrete lorry	Concrete delivery	Concrete pouring for structure construction	12m	20 to 30 minutes	3 per week during concrete pouring
Heavy goods vehicle	Large/heavy materials delivery	Construction materials	12m	10 to 20 minutes	1 per fortnight

## 25. Scaled drawings of highway works

Please note that use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway that includes the extent of any hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions.

- a. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses).

The pavement directly outside 9 Maresfield Gardens will be temporarily suspended during the grab removal process in the interest of public safety. Please refer to drawing 25297-703 in Appendix C for details of the proposed temporary pedestrian diversion. Question 27 provides further details of the temporary pedestrian diversion. Apart from this temporary suspension, the existing footway width and a clear roadway width of 3m for vehicular passage will be maintained at all times.

b. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

Appropriate signage and barriers conformant with the Traffic Signs Manual and NRSWA requirements will be implemented. Signage will be positioned along Maresfield Gardens before and during any construction vehicle movements to warn vulnerable road users.

The contractor will use all necessary safety signage, barriers and accessibility measures to ensure the public are protected at all times. Please refer to drawing series 25297-700 in Appendix C for details of the site characteristics including signage and drawing 25297-703 for the proposed temporary pedestrian diversion. For details of site hoarding please refer to question 27.

For larger pours a truck mounted concrete pump will be used. Both the truck mounted concrete pump and the concrete waggon will be temporarily parked in the temporary suspended bays. The concrete hose will then track directly across the pavement into the site. The hose that runs across the pavement will be covered by an appropriate temporary ramp so safe pedestrian access can be maintained at all times. The hose will also be laid on a matt to catch any potential leakage. Protective hoarding will be erected around the tree trunks to protect the trees, please refer to Question 23 for further details regarding tree protection measures. Please refer to drawing 25297-702 in Appendix C for details of the temporary ramp.

## **26. Diversions**

Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).

It is not envisaged that highway diversions will be required for this development. The contractor will confirm once appointed.

## **27. VRU and pedestrian diversions, scaffolding and hoarding**

Pedestrians and/or cyclist safety must be maintained if diversions are put in place. Vulnerable footway users should also be considered. These include wheelchair users, the

elderly, those with walking difficulties, young children, those with prams, the blind and partially sighted. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/skips/hoardings etc.

A secure hoarding will generally be required at the site boundary with a lockable access.

a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.

Pedestrian and cyclist safety will be a priority of the contractor. Special consideration will be given to vulnerable road users, and safe pedestrian access will be maintained at all times. Pedestrians, the general public and any local residents will be kept separate from the construction activities at all times.

The adjoining public highway will be regularly cleaned down (at least once daily and after concrete mixing), kept free from obstructions and inspected for hazards to ensure road users and pedestrians are protected at all times.

Signage will be positioned along Maresfield Gardens before and during any construction vehicle movements to warn vulnerable road users. The site hoarding used to fully surround the site will be fully and closely sheeted. It will also have clear signage describing the works and have good lighting. The nature of the lighting will be agreed through consultation with local residents. Hoarding will be regularly cleaned and maintained, including redecoration if necessary. The hoarding will also protect the passers-by and reduce the dust and noise emissions from site. 24 hour emergency contact details will be displayed on the site hoarding.

The contractor's operatives are to maintain courteous relations and must be helpful to neighbours and passers-by at all times. The hoarding will be locked to prevent anyone unauthorised from entering the site.

Qualified (LANTRA or similar) banksmen will be in position during the transfer of materials across the footway to ensure that safe pedestrian passage is maintained at all times and priority will be given to members of the public crossing the footway.

The existing clear footway width will be maintained at all times. During the movement of materials across the footway, priority will be given to the pedestrians at all times, activities will temporarily be suspended to allow them to pass safely.

During construction works (such as grab lorry muck away), the footway will need to be temporarily stopped up, in the interest of public safety. Temporary barriers and signs to direct pedestrian traffic will be installed on both sides of the footway in drop kerb areas to aid vulnerable footway users in crossing the road. The temporary pedestrian diversion utilises the existing dropped access outside No. 2 Maresfield Gardens. The Contractors from both sites will liaise to ensure this is clear when the temporary pedestrian diversion is being used. The muck away would take approximately 20-30 minutes to remove the waste material. The pavement and public highway will be cleaned after every spoil removal. Tarpaulin would be laid on the pavement when the grab lorry removes spoil, to reduce the spillage on the pavement and speed up the cleaning and re-opening of the footway.

Due to the presence of Holy Trinity CE Primary School and South Hampstead High School on the proposed egress route, deliveries and collection times will be restricted to of 9:30am to 3:00pm during term time and 9:30am to 4:30pm outside term time (for Monday to Friday). This will mean there will be less pedestrian traffic during times when the grab lorry temporarily attends site, reducing the impact of the work on the area.

Please refer to drawing 25297-703 in Appendix C for further details.

b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.

All materials will be stored on site and the public highway will only be used for deliveries and spoil removal. It is intended that hoarding will be erected along the boundary of the property with Maresfield Gardens. Materials will be stored within the site hoarding at all times and the site hoarding will be maintained to a high quality for the duration of the project.

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# Environment

To answer these sections please refer to the relevant sections of **Camden's Minimum Requirements for Building Construction (CMRBC)**.

28. Please list all [noisy operations](#) and the construction method used, and provide details of the times that each of these are due to be carried out.

Best practice measures will be applied during construction works to minimise noise and vibration impacts arising from construction activities. An assessment of sources of likely noise from the major primary activities will be undertaken prior to the strata of construction including:

- Demolition
- Excavations
- Concreting Operations
- Plant and Equipment
- Drilling and Grinding
- Scaffolding
- Construction Traffic
- Loading/Unloading

29. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

Best practice measures will be applied during construction works to minimise noise and vibration impacts arising from construction activities. An assessment of sources of likely noise from the major primary activities will be undertaken prior to the start of construction which are stated in question 28. Surveys and on-going monitoring will be undertaken and according to best practice. Hoarding will reduce the impact of noise and acoustic barriers will be used where necessary.

30. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

The contractor will adhere to 'Camden's Minimum Requirements for Building/Construction/Demolition Sites'. The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented.

31. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

The contractor will adhere to 'Camden's Minimum Requirements for Building/Construction/Demolition Sites'. The contractor will adhere to key legislation on noise and vibration as detailed in the Control of Pollution Act 1974, Environmental Protection Act 1990 and BS 5228:1997, Code of Practice on Construction and Open Sites.

During the detailed design phase, full assessment of the potential impacts of the demolition and construction works on air quality, noise and vibration will be carried out using the *Mayor's Best Practice Guidance on Control of Dust and Emissions*. The measures that could be adopted to mitigate these nuisances are:

- Setting 'Action Levels' for noise and vibrations;
- Routine monitoring of noise, vibration and dust at the site boundary and sensitive receptors;
- Use of hoarding for as long as practicable to act as acoustic screening;
- Requirement for engines and equipment to be switched off on-site when not in use, use of quieter plant, regular plant maintenance and screening of plant if appropriate;
- Spraying areas with water to dampen down dust when conditions dictate;
- Effective wheel/body washing facilities to be provided and used as necessary;
- Use of road sweepers whenever the need for road cleaning arises;
- Covering of vehicles carrying waste materials off-site; and
- Strictly prohibiting fires on site.

The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented. For further details and mitigation measure relating to the impact of the basement construction works, please refer to the Basement Impact Assessment (8972\_FS\_GB).

The contractor will be aware of the Dust & Air Emissions Mitigation Measures as prepared by the Institute of Air Quality Management and will use their mitigation and control measure to ensure that dust is controlled on site both during demolition and construction.

Occupiers in the vicinity who may be affected by noise from these works will be notified of the nature of the works, a contact name, telephone number (including that to be used outside normal working hours), and address to which any enquiries should be directed. This notification will take place, where possible within 2 weeks but, in any event, at least a week prior to the works commencing.

If noise/vibration/dust complaints arise from the building construction/building works, an immediate review will be completed to remove the problem wherever possible and to establish what levels of noise and vibration have been emitted from the site. The interested parties will also be notified. The complaints will be recorded in a complaint's register which will be made available to the Local Authority, if requested. The complaint register will provide information on day, time, details of complaint, details of monitoring carried out and any additional mitigation works.

Trees and the hoarding that will surround the property will assist in acting as a noise barrier. Deliveries to the site will take place as described in question 23 and scheduled to distribute vehicle movements throughout these hours to avoid periods of intensive activity, therefore limiting noise and vehicle emissions. The noise levels associated with particular materials has, and will continue to be taken in consideration as part of the design specification process and in-line with the Noise Regulations 2006.

Schedule for rock breaking, rock hammering, sheet piling, pile driving and similar activity only between the following hours unless approved otherwise:

- 9am to 12pm, Monday to Friday
- 1pm to 5pm, Monday to Friday
- 9am to 1pm, Saturdays

32. Please provide evidence that staff have been trained on BS 5228:2009

The contractor will add information once appointed.

33. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented.

The contractor will be aware of the Dust & Air Emissions Mitigation Measures as prepared by the Institute of Air Quality Management and will use their mitigation and control measure to ensure that dust is controlled on site both during demolition and construction. The spoil from the basement excavation will be covered to prevent dust and debris blowing around and off the site (prior to be removed). It is proposed to store all waste materials in bags within the site boundary before transfer to a temporary skip. Colour coded bins will be made available and signposted to reduce the risk of cross contamination, and covered to prevent dust and debris blowing around and off the site.

The contractor will add further information once appointed.

34. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

The spoil from the basement excavation will be covered to prevent dust and debris blowing around and off the site (prior to be removed). It is proposed to store all waste materials in bags within the site boundary before transfer to a temporary skip. Colour coded bins will be made available and signposted to reduce the risk of cross contamination, and covered to prevent dust and debris blowing around and off the site. The hoarding will also protect the passers-by and reduce the dust and noise emissions from site.

35. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

Please refer to questions 31 and 33. The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented.

36. Please confirm that a [Risk Assessment](#) has been undertaken at planning application stage in line with the [GLA's Control of Dust and Emissions Supplementary Planning Guidance \(SPG\)](#), and the risk level that has been identified, with evidence. Please attach the risk assessment as an appendix if not completed at the planning application stage.

The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented.

37. Please confirm that all of the GLA's 'highly recommended' measures from the [SPG](#) document relative to the level of risk identified in question 36 have been addressed by completing the [GLA mitigation measures checklist](#).

The contractor will prepare a detailed risk assessment and method statement which will confirm the dust/noise and vibration controls, checks and monitoring which will be implemented.

38. If the site is a 'High Risk Site', 4 real time dust monitors will be required. If the site is a 'Medium Risk Site', 2 real time dust monitors will be required. The risk assessment must take account of proximity to sensitive receptors (e.g. schools, care homes etc), as detailed in the [SPG](#). Please confirm the location, number and specification of the monitors in line with the SPG and confirm that these will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

The contractor will confirm the risk assessment and dust monitors once appointed.

39. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).

The contractors will take the necessary measures to ensure proper control of rodents.

28 days prior to any building works are being carried out the contractor will submit a method statement on how the destruction/dispersion of rodents will be controlled during demolition works. The method statement shall demonstrate if/how the presence of rats and mice has been ascertained and how they will be destroyed if they have been/are found on site. This will also include how existing/new drainage will be sealed during the construction process.

At all times the site shall be kept free, so far as is reasonable practicable, from rats and mice. (Prevention of Damage by Pests Act 1949, part 'H' of the Building Regulations (Drainage & Waste Disposal)).

40. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey will be carried out prior to the commencement of works.

41. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of a suitable smoking area, tackling bad language and unnecessary shouting.

The contractor will adhere to the 'Guide for Contractors Working in Camden' and the project will be registered with the 'Considerate Constructors Scheme'. The contractor will confirm once appointed.

42. If you will be using non-road mobile machinery (NRMM) on site with net power between 37kW and 560kW it will be required to meet the standards set out below. The standards are applicable to both variable and constant speed engines and apply for both PM and NOx emissions.

**From 1<sup>st</sup> September 2015**

**(i) Major Development Sites** – NRMM used on the site of any major development will be required to meet Stage IIIA of EU Directive 97/68/EC

**(ii) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IIIB of EU Directive 97/68/EC

**From 1<sup>st</sup> September 2020**

**(iii) Any development site** - NRMM used on any site within Greater London will be required to meet Stage IIIB of EU Directive 97/68/EC

**(iv) Any development site within the Central Activity Zone** - NRMM used on any site within the Central Activity Zone will be required to meet Stage IV of EU Directive 97/68/EC

Please provide evidence demonstrating the above requirements will be met by answering the following questions:

- a) Construction time period (Estimated 42 weeks from commencement, dates to be confirmed):
- b) Is the development within the CAZ? (The site is outside the Central Activities Zone):
- c) Will the NRMM with net power between 37kW and 560kW meet the standards outlined above? (Y/N):
- d) Please provide evidence to demonstrate that all relevant machinery will be registered on the NRMM Register, including the site name under which it has been registered:
- e) Please confirm that an inventory of all NRMM will be kept on site and that all machinery will be regularly serviced and service logs kept on site for inspection:
- f) Please confirm that records will be kept on site which details proof of emission limits, including legible photographs of individual engine plates for all equipment, and that this documentation will be made available to local authority officers as required:

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# Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

**Please notify that council when you intend to start work on site. Please also notify the council when works are approximately 3 months from completion.**



**Signed:** .....

**Date:** .....

**Print Name:** .....

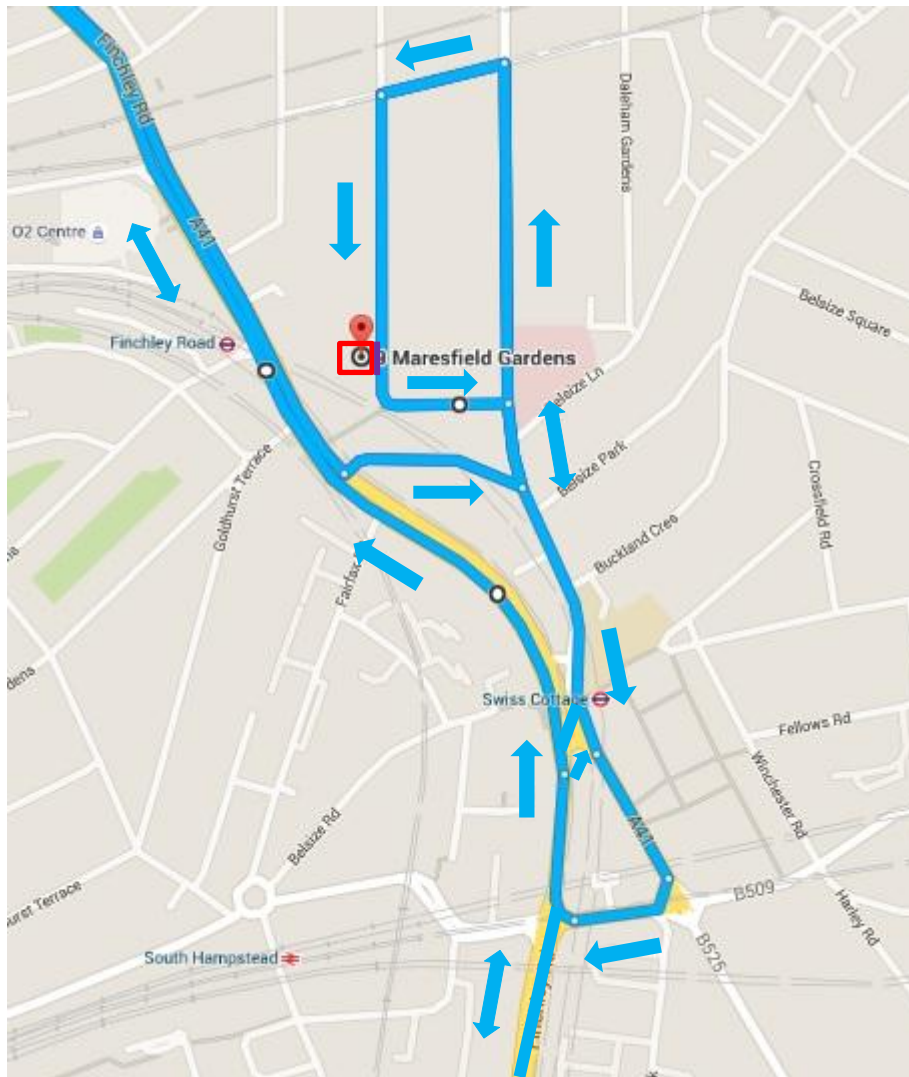
**Position:** .....

Please submit to: [planningobligations@camden.gov.uk](mailto:planningobligations@camden.gov.uk)

End of form.

Appendix B: Route to Site Drawings

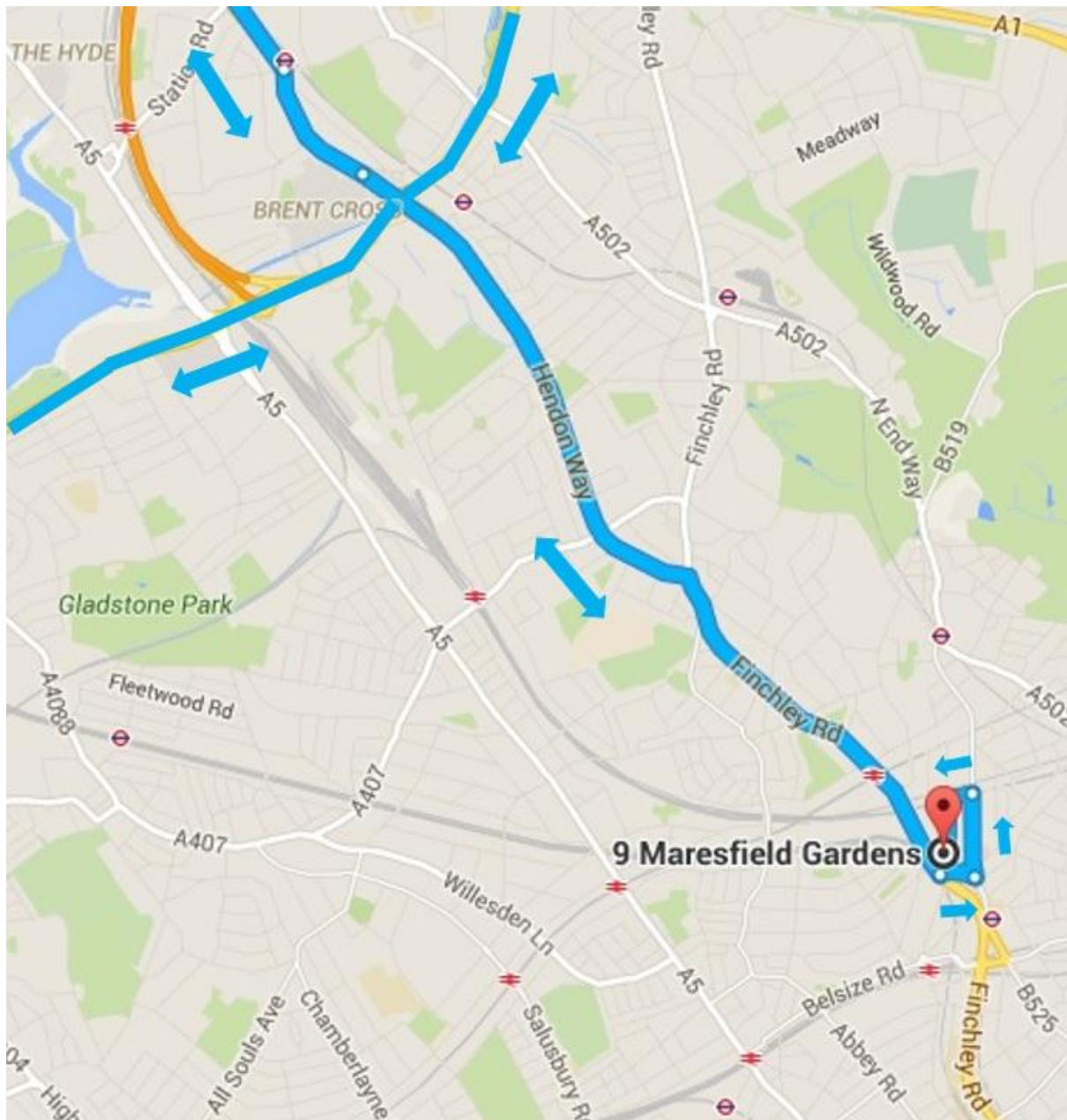
## 9 Maresfield Gardens





Route to and from the site (please refer to wider road network routes for further details)

- █ Site Outline
- █ Deliveries/construction traffic routes
- █ Proposed Unloading/Loading area

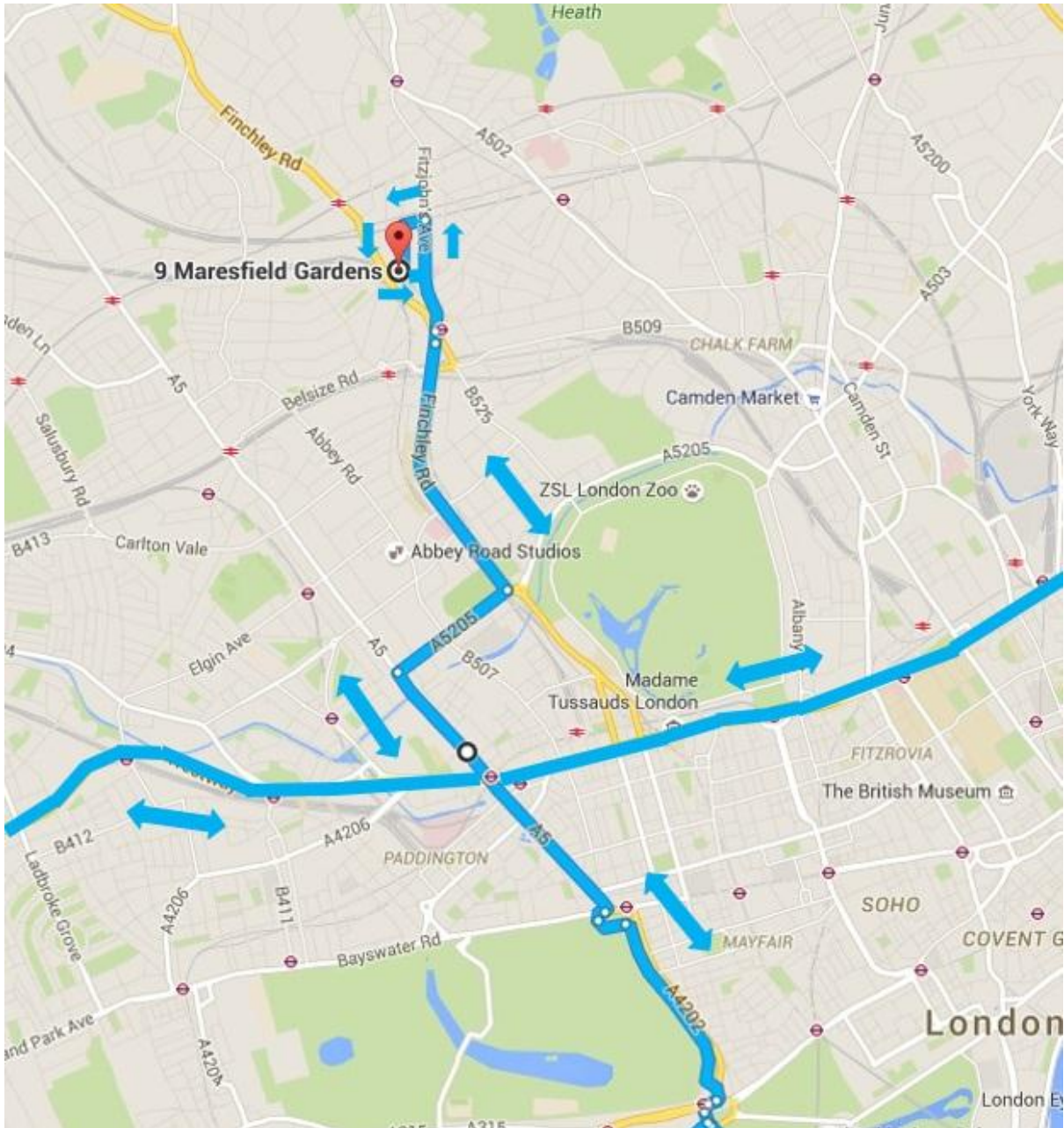
## 9 Maresfield Gardens





Main routes to and from the site using the wider road network to the North

-  Main traffic routes using wider road network in the area
-  Direction of traffic

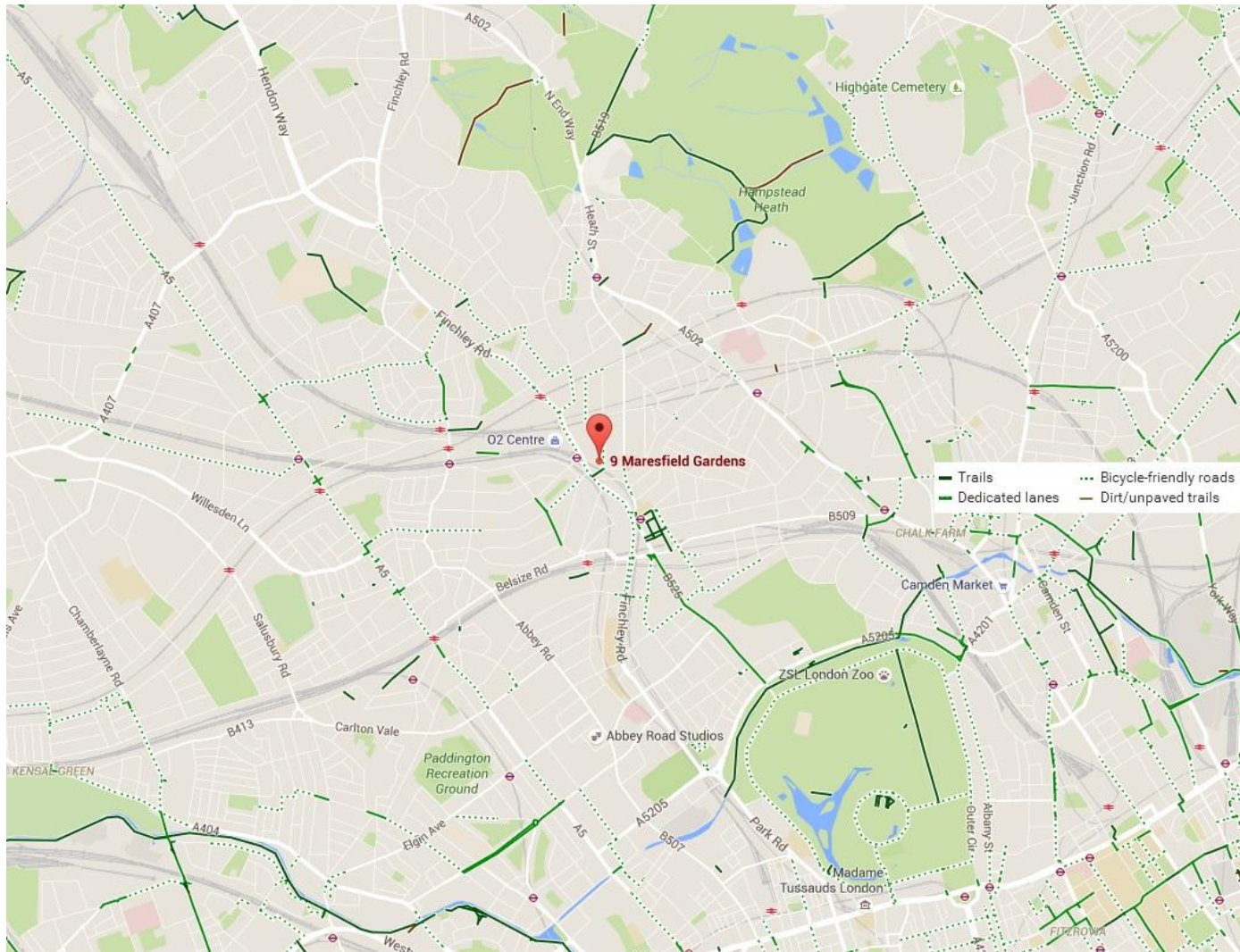
9 Maresfield Gardens



Main routes to and from the site using the wider road network to the South

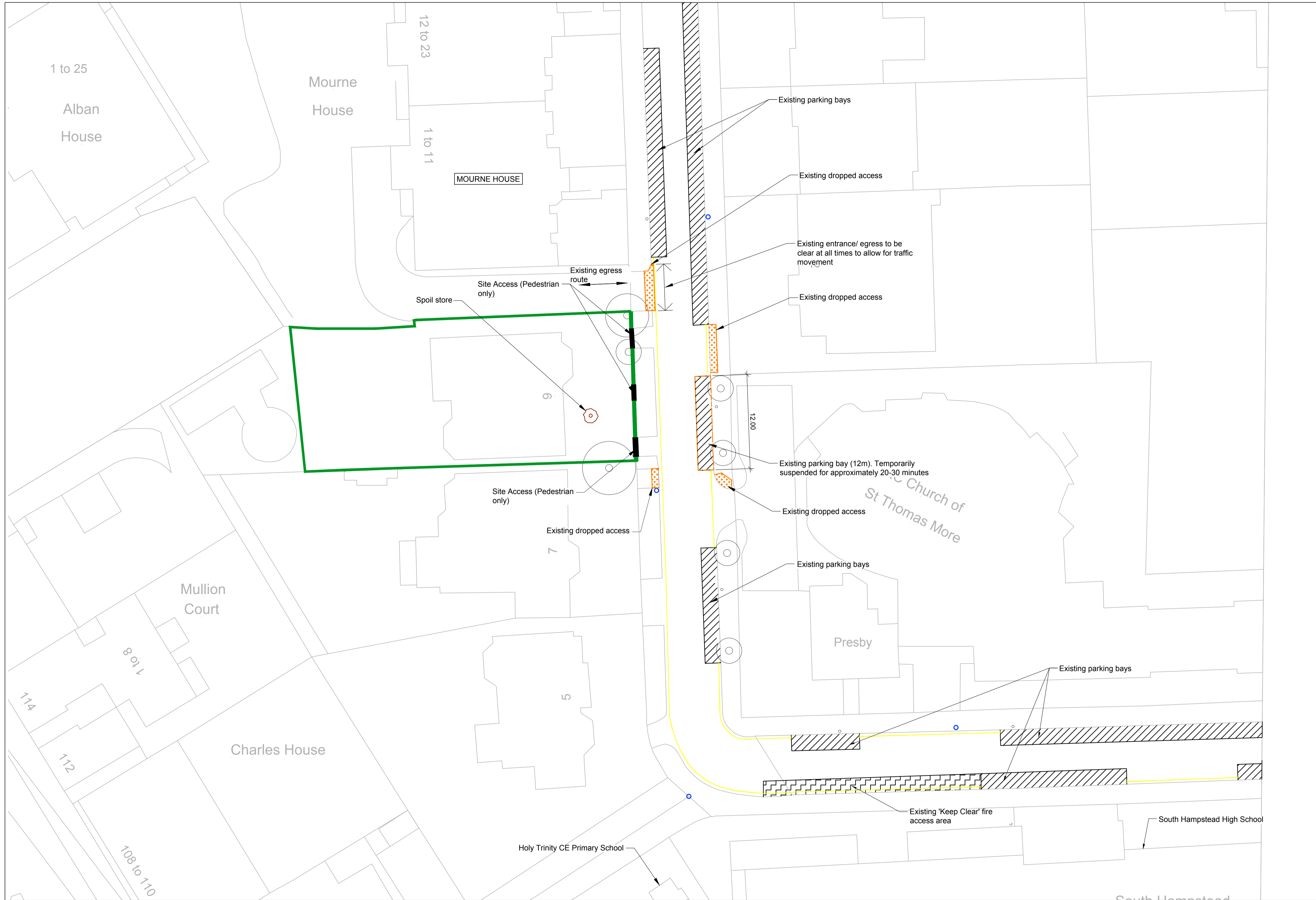
-  Main traffic routes using wider road network in the area
-  Direction of traffic

## 9 Maresfield Gardens



Public cycling routes near the site (shown by green lines)

Appendix C: Tracking Drawings



**SITE SET-UP PLAN AND OFF SITE CHARACTERISTICS**

**Notes :**

1. This drawing is to be read in conjunction with all relevant Architect's, Engineer's and specialists' drawings and specifications.
2. Do not scale from this drawing in either paper or digital form. Use written dimensions only. To check that this drawing has been printed to the intended scale this bar should be 50mm long @ A1 or 25mm long @ A3.
3. Health & Safety : All specific drawing notes are to be read in conjunction with the project "Information Pack" and "Site Rules".

LEGEND	
	Single Yellow Line
	Temporarily Suspended Parking Bays
	Banksman
	Parking Spaces
	Fire Access Area
	Site Hoarding
	Site Access (Pedestrian only)
	Existing Tree
	Street Lighting
	Road Sign
	Drop Kerb

Ver	Date	Drawn	Eng	Amendment
2	15.07.16	RPo	ET	Issued for Information
1	10.06.16	RPo	ET	Issued for Information

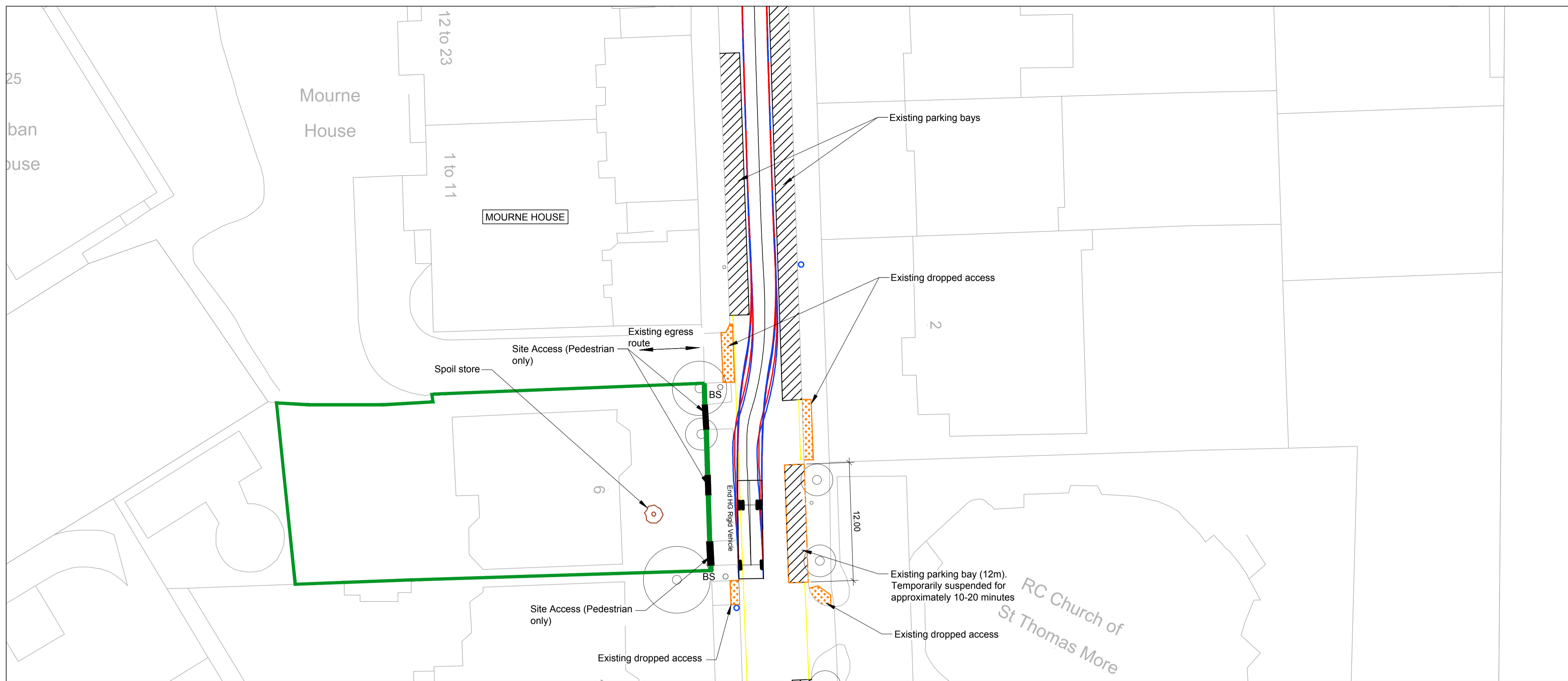
**9 MARESFIELD GARDENS  
LONDON NW3 5SJ**

**SITE SET-UP PLAN AND  
OFF SITE  
CHARACTERISTICS**

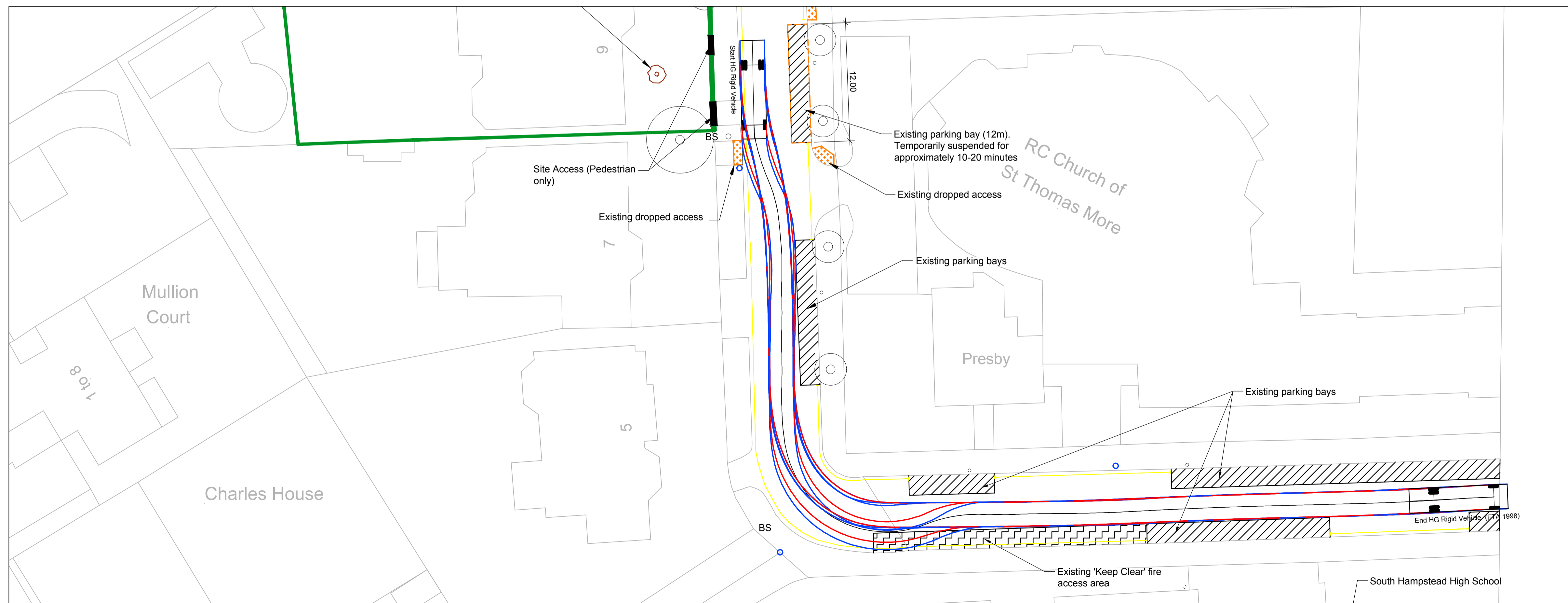
Status  
**ISSUED FOR INFORMATION**  
NOT FOR CONSTRUCTION

Drawn	RPo	Eng	ET
Scales	1:200 @ A1	1:400 @ A3	
Drawing No	Ver		
<b>25297-700</b>	<b>2</b>		





**HG VEHICLE (LARGEST VEHICLE) PULLING UP AT FRONT OF SITE**



**HG VEHICLE (LARGEST VEHICLE) PULLING AWAY FROM SITE**

**Notes :**

1. This drawing is to be read in conjunction with all relevant Architect's, Engineer's and specialists' drawings and specifications.
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3. Health & Safety : All specific drawing notes are to be read in conjunction with the project "Information Pack" and "Site Rules".

**LEGEND**

- Single Yellow Line
- Temporarily Suspended Parking Bays Banksman
- BS
- Parking Spaces
- Fire Access Area
- Site Hoarding
- Site Access (Pedestrian only)
- Existing Tree
- Street Lighting
- Road Sign
- Drop Kerb

**TRACKING LEGEND**

- Vehicle body path
- Vehicle Wheel path

**VEHICLE DIMENSIONS**

Vehicle name : HG Rigid Vehicle (FTA 1998)  
 Overall length : 10.00  
 Overall width : 2.50  
 Max. track width : 2.47  
 Kerb to kerb radius : 11.00

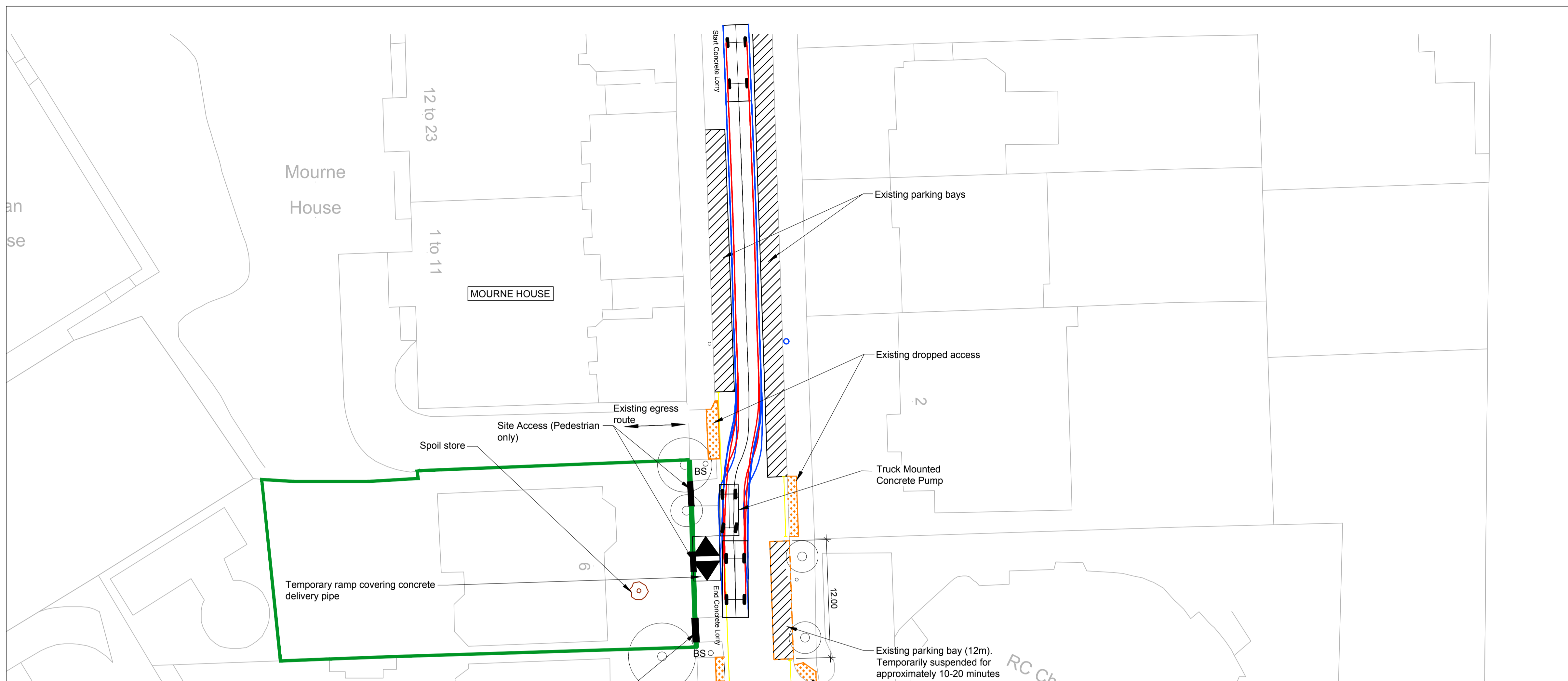
Ver	Date	Drawn	Eng	Amendment
2	15.07.16	RPo	ET	Issued for Information
1	10.06.16	RPo	ET	Issued for Information

**9 MARESFIELD GARDENS  
LONDON NW3 5SJ**

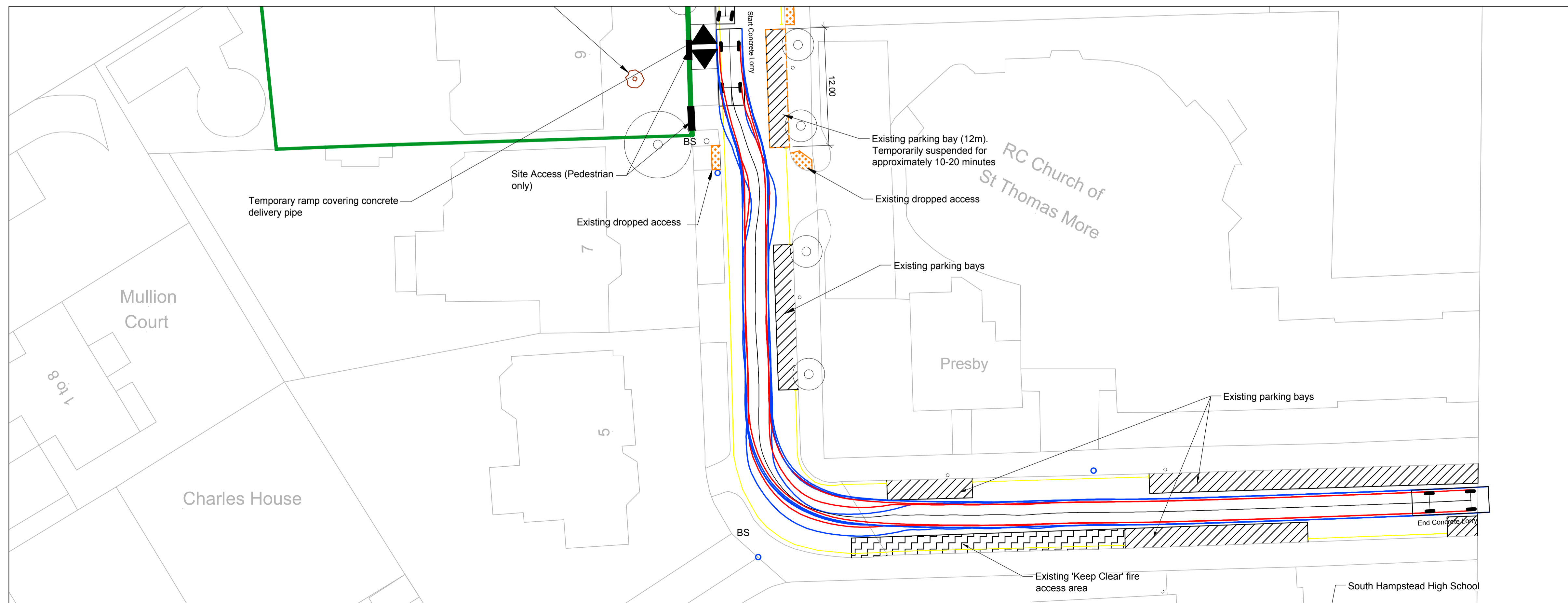
**HG RIGID VEHICLE  
TRACKING**

Status  
**ISSUED FOR INFORMATION**  
NOT FOR CONSTRUCTION

Drawn	RPo	Eng	ET
Scales	1:200 @ A1	1:400 @ A3	
Drawing No	Ver		
<b>25297-701</b>	<b>2</b>		



**CONCRETE LORRY PULLING UP AT FRONT OF SITE**



**CONCRETE LORRY PULLING AWAY FROM SITE**

**Notes :**

1. This drawing is to be read in conjunction with all relevant Architect's, Engineer's and specialists' drawings and specifications.
2. Do not scale from this drawing in either paper or digital form. Use written dimensions only. To check that this drawing has been printed to the intended scale this bar should be 50mm long @ A1 or 25mm long @ A3.
3. Health & Safety : All specific drawing notes are to be read in conjunction with the project "Information Pack" and "Site Rules".

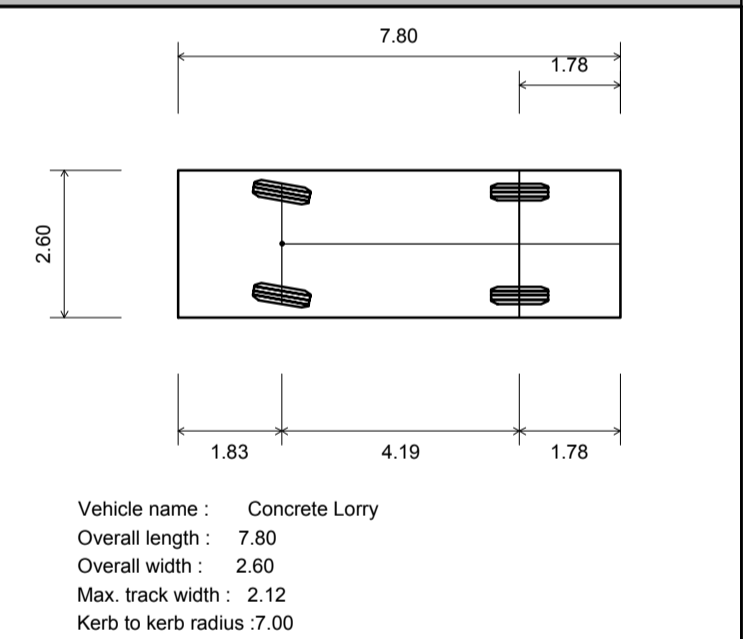
**LEGEND**

	Single Yellow Line
	Temporarily Suspended Parking Bays Banksman
	Parking Spaces
	Fire Access Area
	Site Hoarding
	Site Access (Pedestrian only)
	Existing Tree
	Street Lighting
	Road Sign
	Drop Kerb

**TRACKING LEGEND**

	Vehicle body path
	Vehicle Wheel path

**VEHICLE DIMENSIONS**



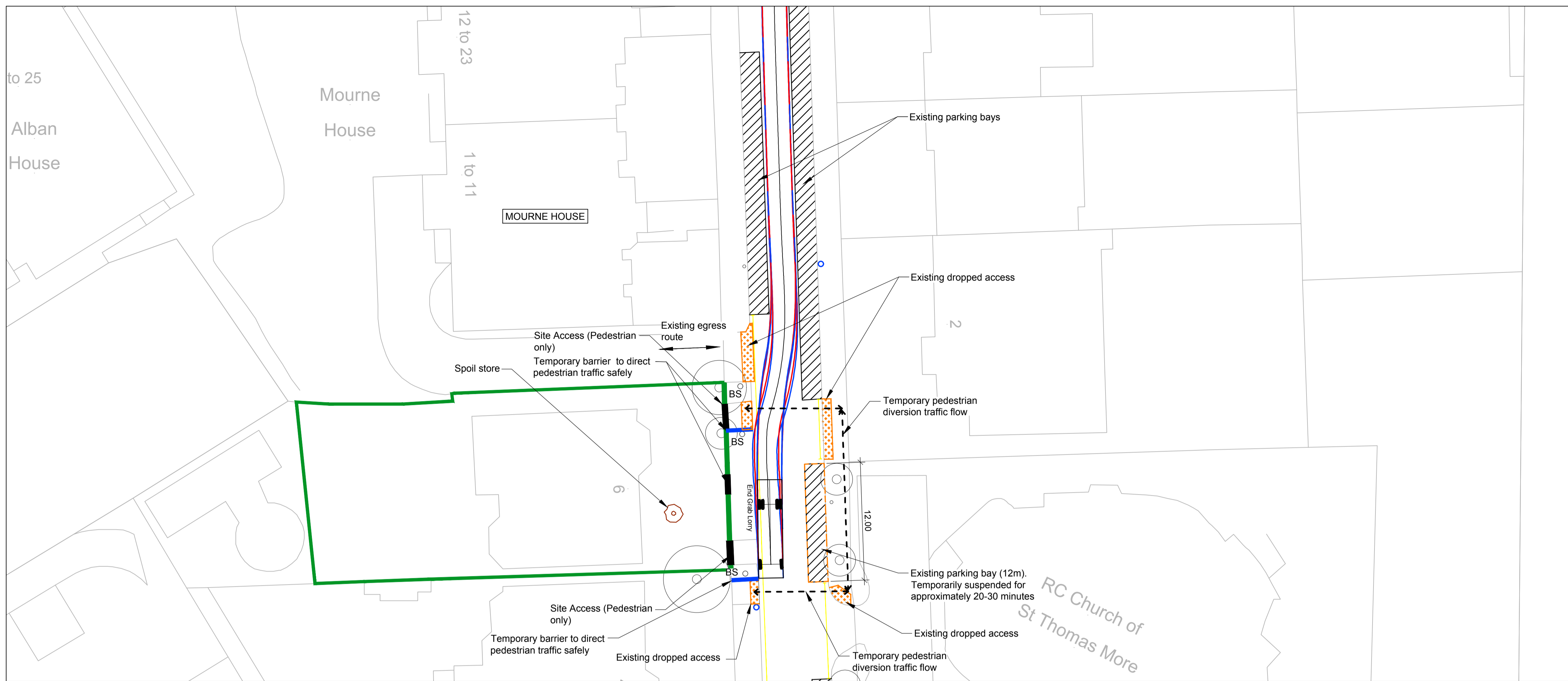
Ver	Date	Drawn	Eng	Amendment
2	15.07.16	RPo	ET	Issued for Information
1	10.06.16	RPo	ET	Issued for Information

**9 MARESFIELD GARDENS  
LONDON NW3 5SJ**

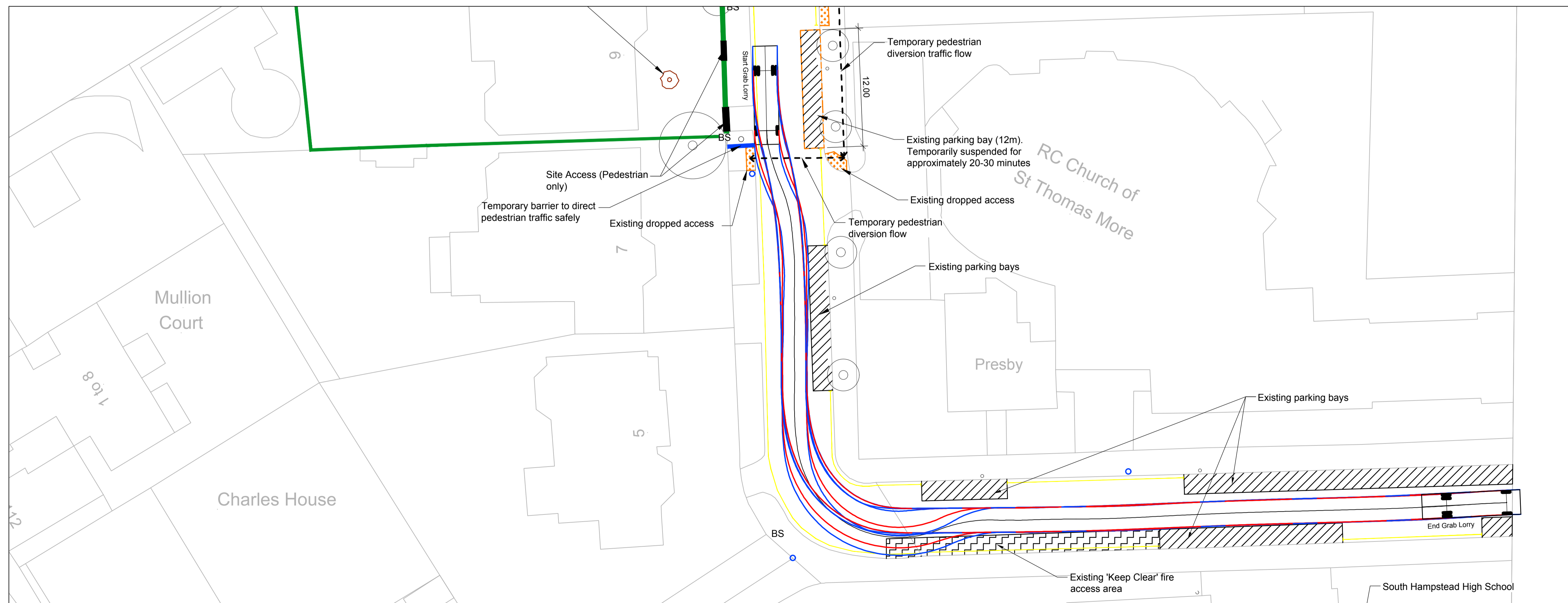
**CONCRETE LORRY  
TRACKING**

Status  
**ISSUED FOR INFORMATION**  
NOT FOR CONSTRUCTION

Drawn	RPo	Eng	ET
Scales	1:200 @ A1	1:400 @ A3	
Drawing No	25297-702		Ver
			2



**GRAB LORRY PULLING UP AT FRONT OF SITE**



**GRAB LORRY PULLING AWAY FROM SITE**

**Notes :**

1. This drawing is to be read in conjunction with all relevant Architect's, Engineer's and specialists' drawings and specifications.
2. Do not scale from this drawing in either paper or digital form. Use written dimensions only. To check that this drawing has been printed to the intended scale this bar should be 50mm long @ A1 or 25mm long @ A3.
3. Health & Safety : All specific drawing notes are to be read in conjunction with the project "Information Pack" and "Site Rules".

LEGEND	
	Single Yellow Line
	Temporarily Suspended Parking Bays
	Banksman
	Parking Spaces
	Fire Access Area
	Site Hoarding
	Site Access (Pedestrian only)
	Existing Tree
	Street Lighting
	Road Sign
	Drop Kerb
	Temporary Barrier
	Temporary Pedestrian Diversion Traffic Flow
	Temporary Ramp

TRACKING LEGEND	
	Vehicle body path
	Vehicle Wheel path

VEHICLE DIMENSIONS	
Vehicle name :	Grab Lorry
Overall length :	10.00
Overall width :	2.50
Max. track width :	2.47
Kerb to kerb radius :	11.00

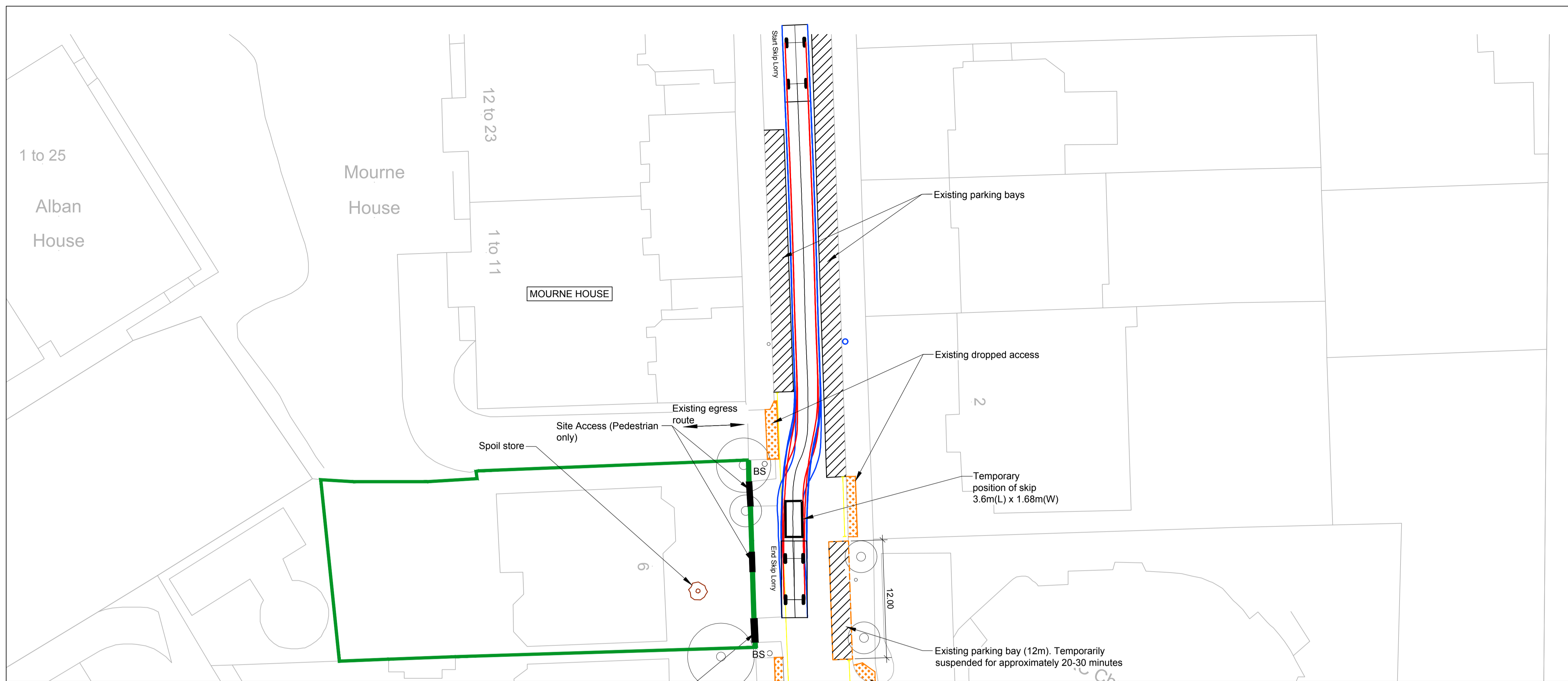
Ver	Date	Drawn	Eng	Amendment
2	15.07.16	RPo	ET	Issued for Information
1	10.06.16	RPo	ET	Issued for Information

**9 MARESFIELD GARDENS  
LONDON NW3 5SJ**

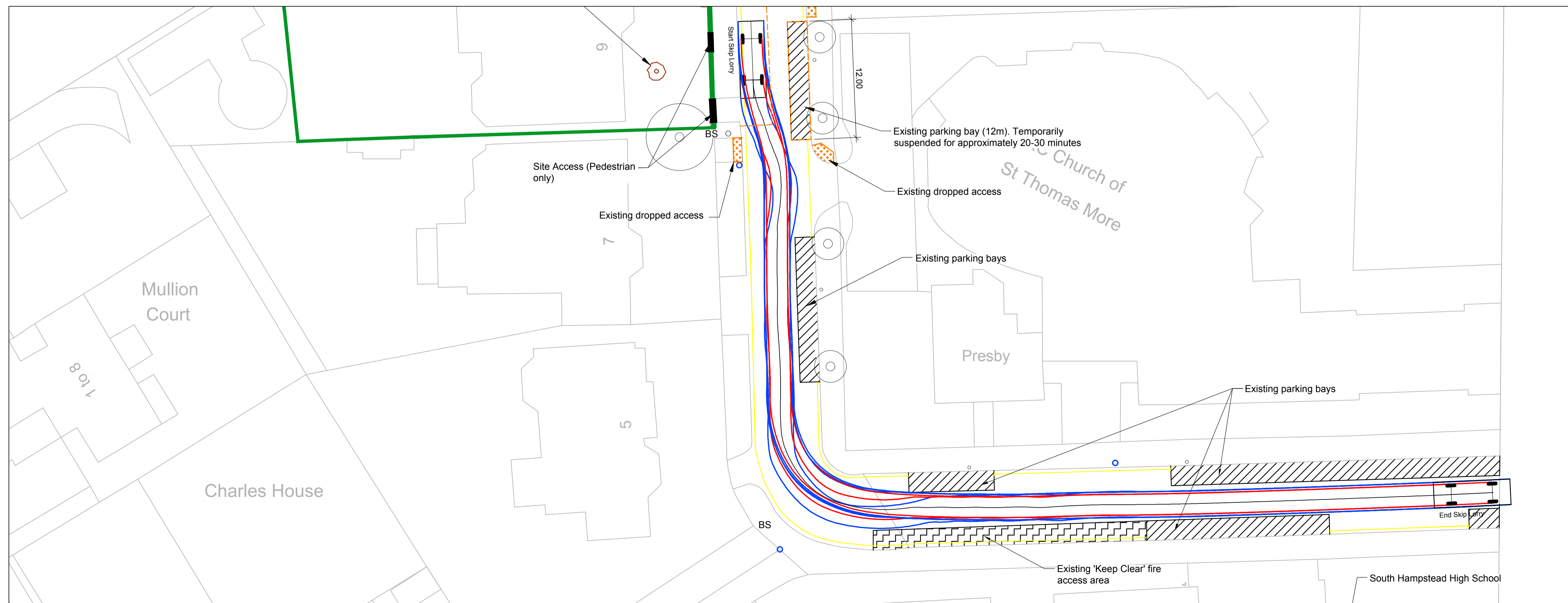
**GRAB LORRY  
TRACKING**

Status  
**ISSUED FOR INFORMATION**  
NOT FOR CONSTRUCTION

Drawn	RPo	Eng	ET
Scales	1:200 @ A1	1:400 @ A3	
Drawing No	Ver		
<b>25297-703</b>	<b>2</b>		



**SKIP LORRY PULLING UP AT FRONT OF SITE**



**SKIP LORRY PULLING AWAY FROM SITE**

**Notes :**

1. This drawing is to be read in conjunction with all relevant Architect's, Engineer's and specialists' drawings and specifications.
2. Do not scale from this drawing in either paper or digital form. Use written dimensions only. To check that this drawing has been printed to the intended scale this bar should be 50mm long @ A1 or 25mm long @ A3.
3. Health & Safety : All specific drawing notes are to be read in conjunction with the project "Information Pack" and "Site Rules".

**LEGEND**

- Single Yellow Line
- Temporarily Suspended Parking Bays Banksman
- BS
- Parking Spaces
- Fire Access Area
- Site Hoarding
- Site Access (Pedestrian only)
- Existing Tree
- Street Lighting
- Road Sign
- Drop Kerb

**TRACKING LEGEND**

- Vehicle body path
- Vehicle Wheel path

**VEHICLE DIMENSIONS**

Vehicle name : Skip Lorry  
 Overall length : 7.80  
 Overall width : 2.60  
 Max. track width : 2.12  
 Kerb to kerb radius : 7.00

Ver	Date	Drawn	Eng	Amendment
2	15.07.16	RPo	ET	Issued for Information
1	10.06.16	RPo	ET	Issued for Information

**9 MARESFIELD GARDENS  
LONDON NW3 5SJ**

**SKIP LORRY  
TRACKING**

Status  
**ISSUED FOR INFORMATION**  
NOT FOR CONSTRUCTION

Drawn	RPo	Eng	ET
Scales	1:200 @ A1	1:400 @ A3	
Drawing No	25297-704		Ver
			2