



51 -52 Tottenham Court Road, London

Design and Access Statement

For Dome Assets Limited

Document History

Rev	Date	Purpose of Issue	Author	Reviewer
A	26.02.16	Client Issue	LLo	PS
B	10.03.16	Planning Issue	AL	PS

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1.0 Development Framework

1.1 Introduction

1.1.1 Squire and Partners have been appointed by Dome Assets Limited to submit a detailed planning application for a mixed use office, residential and retail scheme at 51-52 Tottenham Court Road, London, W1TH 2EH.

1.1.2 The existing buildings include residential and office use at the higher levels and retail units at ground and basement levels. The proposals seek to refurbish and extend the buildings by redistributing its land use and re-incorporating residential, office and retail uses. The existing building contains a GEA total of 845sqm. The proposed scheme involves a maximum GEA of 1,602sqm which has been investigated in various options of retail, office and residential separation.

1.1.3 The site lies on the southern end of Tottenham Court Road, between Goodge Street Station and Tottenham Court Road Station. The site contains two existing buildings that front onto Tottenham Court Road; both buildings have been extended to the rear in different degrees and scales at different points in time. The rear boundaries in both cases are immediately adjacent and overshadowed by the relatively new office development at 6-8 Whitfield Street. There is no physical link between nos. 51 and 52 at present. No. 51 is the earliest of the two, possibly dating back to the mid 19th century; no. 52 dates back to the late 19th century. Both buildings are brick fronted and form part of a continuous built frontage, with no. 51 including a slate clad mansard roof.

1.1.3 The site is located in the Charlotte Street Conservation Area, with the Bloomsbury Conservation Area to the east. There are a number of nearby listed buildings along Tottenham Court Road, in addition to several buildings identified as making a positive contribution to the Charlotte Street Conservation Area. Therefore, the proposed development is sensitive to the heritage context and seeks to contribute to the character of the Conservation Area. The proposals intend to improve the streetscape as a whole by introducing a new high quality mixed use development that meets the key planning policies in Camden.

1.1.4 This Design and Access Statement covers the planning application proposals following an initial pre-application meeting with Camden Council on 5th February 2015. The proposals included in this application have evolved and improved considerably taking on board Camden Council's comments. This statement sets out the constraints and opportunities on this site, the design evolution of our proposals and the technical considerations that have been taken into account.

1.2 Design Team

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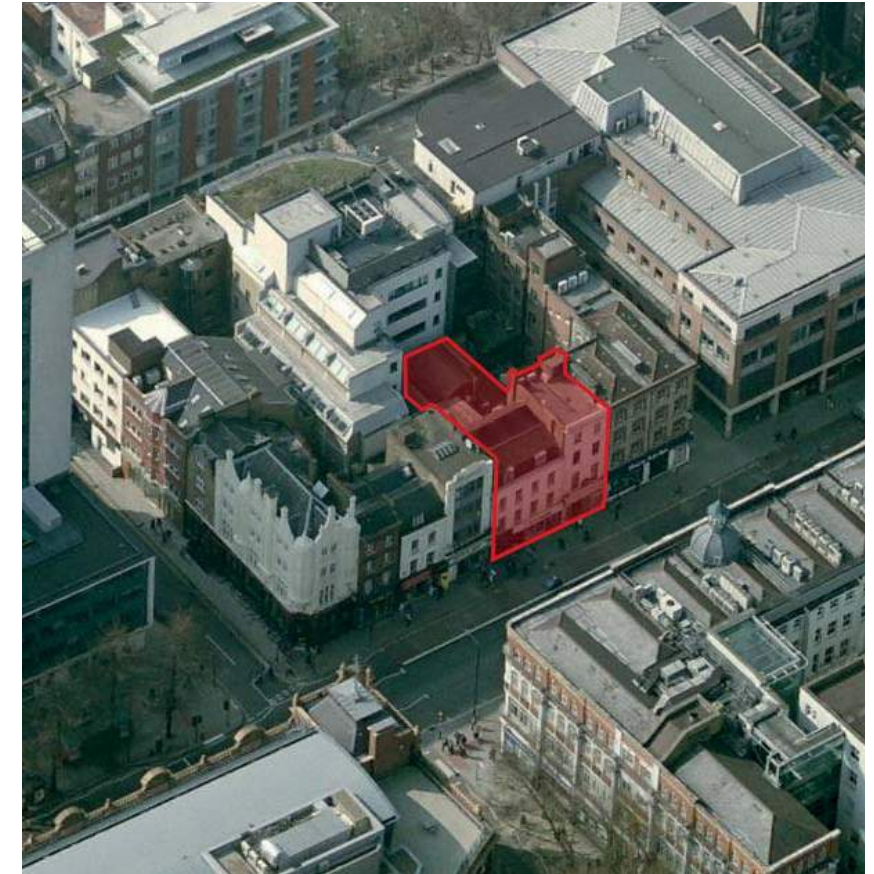


Fig: 1.1 Aerial view of 51-52 Tottenham Court Road



Fig: 1.2 Site Plan

2.0 Context Analysis

2.1 Site Location

2.1.1 Tottenham Court Road lies to the south-east of the London Borough of Camden close to the border with the Borough of Westminster. It runs north-south as a main thoroughfare between Tottenham Court Road Station on Oxford Street and Warren Street Station on Euston Road. The west elevation of Tottenham Court Road constitutes the eastern border of the Charlotte Street Conservation Area. The street is a significant retail centre, best known for its high concentration of consumer electronics shops alongside furniture retailers, a trade that became established in the area since the 19th century.

2.1.2 51-52 Tottenham Court Road are two distinct buildings on the west elevation of the street, standing 150m south of Goodge Street Station. They form part of a continuous frontage that runs between Windmill Street and Kirkman Place. Both rear facades face the back of a large office block that extends 6-10 Whitfield Street. No. 51 has a flying freehold over no. 50 from first floor and above. On ground floor, a shed in the alley can be observed through the gates on Tottenham Court Road. This forms part of the former route into Red Lion Yard, which no longer exists, and it is not in the ownership of the applicant.

2.2 Site Access

2.2.1 The site has excellent public transport connections. It is within two minutes walk from Goodge Street Station which provides a link to the Northern line, within five minutes walk from Tottenham Court Road Station which connects with the Central and Northern lines. To the north, within fifteen minutes walk, Euston Station provides link to the Northern and Victoria lines; in addition to the Euston Railway Station which connects London with the West Midlands, and the North West. An extension and upgrade to Tottenham Court Road Station is currently under development as part of Crossrail due to open in 2018.

2.2.2 In addition to all surrounding stations, there are several bus routes that regularly run along Tottenham Court Road. These routes connect the site to areas of London that currently do not have good underground links.

2.2.3 Pedestrians have access to the site via the Tottenham Court Road frontage. The proposals include cycle storage and refuse access also on Tottenham Court Road.

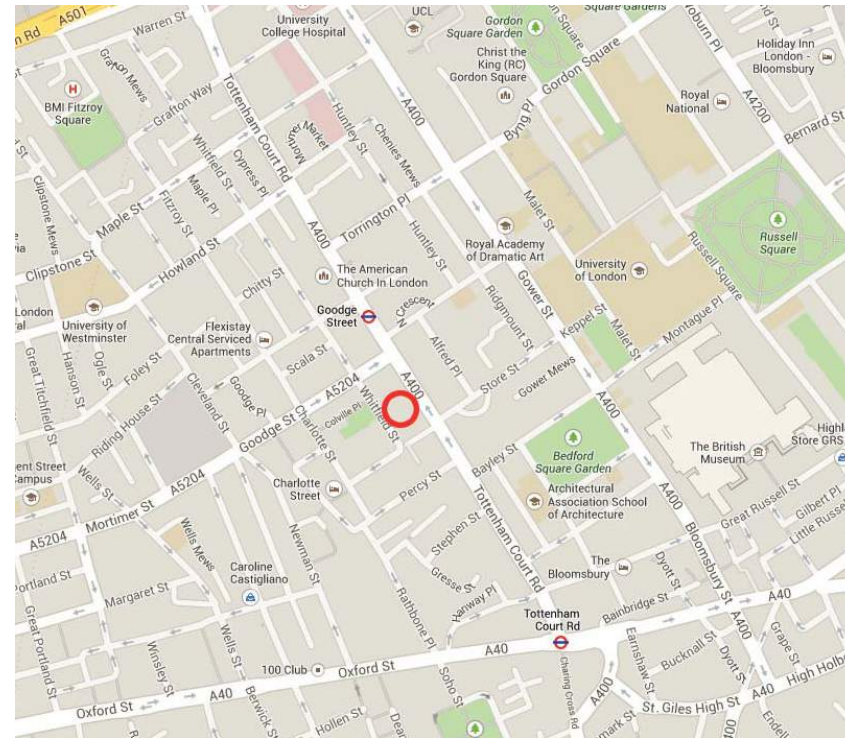


Fig 2.1 Location of no. 51-52 Tottenham Court Road



Fig 2.2 Site Aerial Photograph

2.3 Environmental Analysis

2.3.1 Sun Path

The Site orientates itself along a north-east / south-west axis, with the primary frontage facing north-east onto Tottenham Court Road. East sunlight penetrates onto the front facade during the mornings, while west sun penetrates the rear facade in the afternoon. The proposed terraces to the rear of the buildings will benefit from the afternoon sun, especially at the upper levels.

2.3.2 Wind

The prevailing wind at the site comes from the southwest as shown in Fig: 2.5. As the buildings are mechanically ventilated in the most part, this will have little bearing on the proposals for openable windows. Nor is the buildings' effect on the wind turbulence at street level likely to have any effect, as the proposed heights are clearly appropriate in this location.

2.3.3 Noise and Traffic

Tottenham Court Road is both a major pedestrian and vehicular route, contributing to the main amount of noise and air pollution around the site. However, the residential use in no. 51 is already established and the proposals will not cause any further detrimental impact on the amenity of residential occupants. Where appropriate, the proposals locate main living rooms facing Tottenham Court Road, while keeping bedrooms to the rear of the site.

The office building will include air filters on all mechanical ventilation systems to avoid any air pollution having any impact on the internal environment.

It is important that light pollution in the street is also considered, and the introduction of some form of shutters within the facade could be considered for the residential building.



Fig: 2.3 Sun path diagram



Fig: 2.4 Wind diagram



Fig: 2.5 Noise analysis diagram



Fig: 2.6 Traffic analysis diagram

2.4 Aerial Photographs

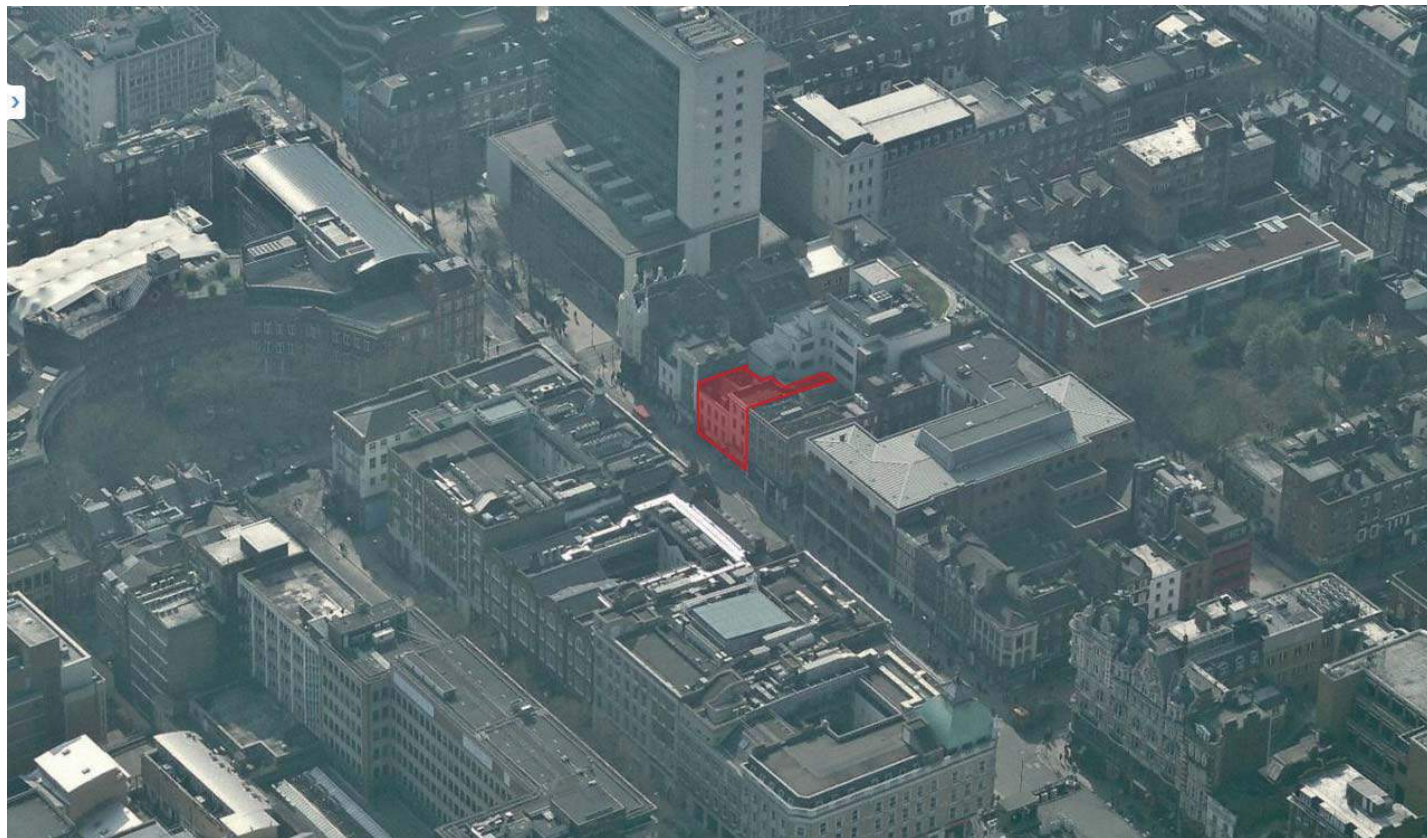


Fig: 2.7 Aerial Photograph from north

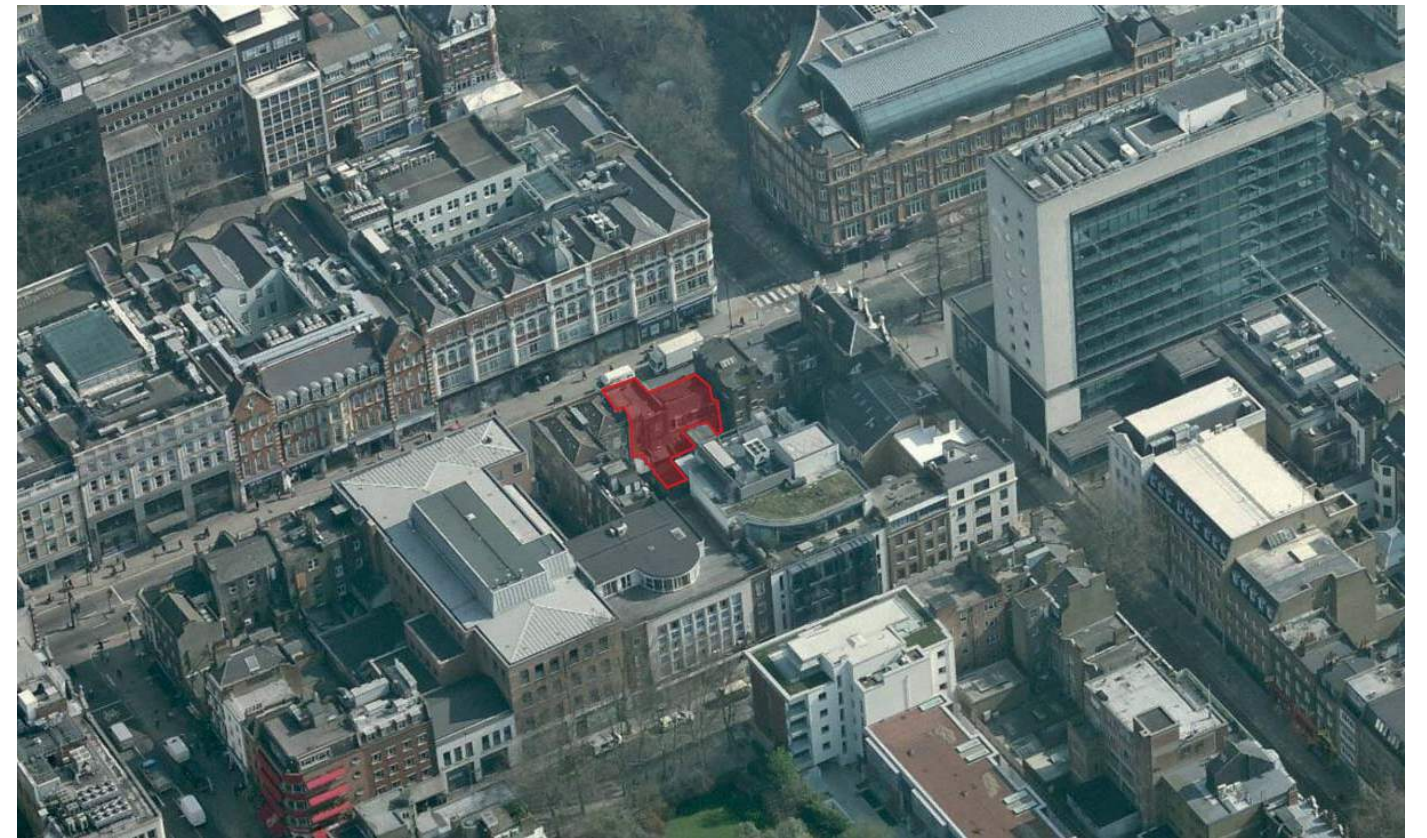


Fig: 2.8 Aerial Photograph from west

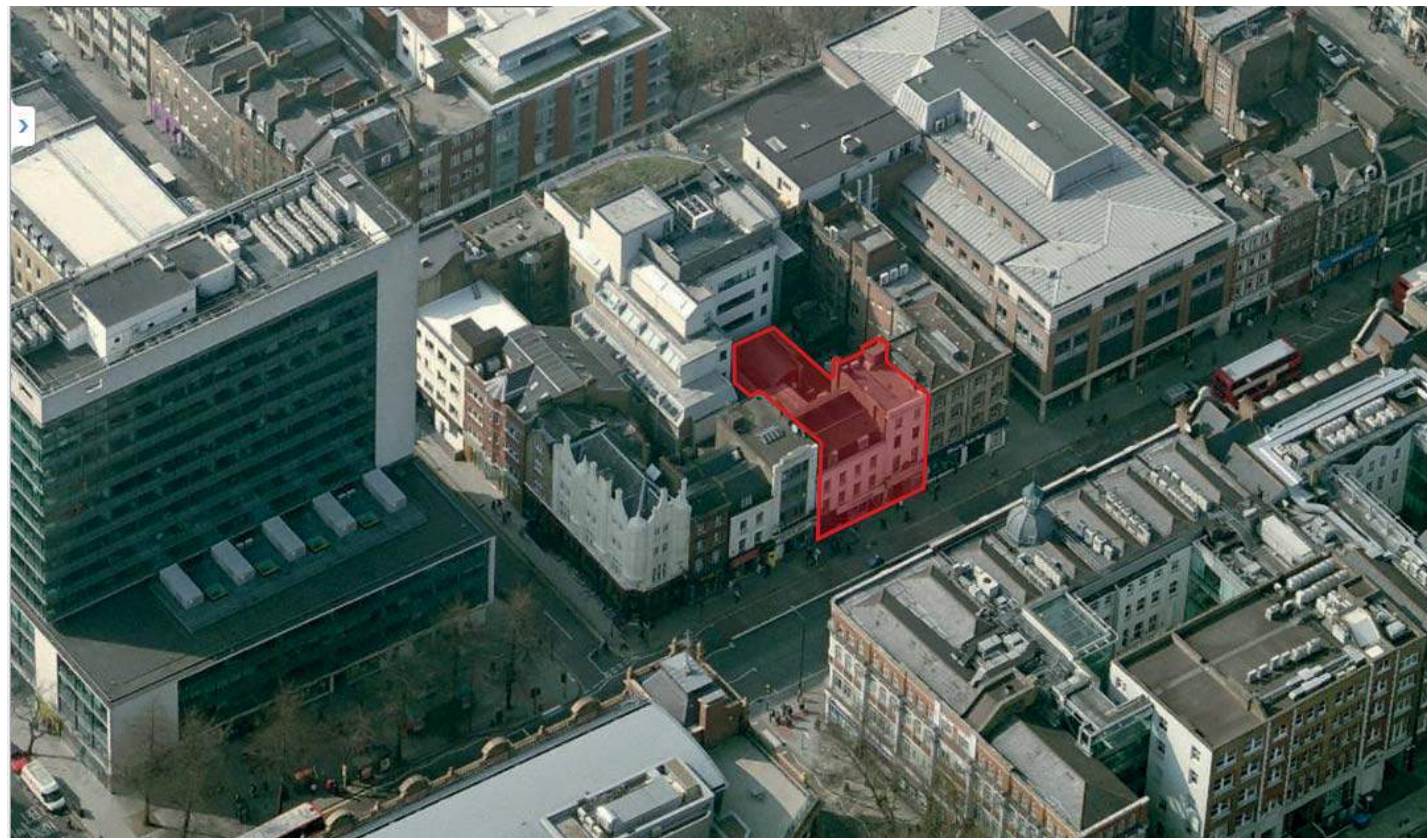


Fig: 2.9 Aerial Photograph from east

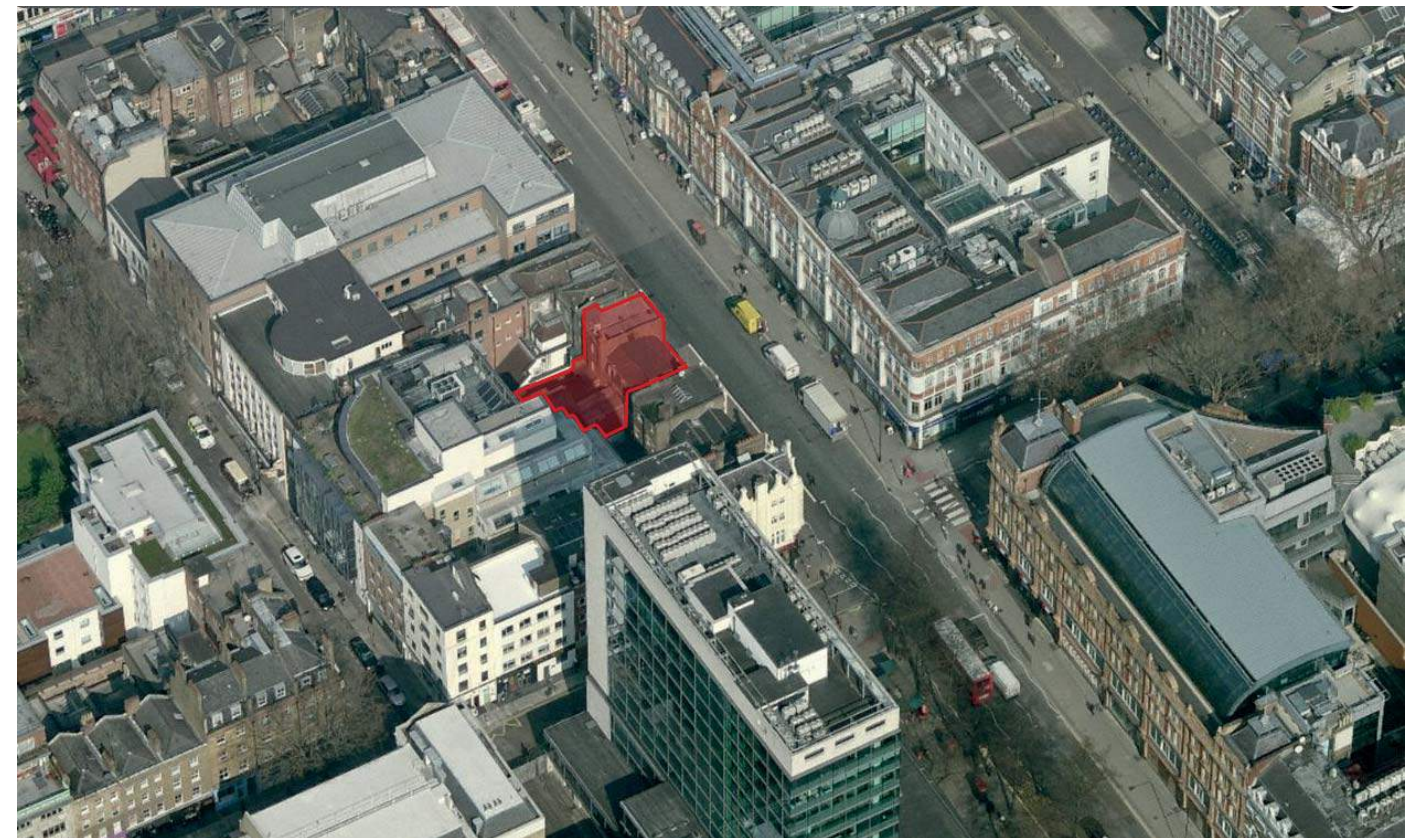


Fig: 2.10 Aerial Photograph from south

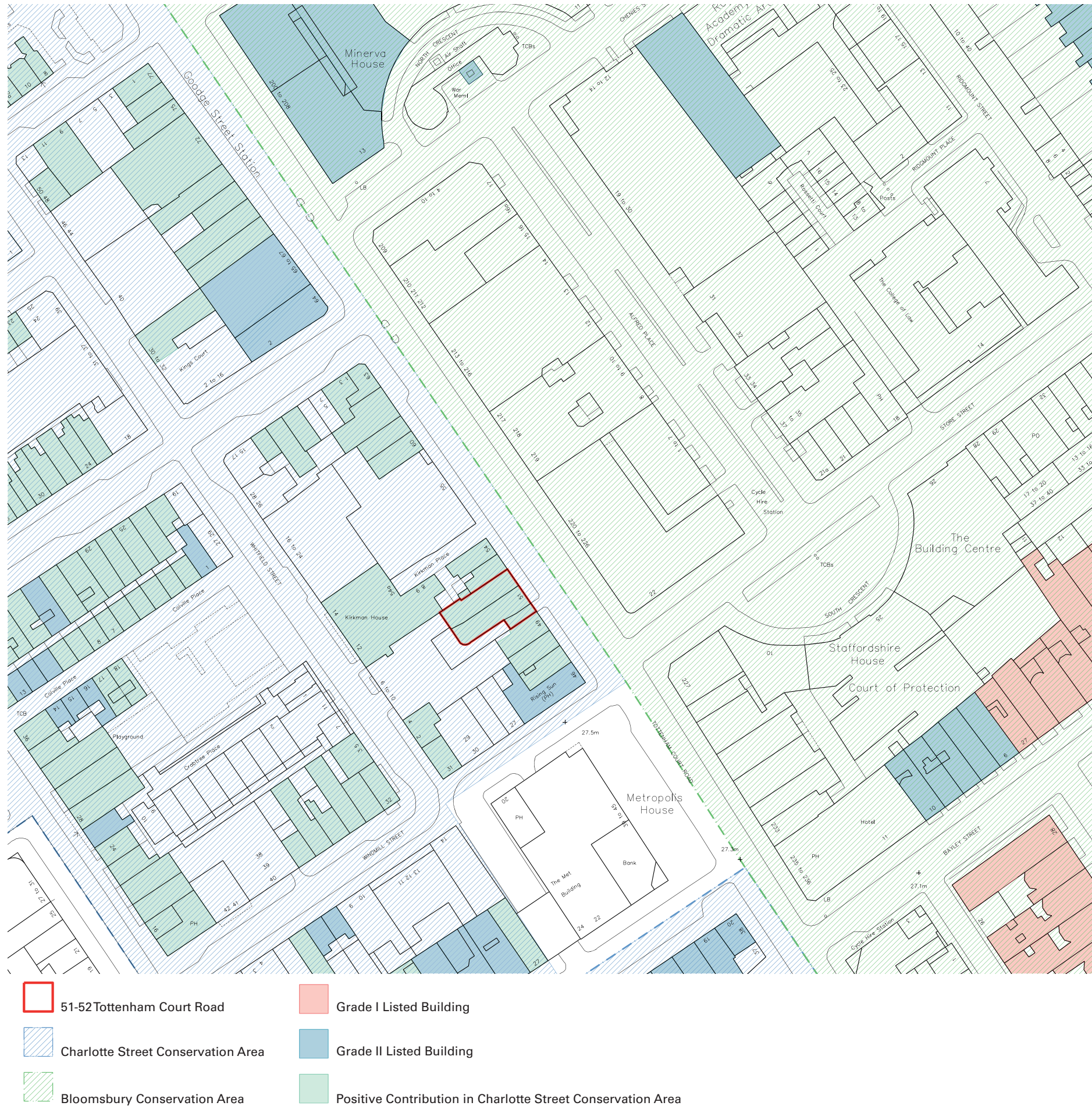


Fig 2.11 Conservation Areas and listed buildings

2.5 Conservation Areas

2.5.1 The Charlotte Street Conservation Area

51-52 Tottenham Court Road lie on the eastern edge of the Charlotte Street Conservation Area. The Conservation Area was designated in March 1974 and has been extended several times. The Site has only been included within the conservation area since its most recent extension in November 1999. The boundary of the Conservation Area runs along Tottenham Court Road. Opposite the Site is Bloomsbury Conservation Area, designated in 1968 and subsequently extended several times.

2.5.2 The area is recognised to be of special historic and architectural interest that is worthy of protection. (See Fig: 2.3). This means that in accordance with Government guidance and Camden's Policy DP25, the council will normally expect all buildings that make a positive contribution to the conservation area to be retained, as a measure to preserve or enhance the area's character. The application is subject to the Charlotte Street Conservation Area Appraisal and Management Strategy in relation to guidance for the types of alterations and developments that are acceptable in the conservation area.

2.5.3 Listed Building and Positive contribution to the Conservation Area

Neither of the buildings on the Site are listed, nor do they lie adjacent to any listed buildings. However, both no. 51 and 52 are listed as making a positive contribution to the character and appearance of the area, alongside various other buildings on the street.

Within the same urban block as nos. 51 and 52 on Tottenham Court Road, No. 46 - The Rising Sun, is grade II listed and dates from 1896. Further north, nos. 64-67 and nos. 2-8 Goadge Street is listed grade II, dating from 1903. There are other listed buildings in the wider area, but given the dense urban character of the area, none have a significant visual relationship to the Site.

2.6 Historic Built Environment

The following section contains extracts from Peter Stewart Consultancy's heritage report included as part of this planning application.

- 2.6.1 The buildings are located within Charlotte Street Conservation Area. This was first designated in 1974 and has been subsequently extended in 1981, 1985, and 1999. The Charlotte Street Conservation Area Appraisal and Management Plan was adopted by Camden Council on 24th July 2008. The Council's Appraisal noted that the area's special character derives from the *densely developed grid pattern of streets and limited open space* of terraced townhouse typical of Georgian London.
- 2.6.2 The area underwent a change in social status from the 1800s onwards, as wealthy residents moved west and the townhouses were subdivided into smaller dwellings, shops and other small scale businesses. The area adopted a character with a mix of residential commercial and retail uses. Many of the properties were re-fronted or altered during the 19th century with infill development and complete redevelopment occurring during the 20th century.
- 2.6.3 There are more modern office blocks and commercial buildings of a significantly larger scale than the original development, such as in Whitfield St. Redevelopment schemes continue as seen in the recent commercial block a few plots north of the Site on Tottenham Court Road.
- 2.6.4 Tottenham Court Road has a varied scale and character of development and eclectic townscape. The Council's Appraisal states that *Tottenham Court Road is notable for the variety of heights, building styles and materials along the frontage. The prevailing height is three and four storeys with a general pattern of vertically proportioned buildings on narrow plots and well-defined parapet at roof level... There are a range of materials including yellow and red brick, render and stone.*
- 2.6.5 Tottenham Court Road exhibits a very varied townscape, in terms of the age and scale of development, and has undergone significant phases of redevelopment over time. It is a major retail street in London and well known for furniture stores. The element that unifies its character are the ground floor retail uses. This type of development has been an integral part of its character, and provides a far more varied an eclectic townscape than the main body of the conservation area.

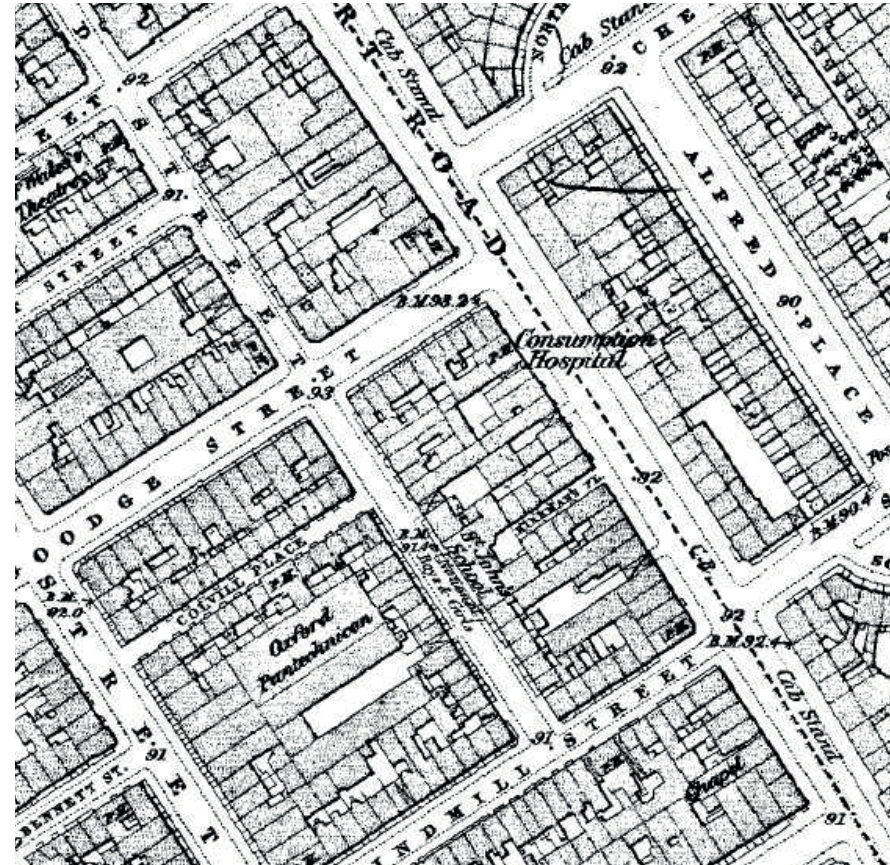


Fig. 2.12 Historic Map: OS 1875

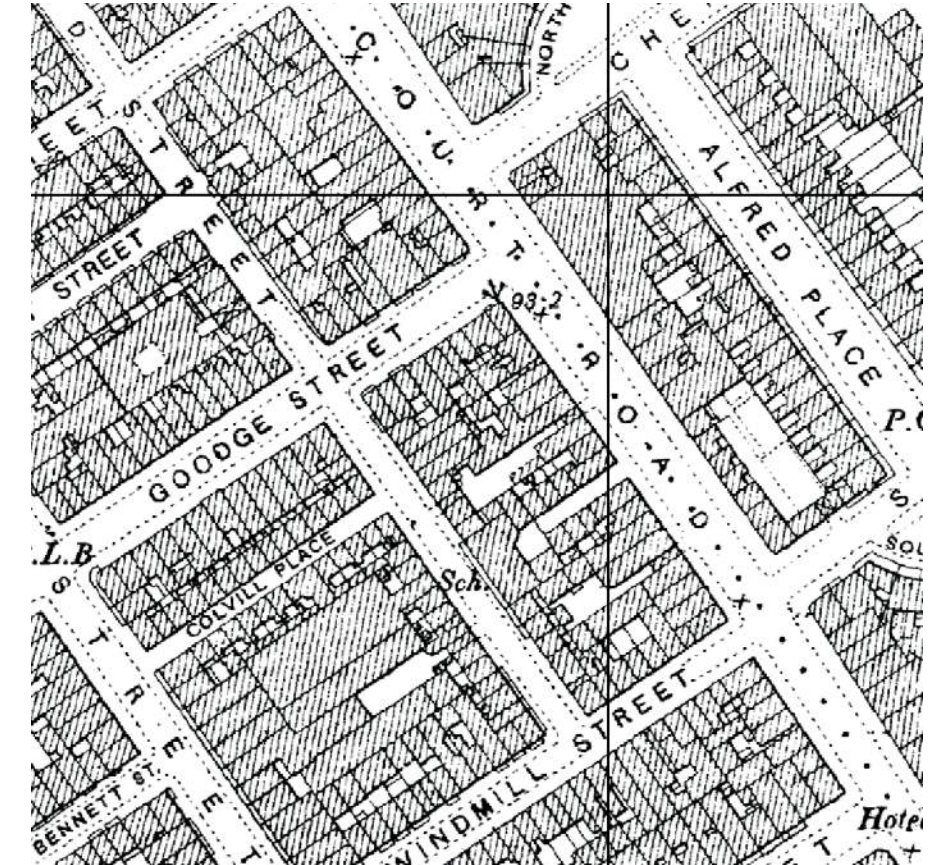


Fig. 2.13 Historic Map: OS 1896

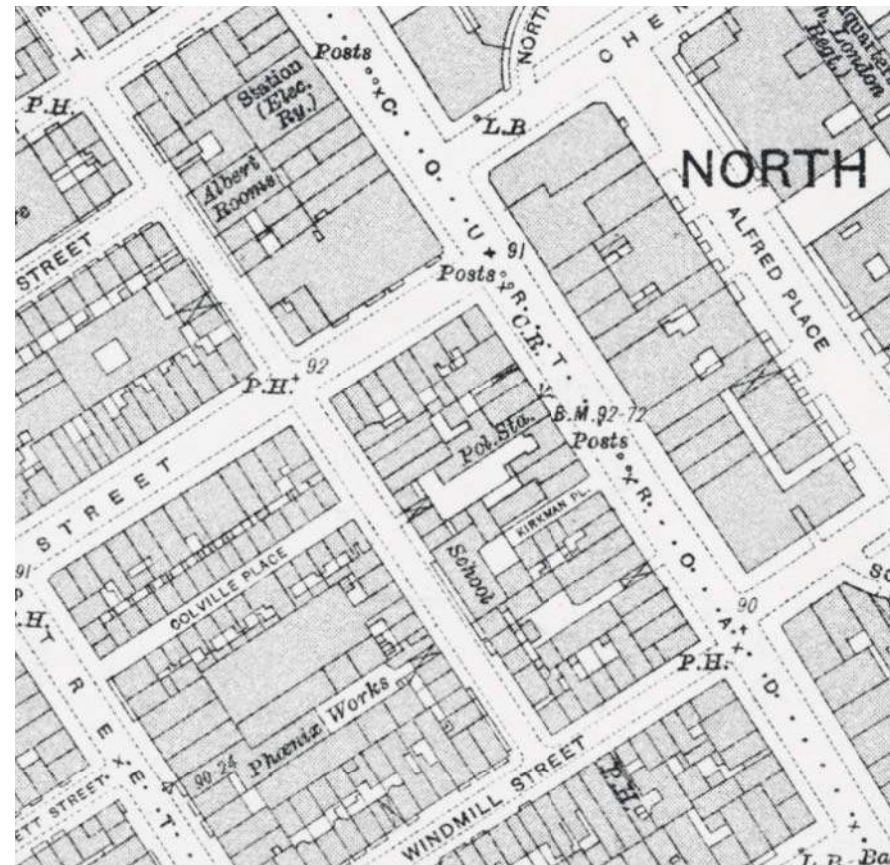


Fig. 2.14 Historic Map: OS 1914



Fig. 2.15 Historic Map: OS 1954



Fig: 2.16 Totten Court called Manor House Totten Hall, 1830

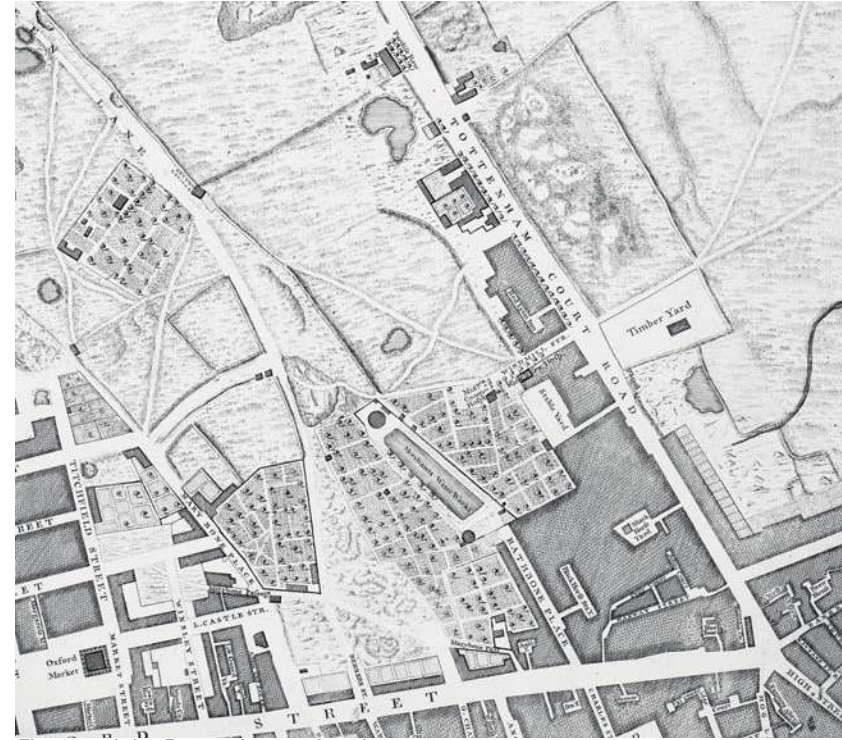


Fig: 2.17 John Rocque's map, London 1764



Fig: 2.18 Photo of Oxford Street / Tottenham Court Road Junction, late 19th C



Fig: 2.19 Photo of Oxford Street / Tottenham Court Road Junction, 1930

2.6.6 Before 1750, the Site and the area around it was agricultural land on the edge of London within the demesnes of the Manor of Tottenham, also known as Tottenham Court. Tottenham Court Road was a market road that connected Tottenham Court, which was popular as a place for entertainment for Londoners, and Oxford Street. It is shown on John Rocque's 1746 map of London.

2.6.7 Originally owned by the Canons of St Paul's Cathedral, the area around the Site changed ownership several times and the piecemeal nature of development in the area reflects this varied ownership. This contrasts with the more regulated and consistent development to the east of Tottenham Court Road in Bloomsbury.

2.6.8 The area south of the Site, including Windmill Street and Rathbone Place, were laid out in the 1720s. The construction of New Road in the 1750s (now known as Euston Road) and the resultant bypassing of Oxford Street, led to the northwards expansion of the suburban residential streets. In just 20 years, the street pattern seen within the Charlotte Street Conservation Area today had been established, with townhouses the most common form of development.

2.6.9 By the turn of the 19th century, the wider area around the Site started to become less fashionable as the generally wealthy occupiers started to move out further west. Rents started to drop and the townhouses were subdivided, creating spaces for studios, and this led to an influx of artists, artisans and craftsmen. The manufacture of furniture, in particular cabinet making, became popular and ground floor shops started to appear in the terraced houses. Towards the end of the 19th century, a wave of immigrants began to come into the area, contributing to a greater variety of businesses, including different shops and restaurants which occupied the ground floor units.

2.6.10 The scale of development in the area and particularly along Tottenham Court Road, changed throughout the 20th century. Ground floor units were merged to provide larger, purpose-built furniture shops, and grand department stores and warehouses that appeared amongst the remaining domestic terraces in the 20th century as a result of furniture making trade that established itself in the area in the 19th century. Pevsner notes that this character has largely gone with only Heal's (196 Tottenham Court Road) surviving in its original buildings.

2.7 Present Built Environment

2.7.1 Larger scale developments continued to appear along Tottenham Court Road following bomb damage during the Second World War, establishing a significant stock of post-1950s buildings in the wider area. On the west side of southern end is the 6 to 10 storeys high granite clad 1980 office building described as a “hulk” in Pevsner and designed by Sidney Kaye Firmin Partnership. This pattern of the intensification of development continues.

2.7.2 There are a mix of buildings along Tottenham Court Road between Windmill Street and Goodge Street, which include the Site. These include, running north from Windmill Street, the Rising Sun public house, listed grade II (see below) which forms a highly decorated stucco fronted local landmark at the junction. This is followed by a two windows wide re-fronted red brick terrace dating from the late 19th century (which has been altered); and a 3 storeys high 2 windows wide building with a mansard roof, painted yellow and with remnants of later 19th century window surrounds. There is then a 4 storeys high purpose built commercial development dating from the 1920s, which abuts the Site. The full height stone frame to the façade, with art deco style swags to the top, give the building a vertical emphasis; within this frame there is an arrangement of casement windows and soffit panels.

2.7.3 To the north of the Site, 53 and 54 are a matching pair of terraced building with yellow brick facades and red brick surrounds to the punched window openings to each floor. They have hipped roofs set behind parapets. There is then an alley leading to a 1920/30s development, Kirkman House, built on a former yard. This is followed by a 4 storeys high modern commercial development with a ground floor retail unit and a 1st floor comprising louvers. Beyond are a pair of red brick terraced fronts with stone window surrounds, dating from the later 19th century; and, a more exuberant red brick commercial building with decorative stone dressing. This run is terminated at the junction with Goodge Street, with a vacant site today (where the 3 storeys high white faience clad commercial development from the 1920s stood until recently).

2.7.4 To the rear of the Site the rear of the extended modern office block fronting Whitfield Street is dominant and defines the edges of the yard with a series of stepped and rendered facades.



Fig: 2.20 Existing view from Tottenham Court Road looking south-east



Fig: 2.21 Existing view from Tottenham Court Road looking north-west



Fig: 2.22 The Rising Sun, no. 46TCR



Fig: 2.23 no. 47TCR



Fig: 2.24 no. 47TCR



Fig: 2.25 No. 51 TCR - Photo of Existing Elevation



Fig: 2.26 No. 51 TCR - Photo of Elevation - Early 20C



Fig: 2.27 No. 52 TCR - Photo of Existing Elevation

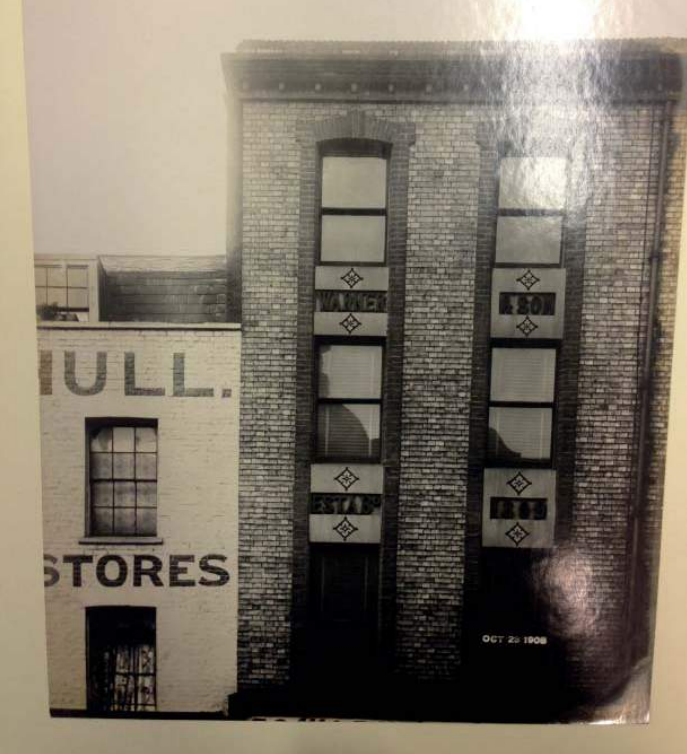


Fig: 2.28 No. 52 TCR - Photo of Elevation - Early 20C

2.8 No. 51-52 Heritage Value

2.8.1 No. 51

No. 51 features in John Tallis's London Street Views (1838-1840), where it can be identified in an illustration as a three storey plus mansard double fronted building with an accessway on the ground floor on the left hand side.

It is evident that the facade, scale and form of the building are largely unaltered since this time and perhaps since the building was constructed.

2.8.2 No. 52

No. 52 is only significant as part of the continuous frontage, reflecting the historic plot pattern and grain of the main body of the conservation area.

It is an unremarkable example of 19th Century terrace building and has been altered. the detailed design, roof form and fenestration does not match that of nos. 53-54 at front or rear.



Fig: 2.29 John Tallis Elevation of Tottenham Court Road - 1838- 1840



Fig: 2.30 Existing view from Tottenham Court Road looking north-west



Fig: 2.31 Existing front elevation of 51-52 TCR



Fig: 2.32 Existing front elevation of 53-54 Tottenham Court Road



Fig: 2.33 Entrance to no. 50TCR (no. 51 TCR Flying freehold)



Fig: 2.34 Gated Entrance to no. 50TCR and adjacent shop fronts at no. 51 - 52 TCR



Fig: 2.35 Entrance to no. 51 TCR



Fig: 2.36 View of rear roof no. 51 TCR



Fig: 2.37 View of rear roof no. 51 - 52 TCR



Fig: 2.38 View of rear roof no. 51 TCR, and rear elevations of buildings on Kirkman Place



Fig: 2.39 View of rear 6-10 Whitfield St. Photo taken from roof of no. 52 TCR

2.10 Existing Site Elevations on Tottenham Court Road



Fig: 2.40 Existing East-facing Elevation - Tottenham Court Road



Fig: 2.41 Existing West-facing Elevation - Tottenham Court Road

3.0 Design Development

The following pages describe the design development. Further to the Pre-Application meeting on 05.02.15, we have reviewed the comments and responded with revisions and explanations to define the scheme. The descriptions define the design response to townscape, street frontages and urban grain. The issues are explained and describe the response in relation to the historical development of the site.



Fig: 3.1 Existing view of Tottenham Court Road looking north-west

3.1 Townscape and Heritage

As explored in the previous section, townscape and heritage have been the main drivers for the refurbishment of no. 51-52 Tottenham Court Road.

The front elevation of no. 51 Tottenham Court Road is illustrated in John Tallis's survey of London dating back to 1838-40 Fig. 3.2 - 3.4. The adjacent images show the design development of the proposed elevation of the scheme. The purpose of the analysis is to compare the original elevation against the existing street elevation and to analyse this against our proposed elevation, particularly with regard to building heights.

Fig. 3.2 shows the original John Tallis's elevation highlighting in red the parapet line, and dotting the mansard roof line throughout the street.

Fig. 3.3 shows the original John Tallis's elevation highlighting the parapet line of the buildings as existing currently, with their corresponding mansard roofs indicated with a dotted line. This shows the addition of no. 46 - The Rising Sun.

Fig. 3.4 shows the original John Tallis's elevation highlighting the parapet line of the proposed buildings at no. 51 - 52, with their corresponding mansard roof indicated with a dotted line.

It is evident from the diagrams that most of the buildings on the street have increased in height at least by one storey. Furthermore, it can also be observed that the proposed height relationship between the nos. 51-52 would be retained as existing.

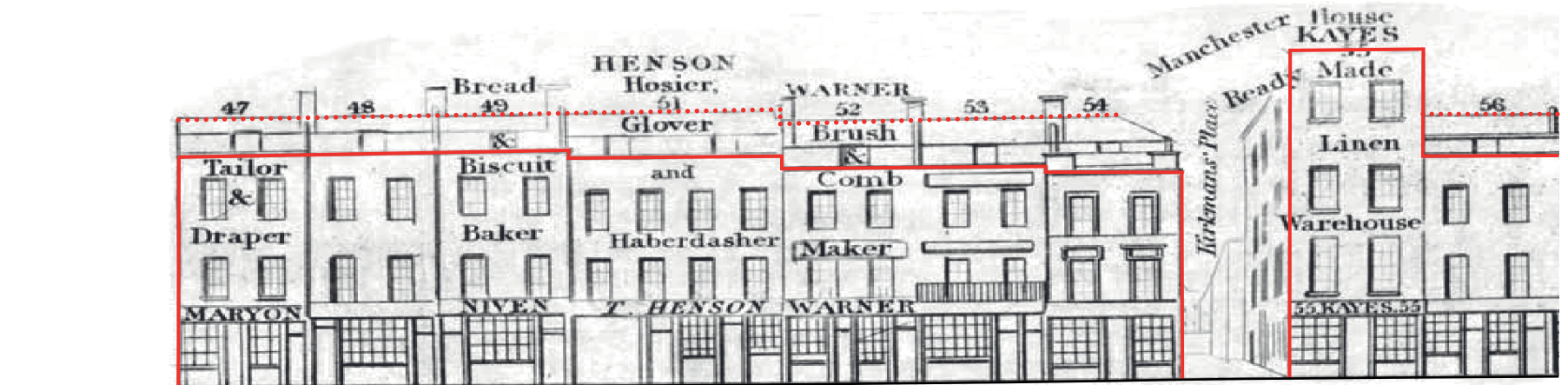


Fig: 3.2 John Tallis Elevation of Tottenham Court Road 1838-40 - Red line indicates building parapet heights; dotted line indicates mansard roof heights - 19C



Fig: 3.3 John Tallis Elevation of Tottenham Court Road 1838-40 - Red line indicates building parapet heights; dotted line indicates mansard roof heights - Present / Existing



Fig: 3.4 John Tallis Elevation of Tottenham Court Road 1838-40 - Red line indicates building parapet heights; dotted line indicates mansard roof heights - Proposed nos. 51 - 52



Fig: 3.5 Existing West-facing Elevation of Tottenham Court Road



Fig: 3.6 Proposed West-facing Elevation of nos. 51-52 Tottenham Court Road

3.2 Variety in Townscape

3.2.1 The street frontage of Tottenham Court Road in the conservation area is varied (age, scale height and detailed design of development) and reads as part of the wider townscape of Tottenham Court Road which has a rich and varied townscape. Tottenham Court Road is not a uniform street or one of a cohesive character, nor is it read in a wider townscape of such development

3.2.2 In terms of the varied height, along the street generally and on site specifically, it is the variety and not the specific relationships that is important.

3.1.3 There will be a change to the roof line on site but it will maintain the same level of variety as exists today and it will not be very noticeable.

3.1.4 Comparing historic street elevations with the street frontage today highlights this type of change, which has been ongoing since the 19th century and forms part of the character of the conservation area.



Fig: 3.7 Existing view ofTCR looking south indicating building heights and difference in floor levels across the street

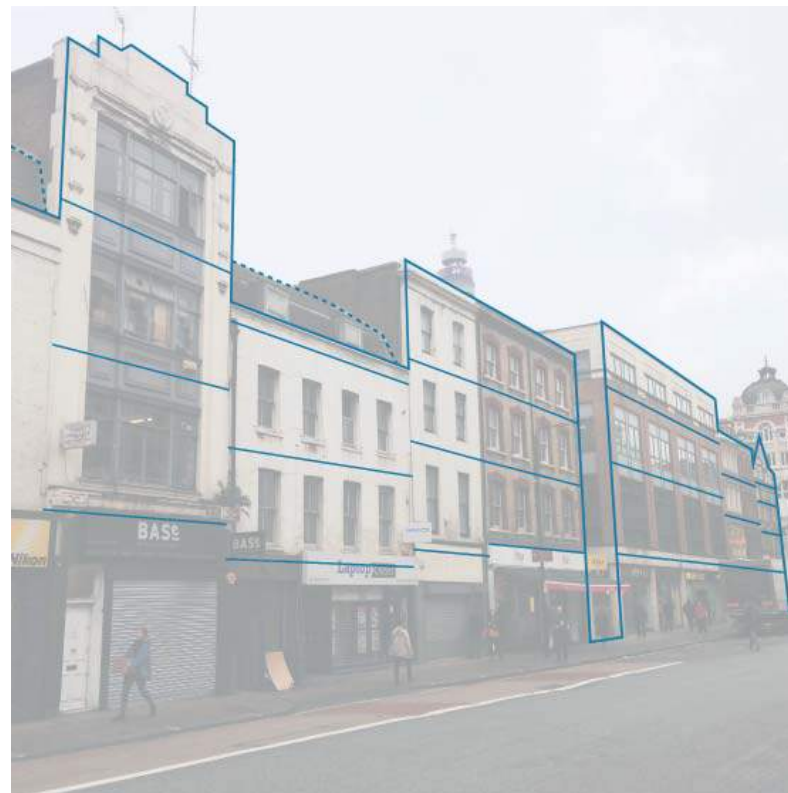


Fig: 3.8 Existing view ofTCR looking north indicating building heights and difference in floor levels across the street



Fig: 3.9 Proposed view ofTCR looking north indicating building heights and difference in floor levels across the street



Fig: 3.10 Existing view ofTCR looking south indicating building heights and plot widths across the street



Fig: 3.11 Existing view ofTCR looking north indicating building heights and plot widths across the street



Fig: 3.12 Proposed view of TCR looking north indicating building heights and plot widths across the street

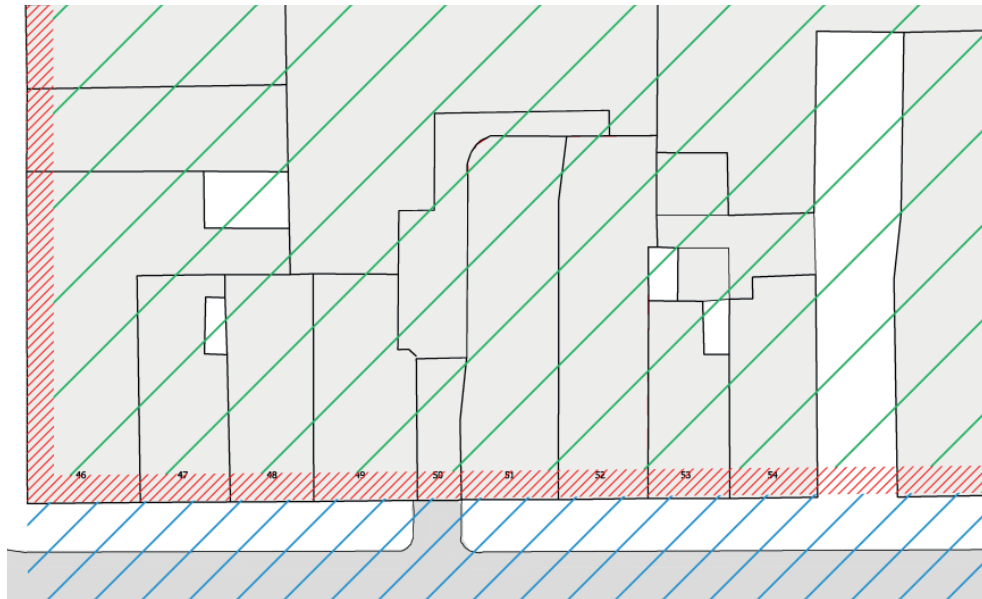


Fig: 3.13 Public A road vs. Private yard to rear of the Site.



Fig: 3.14 Public (Front) Elevation - Urban grain clearly represented through varying ploth widths along street

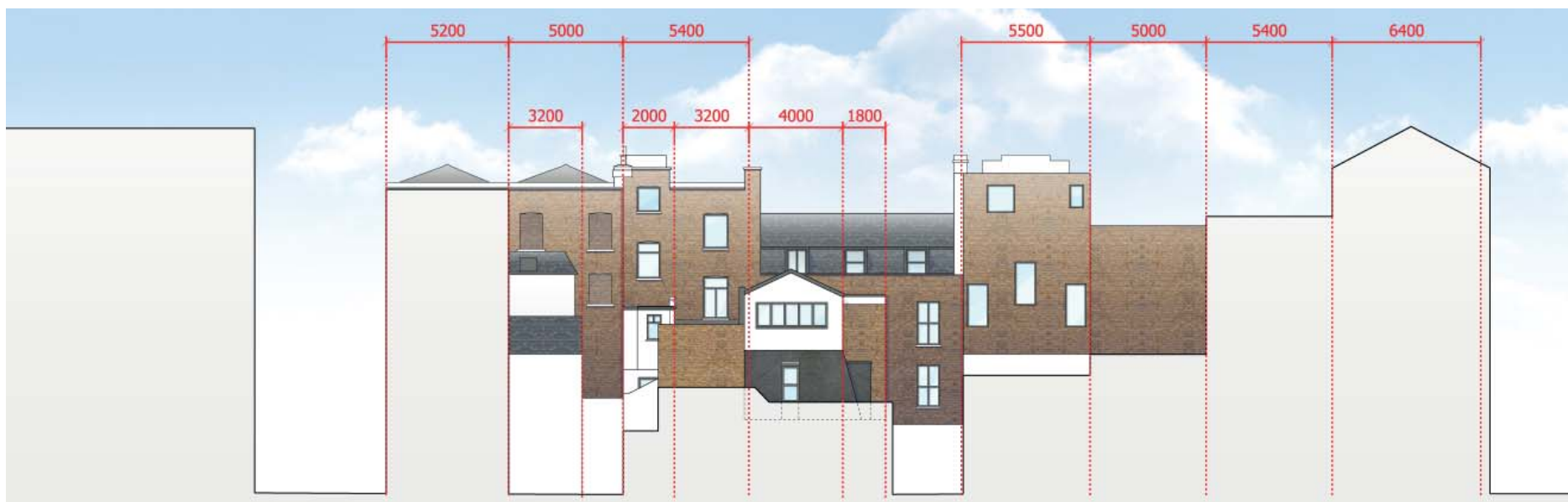


Fig: 3.15 Private (Rear) Elevation - Jumble of rear extension is not representative of historic grain

3.3 Fronts and Backs - Frontage

3.3.1 The street and rear of the site are distinct and differing in character:

The front or street facade is public facing, whilst the rear is private.

The front forms part of a main A-Road and is a popular shopping street; the rear is compromised yard /alley with no public access.

3.4 Urban Grain

3.4.1 The grain of the area is a result of the plot widths, as read in the rhythm of street elevations and reinforced by the varied height, design and materials of individual buildings. This is more evident in the street frontage and it is principally this element that contributes to the significance of the conservation area. The contribution of views to the rear is far less important.

3.4.2 The historic plot rhythm and therefore grain of the street and site is less apparent to the rear. The poor quality extensions to buildings on Site, adjoining buildings and large scale redevelopment of surrounding buildings detract from this. In particular:

3.4.3 The former uninterrupted route / layout of the yard is lost.

3.4.4 There is no clear building line to the rear of Tottenham Court Road.

3.4.5 The development fronting Windmills Street (also in the conservation area) has an overpowering presence, particularly nos. 6 to 10 wrapping around the site.

3.4.6 The rear extension on site will reflect the plot widths in its detailed design and stop at the eaves level of the frontage buildings. The main body of each building will remain discernible and be expressed differently at roof level. The extension will not appear over dominant and the existing grain will be maintained and better expressed.

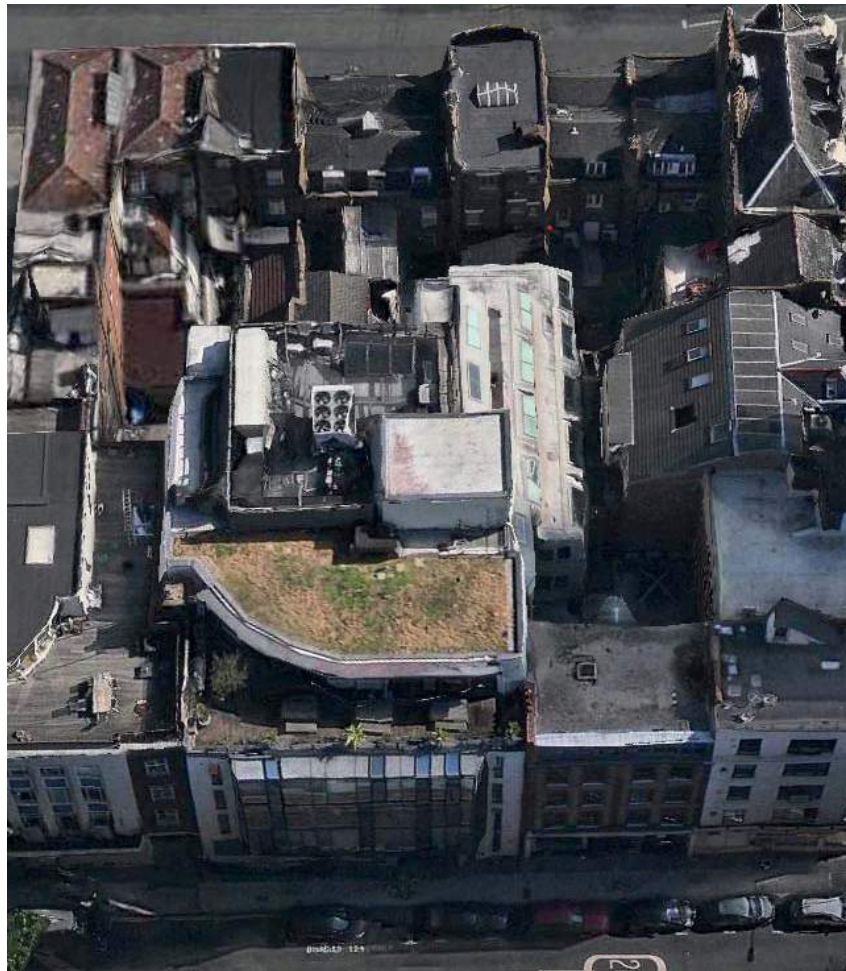


Fig: 3.16 Aerial view of Site - jumble of rear extensions to Tottenham Court Road; greater scale of building plots and heights on Whitfield Street



Fig: 3.17 Existing Satellite view of Site



Fig: 3.18 Satellite view of Site - colour indicating urban grain in 1875

1875



Fig: 3.19 Satellite view of Site - colour indicating urban grain and growth from 1875 to 1954

1875 1945



Fig: 3.20 Satellite view of Site - colour indicating urban grain and growth from 1875 to 1954

1875 1945 Urban grain

3.4 Urban Grain (continued)

3.4.7 The proposed roof plan (Fig. 3.21) indicates our intention to maintain the implied urban grain by creating an architecture that respects existing plot widths. Furthermore, the proposed rear and roof extensions are subservient to the existing buildings. This helps to create a hierarchy of building elements that highlight the existing frontage of the buildings, concealing the extensions to the rear.

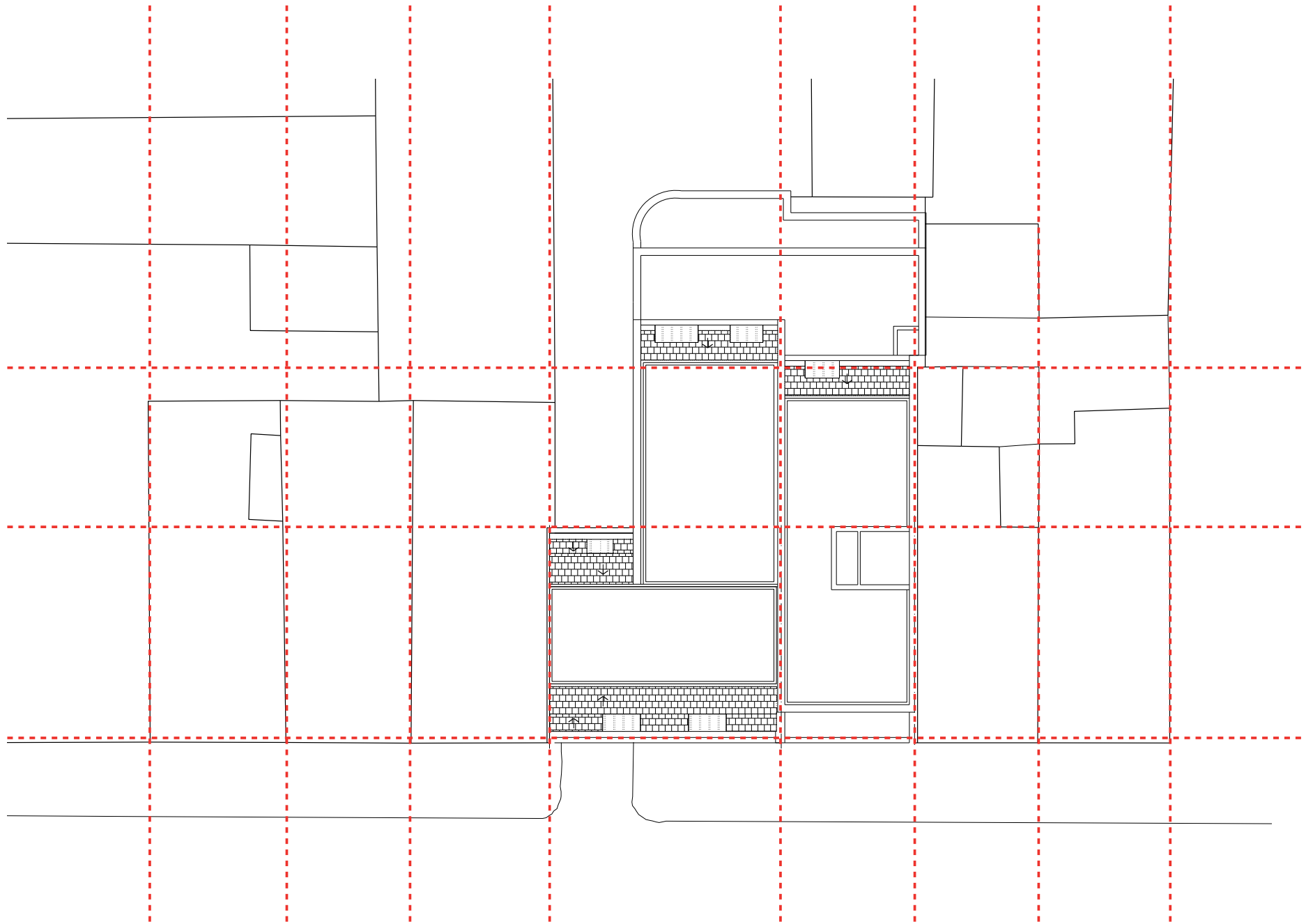


Fig: 3.21 Proposed Roof Plan

3.5 Proposed Design

3.5.1 The proposed view of Tottenham Court Road (Fig. 3.22) illustrates our intention to maintain the character, materials and individuality of the original buildings.

The street frontage will express a continuity of materials with the existing buildings; a painted brick frontage and slate clad mansard roof to nos. 50-51 will reflect the existing character of the building, while a set-back metallic clad mansard roof in bronze finish to no.52 will express the distinction which exists in the original plot pattern of the site.



Fig: 3.22 Proposed view of Tottenham Court Road looking north-west

4.0 Access Statement

4.1 Summary

Squire and Partners and the applicant set excellent standards of accessibility through consideration of inclusive design from the conception of the project and the consideration of the needs of all users. All aspects of the building are designed to ensure an inclusive and enjoyable environment for everybody.

The purpose of this statement is to outline Squire and Partners overall holistic approach to inclusive design within the scheme in accordance with the relevant local and national planning guidance, along with how the different access principles will be implemented into the scheme and managed.

4.2 Pedestrian Access

The scheme will provide a safe, legible, high quality inclusive environment that will be easy to use for as wide a range of people as possible without undue effort, special treatment or separation. The site's highly sustainable location in transportation and accessibility terms includes proximity to underground stations, rail stations, buses and roads for taxis and cars. In addition, level pedestrian access is provided on the site and around the site to Tottenham Court Road. Collectively, these transport modes provide the site with a large public transport catchment area which benefits from excellent accessibility.

The development proposals will continue this existing good level of accessibility to the mobility impaired, in line with requirements set out in National Guidance and Camdens UDP. The pavement around the site is relatively flat. Entrances to the retail unit and office will have step free access from the pavement through doors designed to suit wheelchair and impaired access requirements.

The circulation core through the building is designed with a wheelchair accessible lift that provides level access to all floors and the new escape stair will have allocated space for disabled refuge as outlined in the Building Regulations.

The following documents have been referred to in the development of the scheme:

- Relevant British Standards
- Part M of the Building Regulations
- Part B of the Building Regulations
- Lifetime Homes
- London Housing Design Guide
- Wheelchair Housing Design Guide

The Camden UDP has been reviewed carefully with regards to mobility impaired access and policies have been accounted for in the design.

4.3 Trains, Buses, Boat and Cycle Services

The local underground station at Tottenham Court Road, has good mobility impaired access as it is a step-free station. Street level to platform can be achieved by lift and ramp. Level access to the trains are also provided. The station has recently undergone major improvements with the cross rail interchange development. All buses operating around the site have wheel chair access, designated priority seating and wheelchair spaces.

Cycle parking for the building is provided in accordance with Camden cycle parking requirements. They are located within secure storage facilities and all are served by lift or stair.

4.4 Access to and around the building

Access to Tottenham Court Road will be level with the pavement. Pavement surfacing is as per Camden requirements and an appropriate level of external lighting is provided in the street.

Access into the Building

- Flush thresholds are provided into all ground floor areas
- Flush thresholds are provided from all ground floor main lobbies to the lift cores.
- Handrails to ramps, lifts and stairs provided are suitably detailed in line with Part M as required.

Within the Building - Offices

The office entrance lobby provides adequate space to manoeuvre as required under Part M. All common corridors are designed for wheel chair access. All floors are served by adequately sized lifts for wheelchair users.

Within the Building - Elsewhere

All service corridors will be 1.2m clear width. Refuse stores are all located at basement level.

4.5 Communications and Controls

This will be addressed during the detailed design stages. Generally signage will be clear, legible and consistent and consideration will be given to provide auditory signals for the visually impaired and visual signals for the auditory impaired. All fire alarms will be both visual and auditory in line with Part B of the building regulations.

4.6 Evacuation and means of escape

The building will be managed separately by either the occupier or an appointed management company who will also take responsibility for the development.

In the event of an emergency, evacuation from the building is by stairwell and a protected refuge for the mobility impaired is provided within the the stair core. The residential apartments are subject to detail design but are envisaged as *stay put strategy* for Fire brigade rescue.

5.0 Application Drawings

5.1 Existing Drawings

Drawing No. / Rev.	Description	Scale
13081_G100_P_00_001	Site Location Plan	1:250@A1
13081_JA12_P_B1_001	Existing Basement Plan	1:100@A1
13081_JA12_P_00_001	Existing Ground Floor Plan	1:100@A1
13081_JA12_P_01_001	Existing First Floor Plan	1:100@A1
13081_JA12_P_02_001	Existing Second Floor Plan	1:100@A1
13081_JA12_P_03_001	Existing Third Floor Plan	1:100@A1
13081_JA12_P_RF_001	Existing Roof Plan	1:100@A1
13081_JA12_E_NE_001	Existing Front Elevation	1:100@A1
13081_JA12_E_SW_001	Existing Rear Elevation	1:100@A1
13081_JA12_E_NW_001	Existing NW Elevation	1:100@A1
13081_JA12_E_SE_001	Existing SE Elevation	1:100@A1
13081_JA12_S_AA_001	Existing Section AA	1:100@A1

5.2 Alterations Drawings

13081_C645_P_B1_002	Alterations Basement Plan	1:100@A1
13081_C645_P_00_002	Alterations Ground Floor Plan	1:100@A1
13081_C645_P_01_002	Alterations First Floor Plan	1:100@A1
13081_C645_P_02_002	Alterations Second Floor Plan	1:100@A1
13081_C645_P_03_002	Alterations Third Floor Plan	1:100@A1
13081_C645_P_RF_002	Alterations Roof Plan	1:100@A1
13081_C645_E_NE_002	Alterations Front Elevation	1:100@A1
13081_C645_E_SW_002	Alterations Rear Elevation	1:100@A1
13081_C645_E_NW_002	Alterations NW Elevation	1:100@A1
13081_C645_E_SE_002	Alterations SE Elevation	1:100@A1

5.3 Proposed Drawings

Drawing No. / Rev.	Description	Scale
13081_C645_P_B1_001	Proposed Basement Plan	1:100@A1
13081_C645_P_00_001	Proposed Ground Floor Plan	1:100@A1
13081_C645_P_01_001	Proposed First Floor Plan	1:100@A1
13081_C645_P_02_001	Proposed Second Floor Plan	1:100@A1
13081_C645_P_03_001	Proposed Third Floor Plan	1:100@A1
13081_C645_P_04_001	Proposed Fourth Floor Plan	1:100@A1
13081_C645_P_RF_001	Proposed Roof Plan	1:100@A1
13081_C645_S_AA_001	Proposed Section AA	1:100@A1
13081_C645_E_SW_001	Proposed Rear Elevation	1:100@A1
13081_C645_E_NE_001	Proposed Front Elevation	1:100@A1
13081_C645_E_NW_001	Proposed NW Elevation	1:100@A1
13081_C645_E_SE_001	Proposed SE Elevation	1:100@A1