

# ATHLONE HOUSE LONDON NW3

Draft Construction Management Plan  
June 2016, Rev 1



WALTER LILLY

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# 1 REVIEW

## FOR INTERNAL USE ONLY

Please initial and date in the relevant section of the table.

The **highlighted areas** of the Draft table will be deleted by their respective teams during pre app review if these sections are no longer applicable.

### Pre app

Community liaison	
CLOCS	
Transport	
Highways	
Parking	
Environmental health	
Sustainability	(Attach appendix if necessary)
Sign off	

### Draft

Community liaison	
CLOCS	
Transport	
Highways	
<b>Parking</b>	
Environmental health	
<b>Sustainability</b>	
Sign off	

- Indicates input required from multiple teams through document

## 2 INTRODUCTION

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

The completed and signed CMP must address the way in which any impacts associated with the proposed works, and any **cumulative impacts of other nearby construction sites**, will be mitigated and managed. The level of detail required in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance (CPG) 6: Amenity and (CPG) 8: Planning Obligations.

This CMP follows the best practice guidelines as described in Transport for London's (TfL's Standard for Construction Logistics and Cyclist Safety (**CLOCS**) scheme) and Camden's Minimum Requirements for Building Construction (**CMRBC**).

The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as for road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "Demolition Notice".

Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary. It is preferable if this document is completed electronically and submitted as a Word file to allow comments to be easily documented.

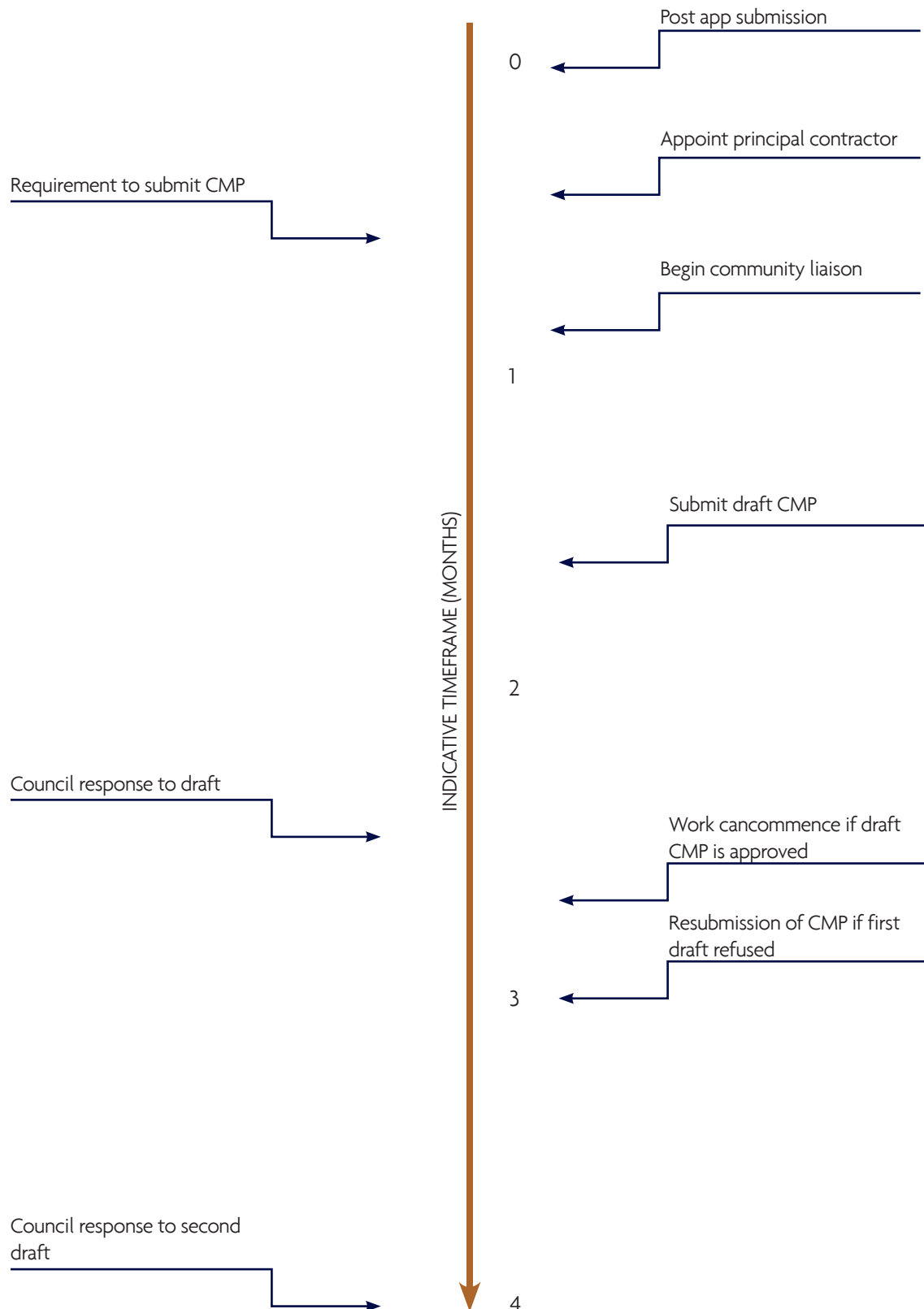
(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction, etc.)

Revisions to this document may take place periodically.

### 3 TIMEFRAME

#### COUNCIL ACTIONS

#### DEVELOPER ACTIONS



## 4 CONTACT

*1. Please provide the full postal address of the site and the planning reference relating to the construction works*

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Address: Athlone House, Hampstead Lane London N3  
 Planning ref: Unknown

*2. Please provide contact details for the person responsible for submitting the CMP*

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Name: Walter Lilly & Co Ltd  
 Address: Knollys House, 17 Addiscombe Road, Croydon CRO 6SR  
 Email: contact@walterlilly.co.uk  
 Phone: 0207 8730 6200

*3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses*

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Name: Walter Lilly & Co Ltd  
 Address: Knollys House, 17 Addiscombe Road, Croydon CRO 6SR  
 Email: contact@walterlilly.co.uk  
 Phone: 0207 8730 6200

*4. Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and businesses if different from question 3*

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Name: Walter Lilly & Co Ltd  
 Address: Knollys House, 17 Addiscombe Road, Croydon CRO 6SR  
 Email: contact@walterlilly.co.uk  
 Phone: 0207 8730 6200

*5. Please provide full contact details of the person responsible for community liaison/dealing with any complaints from local residents and businesses if different from question 3. In the case of Community Investment Programme (CIP), please provide contact details of the responsible Camden officer*

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Name: Walter Lilly & Co Ltd  
 Address: Knollys House, 17 Addiscombe Road, Croydon CRO 6SR  
 Email: contact@walterlilly.co.uk  
 Phone: 0207 8730 6200

*6. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP*

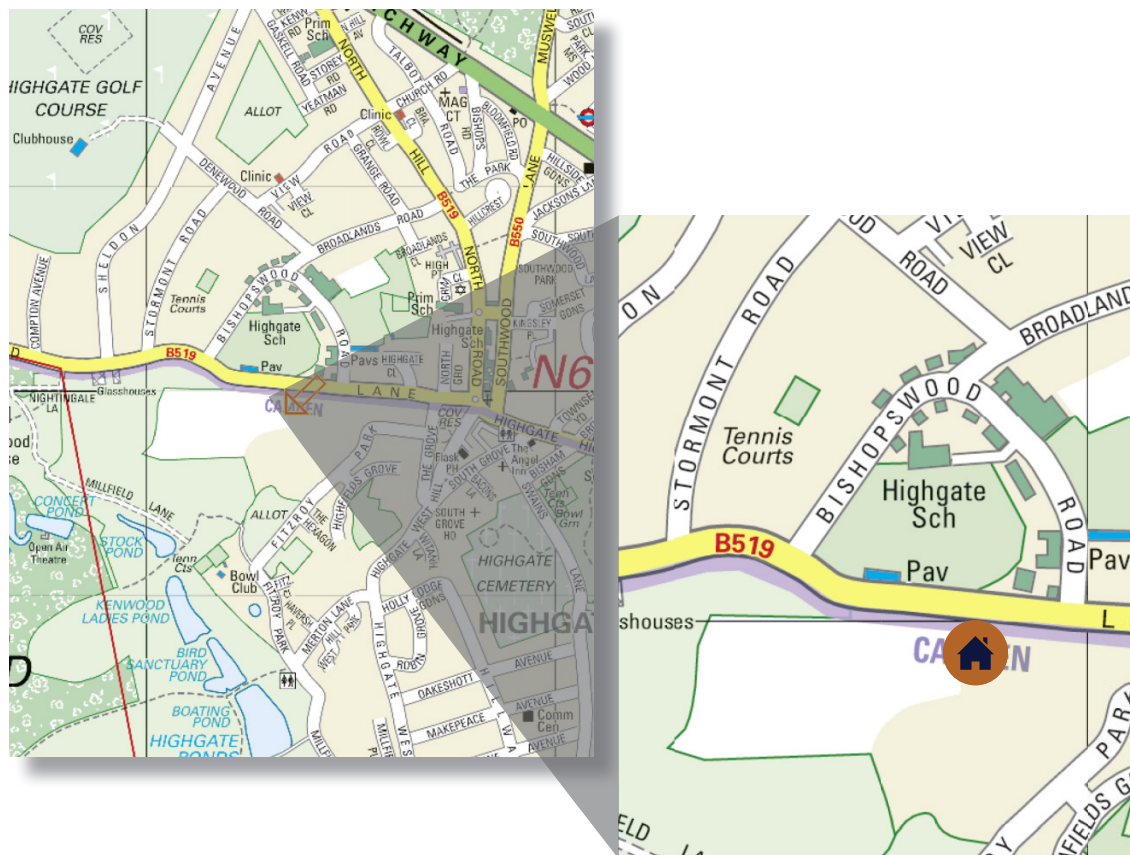
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Name: Walter Lilly & Co Ltd  
 Address: Knollys House, 17 Addiscombe Road, Croydon CRO 6SR  
 Email: contact@walterlilly.co.uk  
 Phone: 0207 8730 6200



## 5 SITE

1. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies



The Athlone House Estate site is situated within its own Private Open Space off Hampstead Lane (B519) within a residential area backing on to Hampstead Heath and the Kenwood House Estate to the west.

The site is in close proximity to Caenwood Court residential apartment blocks shown to the right of the location plan above. The site currently shares an entrance driveway with Caenwood Court.

The site is opposite Highgate School and playing fields.

The site is situated approximately 1km from the village of Highgate to the east of the site within the London Borough of Camden and is within the Metropolitan Open Land and Highgate Conservation Area.

The development for extension, restoration and refurbishment of Athlone House; extension and refurbishment Caen Cottage; and the refurbishment of the Gate House.

*2. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings)*

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The existing building is to be carefully restored with remodelling to extend the building in a contemporary fashion but one that pays homage to the original architectural fabric of the building.

The proposed development would be in the region of 27,877 sq. ft. This will include a new contemporary leisure wing housing a pool, spa and gym. The proposed Summer House would measure 1,872 sq. ft. Caen Cottage would measure 2,421 sq. ft. The Gate House would measure 990 sq. ft.

*3. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting, etc.)*

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The site is situated within a residential area and opposite Highgate School and playing fields. Caenwood Court is within close proximity with a shared access off Hampstead Lane.

The village of Highgate with residential dwellings, shops and businesses lies to the east of the site within a distance of approximately 1km.

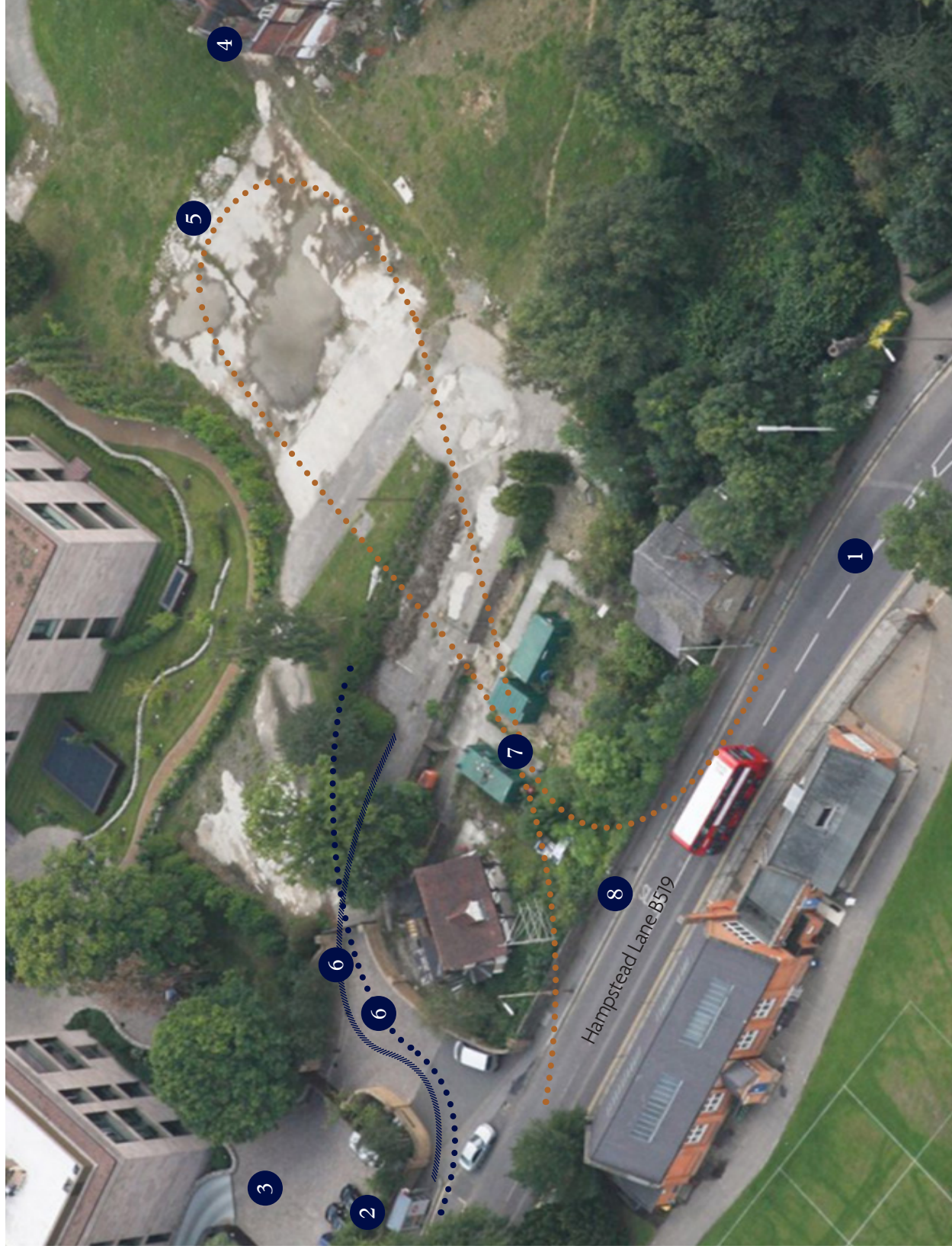
Kenwood House and Gardens (English Heritage) lies to the west of the site open to visitors and tourists.

*4. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations*

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Please see proposed site plan overleaf.





- 1 B519 Hampstead Lane
- 2 Bus stop route 210
- 3 Caenwood Court
- 4 Athlone House
- 5 Turning circle on site
- 6 Option 1: Access Route
- 7 Option 2: Access Route
- 8 Pedestrian route

The site is located off the B519 Hampstead Lane. There are double yellow line restrictions either side of the road. There is a 20 mph speed restriction in place for traffic. There is a public footpath either side of the road. Currently the proposed site has a shared access with Caenwood Court a residential development that is adjacent to the site. It is proposed that a new primary site entrance is set up in the location shown above with a turning circle for vehicle traffic within the confines of the site. There is a bus stop (Route 210) located near the site entrance for pedestrian access to the site.

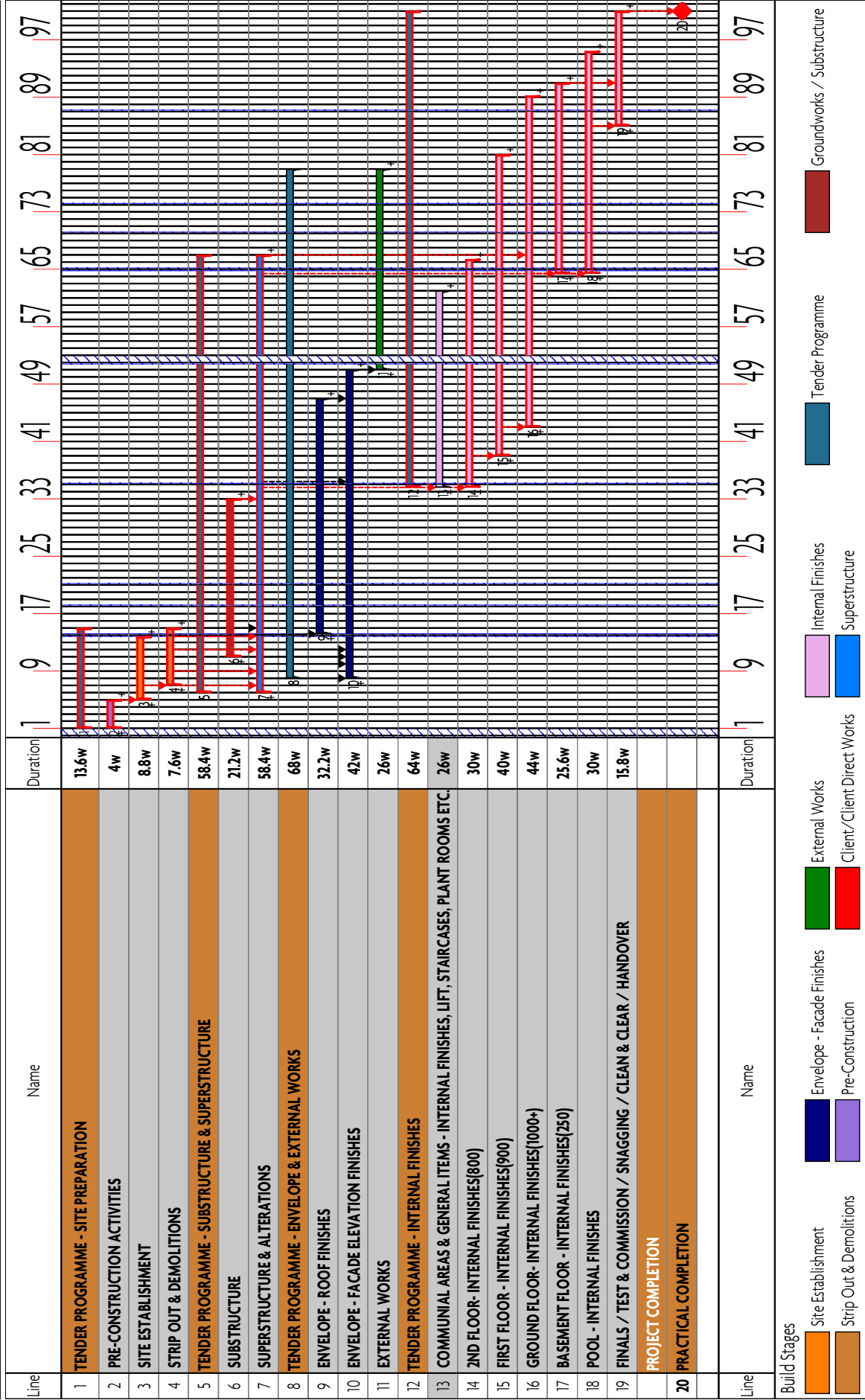


WALTER LILLY

*5. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be ideal)*

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Please see proposed construction programme overleaf.



6. Please confirm the standard working hours for this site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
  - 8.00am to 1.00pm on Saturdays
  - No working on Sundays or Public Holidays
- 

The site will be fully compliant to the times stated above for working hours in accordance with the London Borough of Camden requirements.

7. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT. etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions

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New incoming utility services will be installed under the proposed scheme. It will be the intention where possible to share the same excavation and traffic management proposals to reduce the impact on the environment and works on the public highway.



## 6 COMMUNITY LIAISON

**A neighbourhood consultation process must have been undertaken prior to submission of the CMP first draft.**

Significant time savings can be made by running an effective neighbourhood consultation process. This must be undertaken in the spirit of cooperation rather than one that is dictatorial and unsympathetic to the well-being of local residents and businesses.

These are most effective when initiated as early as possible and conducted in a manner that involves the local community. Involving locals in the discussion and decision making process helps with their understanding of what is being proposed in terms of the development process. The consultation and discussion process should have already started, with the results incorporated into the CMP first draft submitted to the Council for discussion and sign off. This communication should then be ongoing during the works, with neighbours and any community liaison groups being regularly updated with programmed works and any changes that may occur due to unforeseen circumstances through newsletters, emails and meetings.

Please note that for larger sites, details of a construction working group may be required as a separate S106 obligation. If this is necessary, it will be set out in the S106 Agreement as a separate requirement on the developer.

### **Cumulative impact**

Sites located within high concentrations of construction activity that will attract large numbers of vehicle movements should consider establishing contact with other sites in the vicinity in order to manage traffic routing and volumes. Developers in the Tottenham Court Road area have done this to great effect.

**The Council can advise on this if necessary.**

#### *1. Consultation*

*The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents prior to submission of the first draft CMP.*

*Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. Details of meetings including minutes, lists of attendees etc. must be included.*

*In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason should be given. The revised CMP should also include a list of all the comments received.*

*Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.*

*Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.*

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Consultation will take place as the scheme is developed with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.



## 2. Construction Working Group

*The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents prior to submission of the first draft CMP.*

*Evidence of who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. Details of meetings including minutes, lists of attendees etc. must be included.*

*In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason should be given. The revised CMP should also include a list of all the comments received.*

*Developers are advised to check proposed approaches to consultation with the Council before carrying them out. If your site is on the boundary between boroughs then we would recommend contacting the relevant neighbouring planning authority.*

*Please provide details of consultation of draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.*

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During the pre-construction stage, prior to commencement of works on site, community liaison following appointment between Walter Lilly as principle contractor and the local community will take place.

The resident site Project Manager will be liaising on a day to day basis as required with the local community and residents.

Regular newsletters will be produced during the Construction stage to keep neighbours advised of future events, general progress of the works and the requirements of any abnormal works.

## 3. Schemes

*Please provide details of any schemes such as the ‘Considerate Constructors Scheme’, such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the “Guide for Contractors Working in Camden” also referred to as “Camden’s Considerate Contractors Manual”.*

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Walter Lilly will register and comply with the requirements of the Considerate Constructors Scheme for the duration of the project.

The works will be carried out in accordance with the Considerate Constructors Scheme and in such a way as to minimise the impact on the local environment and amenities.

A contact board will be displayed outside the site providing contact details. This will include names and telephone numbers of key construction staff so that neighbours and the general public can contact us should they have cause to do so. This will include 24 hour contact details.

A complaints / contact book will be kept on site, which will be used to record details of any complaints. This will include the name of the person making the complaint, the date, time and nature of the complaint and the action necessary to resolve the complaint. The complaints book will be regularly reviewed to ensure that any complaints are dealt with and resolved promptly.

#### 4. Neighbouring sites

*Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site. The council can advise on this if necessary.*

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Wates Construction has an existing development east of the site off Hampstead Lane. Consideration will be given to mitigate the accumulative impact of the construction activities.

The site is within a residential area. Maintaining good neighbourly relations is assisted greatly by good communication, and by keeping third parties regularly informed of the site activities which are likely to impact on adjoining residents or sites. Walter Lilly have found that listening to reasonable concerns and demonstrating a considerate and professional approach will always maintain a well-balanced relationship.

Regular newsletters will be produced to keep neighbours advised of future events, general progress of the works and the requirements for any abnormal works.

Appropriate signage and information boards will be displayed on site hoardings.

## 7 TRANSPORT

**This section must be completed in conjunction with your principal contractor. If one is not yet assigned, please leave the relevant sections blank until such time when one has been appointed.**

Camden is a CLOCS Champion, and is committed to maximising road safety for Vulnerable Road Users (VRUs) as well as minimising negative environmental impacts created by motorised road traffic. As such, all vehicles and their drivers servicing construction sites within the borough are bound by the conditions laid out in the CLOCS Standard.

This section requires details of the way in which you intend to manage traffic servicing your site, including your road safety obligations with regard to VRU safety. It is your responsibility to ensure that your principal contractor is fully compliant with the terms laid out in the CLOCS Standard. It is your principal contractor's responsibility to ensure that all contractors and sub-contractors attending site are compliant with the terms laid out in the CLOCS Standard.

Checks of the proposed measures will be carried out by the council to ensure compliance. Please refer to the CLOCS Standard when completing this section. Guidance material which details CLOCS requirements can be accessed [here](#), details of the monitoring process are available [here](#).

Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.

**Please refer to the CLOCS Overview and Monitoring Overview documents which give a breakdown of requirements.**

### CLOCS CONSIDERATIONS

#### *1. Name of Principal contractor*

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Walter Lilly & Co Ltd  
Knollys House, 17 Addiscombe Road, Croydon CRO 6SR

#### *2. Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our CLOCS Overview document in the appendix and CLOCS Standard point 3.4.7)*

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All vehicles on arrival will be checked for compliance under the CLOCS standards.

Regular audits will be undertaken with random vehicle compliance checks and at each reporting period a summary of the checks undertaken and any corrective action taken for non-compliance.

#### *3. Please confirm that you as the client/developer and your principal contractor have read and understood the CLOCS Standard and included it in your contracts. Please sign-up to join the CLOCS Community to receive up to date information on the standard by expressing an interest online.*

*I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers*

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*Please contact [CLOCS@camden.gov.uk](mailto:CLOCS@camden.gov.uk) for further advice or guidance on any aspect of this section.*

## SITE TRAFFIC

**Sections below shown in blue directly reference the CLOCS Standard requirements. The CLOCS Standard should be read in conjunction with this section.**

*4. Traffic routing: “Clients shall ensure that a suitable, risk assessed vehicle route to the site is specified and that the route is communicated to all contractors and drivers. Clients shall make contractors and any other service suppliers aware that they are to use these routes at all times unless unavoidable diversions occur.” (P19, 3.4.5)*

*Routes should be carefully considered and risk assessed, taking into account the need to avoid where possible any major cycle routes and trip generators such as schools, offices, public buildings, museums etc. Where appropriate, on routes that use high risk junctions (ie. those that attract high volumes of cycling traffic) installing Trixi mirrors to aid driver visibility should be considered.*

*Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. The route(s) to and from the site should be suitable for the size of vehicles that are to be used.*

*a. Please indicate routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of links to the Transport for London Road Network (TLRN).*

*b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.*

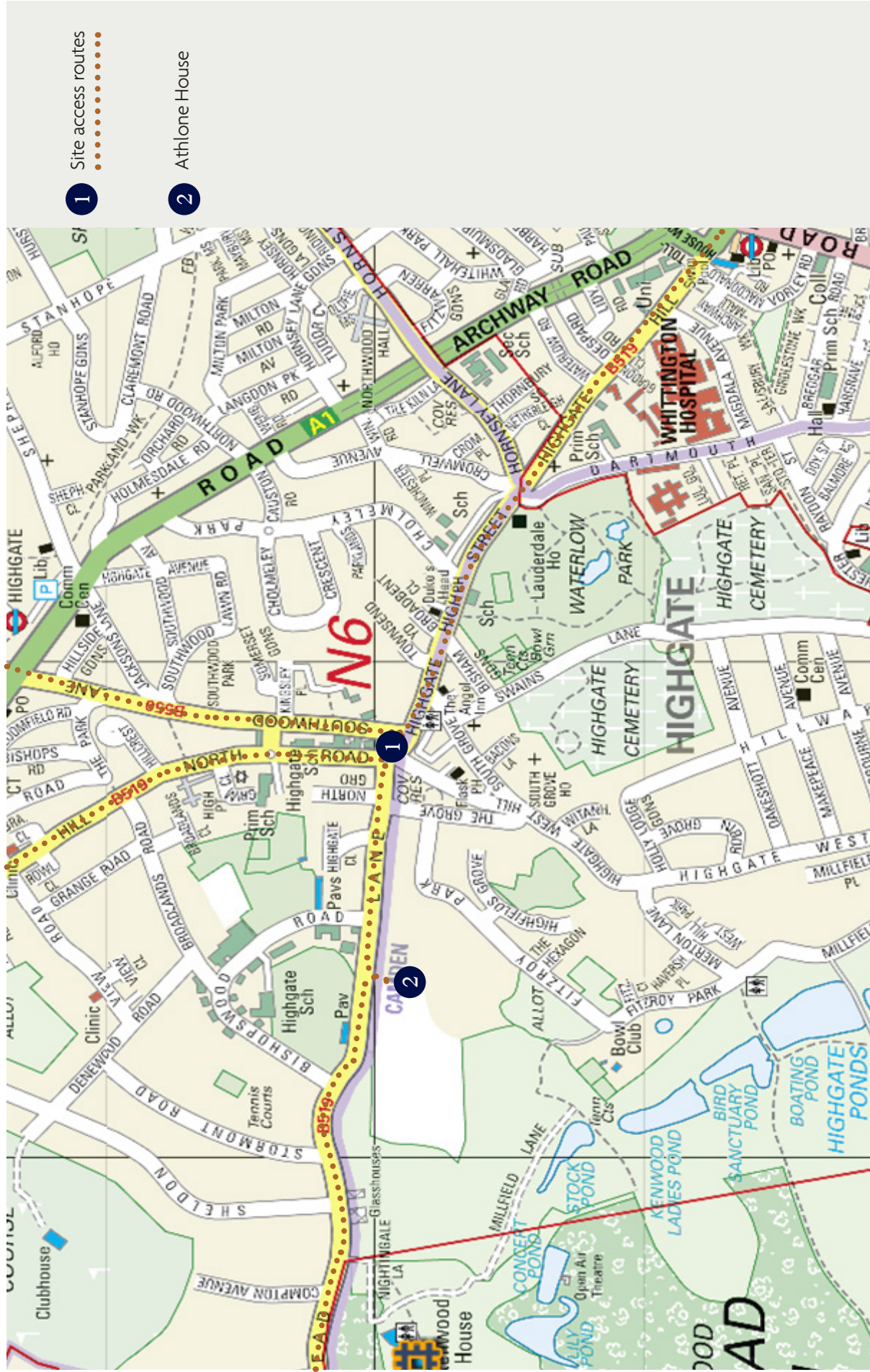
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a. Please refer to site route map overleaf.

b. All contractors and suppliers will be informed at the pre-construction stage and made aware of the routes and any on site restrictions as part of their order.

Appropriate signage will be displayed as permitted by the Local Authority of the London Borough of Camden.





The primary route to the site would be via the A1-B519 Hampstead Lane Road networks. Archway and Highgate tube stations are within a 2 mile of the site with the bus route 210 from Archway station



5. Control of site traffic, particularly at peak hours: “Clients shall consider other options to plan and control vehicles and reduce peak hour deliveries” (P20, 3.4.6)

*Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. (Refer to the Guide for Contractors Working in Camden).*

*A delivery plan should ensure that deliveries arrive at the correct part of site at the correct time. Instructions explaining such a plan should be sent to all suppliers and contractors. Consideration should be given to the location of any necessary holding areas for large sites with high volumes of traffic. Vehicles must not wait or circulate on the public highway. Whilst deliveries should be given set times to arrive, dwell and depart, no undue time pressures should be placed upon the driver at any time.*

*a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.*

*b. Please provide details of other developments in the local area or on the route.*

*c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of site at the correct time.*

*d. Please identify the locations of any off-site holding areas (an appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected) and any measures that will be taken to ensure the prompt admission of vehicles to site in light of time required for necessary compliance checks. Please refer to question 5 if any parking bay suspensions will be required for the holding area.*

*e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of construction material consolidation centres).*

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a. Please refer to traffic movement graph overleaf.

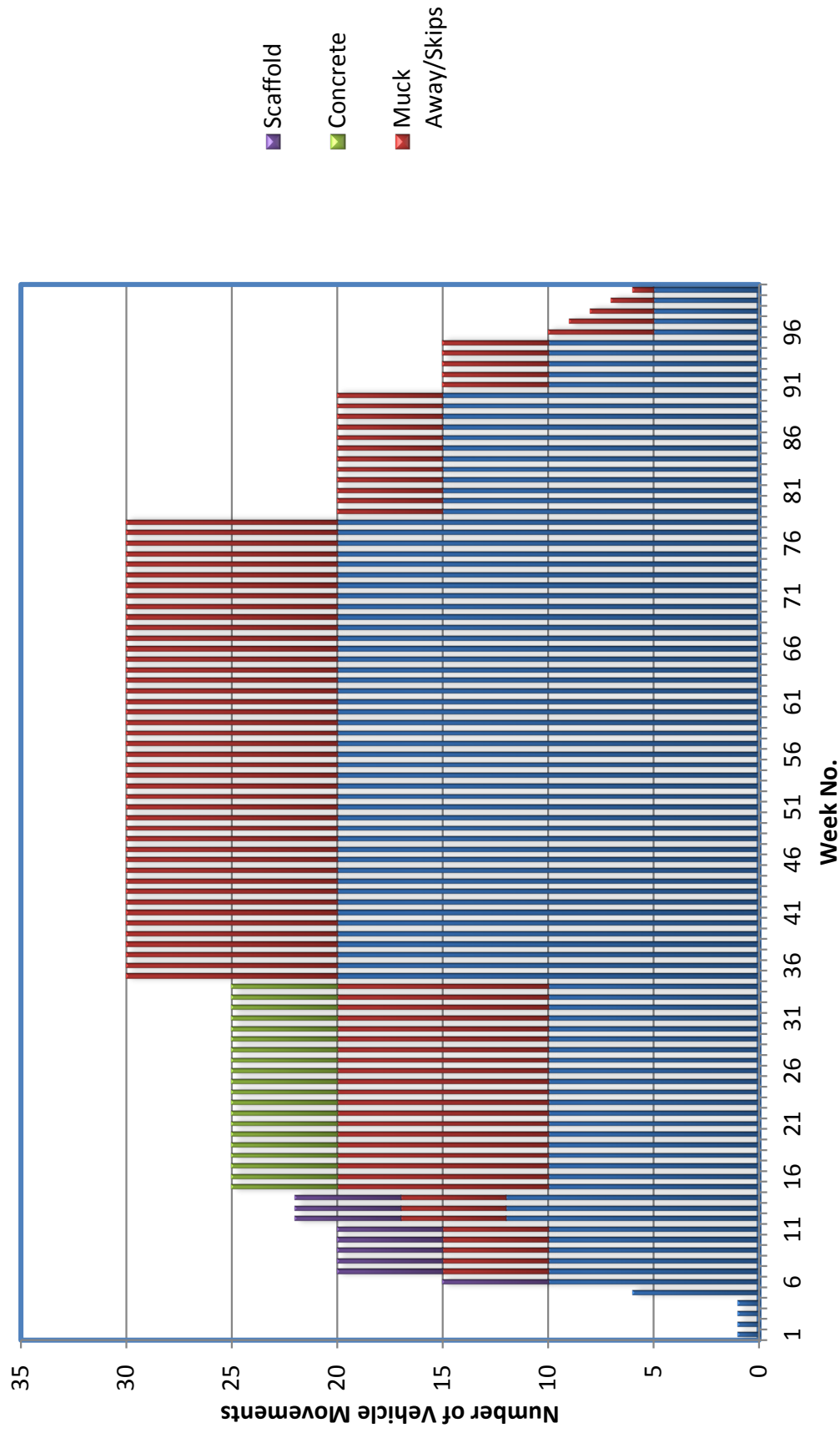
b. There is a Wates Construction development to the east of the proposed site location.

c. Deliveries will be managed on a ‘just-in-time’ basis. Deliveries will be carefully planned, pre-booked and managed on site to ensure no back up of vehicles on Hampstead Lane B519 or the adjacent roads and timed to minimise disruption to neighbours.

d. No parking restrictions are required. The intention is to contain all deliveries within the site demise with carefully plan deliveries to minimise disruption to neighbours. Suitable on site storage will be provided as part of the site establishment and logistic plan

VEHICLE MOVEMENTS

# Athlone House London NW3



## 6. Site access and egress

*“Clients shall ensure that access to and egress from the site is appropriately managed, clearly marked, understood and clear of obstacles.” (P18, 3.4.3)*

*Vehicles entering and leaving the site should be carefully managed, using gates that are clearly marked and free from obstacles. Traffic Marshalls must ensure the safe passage of pedestrians, cyclists and other traffic when vehicles are entering and leaving site, particularly if reversing.*

*a. Please detail the proposed access and egress routes to and from the site*

*b. Please describe how the access and egress arrangements for construction vehicles will be managed.*

*c. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).*

*d. Provision of wheel washing facilities should be considered if necessary. If so, please provide details of how this will be managed and any run-off controlled.*

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As mentioned above. Deliveries will be managed on a ‘just-in-time’ basis. Deliveries will be carefully planned, pre-booked and managed on site to ensure no back up of vehicles on Hampstead Lane B519 or the adjacent roads and timed to minimise disruption to neighbours.

Numbers of deliveries will be carefully controlled on a daily basis to minimise disruption to neighbours.

a. Please refer to proposed access and egress routes plan overleaf.

b. All deliveries to site will be undertaken with full regard paid to:

- Reduction and control of plant movements
- Reversing vehicles directed by a Competent Person
- Pedestrian and vehicle directional signage – suitable barriers will be erected when deliveries arrive to prevent pedestrians accessing the unloading area.
- Mobile plant will only be operated by a competent person with a banks person in attendance to any movements

Consultation with the London Borough of Camden will continue throughout the project to ensure:

- Construction methods minimise the potential impact on nearby residents
- Maintenance of the existing public highway
- Segregation of all pedestrians, public or employees, on or in the vicinity of the site

c. Swept paths will be developed if required following confirmation of proposed site entrance location. This is shown on the attached site access plan AH/CMP/SLP/03 overleaf.

d. Wheel wash facilities will be used during the substructure and ground works phases to minimise the effects of the construction works.

This will be managed by dedicated site personnel on every departure.

The run- off will be controlled and contained. No effluent/waste will enter the public sewer