



STABLES MARKET

**DESIGN AND ACCESS
AND
HERITAGE STATEMENT**

for Long Stable External Stair and Canopy Works

June 2016

**Prepared for
Stanley Sidings**

By



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Stables Market: Design and Access and Heritage Statement

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1 INTRODUCTION

Executive Summary

This Design & Access and Heritage Statement has been prepared as supporting documentation for the Planning and Listed Building Consent application for the replacement of the existing Long Stable external metal staircase, landing and canopy with a new external metal staircase, landing and canopy.

Previous consent was granted (2015/5825/L & 2015/4960/P) for the proposed refurbishment of the existing metal stair and landing and the replacement of the canopy with a new glass canopy. Following the structural engineer's inspection of condition, it has been considered necessary to replace the existing staircase and link rather than repair and retention.

Like the existing external arrangement, the new proposal will allow pedestrian access at first floor level between the Grade II listed Long Stable and Provender Store buildings located within The Stables Market, Chalk Farm Road, London NW1 8AH. Both buildings are included in the Heritage at Risk Register.

The design documents have been prepared by and with the input of the project team who are:

Client: Stanley Sidings Limited

Local Authority: London Borough of Camden

Planning Consultants: Gerald Eve

Heritage Architects: Stephen Levrant Heritage Architecture Ltd

Structural Engineers: Walsh

1.1 Reference to other documentation

This document should be read in conjunction with:

Information prepared by Stephen Levrant Heritage Architecture Ltd:

- **CB-LP:** Location Plan
- **CB-01:** Long Stable – existing external stair, link & canopy-plans, elevations & details
- **CB-02:** Long Stable – proposed external stair, link & canopy- plans, elevations & details
- **Structural Engineering drawings:** 3788-sk-160608-CS / 3788-sk-160613-CS / 3788-sk-160615 / 01

1.2 Consultation

Proposals have been discussed with Catherine Bond who expressed an agreement in principle stating in an email to Nadina Reusmann (Stanley Sidings) of 30th May 2016 that '*works were generally in accordance with listed building and conservation area requirements*'. (Refer to Appendix III).

1.3 Planning Policy Guidance and Legislation

The assessments of the listed buildings and conservation area have been prepared taking into account the information contained in:

- NPPF National Planning Policy Framework, 27 March 2012.
- Planning practice guidance for the National Planning Policy Framework and the planning system. , 12 June 2014.
- Conservation principles, policies and guidance for the sustainable management of the historic environment, EH, April 2008.

- The Setting of Heritage Assets: English Heritage Guidance, October 2011.
- BS 7913:1998 Guide to the Principles of the Conservation of Historic Buildings.
- Understanding Place: Historic Area Assessments in a Planning and Development Context, EH, June 2010;
- Camden Development Policies 2010 – 2025, Local Development Framework
- Local Development Framework - Camden Core Strategy 2010 – 2025

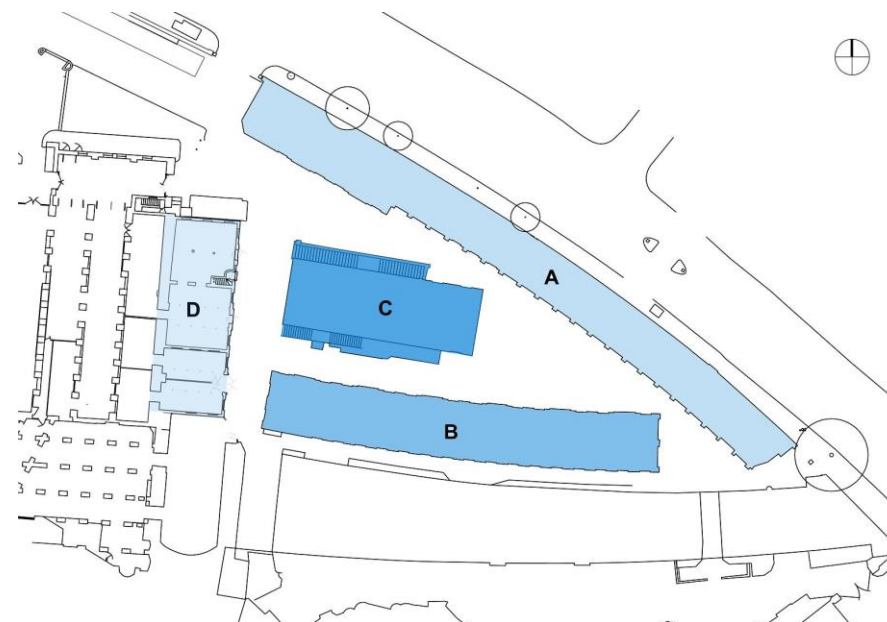


Figure 1 Stables Market - Four western ranges, subject site Buildings A & C.

2 CONTEXT

The complex of multi-storey stables was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

'The heart of Stables Market was previously a part of the Camden Goods Yard and a large area was occupied by warehousing for W. A. Gilbey's wines and spirits from the mid 19th century. The remaining buildings on the site comprise the stables for railway horses known as Stanley Sidings [...], a later block of 1883-85, and the surviving bonded

warehouse, *Gilbey's No.2 Bond*, built c.1885' (Regent's Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.17). The Long Stable and Chalk Farm Building, subject of this application, was built during the first phase of stabling. These four western ranges, which also include the Tack Room, the Provender Store, form the triangular group A-D built between 1854 and 1856 as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072):

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright. Unless stated otherwise.

Location

The Long Stable (Building C) lies in the centre of the triangular site described above and between the Chalk Farm Building (Building A) and the Provender Store (Building B) part of the internationally recognised Stables Market, in the northwest corner of Regent's Canal Conservation Area. (Fig. 1). The area is bounded to the north by Chalk Farm Road (Hampstead Road as it was known until 1862), and is separated from the former lands of the goods yard to the south by the railway viaducts (Fig. 2).

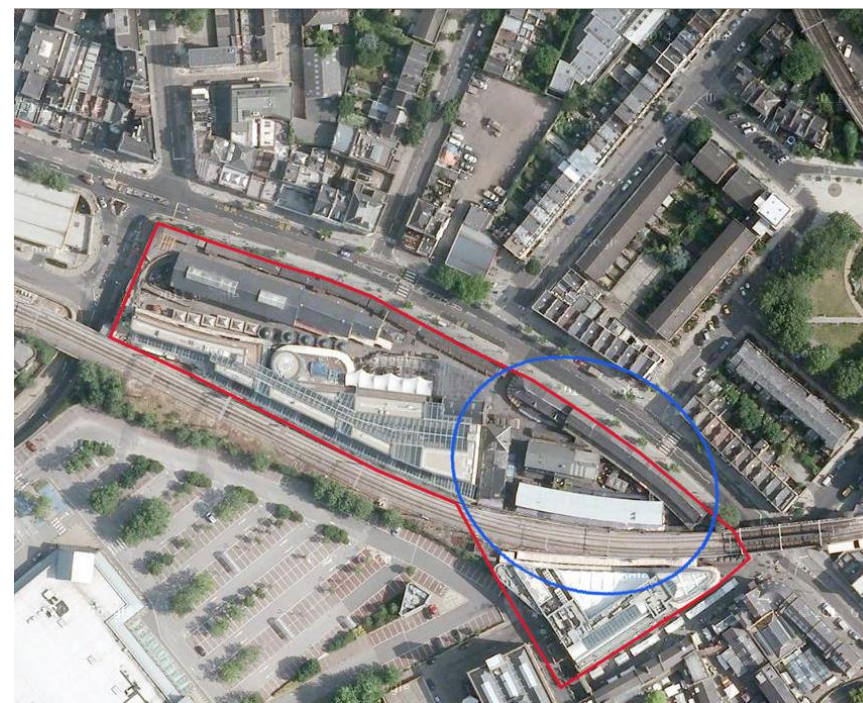


Figure 2 The Stables Market shown in red line; the stables range circled in blue.

2.1 Regent's Canal Conservation Area

The Regent's Canal Conservation Area was originally designated in 1974 and subsequently extended in 1981 to include the Stanley Sidings and the Stable Buildings. The conservation area was designated due to its unique character. *"It is an important feature of historic and visual interest in the wider townscape and, following the decline of traditional canal-related commercial activities, has been increasingly recognized as a valuable resource for water-based leisure activities, for its tranquil seclusion, for its ecological value and its potential for transportation and informal recreation"* (Regent's Canal Conservation Area Appraisal and

Management Strategy, adopted 11 September 2008, pp.5).

Many of the industrial buildings and structures are fine examples of industrial brickwork, illustrating styles of engineering construction characteristics of the 19th and early 20th centuries and using various types of brick, some produced in London and others brought in by the railways from their respective regions. Cast iron and wrought iron are also represented on the site.

2.2 Setting

The National Planning Policy Framework (NPPF) defines 'setting of a heritage asset' as 'the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral' (NPPF, Annex 2: Glossary, p.52)

The Setting of Heritage Assets: English Heritage Guidance 2011, highlights para 114 of the NPPF: *'The extent and importance of setting is often expressed by reference to visual considerations. Although views of or from an asset will play an important part, the way in which we experience an asset in its setting is also influenced by other environmental factors such as noise, dust and vibration [...]'* (NPPF, para 114).

The site as a whole falls within the Regent's Canal Conservation Area, a heritage corridor of unique character, but the elements which contribute towards the neighbourhood identity, are not necessarily confined to the conservation area or other boundaries.

The character of the environs developed over the years and, today largely derives from the presence of four distinct but interrelated elements: the Regent's Canal, Chalk Farm Road, the Railway viaduct and the market activities

Chalk Farm Road is the principal thoroughfare of the area. The sequence of diverse views along Chalk Farm Road, defined by the built form and the curve of the road, prevents long distance views.

The built environment is defined on the East side by 19th century terraced buildings which have shops, café, and restaurants at the ground floor; and on the West side by predominant structures, such as the Roundhouse, the 'Camden Wall' and the railway bridge, which come into view on a northerly progression.

The Roundhouse is a major focus point, almost providing a gateway to the area from the north.

The 'Camden Wall' confers to Chalk Farm Road a sense of continuity and calmness with its regular progression of openings, but it is also a distinct barrier with its non-active frontage. A strong change of character occurs behind the Stables Market gate, where the original industrial stables subject of this application, still remain. The market is enclosed and separated from the public realm by the impenetrable wall.

The railway bridge, crossing diagonally across Chalk Farm Road, is not a simple barrier. It creates a visual break, emphasised by the traffic island and one-way traffic system. It also frames with its portal structure the view behind that change again in character. In fact, the buildings aligned on this stretch of the street have a narrow pavement establishing a different relationship with the street.

The immediate setting of the subject site is the larger Stables Market at the heart of the area widely known as Camden Market. The complex comprises the industrial horse stables and tightly enclosed courts leading off one another; their plan form being influenced by the horse towpath and stabling serving the vast railway goods yard and interchange traffic.

The redevelopment of the underused site after the decline of canal-related activities and the conversion of wharves for the craft markets in the 1970s, contributed greatly to the economic and physical regeneration of Camden and to the present character of the area.

The craft markets developed into one of London's top tourist attractions

giving a new iconography to Camden. Today the place is mostly known for its famous market, restaurants, café and entertainment facilities but this was never part of any planned development policies.

This market attracts large numbers of Londoners and tourists because of the character, the goods on sale and the uniqueness of the location. Boat trips, walks along the canal, and watching the barges pass through the lock gates from the Dingwall's beer garden, are important parts of the attraction of the Camden Lock area. Sunday trading was permitted on this private site while disallowed in many places elsewhere, and this also contributed to its success.

2.3 Historical Background

This complex of multi-storey stables, also known as Stanley Sidings, was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

Bounded to the north by Chalk Farm Road (known as Hampstead Road before 1862), the triangular site was separated from the goods yard to the south by the viaducts of the North London Railway but was linked to it by underground tunnels.

A 22 acre site at Chalk Farm was acquired by the London and Birmingham Railway company soon after its formation, for a terminus and depot. When development began in 1835-37 it was as a goods depot, linked to the Thames by the Regent's Canal and subsequently to the docks by the North London Railway, built in 1850-1.

In the 1840s and 1850s substantial buildings were constructed around the edge of the site, including the Southern Goods Shed, built in 1845 and rebuilt after a fire in 1857, and a goods shed north of the canal, replaced by the Interchange Warehouse in 1900-5. The Chalk Farm stables were linked to both of these buildings by underground tunnels.

Horses were required for a wide range of haulage duties in the yard as

well as for distributing goods outside; others were needed for shunting locomotives. Early provision for the horses was apparently in makeshift accommodation or below ground in the vaults of the goods station. By 1849 some horses were being stabled in the south eastern corner of the goods yard, and it seems likely that the expansion of this part of the yard in 1855 led to the construction of the stables at the triangle site.

The first phase of stabling, the four western ranges forming the triangular group A-D in the modern site plan, was built between 1854 and 1856, as indicated as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072). They were designed by the LNWR staff on the Stafford office and had a capacity of 148 horses. The ranges were mainly one and a half storeys, with ground floor stabling and haylofts above.

In the early 1880's the whole site underwent a phase of expansion and alteration with additional storeys added to the western ranges between 1880-3. During this period, a bridge connection between the Provender Store and the Tack Room was also created.

The site was vacated in 1973, and many of the buildings were demolished, tracks lifted, and various parcels of the land were sold into different ownerships. After 1973 the sidings and Camden Lock were used for various light industrial and storage uses including garaging. The market activities started at this time in a rather ad hoc fashion, commencing with small stalls located externally to the stable buildings. Gradually shop units and canopies were added to the buildings and by the 1980s it was a fully-fledged market place.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright.

2.4 Long Stable

The Long Stable building was built in 1853-5 as a single storey stable block with hay lofts. In 1881-3 it was raised (builders, Bradney and Company) to provide an upper level of stabling and a ramp was provided on the south side for horses to gain access to the upper storey (Fig.3). A second ramp to the north was added c1895 to provide access to the upper storey of the newly raised Building A. Only the latter ramp now survives (Fig.4).

The building is seven bays long, with the centre bay breaking slightly forward on both sides; there was originally an entrance and a loophole to the south. On the ground floor there are round-headed openings with high level sills to light the stalls from above, as seen elsewhere on the site. The later first-floor openings have segmental heads. The east gable end has a small upper-storey hatch over a widened ground-floor opening. The west gable end has had its upper-storey opening enlarged as a goods hatch.

The 1880s 'horse road' to the south was not strictly a ramp as it had steps with 4 ½ inches risers; its position is evident on the south wall. The ramp to the north is brick built with concrete-paved road surfaces and an intermediate landing.

Interior:

There were originally 26 stalls in a single undivided space giving rise to the naming of this block as the 'long stable', even though this is the shortest of the stable blocks built on the site. The upper floor rests on cast-iron stanchions with moulded heads along the north and south walls; these bear marks that they were made by Barton & Son, of Darlaston. They support wrought-iron I-section girders carrying rolled joists to brick jack arches. There are timber king-post roof trusses, but no traces of the original stable fittings.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables, Report by Peter Guillery, September 1995. Crown Copyright.



Figure 3 1975 photograph showing horse ramp to south side (now lost and replaced with a modern steel staircase).

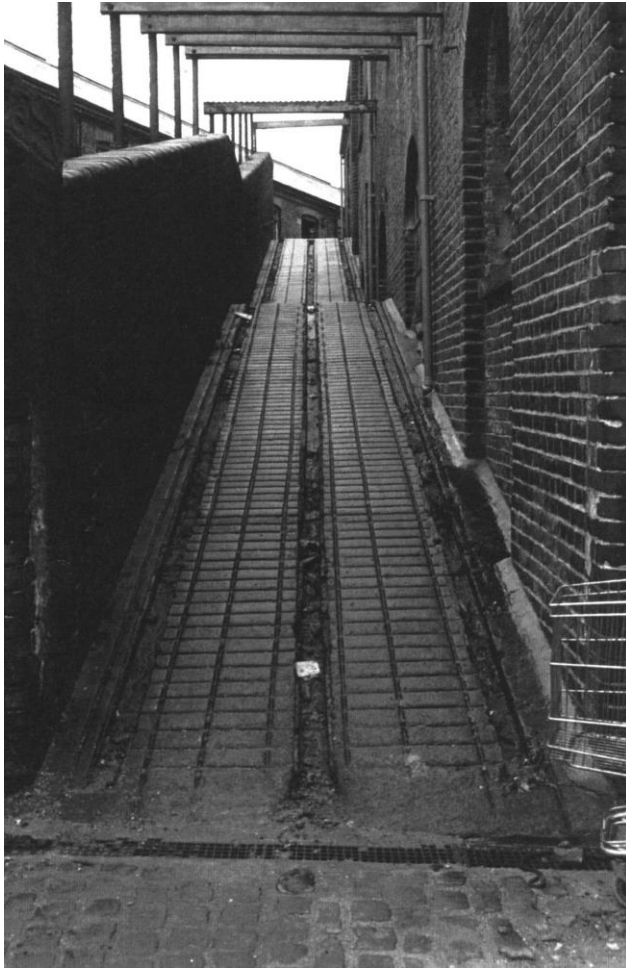


Figure 4 1975 photograph showing horse ramp to the north side of the Long Stable (still extant). Note the remains of a canopy.



Figure 5 1975 photograph showing relationship between the Long Stable and the Chalk Farm Building viewed from the west. Note the canopy to the Long Stable horse ramp and the Chalk Farm Building walkway and balustrade.

2.5 Provender Store Building

This long range backing onto the line of the North London Railway and shallowly curved accordingly was built in 1853-5 as a single storey stabling with hay lofts. In 1880-3 it was raised by the addition of two storeys replacing the hay lofts to provide extended provender storage (figs.6&7).

The range is 18 bays long, in yellow stock brick. The ground floor stabling matches that of the rest of stable on site; the round-arched bays

open to their heads with semi-circular stable windows grouped as threes with central entrances.

The first floor has round-headed windows, the second floor segmental heads. There were formerly external timber staircases giving access to the pitching holes of the hay lofts.

At the east end of the north elevation there was a stepped link at first floor level joining with the cantilevered walkway of Building A. The east gable end wall has been rebuilt in blue brick. The west gable end had a bridge link to Building D at first floor level – now lost, save for a single timber beam.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables, Report by Peter Guillery, September 1995. Crown Copyright.



Figure 6 The Provender Store in 1975.



Figure 7 The Provender Store in 1975.

3 Assessment of Significance

3.1 Criteria

As recommended by NPPF (March 2012) proposals for the alteration or redevelopment of listed building or buildings within a Conservation Area should be considered and be based on an understanding of the site's significance.

Paragraph 128 of NPPF states that '*In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should also be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance*'.

The criteria for the assessment of significance according to English Heritage's 'Conservation Principles' consists primarily of:

- Evidential Value – relating to the potential of a place to yield primary evidence about past human activity;
- Historical Value – relating to ways in which the present can be connected through a place to past people, events and aspects of life;
- Aesthetic Value – relating to the ways in which people derive sensory and intellectual stimulation from a place;
- Communal Value – relating to the meanings of place for the people who relate to it, and whose collective experience or memory it holds.

3.2 Long Stable & Provender Store Building

Evidential Value

There are numerous surviving features providing evidence of the original function of the buildings, although disturbed by many alterations. The legibility of the former uses is, however, clear.

The extent of historic fabric preserved contributes greatly to the special interest and character of both buildings and to that of the group in general. **Evidential value is therefore high.**

Historical Value

The historical value of the buildings is recognised by their statutory Grade II listing, as part of a distinctive group of former stables.

In this case the historic value is closely associated to the evidential value, providing a significant example of stables construction of their historic period beyond the numerous alterations suffered through the years.

Historical value is therefore medium to high.

Aesthetic Value

Both buildings, as well as the whole stables complex, are utilitarian and purpose-made. There is no artistic 'design ethos' in these buildings as such; utilitarian buildings are economical: there are no added embellishments, wasted space, nor selection of materials but those easily sourced.

Although both buildings are exemplary Mid-Victorian stables, they have little artistic or architectural merit. **Aesthetic value is therefore low.**

Communal Value

The Stables Market and its listed buildings have a **high communal value** as there is a strong sense of identity with the place, nurtured by the historical industrial use.

The original purpose of the stables has been superseded. There are no possible horse-related activities that may be carried out within the site, and there have not been since the 1950's/60's. The market use, established since the late 1970's/80's, is the reason why this area of Camden achieved worldwide recognition and attracts over 100,000 visitors per week. Furthermore, a poll carried out by NOP World in late 2003, shows that 95% of visitors agreed that the Camden markets are a valuable tourist attraction and 89% agreed the markets are a valuable amenity to the local community as well as visitors (London Borough of Camden, the role the markets play in the vitality & viability of Camden Town, 2006.). The communal values of the site can be associated to the later but firmly established market function, and as proved by the studies and polls carried out since its inception. The fact that the buildings are still in use brings together past and present common uses by the community, reaffirming the communal value as high.

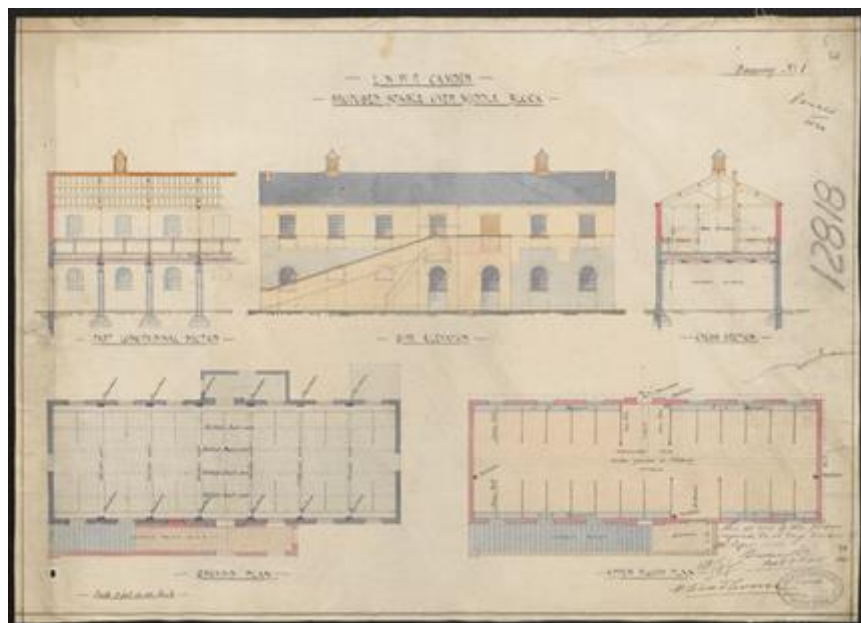


Figure 8 Drawing dated 20th September 1881, showing first floor extension to the Long Stable.



Figure 9 Detail of drawing. Long Stable - Longitudinal Cross Section.

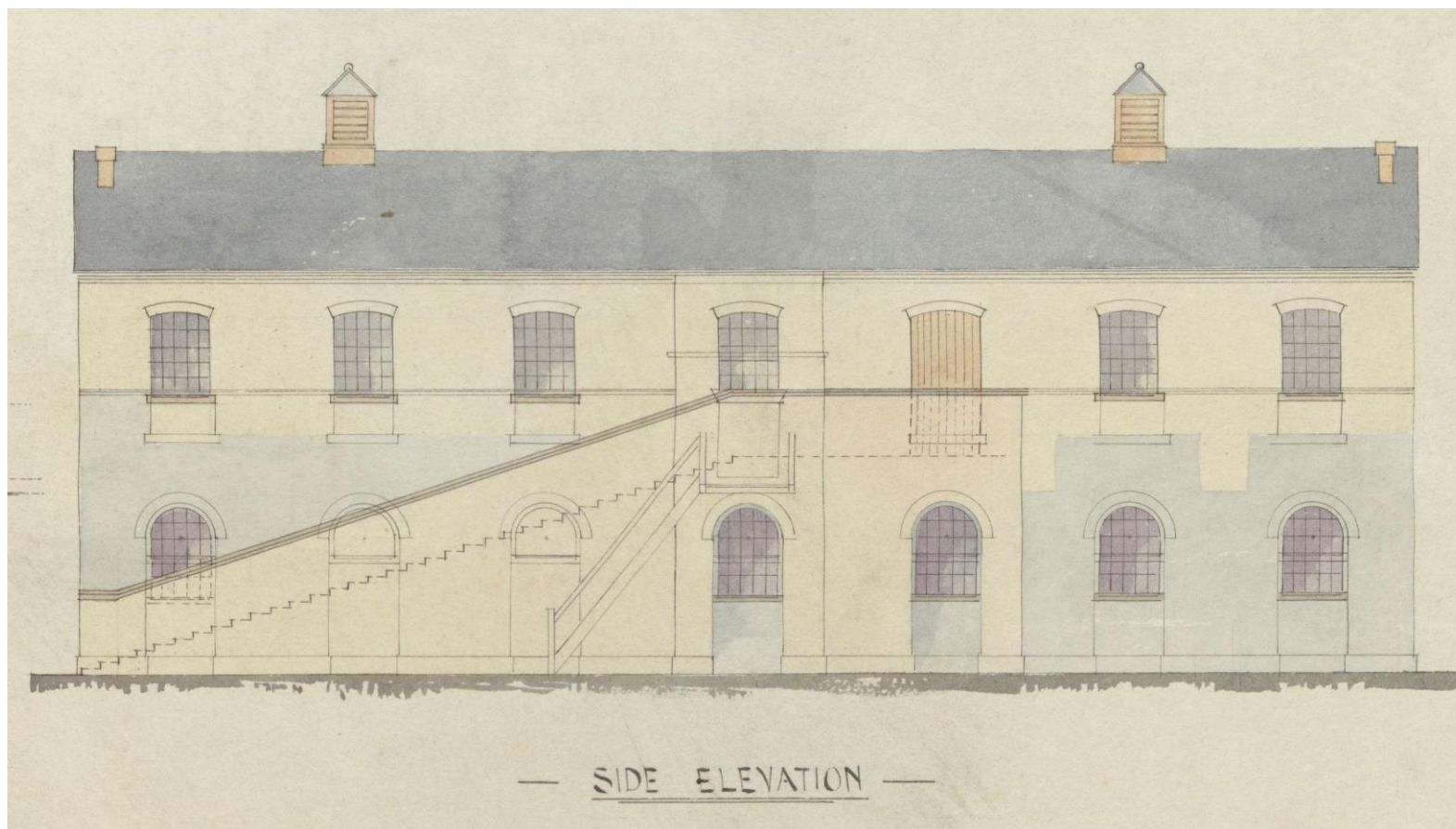


Figure 10 Long Stable South Elevation drawing. Note the horse ramp providing access to the first floor (now lost).

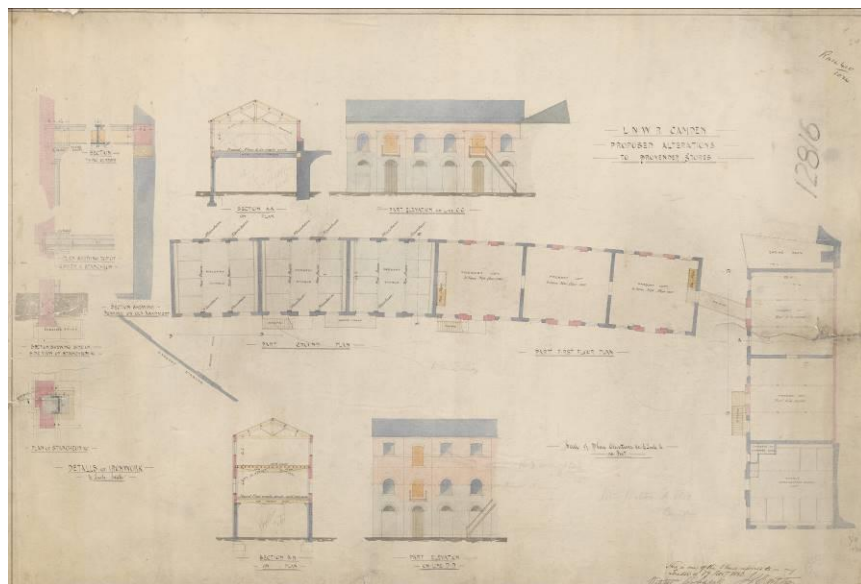


Figure 11 Drawing dated 17th November 1880 showing proposed alterations to Provender Store.

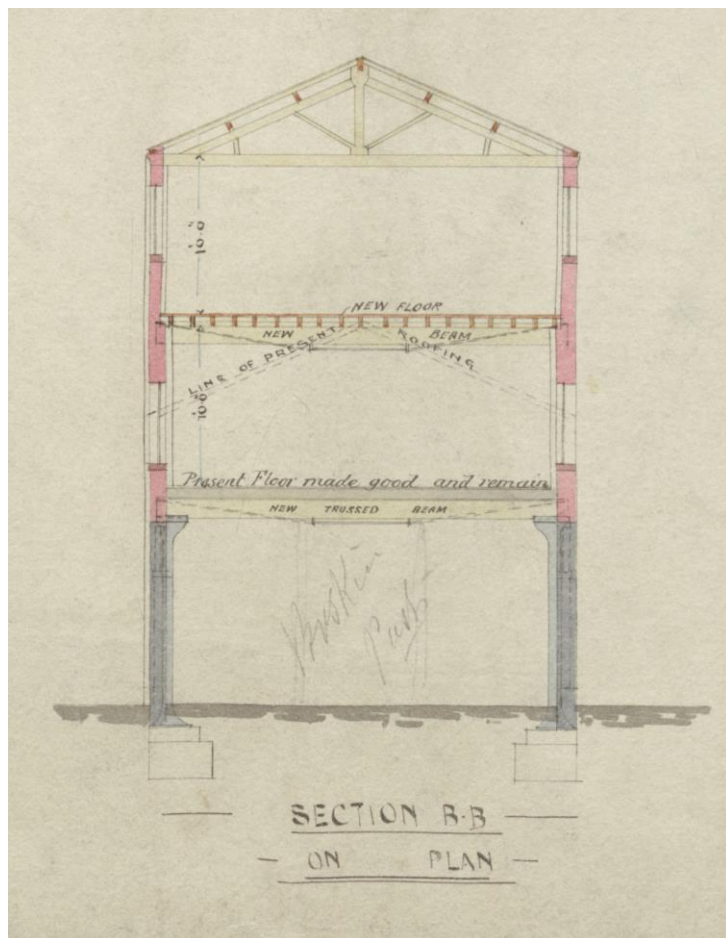


Figure 12 Drawing dated 17th November 1880 Proposed alterations to Provender Store (Section).



Figure 13 Drawing dated 17th November 1880 Proposed alterations to Provender Store (Elevation).

4 PROPOSED WORKS

4.1 Description

Long Stable external stair and canopy works

The proposal is to replace the existing modern steel staircase, fabric canopy and link structure with a new steel staircase, glass canopy and link structure.

The existing staircase is constructed from steel flat sections and angles with a chequer plate finish. The link is formed from steel I beams spanning between the Long Stable and Provender Store with a similar chequer plate finish.

The Structural Engineer has confirmed following a site inspection and examination of the structure that the chequer plate deck and structural supporting beams and padstones are in poor condition. They subsequently have recommended, in an email dated 25th May 2016 (refer to Appendix II), for the replacement of the primary link supporting beams and padstones. The replacement of this arrangement with new stairs, a canopy and link structure will ensure the sustainable long term functioning of both buildings and provide the opportunity to improve the current arrangement by installing the link at the level of the current existing primary cross beams such that a flush entry into the Provender Store can be achieved.

Like the existing stair, the new stair will be constructed from steel flats and angles and finished with a chequer plate deck. The new stair has the benefit over the existing arrangement of being shorter in its overall length due to the reduced total rise as a result of the new link deck level.

The new balustrade will be formed from T shaped uprights and an architectural metal mesh infill to match that proposed for the Chalk Farm walkway. The uprights will extend to support a new glass canopy over formed from flat glass sheets interrupted by pressed metal box gutters at

the changes in plane.

The works will improve the external appearance of both the Long Stable and the Provender Store as well as providing an improved and safer access to both buildings at first floor level. The detail design will closely relate to the refurbished Chalk Farm building walkway and thus establish a coherent architectural and tectonic language across the wider site.



Figure 14 2014 View between the Long Stable and Provender Store showing the external staircase and canopy (left) and link.



Figure 15 Current high level view of the Long Stable from the west showing the staircase canopy and link.

5 IMPACT ASSESSMENT

It is contended there will be no harm caused to any of the historic buildings by the proposals. Moreover, the works will act to enhance the historic character and appearance of the buildings by appearing less visually intrusive than the existing arrangement, thus having a highly positive impact overall.

The new works are designed to have a negligible impact on the buildings' fabric. The Long Stable new glass canopy design takes as its point of departure that canopy shown remaining over the horse ramp in a 1975 photograph of the north side of the Long Stable building.

The new stair has the benefit over the existing arrangement of being shorter in its overall length due to the reduced total rise as a result of the new link deck level.

The canopy bracket fixings adjacent the Long Stable external elevation are such that their particular locations can be determined to avoid windows and other details of the elevation and fixed into the mortar joints rather than the brickwork.

The proposed works taken together are considered to enhance the significance of the three listed buildings and that of the stables complex generally.

The proposed works are, therefore, considered appropriate and highly beneficial and will result in much needed improved facilities for site visitors.

6 NPPF CONSIDERATIONS

All policies in the NPPF adopted 27th March 2012 constitute the government's view of what sustainable development means in practice. The NPPF contains an express presumption in favour of sustainable

development which should be taken into account when making planning decisions.

'Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy.' (NPPF Paragraph 7).

This statement deals principally with Section 12 of the NPPF, "Conserving and enhancing the historic environment", however Heritage considerations and issues are prevalent throughout the framework.

NPPF Paragraph 126 states that *"Local planning authorities should set out in their Local Plan a positive strategy for the conservation and enjoyment of the historic environment, including heritage assets most at risk through neglect, decay or other threats. In doing so, they should recognise that heritage assets are an irreplaceable resource and conserve them in a manner appropriate to their significance. In developing this strategy, local planning authorities should take into account:*

- *the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;*

NPPF Paragraph 128 states that *"In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contributions made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance".*

As recommended in Paragraph 128, an assessment of the significance of the Long Stable and Provender Store building has been provided earlier in this report. Appropriate and proportionate historic research and analysis was undertaken to identify the heritage values and character of the heritage assets, including the origins of the buildings and how they have changed over time.

The significance assessment was based on historic research and consultation of relevant historic records and was examined according to the criteria set out in English Heritage's 'Conservation Principles'.

It is our belief that the NPPF requirement for provision of proportionate information which will enable the assessment of the likely impacts of proposed development on the special historic and architectural interest of the buildings has been therefore fulfilled.

NPPF Paragraph 131 states: *'In determining applications, local planning authorities should take account of:*

- *The desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;*
- *The positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and*
- *The desire of new development making a positive contribution to local character and distinctiveness'.*

The proposal has been driven by the need to ensure the historic character of the listed buildings is preserved and enhanced and meets all current standards in a respectful manner towards the historic environment. In summary, the proposals will promote the continued effective use and operation of the Long Stable and the Provender Store building as part of the Stables Market complex.

NPPF Paragraph 134 states: *'Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including its optimum viable use'.*

There is no harm involved in the proposed replacement Long Stable external staircase, link and new glass canopy.

The established market use of the former stables is not challenged by the proposals; the works will greatly improve not only the appearance of the buildings but will also result in an improved circulation and safer access route for site staff and customers alike.

The public benefit the proposals provide is in preserving not only the listed buildings but also the already established "optimum viable use" for the site, as well as enhancement of the wider historic environment in general.

The proposals will aid in safeguarding the continued use and long term preservation of the buildings. It is therefore concluded that the significance of the heritage assets will be preserved and enhanced in compliance with NPPF requirements.

7 LOCAL PLANNING POLICY

Camden Local Development Framework, Camden Core Strategy, 2010 - 2025, Adopted Version, November 2010.

The Camden Core Strategy Policy CS14 – 'Promoting high quality places and conserving our heritage', sets out the requirements to safeguard Camden's heritage. The overall strategy is to sustainably manage growth in Camden in a way that conserves and enhances the heritage and valued places that give the borough its unique character.

Camden Planning Guidance provides advice and information on how the Local Authority applies its planning policies. The guidance is consistent with the Core Strategy and the Development Policies, and forms a Supplementary Planning Document (SPD) which is an additional "material consideration" in planning decisions.

CPG 1- Design deals with heritage issues in Section 3. This section sets out further guidance on Core Strategy Policy CS14 Promoting high quality places and conserving our heritage and Development Policy DP25 Conserving Camden's Heritage.

Paragraph 3.22 refers to the statutory requirement, when assessing applications for listed building consent, to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

It is considered that the proposal not only preserves but enhances the character of the historic environment through the replacement of the existing Long Stable staircase and link, which in turn, retains and enhances the elements of significance within and in the vicinity of the site. The new glass canopy replacing the existing fabric canopy further contributes to this objective.

Paragraph 3.23 expands on the desirability to retain original or historic features and to carry out repairs in matching materials. The proposals should seek to respond to the special historic and architectural constraints of the listed buildings, rather than significantly change them.

As stated earlier in this report, the proposal actively seeks to restore historic finishes and features. The Long Stable stair, link and canopy are not historic and date from the later part of the twentieth century. The works will be carried out using traditional materials and techniques where appropriate in order to reinforce the integrity of the listed buildings.

8 CONCLUSION

The buildings are Grade II listed and on the Heritage at Risk Register. The buildings are open to the public and in continuous use. They have been subject to change and in order to remain sustainable, welcoming and pleasing places, will continue to change. The proposed scheme is driven by the need to not only conserve and enhance the buildings, but also to ensure a sustainable solution that safeguards the current and future use of the market.

The proposed works, including the new glass canopy, will enhance the historic character and appearance and architectural importance of the heritage assets safeguarding their long term conservation.

This proposal complies with policy at the heart of NPPF in respect of sustaining and enhancing not just the historic fabric but the significance of the Regent's Canal Conservation Area and the heritage assets within and in the vicinity of the site.

The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions. Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy (NPPF Para 7).

For the above reasons, it is considered that the proposal would be acceptable in the context of the setting of the heritage assets in the immediate surroundings and the conservation area. It is therefore concluded that the proposed works satisfy the relevant clauses of the NPPF as detailed above and is consistent with the spirit of local policies and national conservation principles.

APPENDIX I LISTED BUILDING DESCRIPTION

List entry Summary

List entry Number: 1258101

Grade: II

Four blocks of industrial stabling, now workshops and warehousing. c1855-1870, with later Victorian additions. For the London and North-Western Railway Company's Camden Goods Yard. Stock brick, with hipped slate roofs, some stone lintels. Some iron columns internally, but floors and roofs generally of timber. EXTERIOR: mostly of 2 storeys.

Northernmost block (A) abutting on Chalk Farm Road, c1855, with upper storey of c1895. Long curved front road, mostly of 2 storeys with eaves cornice but western end of one and a half storeys, somewhat altered, with chimney on roof. Round-headed half windows for stabling on ground storey, segment-headed industrial windows in upper storey (eastern end only). Elevation towards yard irregular. Eastern portion has cantilevered open balcony at first-floor level retaining some concrete horse troughs and connected by bridge to Block B and separately to ramp on Block C. Ceilings of ground storey have jack-arch iron and brick construction. INTERIOR of upper storey has separate compartments and paving for horses but no stalls.

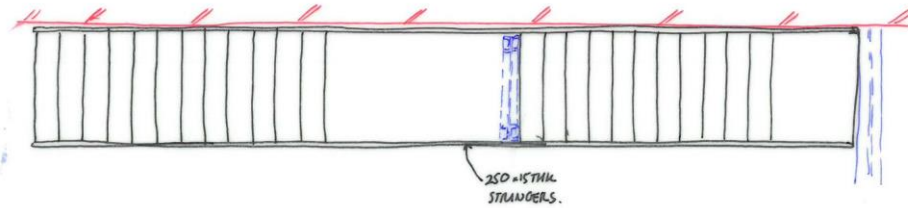
Block B immediately to north of North London railway line. 3 storeys. Ground storey c1868, originally provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A.

Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A. Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper storey 1881; horse ramp on north side of block c1895, connected with balcony on Block A. Round-headed windows on ground storey, segment-headed industrial windows above on both north and south sides. South side formerly had another horse ramp, of 1881, and covered bridge connecting with Block B, demolished in 1980s. Ground storey has iron and brick jack-arch construction and iron stanchions against walls stamped 'Norton and Son Darlaston'.

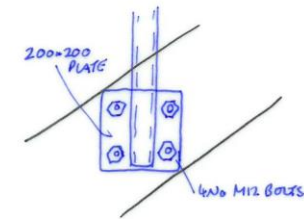
Block D at right-angles and to west of Blocks B and C. 2 storeys. Ground storey c1868, upper storey c1881. Main elevation faces eastwards, with return northwards. Round-headed half-windows for stabling at ground level, some segment-headed sash windows above. Tall brick chimneys. INTERIOR with original timber benching, one timber partition and some harness hooks. Said to have been formerly the Tack Room for the stabling. Formerly connected by a bridge at south end to Block B. Included as a rare example of substantial industrial stabling and a major surviving portion of the former Camden Goods Yard. Forms a group with the 'Horse Hospital' to north-west (qv) and with further remnants of stabling and warehouses west of Block D (qv). A tunnel (now blocked) south of the North London line connects the complex with further LNWR buildings and the Regent's Canal south of the North London Line.

Listing NGR: TQ2862684201

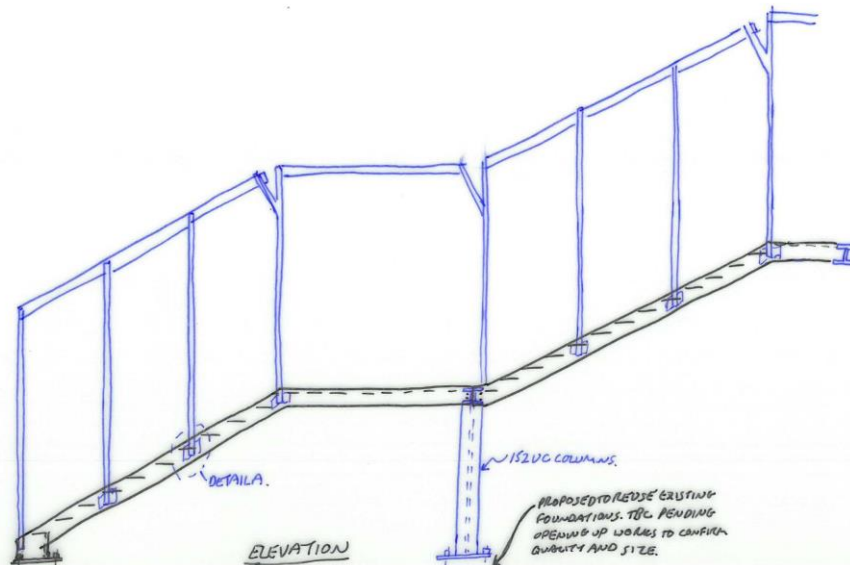
APPENDIX II STRUCTURAL ENGINEERING



PLAN VIEW



DETAIL A.



ELEVATION

NOTES

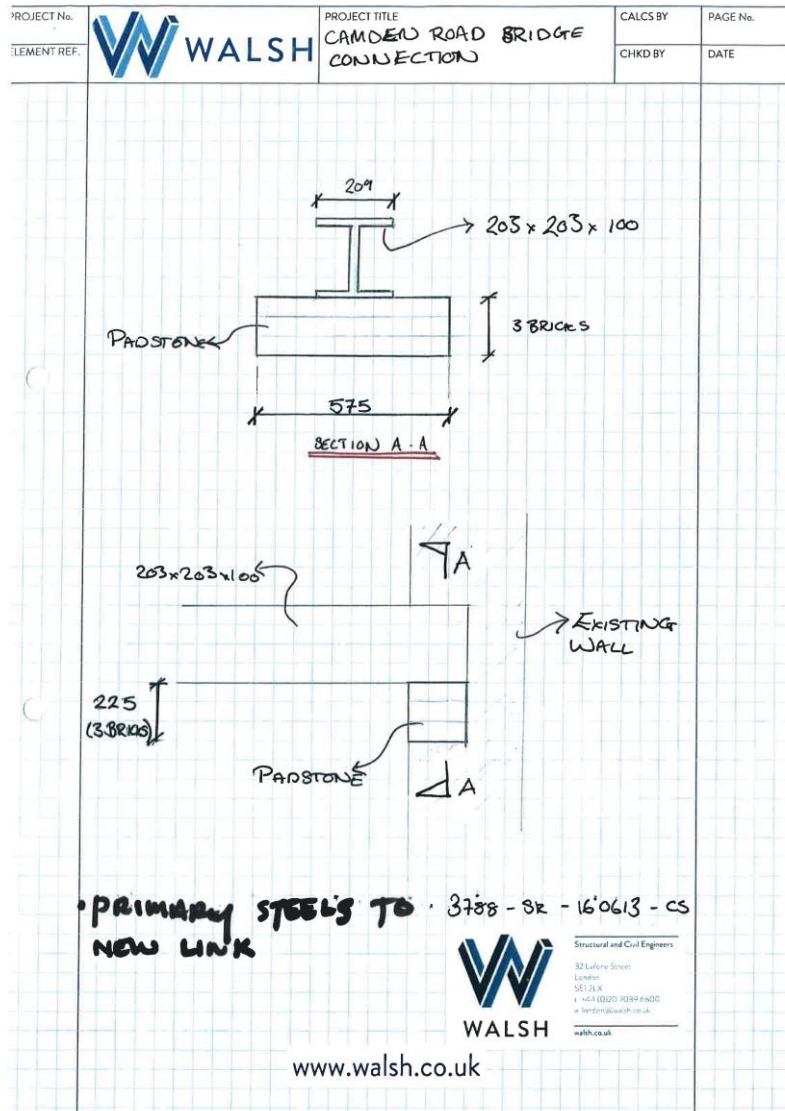
0.7% STEELWORK TO BE GALVANISED.

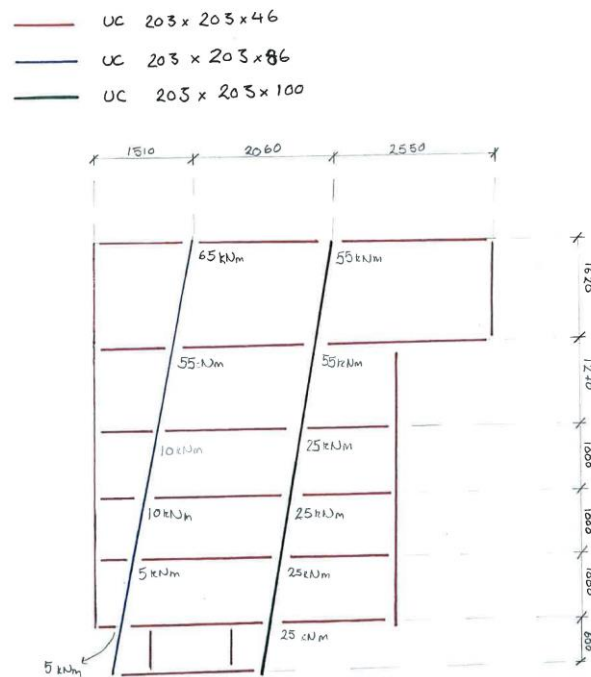
FOR COMMENT.

STAIR DETAILS.
LONG STABLE STAIR AND BRIDGE
3788/SH/160615/MW/01



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NEW LINK - STRUCTURAL PLAN

From: Leighton, Thomas [<mailto:Thomas.Leighton@walsh.co.uk>]
Sent: 25 May 2016 15:00
To: 'Nadina Reusmann' <nadina@stanleysidings.com>
Cc: Ben Blackledge <ben.blackledge@stanleysidings.com>
Subject: RE: Provender/Long Stable bridge and stairs

Hi Nadina,

Further to our site visit last week, please see below for a summary.

We have looked at the new structure for the bridge we can use 203 UC's to support the bridge and the small flight of stairs. Please see attached for preliminary proposals. We will detail up the connections and finalise the sizes in the next few weeks. I would allow an extra 30mm depth in the structure to allow for connections. Please let me know if you require any further information for the immediate future.

With regards to the existing beams spanning between the buildings - In order to determine their structural capacity we would unfortunately need to do intrusive investigation works to determine the embedment into the walls and to determine the size and condition of the padstones. We would unfortunately require this on all four supports. Given the poor appearance of the padstones we expect that the build quality may be poor and that the capacity of the embedment into the walls may not be sufficient. We would therefore recommend allowing for the replacement of these beams along with new padstones within the walls to support them.

Hope this helps, and hope you have a good two weeks off. If you want this in formal letter please let me know. I'm not in the office this afternoon but can write it for Ben tomorrow if required.

Thanks,

Tom

Thomas Leighton MEng MA (Cantab) CEng MICE

Project Engineer

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APPENDIX III CONSULTATION WITH LA

----- Forwarded message -----

From: **Bond, Catherine** <Catherine.Bond@camden.gov.uk>

Date: Tuesday, 31 May 2016

Subject: Provender/Long Stable bridge and stairs

To: Nadina Reusmann <nadina@stanleysidings.com>

Cc: "Nicholls, John" <John.Nicholls@camden.gov.uk>, "Gracie, Ian" <Ian.Gracie@camden.gov.uk>

Dear Nadina

Thanks for your message regarding the fragile structure of the staircase and link bridge accessing the Provender Store and the Long Stable.

We fully appreciate that the structure is not as sound as it should be, and understand the urgency of the works. Notwithstanding, the works do require planning permission and listed building consent.

The proposed designs you have sent us are generally in accordance with the listed building and conservation area requirements. I agree that levelling out the bridge to match the internal floor levels of the Provender Store makes sense, subject to detailed design. However, the Council will need to consult on the designs as part of the planning process.

This means that if you wish to proceed with the works at this stage, you will be doing so at your own risk, and the relevant applications you need to submit will by necessity be retrospective. Obviously, if you choose to go ahead now, you will be doing so on the basis that they are emergency works.

Please get in touch if you have any questions.

Kind regards

Catherine

Catherine Bond
Principal Planner (Conservation and Heritage)

Telephone: 020 7974 2669



You can [sign up](#) to our new and improved planning e-alerts to let you know about new planning applications, decisions and appeals.

From: Nadina Reusmann [<mailto:nadina@stanleysidings.com>]

Sent: 25 May 2016 16:11
To: Bond, Catherine
Cc: Ben Blackledge
Subject: Provender/Long Stable bridge and stairs

Dear Catherine,

As I mentioned recently, the stairs and bridge between Provender and Long Stable are in quite worse condition than expected.

I would appreciate if you could consider carrying out the proposed works (please engineer's notes below and attached sketch) as emergency works. If you think it is possible, my colleague Ben –copied in- will forward to you the formal letter and engineer's report and final drawings (I will be on leave from tomorrow to 13/06).

We would also appreciate your input on our proposal to reduce the level of the platform floor only –the structure will remain at existing level-, to remove the steps in front of the Provender door (attached as existing, as approved, and as proposed).

Best wishes

Nadina

Nadina Reusmann
Heritage Director

For and on behalf of

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