

**STABLES MARKET
HORSE HOSPITAL**



**DESIGN AND ACCESS
AND
HERITAGE STATEMENT**

For proposed works to Unit 92

April 2016

**Prepared for
Stanley Sidings**

By



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Stables Market: Horse Hospital – Unit 92 – Design and Access and Heritage Statement

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1 INTRODUCTION

Executive Summary

This statement accompanies the application for planning permission and listed building consent for the refurbishment of Unit 92 of the Grade II* listed Horse Hospital at The Stables Market, Chalk Farm Road, NW1 8AH London.

Unit 92, which forms part of the south range of the Horse Hospital building, is to receive an internal fitting-out in order to facilitate its use as a Class A3 restaurant (measuring 104 sq m internal area). The works include the replacement of the existing modern doors into the unit with an appropriate and suitable arrangement of new doors and the installation of new heating and cooling, extract servicing and drainage.

This report sets out:

- A historical background of the building, the site and the surrounding area
- A detailed assessment of the significance of the heritage asset
- A detailed description of the proposed works and their impact on the building and heritage assets

A pre-application meeting was held on 8th March 2016 to discuss the general strategy and works, followed by written advice on 30th March 2016 by Catherine Bond (Principal Planner – LB of Camden, Conservation and Heritage) and Claire Brady (Historic England). The Regent's Canal Conservation Area Advisory Committee (RCCAAC) were also consulted as part of the design process. All issues raised in the planning officers' follow-up emails were addressed and the proposal has been revised accordingly.

This report describes the proposed works and provides full information in order to assess the likely impact on any significant historic fabric and the wider conservation area.

The design documents have been prepared by and with the input of the Project Team who are:

Client: Stanley Sidings Limited

Planning Consultants: Gerald Eve

Heritage Architects: Stephen Levrant Heritage Architecture Ltd.

Mechanical & Electrical Engineers: Optima BES

1.1 Reference to other documentation

This document should be read in conjunction with the design drawings prepared by Stephen Levrant Heritage Architecture Ltd. (April 2016)¹; the M&E drawings and report prepared by Optima BES (April 2016).

1.2 Authorship

This statement has been prepared by Stephen Levrant Heritage Architecture Ltd, which specialises in the historic cultural environment.

- Stephen Levrant - Principal Architect

¹ HH-unit 92-01 / HH-unit 92-02 / HH-unit 92-03 / HH-unit 92-04 / HH-unit 92-05 / HH-unit 92-06/ HH-unit 92-07

- Stephen Jeffrey – Senior Conservation Architect
- Francesca Cipolla – Senior Associate Conservation Architect

- Camden Development Policies 2010 – 2025, Local Development Framework
- Local Development Framework - Camden Core Strategy 2010 – 2025

1.3 Planning Policy Guidance and Legislation

The assessment of the alteration on the building and conservation area has been prepared taking into account the information contained in: planning policy guidance and legislation

- National Planning Policy Framework (NPPF), 27 March 2012.
- DCLG Online Planning Guidance to the NPPF (NPPG)
- Conservation principles, policies and guidance for the sustainable management of the historic environment, English Heritage (now Historic England), April 2008.
- Understanding a Place: Conservation Area Designation, Appraisal and Management, English Heritage Guidance (now Historic England), 2011.
- Historic England. Historic Environment Good Practice Advice in planning note 3 March 2015.
- Seeing the History in the View, English Heritage (now Historic England), May 2011.
- Historic England Good Practice Advice in Planning – Notes 1-3, 2015.
- BS 7913:1998 Guide to the Principles of the Conservation of Historic Buildings.

2 CONTEXT

This complex of multi-storey stables was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

‘The heart of Stables Market was previously a part of the Camden Goods Yard and a large area was occupied by warehousing for W. A. Gilbey’s wines and spirits from the mid-19th century. The remaining buildings on the site comprise the stables for railway horses known as Stanley Sidings [...], a later block of 1883-85, and the surviving bonded warehouse, Gilbey’s No.2 Bond, built c.1885’ (Regent’s Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.17). The four western ranges, which include the Chalk Farm Stable, the Tack Room, the Provender Store, and the Long Stable, form the triangular group A-D built between 1854 and 1856 as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072).

The Horse Hospital building which lies to the northwest was originally constructed later in 1883.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright. Unless stated otherwise.

2.1 Location

The Horse Hospital building is part of the internationally recognised Stables Market, in the northwest corner of the Regent's Canal Conservation Area. (Fig. 1). The area is bounded to the north by Chalk Farm Road (Hampstead Road as it was known until 1862), and is separated from the former lands of the goods yard to the south by the railway viaducts (Fig. 2).

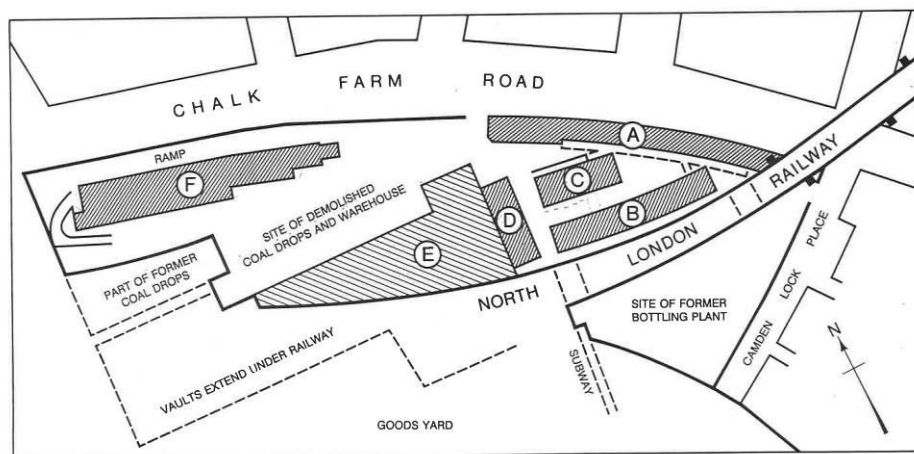


Figure 1 The Stables Market – Plan identifying the six buildings. The subject Building is F (from the Royal Commission on the Historical Monuments of England, Historic Building Report, September 1995).



Figure 2 The Stables Market area shown within the red line.

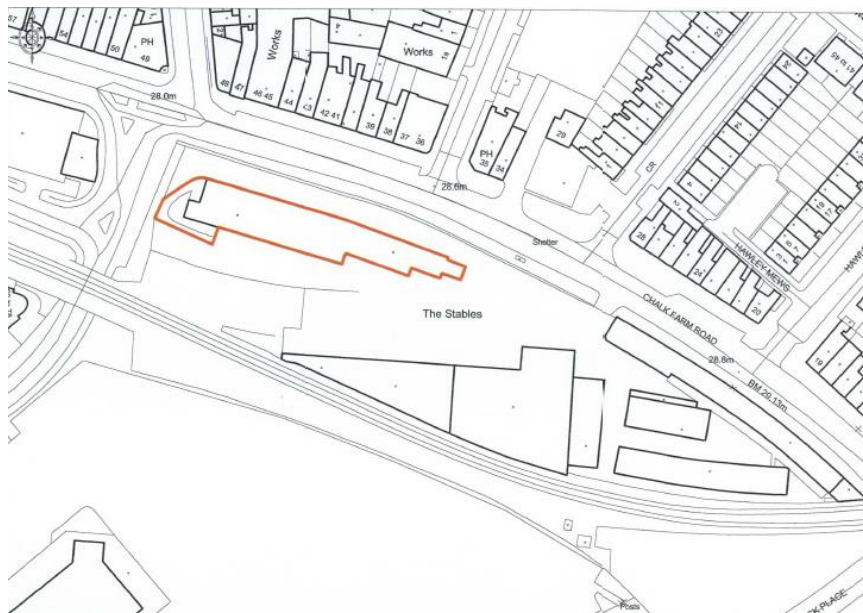


Figure 3 The site location Plan. The Horse Hospital shown within the red line.



Figure 4 The Horse Hospital shaded in orange. The subject site (Unit 92) shown within the red line.

2.2 Statutory site

The Horse Hospital is a Grade II* listed building, designated on 30th September 1981. For the complete listing description refer to the Appendix 2.

There are four listed buildings in the vicinity of the former Horse Hospital:

- (West side) Stanley Sidings, Stables to the east of the Bonded Warehouse (fig.5)
- (South West Side) The Roundhouse
- (South West side) Drinking fountain in wall next to the Roundhouse
- (South West side) Cattle through SE of the Roundhouse

The site partially lies within the Regent's Canal Conservation Area.



Figure 5 Map showing Regent's Canal CA and listed buildings.

2.3 Regent's Canal Conservation Area

The Regent's Canal Conservation Area was originally designated in 1974 and subsequently extended in 1981 to include the Stanley Sidings and the Stable Buildings. The conservation area was designated due to its unique character. *"It is an important feature of historic and visual interest in the wider townscape and, following the decline of traditional canal-related commercial activities, has been increasingly recognized as a valuable resource for water-based leisure activities, for its tranquil seclusion, for its ecological value and its potential for transportation and informal recreation"* (Regent's Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.5).

Many of the industrial buildings and structures are fine examples of industrial brickwork, illustrating styles of engineering and construction characteristics of the 19th and early 20th centuries and using various types of brick, some produced in London and others brought in by the railways from their respective regions. Cast iron and wrought iron are also well represented on the site.

2.4 Setting

The National Planning Policy Framework (NPPF) defines 'setting of a heritage asset' as *'the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral'* (NPPF, Annex 2: Glossary, p.52).

The Setting of Heritage Assets: English Heritage Guidance 2011, highlights para 114 of the NPPF: *'The extent and importance of setting is often expressed by reference to visual considerations. Although views of or from an asset will play an important part, the way in which we experience an asset in its setting is also influenced by other environmental factors such as noise, dust and vibration [...]'* (NPPF, para 114).

The site partly falls within the Regent's Canal Conservation Area, a heritage corridor of unique character, but the elements which contribute towards the neighbourhood identity, are not necessarily confined to the conservation area or other boundaries.

The character of the environs developed over the years and, today largely derives from the presence of four distinct but interrelated elements: the Regent's Canal, Chalk Farm Road, the

Railway viaduct and the market activities.

Chalk Farm Road is the principal thoroughfare of the area. The sequence of diverse views along Chalk Farm Road, defined by the built form and the curve of the road, prevents long distance views.

The built environment is defined on the east side by 19th century terraced buildings which have shops, café, and restaurants at the ground floor; and on the west side by predominant structures, such as the Roundhouse, the 'Camden Wall' and the railway bridge, which come into view on a northerly progression.

The Roundhouse is a major focal point, almost providing a gateway to the area from the north.

The 'Camden Wall' confers to Chalk Farm Road a sense of continuity and calmness with its regular progression of openings, but it is also a distinct barrier with its non-active frontage. A strong change of character occurs behind the Stables Market gate, where the original industrial stables subject of this application, still remain. The market is enclosed and separated from the public realm by the impenetrable wall.

The railway bridge, crossing diagonally across Chalk Farm Road, is not a simple barrier. It creates a visual break, emphasised by the traffic island and one-way traffic system. It also frames with its portal structure the view behind that change again in character. In fact, the buildings aligned on this stretch of the street have a narrow pavement establishing a different relationship with the street.

The immediate setting of the subject site is the larger Stables Market at the heart of the area widely known as Camden Market. The complex comprises the industrial horse stables and tightly enclosed courts leading off one another; their plan form being

influenced by the horse towpath and stabling serving the vast railway goods yard and interchange traffic.

The redevelopment of the underused site after the decline of canal-related activities and the conversion of wharves for the craft markets in the 1970s, contributed greatly to the economic and physical regeneration of Camden and to the present character of the area.

The craft markets developed into one of London's top tourist attractions giving a new iconography to Camden. Today the place is mostly known for its famous market, restaurants, café and entertainment facilities but this was never part of any planned development policies.

This market attracts large numbers of Londoners and tourists because of the character, the goods on sale and the uniqueness of the location. Boat trips, walks along the canal, and watching the barges pass through the lock gates from the Dingwall's beer garden, are important parts of the attraction of the Camden Lock area. Sunday trading was permitted on this private site while disallowed in many places elsewhere, and this also contributed to its success.

2.5 Historical Background

A 22 acre site at Chalk Farm was acquired by the London and Birmingham Railway company soon after its formation, for a terminus and depot. When development began in 1835-37 it was as a goods depot, linked to the Thames by the Regent's Canal and subsequently to the docks by the North London Railway, built in 1850-1.

In the 1840s and 1850s substantial buildings were constructed

around the edge of the site, including the Southern Goods Shed, built in 1845 and rebuilt after a fire in 1857, and a goods shed north of the canal, replaced by the Interchange Warehouse in 1900-5. The Chalk Farm stables were linked to both of these buildings by underground tunnels.

Horses were required for a wide range of haulage duties in the yard as well as for distributing goods outside; others were needed for shunting locomotives. Early provision for the horses was apparently in makeshift accommodation or below ground in the vaults of the goods station. By 1849 some horses were being stabled in the south eastern corner of the goods yard, and it seems likely that the expansion of this part of the yard in 1855 led to the construction of the stables at the triangle site.

The first phase of stabling, the four western ranges forming the triangular group A-D in the modern site plan, was built between 1854 and 1856, as indicated as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072). They were designed by the LNWR staff on the Stafford office and had a capacity of 148 horses. The ranges were mainly one and a half storeys, with ground floor stabling and haylofts above.

In the early 1880's the whole site underwent a phase of expansion and alteration with additional storeys added to the western ranges between 1880-3. A new stable block, known as the Horse Hospital, was also built at this time to accommodate a further 92 horses.

To the north of the Horse Hospital is a boundary wall that abuts Chalk Farm Road. This was built in 1854-6 as a retaining wall to raise the level Camden Goods Depot. The wall retains infill and an adjacent roadway finished with granite setts. The roadway slopes up towards the west end where the horse ramp begins and curves around the Horse Hospital. Materials of the boundary wall include

multi-coloured stock brick and stone used for the coping and sub-coping.

The site was vacated in 1973, and many of the buildings were demolished, tracks lifted, and various parcels of the land were sold into different ownerships. After 1973 the sidings and Camden Lock were used for various light industrial and storage uses including garaging. The market activities started at this time in a rather ad hoc fashion, commencing with small stalls located externally to the stable buildings. Gradually shop units and canopies were added to the buildings and by the 1980s it was a fully-fledged market place.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright.

2.6 The Horse Hospital Building

The building known as the former Horse Hospital has one or two storeys, stepping uphill westwards and northwards. The list description describes the stables as '*the outstanding architectural survival from the Camden Good Yard*', forming a group with the block of stables to the south-east and to the east of Bonded Warehouses.

The original 1883 section of this building - to the western side of the Horse Hospital, is five bays wide delineated by pilaster strips with two sets of two windows in each bay. It was constructed by the London and North-Western Railway's Engineer's Department as stables for 92 horses, with a curved ramp (horse creep) at the western end leading to the first floor level of stables. A now-vanished smaller curved ramp running at a right angle to the building may have been removed when the block was extended

c.1895, also by the L&NW Engineer's Department. This extension can be seen when comparing the 1891 Goad Fire Insurance Plan and 1916 OS Map (see Appendix 1).

Original plans and elevations for the 1883 building by the London and Northwest railway are shown in Appendix 3. These show both loose boxes and stables. A small harness room was located at the far eastern end and ventilation shafts appear along the length of the building.

The later section of the building was added on at the eastern end and comprises five bays with 7 windows to the north and 9 to the south. Built in the same style and materials, the two buildings appear as one.

The building contains features demonstrating its original use that are of special interest, namely the iron columns and capitals supporting timber roof trusses, complete and surviving timber and iron partitions for stabling with mangers and troughs, the stone sets in two of the rooms and the horse ramp running up the side of the building. The building survives in a relatively intact state, although works to the outside have taken place in order to maintain the building in use. Until 2007, the Horse Hospital was on the Buildings at Risk register but was subsequently removed following both external and internal repair works.

A boundary brick wall, running near parallel to the Horse Hospital to the north, pre-dates the Horse Hospital and was built between 1854-6 as a retaining wall in order to allow the raising of the level of the Camden Goods Depot.

Architect

Pevsner names the architect as H. Woodhouse. However, archival research did not uncover any information about him.

Occupants

The Horse Hospital was not included in the trades and the Street Index sections of London Directories from 1884 onwards. Prior to 1890 the south side of Chalk Farm Road is not included, after this the London & North-Western Railway (L&NWR) Goods Depot as contained several businesses. The main type being coal merchants and brewers although by the 1940s these were not so prevalent and by the 1970s motor related businesses began to dominate. In 1985 eight of thirteen businesses listed as being in the railway Yard were motor related. The others included a rehearsal studio and a builder's and also Old Stables indoor markets. The Directories show that ownership of Camden Goods Yard change from L&NWR to British Railways (London Midland Region) by 1950.



Figure 6 1992 photograph of the south elevation looking towards Unit 92. Photograph from the National Monuments Record in the Historic England Archive in Swindon, taken by Derek Kendall of the former Royal Commission on the Historic Monuments of England (RCHME), on 08/10/1992 (Crown Copyright).



Figure 7 2009 photograph of the south elevation looking towards Unit 92 showing previous restoration works in hand.



Figure 8 2009 photograph of the north elevation looking south.

3 ASSESSMENT OF SIGNIFICANCE

3.1 Criteria

As recommended by NPPF (March 2012) proposals for the alteration or redevelopment of listed building or buildings within a Conservation Area should be considered and be based on an understanding of the site's significance.

Paragraph 128 of NPPF states that 'In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should also be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance'.

The criteria for the assessment of significance according to English Heritage's 'Conservation Principles' consists primarily of:

Evidential Value – relating to the potential of a place to yield primary evidence about past human activity;

Historical Value – relating to ways in which the present can be connected through a place to past people, events and aspects of life;

Aesthetic Value – relating to the ways in which people derive sensory and intellectual stimulation from a place;

Communal Value – relating to the meanings of place for the people who relate to it, and whose collective experience or memory it holds.

3.2 Significance of the Horse Hospital Building

Evidential Value

There are numerous surviving features throughout the building providing evidence of the original function of the building, although some disturbed or altered by subsequent changes of use and alteration. The legibility of the former use is, however, clear.

The great extent of historic fabric preserved contributes to the special interest and character of the Horse Hospital and to that of the wider group in general.

Evidential value is therefore considered **high**.

Historical Value

The historical value of the Horse Hospital is recognised by its statutory Grade II* listing, as an important part of a distinctive group of former stables.

In this case the historic value is closely associated with the evidential value, providing a significant example of stables construction of their historic period despite the numerous alterations suffered throughout the years.

Historical value is therefore considered **medium to high**.

Aesthetic Value

The Horse Hospital, like the other Stables Market former stable buildings is essentially a utilitarian building, but it does, unlike the other buildings, exhibit a certain aesthetic intent in its detailing which sets it somewhat apart from its neighbours. This quality is essentially defined by the treatment of its elevations and the

arrangement of red brick dentil courses, segmental window heads, an oculi window to the west elevation and the cast iron pilasters framing openings to the ground floor level south elevation. Indeed, it is this external aesthetic quality together with the remaining interior original stable fixtures and fittings which have allowed the buildings designation as grade II*.

The building is an exemplary Mid-Victorian stable building.

Aesthetic value is therefore considered **medium**.

Communal Value

The Stables Market and its listed buildings have a strong identity and sense of place nurtured by the former historical industrial use.

The original purpose of the stabling has been superseded. There are no possible horse-related activities that may be carried out within the site, and there have not been since the 1950's/60's. The market use, established since the late 1970's/80's, is the reason why this area of Camden achieved worldwide recognition and attracts over 100.000 visitors per week. Furthermore, a poll carried out by NOP World in late 2003, shows that 95% of visitors agreed that the Camden markets are a valuable tourist attraction and 89% agreed the markets are a valuable amenity to the local community as well as visitors (London Borough of Camden, the role the markets play in the vitality & viability of Camden Town, 2006.). The communal values of the site can be associated to the later but firmly established market function, and as proved by the studies and polls carried out since its inception. The fact that the buildings are still in use brings together past and present common uses by the community, reaffirming its communal value.

Communal value is therefore considered **high**.

4 PROPOSED WORKS

Unit 92, at ground floor level, is to be refurbished to facilitate its continued use as a A3 restaurant. The proprietor is to be Voodoo Ray's, an independent gourmet pizza restaurateur, and will include 45 covers in total.² The unit is currently used as an A3 restaurant. The unit comprises two interconnecting rooms each with access door arrangements to the south elevation. The unit will receive an internal fitting out together with the replacement of the existing modern external door arrangements and the provision of new drainage, heating and cooling and extract servicing.

The new doors proposal will reinstate a historic stable door arrangement to the single door and provide a sympathetic double folding door arrangement to the adjacent later wider opening. The proposed design has been determined following a site inspection and analysis of the historic door operations, and developed in line with our established design rationale for the Stables Market buildings. The proposal is thus considered to improve the character and appearance of the historic building.

In addition, it is proposed to install glazed steel framed doors inset internally behind the external timber doors as detailed on the design drawings accompanying this application. The purpose of the inset glazed steel framed doors is to better regulate the environmental conditions within the unit (temperature and heat loss) as it is to function in the preparation and serving of food stuffs and importantly, to prevent the egress of vermin. We consider these doors will be perceived as a recessed modern

² Voodoo Ray's speciality is New York style pizza. They opened their first restaurant in Dalston in November 2012 followed by Shoreditch in May 2015.

addition behind the principle brick elevation and therefore the character and appearance of the listed building will be preserved whilst allowing for the proposed use of the unit.

Hanging signage for the unit is proposed at high level to be located in the space created between the internal steel doorset and the external timber double folding doors. This is a discrete arrangement, the sign inset and not visible when the unit is closed.

The fit out will comprise the installation of partitions to enclose a number of functions, including a WC and small office. These new elements are conceived as contemporary elements or 'pods' within the volume of the space; the partition walls not being full height to the ceiling. The first floor structure and the internal columns will be left exposed and untouched, in order to maintain the open spatial characteristic of the room. The proposed work is also considered reversible.

A new drain, serving the WC and kitchen facilities, is proposed to run below floor level to an externally located inspection chamber.

Heating is to be provided from both a supply ducted ventilation system and an over door electrical curtain heater. The air supply intake will be via a metal louvre fitted within the existing timber fanlight frame over the single door (currently penetrated by a highly obtrusive large metal external extract duct).

A new mechanical ducted extract system serving the oven and general areas will be routed at high level and vertically through the existing floor construction and the first floor room above to terminate at roof ridge level via a reconstructed existing ventilator cowl. In this way, the extract duct will not be visible and attached to the building's external elevation as is currently the case.

The design is considered to appropriately enable the continued

viable use of the unit as a restaurant while contributing to the improvement of the building's character and appearance and the better revealing of its historic significance.

The paint colour for the timber doors will be Obsidian Green (no.216) by Little Greene.



Figure 9 2016 photograph showing the two door arrangements on the south elevation giving access to Unit 92.

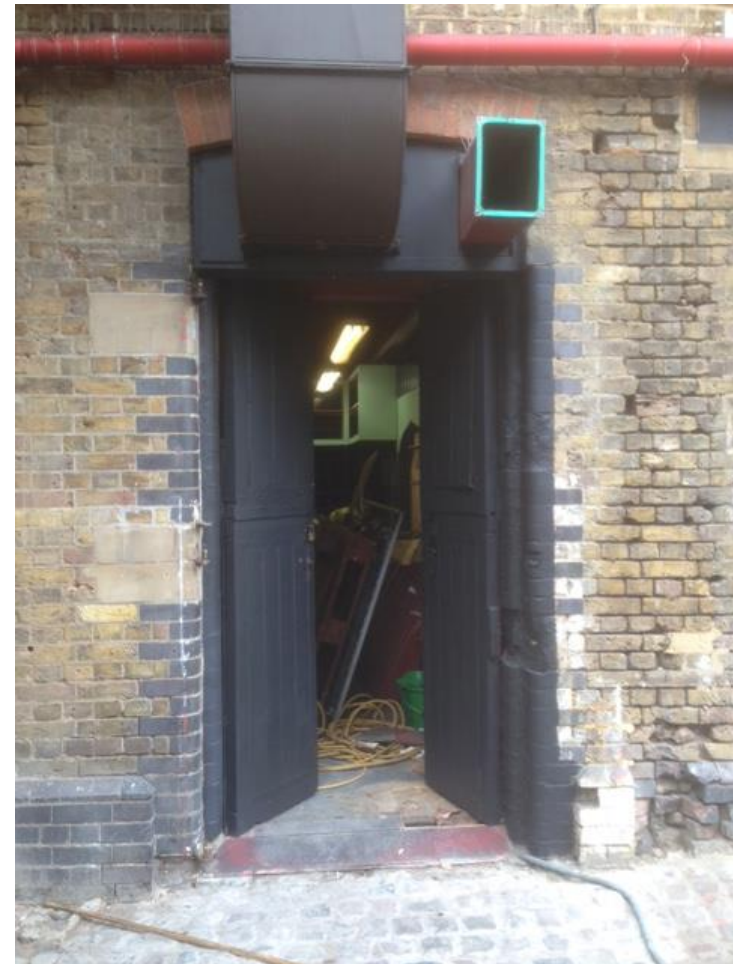


Figure 10 2016 photograph showing the inserted modern timber double door and service ducts passing through the timber fanlight over (Unit 92).



Figure 11 2016 photograph showing the later modified widened opening with steel lintel over (Unit 92).

5 IMPACT ASSESSMENT

The heritage objective of the proposal has been to ensure that works better reveal the character and appearance of the listed building with the minimum possible impact on the historic fabric of the building.

The proposed works will replace the existing modern ad hoc external doorsets with new doors that replicate the historic doors where original and remove the service ducts which penetrate the single door fanlight.

The new timber doors will be of a traditional framed, ledged and braced construction and follow the pattern of door types previously established and approved for the other Stables Market former stable buildings. Historic ironmongery – like the pintles extant on the stable door will be overhauled and retained for use with the new door. There will be no adverse impact on historic fabric – on the contrary, existing building fabric around the door openings will be fully restored including the sandstone hinge/lock blocks and the brick plinths.

The new external timber doors will not necessitate any significant intervention into the masonry wall reveals and new internal partitions and the steel framed and glazed double doors and screen will have a minimal abutment interface with the historic envelope.

The design of the internal partitions and the framed and glazed doors and screen will ensure they are perceived clearly as modern interventions within the historic envelope. In this way they will not impede or obscure the fundamental historic character of the spaces. The new inset framed and glazed doors and screen and the hanging sign will appear discrete when viewed from the

outside and be hidden when the unit is not open for business.

All existing ductwork which penetrates the external door fanlight will be removed – including the large cooker extract duct which rises externally to the roof³ (fig.9, 10, 12). New extract ductwork will be located internally to rise within the building to terminate at roof ridge level within a reconstructed roof ventilator (fig.13). The use of the roof ventilator in this manner is considered a discrete solution and one that importantly removes the ductwork from the building's exterior. An aperture through the first floor jack brick arch structure will be necessary to allow the new ducts but it is considered this limited loss of fabric will be balanced by the benefit gained from the removal of the existing highly obtrusive external ductwork.

The majority of new ductwork is confined to the Kitchen area of the unit as opposed to the main servery and dining space and consequently a high level aperture through the wall above the north internal existing opening is necessary (fig.15, 16). This work will have the advantage of allowing the removal of an existing modern steel lintel and its replacement with a traditional brick lintel arrangement.

The air supply intake will be via a discrete metal louvre fitted within the existing frame of the fanlight over the single door.

The new drainage below the Kitchen floor will require the removal of modern floor finishes and allow for the reinstatement and repair of the underlying existing floor. The drainage from a new sink is to reuse an existing pipe aperture through the existing dividing brick wall (fig.15).

³ The duct is identified in the Local Authorities 'Schedule of unauthorised works' as item 111 and classified as 'severe harm'.

It is considered that the proposed works to Unit 92 will significantly enhance the historic character and appearance of the building, and have a beneficial impact on the historic building in both the short and long term.

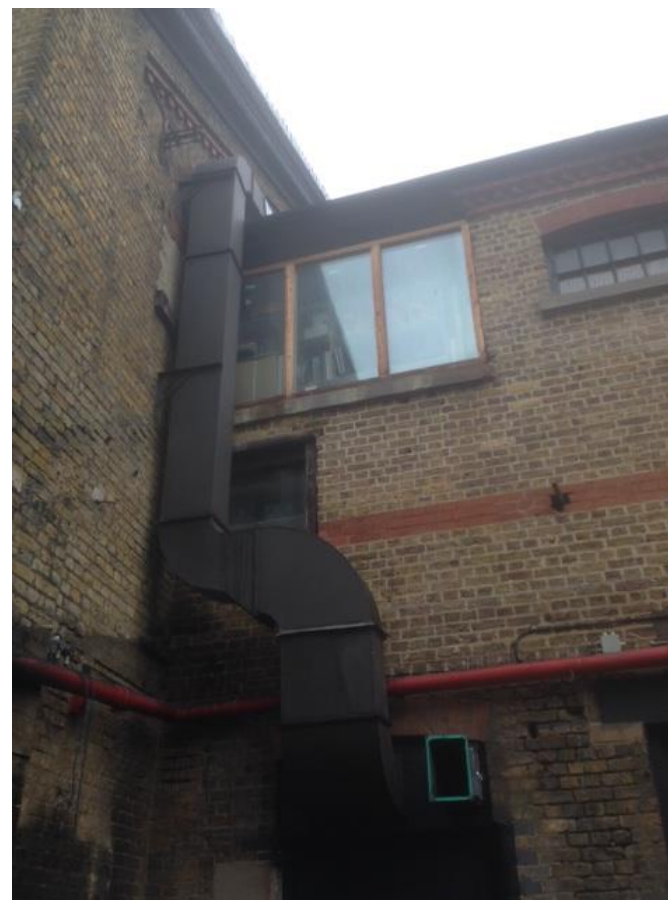


Figure 12 2016 photograph showing the extract duct routing through the fanlight and over the roof eaves.



Figure 13 2016 photograph showing the extant former stable roof ridge ventilator which is to be reconstructed to serve as the extract cowl for the new extract system.

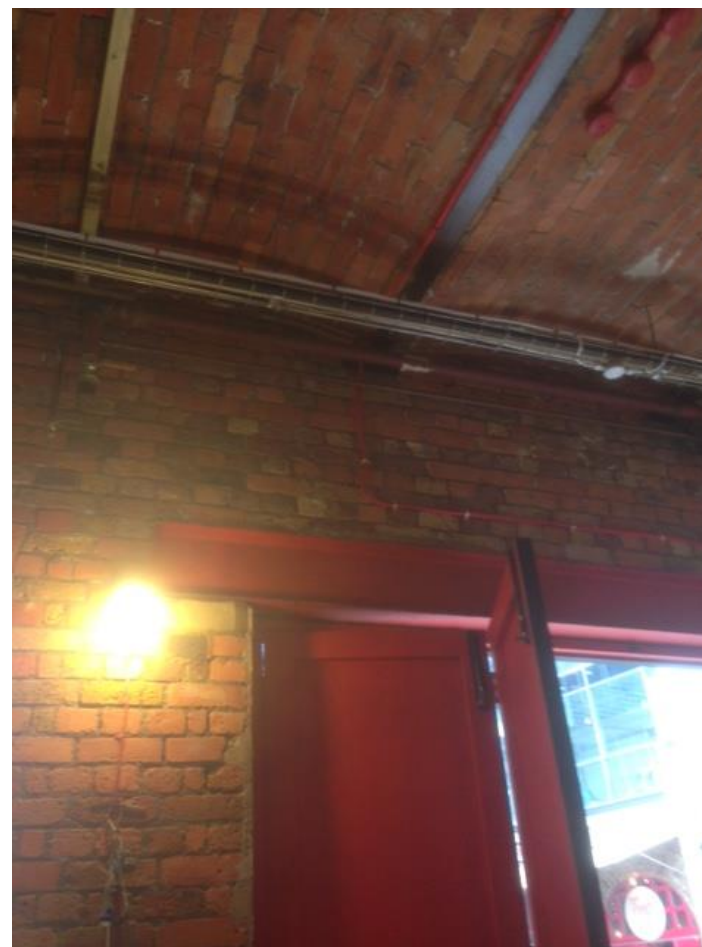


Figure 14 2016 photograph showing interior of Unit 92.

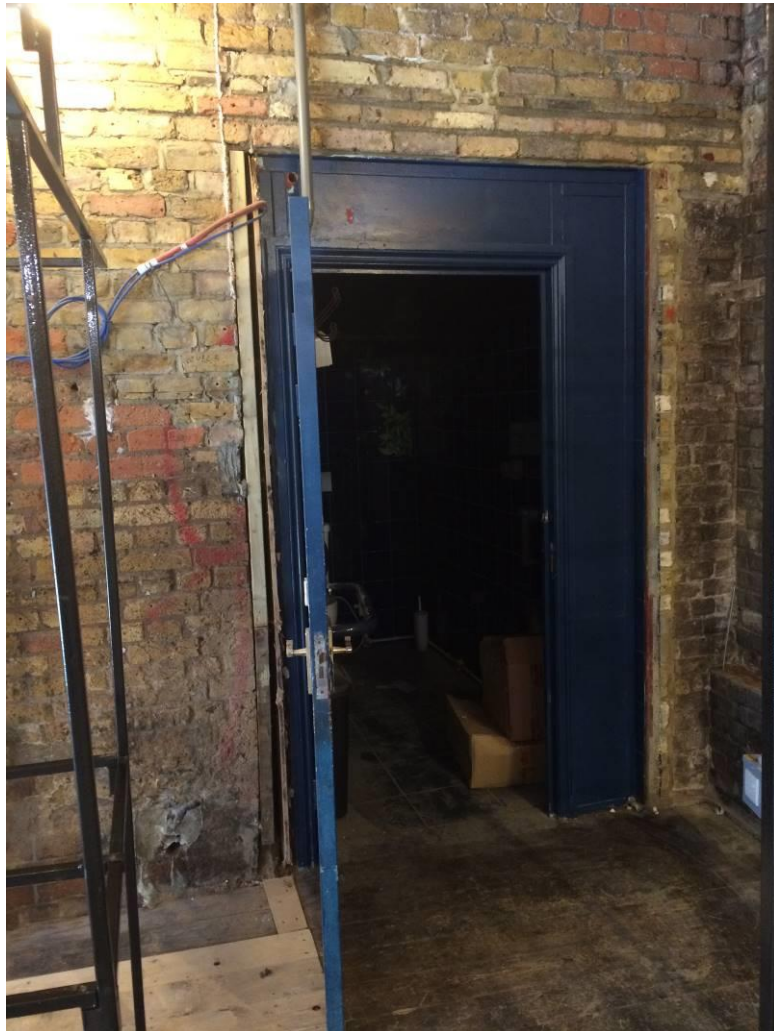


Figure 15 2016 photograph showing interior opening in north west corner. The existing pipe aperture through the wall at low level will be reused in the proposal to take the new sink drain.



Figure 16 2016 photograph showing interior ceiling soffit in north west corner. A high level aperture above the opening will be formed through the wall to allow the routing of the supply air duct.



Figure 17 2016 photograph showing double bullnose reveal detail to the narrower door opening. Note the brick plinth and the remaining pintle from the original stable door.

6 NPPF CONSIDERATIONS

The NPPF does not contain an express presumption in favour of the conservation of designated heritage assets as PPS5 did, but the presumption in favour of sustainable development is a presumption in favour of development that meets the objectives and policies of the NPPF, which has the conservation of heritage assets as one of its 12 core principles.

The policies contained within the NPPF seek to attain the Government's aim of achieving sustainable development. Resolution 42/187 of the United Nations General Assembly defined "sustainable development" as development that meets the needs of the present without compromising the ability of future generations to meet their own needs".

Paragraph 6 of the NPPF states: 'The purpose of the planning system is to contribute to the achievement of sustainable development. The policies in paragraphs 18 to 219, taken as a whole, constitute the Government's view of what sustainable development in England means in practice for the planning system.' The entire Framework constitutes a definition of "sustainable development" with no one part assuming greater weight than any other.

'Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy.' (NPPF Paragraph 7).

This statement deals principally with Section 12 of the NPPF, "Conserving and enhancing the historic environment", however Heritage considerations and issues are prevalent throughout the

framework.

NPPF Paragraph 128 states that *'In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contributions made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance'*.

Furthermore, **NPPF Paragraph 129** states that *'Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this assessment into account when considering the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposal'*.

As recommended in **Paragraphs 128 and 129**, an assessment of the significance of the heritage asset has been provided earlier in this report.

This report aims at fulfilling the National Policy requirement for provision of proportionate information which will enable the assessment of the likely impacts of proposed development on the special historic and architectural interest of the Horse Hospital by the Local Planning Authority. The significance assessment was based on historic research and consultation of relevant historic records and was examined according to the criteria set out in English Heritage's 'Conservation Principles'.

It is contended the significance of the heritage asset will be preserved and enhanced in compliance with the NPPF requirements.

Paragraph 131 states: *'In determining applications, local planning authorities should take account of:*

The desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;

The positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and

The desire of new development making a positive contribution to local character and distinctiveness'.

'Conservation' is defined in the NPPF as: *'the process of maintaining and managing change ... in a way that sustains and, where appropriate, enhances its significance'*. The proposals will contribute to the achievement of this objective.

Paragraph 134 states: *'Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including its optimum viable use'*.

The proposed works to Unit 92 of the Horse Hospital will enhance the listed building significance and that of the Stables Market complex in general. We contend there is, in fact, no harm caused by the proposed works. The established market use of the former stables is not challenged by the proposals; the replacement doors will improve not only the appearance of the building but its environmental efficiency and ability to function in the manner proposed. The removal of the highly obtrusive external extract duct is a tangible enhancement that should be weighed against the very limited intervention in building fabric.

The public benefit the proposal provides is in preserving not only the listed building but also the already established "optimum

viable use" for the site, as well as the enhancement of the wider historic environment in general.

The proposal will aid in safeguarding the continued use and long term preservation of the building. It is therefore concluded that the significance of the heritage asset will be preserved and enhanced in compliance with the NPPF requirements.

7 LOCAL PLANNING POLICY

Camden Local Development Framework, Camden Core Strategy, 2010 -2025, Adopted Version, November 2010.

The Camden Core Strategy Policy CS14 – ‘Promoting high quality places and conserving our heritage’, sets out the requirements to safeguard Camden’s heritage. The overall strategy is to sustainably manage growth in Camden in a way that conserves and enhances the heritage and valued places that give the borough its unique character.

Camden Planning Guidance provides advice and information on how the Local Authority applies its planning policies. The guidance is consistent with the Core Strategy and the Development Policies, and forms a Supplementary Planning Document (SPD) which is an additional “material consideration” in planning decisions.

CPG 1 – ‘Design’; deals with heritage issues in Section 3. This section sets out further guidance on Core Strategy Policy CS14 – ‘Promoting high quality places and conserving our heritage’ and Development Policy DP25 – ‘Conserving Camden’s Heritage’.

Paragraph 3.22 refers to the statutory requirement, when assessing applications for listed building consent, to have special

regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

It is considered that the proposal not only preserves but enhances the character of the historic environment through the replacement of the existing doors and the manner of the interior fitting out.

Paragraph 3.23 expands on the desirability to retain original or historic features and to carry out repairs in matching materials. The proposals should seek to respond to the special historic and architectural constraints of the listed building, rather than significantly change them. As stated earlier, the proposals actively seek to enhance the historic character of the building, and retain surviving historic features (like door pintle hinges).

The works will be carried out using traditional materials and techniques to match the existing and thus strengthen the historic integrity of the listed building.

Development Policy DP25 – ‘Conserving Camden’s Heritage’.

The proposals are considered to preserve and enhance the character and appearance of the conservation area. There is no harm to the special interest of the building or the setting, indeed the heritage assets significance will be better revealed on account of the works.

Development Policy DP12 – ‘Supporting strong centres and managing the impact of food, drink, entertainment and other town centre uses’.

The proposal complements the existing market and town centre by introducing a quality restaurateur. The proposal seeks to introduce ventilation into the unit to allow high level extraction thus complying with Policy DP12.

8 CONCLUSION

The Horse Hospital is the only Grade II* listed building among the former stables buildings on the site. The building is open to the public and in continuous use. The building has been subject to change over time to accommodate new uses and in order to remain sustainable, welcoming and pleasing, will doubtless continue to evolve. The proposed scheme is driven by a need not only to conserve and enhance the building, but also to ensure a sustainable solution that safeguards the current and future use of the building and wider market.

The proposed works will replace the existing modern ad hoc doors with new doors of an appropriate style and construction and allow for the operation of Unit 92 in a viable and sustainable manner, thus enhancing the historical character and architectural importance of the heritage asset and safeguarding its long term conservation.

The proposal complies with policy at the heart of the NPPF in respect of sustaining and enhancing not just the historic fabric but the significance of the wider Regent's Canal Conservation Area and the heritage assets within and in the vicinity of the site.

The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions. Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings are required to change in order to adapt to climate change and move towards a lower carbon economy (NPPF Para 7).

For the above reasons, it is considered the proposal is acceptable given the context of the setting of the heritage asset and the immediate surroundings and the conservation area. It is therefore concluded that the proposed works satisfy the relevant clauses of the NPPF as detailed above and is consistent with the spirit of local policies and national conservation principles.

APPENDIX 1: HISTORIC MAPS

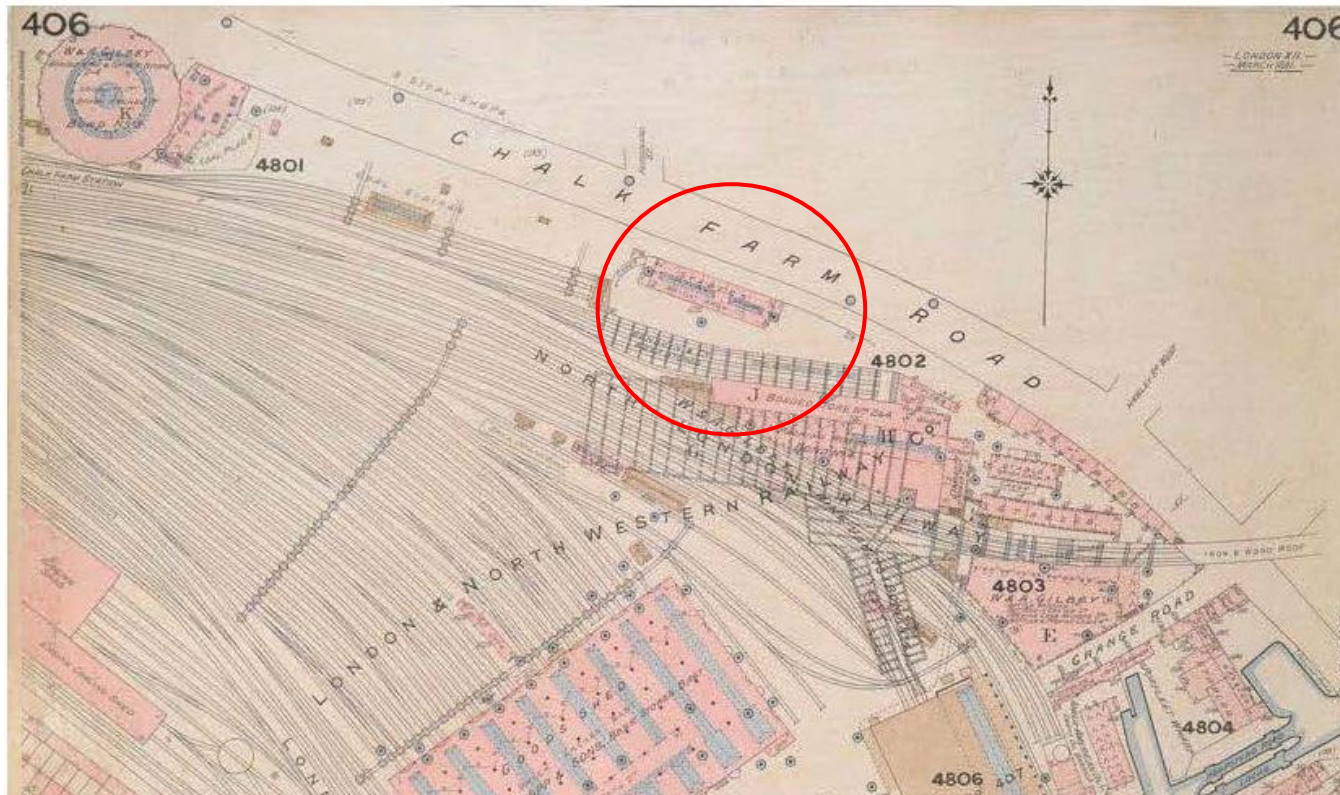


Figure 18 1891 - Insurance Plan of London Vol. xi: sheet 406



Figure 19 1893-1895 OS Map

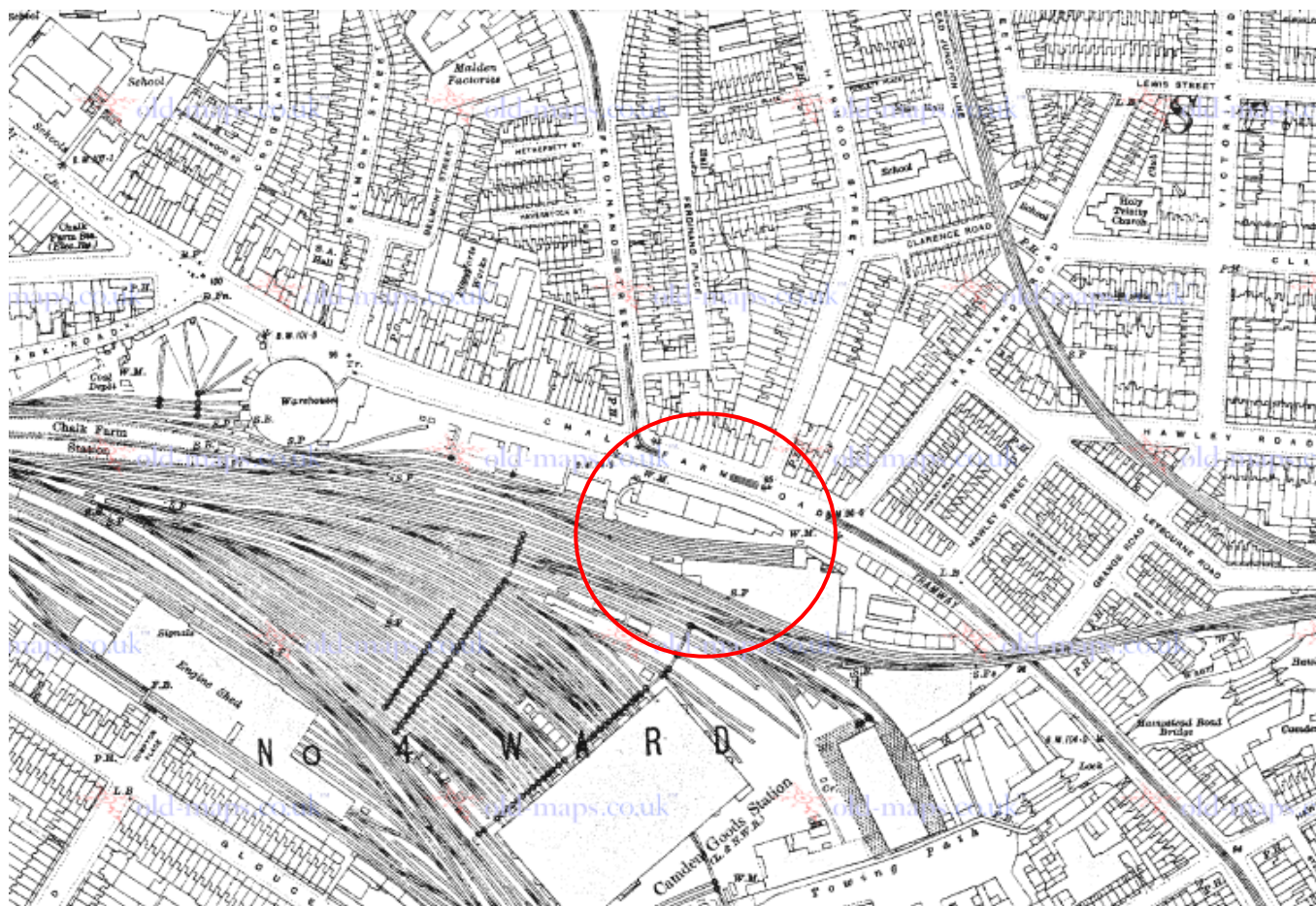


Figure 20 1916 OS Map

APPENDIX 2: LISTED BUILDING DESCRIPTION

Name: HORSE HOSPITAL WITH RAMPS AND BOUNDARY WALL AT NORTH OF SITE

List entry Number: 1258100

Location: STABLES YARD, STABLES MARKET, CHALK FARM ROAD

Grade: II*

Date first listed: 30-Sep-1981

Date of most recent amendment: 28-Jan-2013

List entry Description

Summary of Building

Stables. Built 1882-3 for the London and North-Western Railway. Designed by the London and North Western Railway (LNWR) Engineer's Department. Extended 1897. C20 conversion to market use.

Reasons for Designation

The Horse Hospital, Stables Yard is listed at Grade II* for the following principal reasons:

* Architectural interest and intactness: a fine example of a C19 industrial stabling complete with horse ramps and interior fittings, including stalls, mangers and hay racks;

* Historic interest and group value: an important component of the Camden Goods Depot, one of the most complete groups of C19 railway buildings and associated canal structures in England.

History

The Camden Goods Depot was originally constructed as the London terminus for goods traffic on the London and Birmingham Railway (L&BR), the capital's first inter-city main line railway and the largest civil engineering project yet attempted in the country. The site was chosen by Robert Stephenson (1803-59), the company's engineer, since it allowed interconnection for freight with the London docks via the Regent's Canal, built 1812-1820. Work started on a 25-acre site north of the canal purchased from Lord Southampton in January 1837 and the goods depot opened to traffic in 1839. The site included the stationary winding engine house for pulling trains up the inline from Euston to Camden (listed at Grade II*); a locomotive house; 18 coke ovens for making smokeless fuel for locomotives; two goods sheds and stabling for 50 horses; stores and a wagon repair shop. There were also cattle pens and offices. The sidings, the locomotive shed and No.1 Goods Shed were all constructed on brick vaults. Further goods sheds and stabling was subsequently built for the public carriers, such as Pickford & Co, who had rights to the distribution of goods on the L&BR until 1846 when the L&BR decided to carry out the carriage of goods through their own agents – the same year L&BR merged with other lines to become the London and North-Western Railway (LNWR). The Pickford goods shed was built in 1841 (enlarged in 1845) by William Cubitt (1791-1863) on the south side of the canal and linked to the goods yard by a second wooden railway bridge and was the first such rail, road and canal interchange building. In 1846-8 due to the rapid growth in passenger and goods traffic and the increase in locomotive size, the Goods Depot was overhauled to the designs of the Resident Engineer, Robert Dockray (1811-71). New structures were built, including two engine houses, notably that for goods engines (now the Roundhouse – listed at Grade II*) to the north of the main line tracks, and one for passenger engines to the south (demolished in 1966). There was also a construction shop for repairs to the north of No. 1 Goods Shed and other structures including a new railway bridge to the former Pickford & Co warehouse.

In 1854-6 another major upgrading of the site was undertaken following the construction of the rail link to the London docks in 1851, and further increases in goods traffic which required a larger marshalling yard. The North London Railway (NLR) lines were repositioned to the north of the site and the recently built construction shop dismantled (leaving its vaults) to make way for this. Sidings were extended to the edge of the canal either side of the

interchange basin which was realigned and enlarged to its present size. As a result of these changes in layout a new stables yard was constructed between the NLR tracks and the Hampstead Road. This contained four new stable ranges with a horse tunnel (the Eastern Horse Tunnel) linking them to the marshalling yards to the south. At the same time further stables were built on the western side of the mainline tracks off Gloucester Road (now Gloucester Avenue) and linked to the goods depot by the Western Horse Tunnel.

Further changes to the site took place in the later C19 including the construction of the LNWR goods shed in 1864, then the largest in the country (enlarged in 1931 and subsequently demolished). The goods depot closed around 1980.

The surviving elements of Camden Goods Yard, along with the Roundhouse, stationary winding engine house, Primrose Hill Tunnel Eastern Portals (also listed at Grade II*) and Regent's Canal represent a particularly important concentration of C19 transport and industrial buildings illustrating the development of canal and rail goods shipment.

The stables and 'Horse Hospital' Victorian railway goods depots required large numbers of horses for the transfer of goods and shunting of wagons. At its peak, around 700-800 horses were used at the Camden Goods Depot and by the early 1900s the LNWR provided accommodation for something like 6,000 horses nationally.

Stabling for 50 horses at the original 1839 goods depot was provided in the vaults below the railway sidings. By 1849, increased goods traffic meant that 427 horses were employed on the site. As part of the 1846-7 remodelling, four stable blocks, with stalls for 168 horses, were built between the sidings and Chalk Farm Road and let to tenants, whilst other horses were stabled in vaults below the Construction Shop and the Pickford's warehouse on the east side of the canal. In 1854-6, the further remodelling of the depot resulted in the demolition of the original free-standing stable blocks and the construction of the present blocks to the south-east. The four blocks are estimated to have stabled 162 horses and Stables Yard was linked to the rest of the depot by the Eastern Horse Tunnel. The Horse Hospital, as it came to be known, was built to the north-west of the other stables in 1882-3 and extended to the south-east in 1897. The first phase accommodated 92 horses with 40 more in the second phase. Major additional stabling had also been provided in about 1855 on the southwest side of Gloucester Road and more stable ranges on the north side in 1876. Both were linked to the Western Horse Tunnel, the second group by the existing horse stairs. The first group was demolished in the 1960s (to make way for Waterside Place) and the second group in 2000. The Horse Hospital has been converted to use as shops with a music venue on the upper floor.

Details

EXTERIOR The building consists of two adjoining ranges, the larger western range dating to 1882-3 and the eastern to 1897, built on a narrow sloping site along the boundary wall to Chalk Farm Road. The building is of yellow stock brick laid in English bond and a pitched slate roof with two sets of wooden ventilation louvers on the ridge of the western range. Details are in red brick consisting of floor bands, dentilled cornices, segmental window heads and oculi to the end gables of the western range (that to the eastern gable obscured by the later range). The two-storey southern elevation is stepped back to mark the building phases. The first phase comprises five bays and had accommodation for 92 horses using both storeys. The second phase comprises three two-storey stable bays (with the easternmost bay stepped back) and a single-storey mess with a hipped roof on the eastern end. This accommodated a further 40 horses.

The bays of the western range are divided by brick pilaster strips into panels of plain brickwork, relieved by pairs of small segmental-headed windows set high up under a red brick dentil cornice. The ground-floor bays have pairs of cast-iron pilasters with classical detailing either side of wide openings and supporting cast-iron girders. The openings were originally flanked by large multi-pane wooden windows but this arrangement survives intact only in the central bay, others having been altered to incorporate varying modern shop fronts, some retaining the original upper windows. The large openings indicate that the building was probably originally intended to be used as cart sheds rather than solely as stabling. Due to the slope of the land, the northern elevation is expressed externally as a single-storey, detailed in the same manner as the upper storey of the south elevation. Two window openings towards the centre of the elevation have been converted into doorways opening onto a modern entrance platform. The upper storey of the west gable end has a central doorway flanked by paired windows and opening onto a raised brick platform reached from the horse ramp which curves round the west end of the building. At ground floor level is a small lean-to with sloping slate roof, originally the boiler house.

The eastern range is simpler with the side elevations having a continuous run of upper storey windows of the same pattern as the west range. This arrangement was repeated, with larger windows, on the ground floor but some windows have been converted into doors including a large carriage entrance. The northern elevation has low windows on the ground floor due to the slope of the land and a large arched entrance with blue engineering brick quoins at the west end. This was originally entered via a short horse ramp from the setted roadway on the embankment running along the north of the building but has now been re-modelled as steps.

INTERIOR The 1883 range has cast-iron columns with bell capitals, supporting brick jack arching on the ground floor and timber roof trusses on the upper floor. The original brick-paved floors survive on both floors. The western section of the first floor retains twelve horse stalls with iron doorposts and timber boxes below the iron grilles and rails. Some stalls retain their mangers and hay racks and the remains of the wooden ventilation shafts. The stalls were used for the resting of tired or lame horses and their existence probably accounts for the building becoming known as the 'Horse Hospital' although it was unlikely to have been used for veterinary purposes. No stable fittings survive on the ground floor.

The interior of the 1897 range is plainer with I-section stanchions supporting the brick jack arching. No stable fittings survive in this range.

SUBSIDIARY FEATURES The high boundary wall to Chalk Farm Road, north of the Horse Hospital, was built in 1854-6 to retain the fill deposited to raise the level of the Camden Goods Depot. The wall is of multi-coloured stock brick laid in English bond with broad brick piers and stone coping. The infill between the wall and the horse hospital is topped by a sloping roadway with stone setts and kerbs of stone sleeper blocks from the early days of the railway (the modern stalls which line the northern side of the roadway are not of special interest). At the west end of the building it joins the horse ramp which curves round the western end of the Horse Hospital and gave additional access to its upper storey. The horse ramp has brick retaining walls with stone copings and a stoned setted ramp. The curve to the east is a later realignment.

Selected Sources

Books and journals

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Cherry, B, Pevsner, N, The Buildings of England: London 4, North, (1999), 365-366

Other

Peter Darley, Stables Complex and Underground Features in Former Camden Goods Depot , 2010,

National Grid Reference: TQ2850984260

APPENDIX 3: ORIGINAL DESIGN DRAWINGS

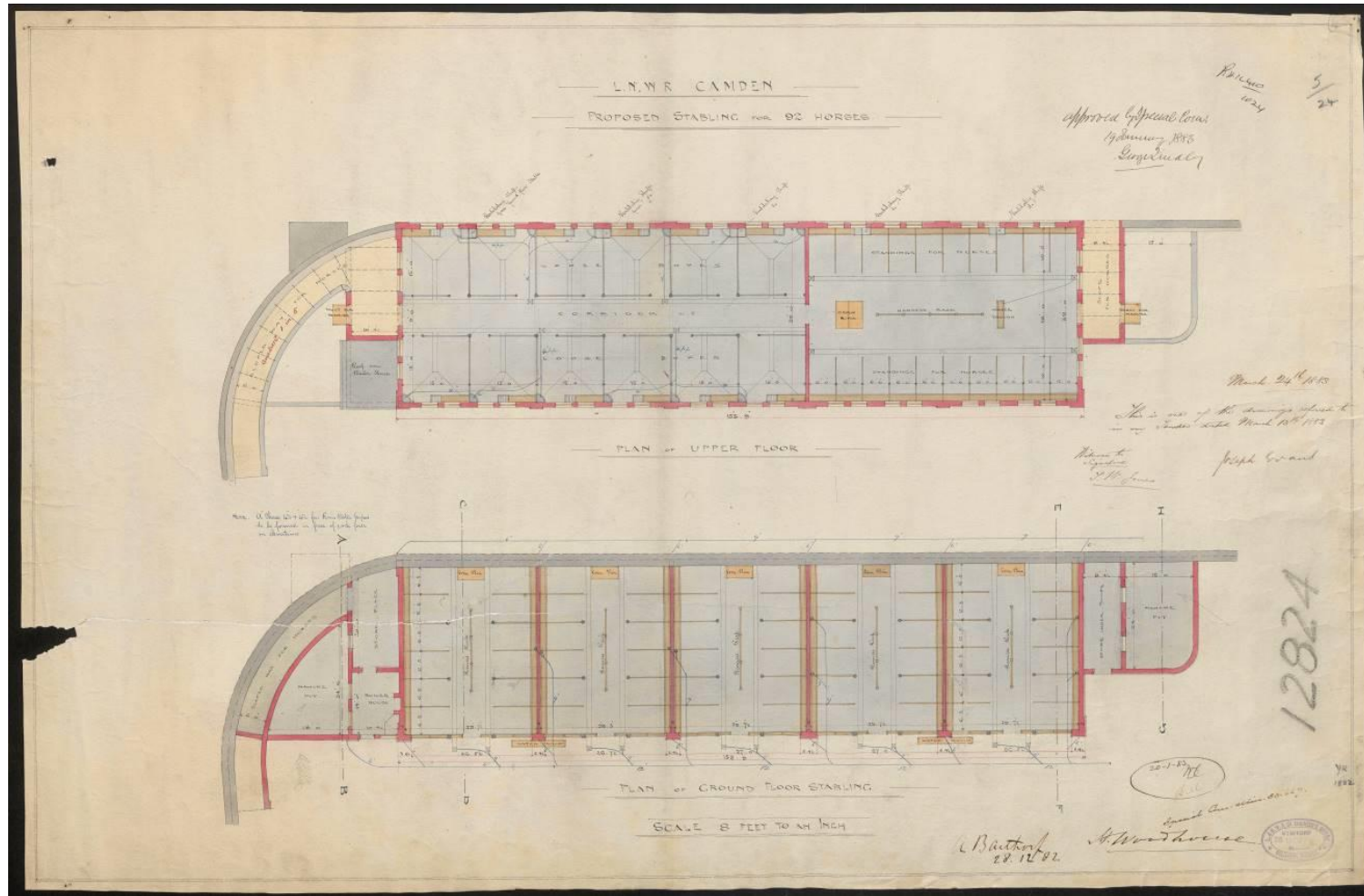


Figure 21 1883 original plans.

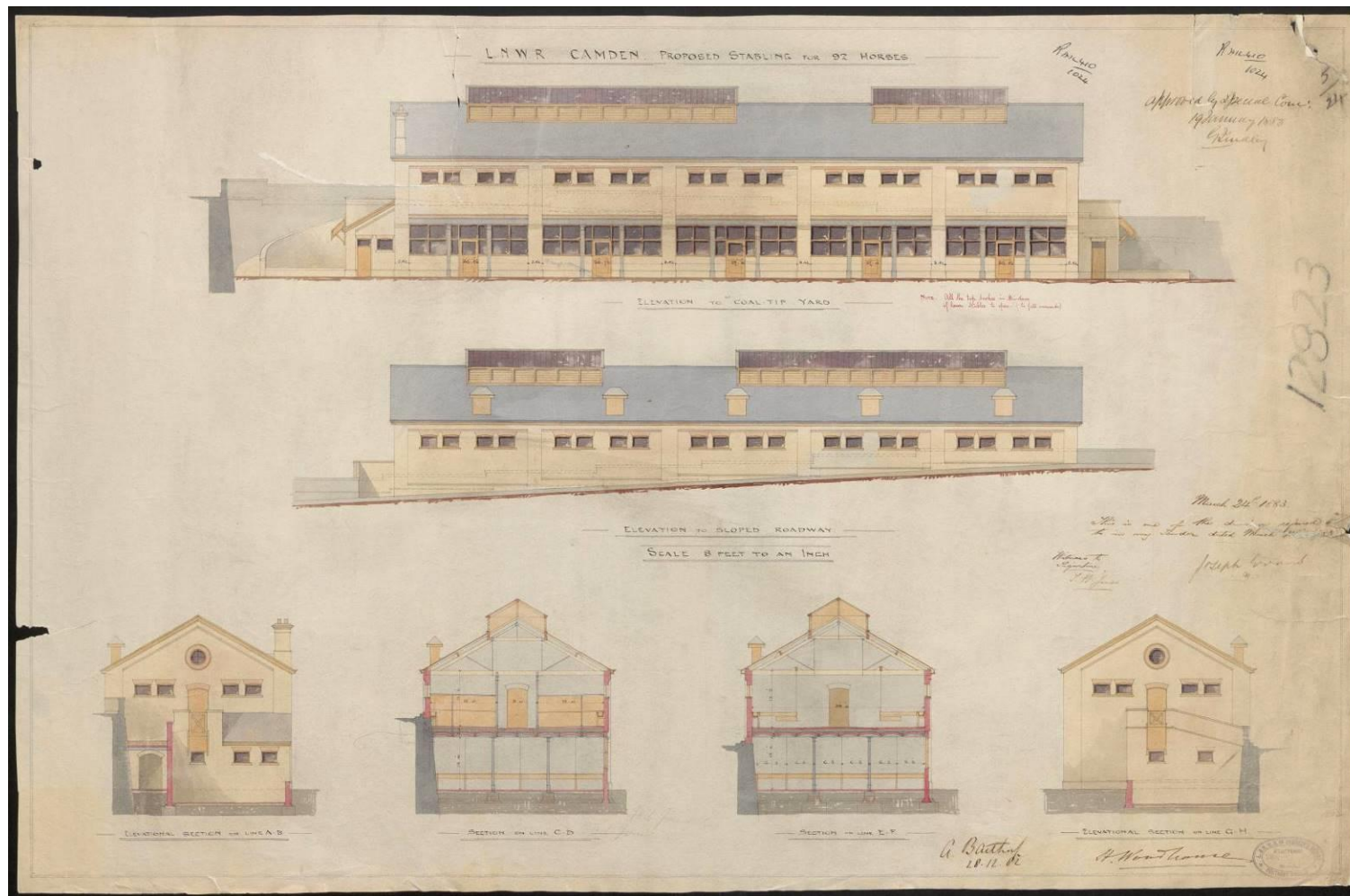


Figure 22 1883 original elevations & sections.

APPENDIX 4: STEEL FRAMED DOOR IMAGE



Figure 23 Typical steel framed and glazed door arrangement.