

**Construction Management**  
**Plan**

**New Church Hall at  
Roman Catholic Church of St  
Thomas More  
and Presbytery Maresfield  
Gardens Hampstead London  
NW3 5SU**

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## Introduction:

This Construction Management Plan has been produced by RJM Luton Ltd for the demolition, and the reconstruction of a new two storey church hall at St. Thomas More Church, Maresfield Gardens, London, NW3 5SU

The purpose of the **Construction Management Plan (CMP)** is to help developers to minimise construction impacts, and relates to both on site activity and the transport arrangements for vehicles servicing the site.

It is intended to be a live document whereby different stages will be completed and submitted for application as the development progresses.

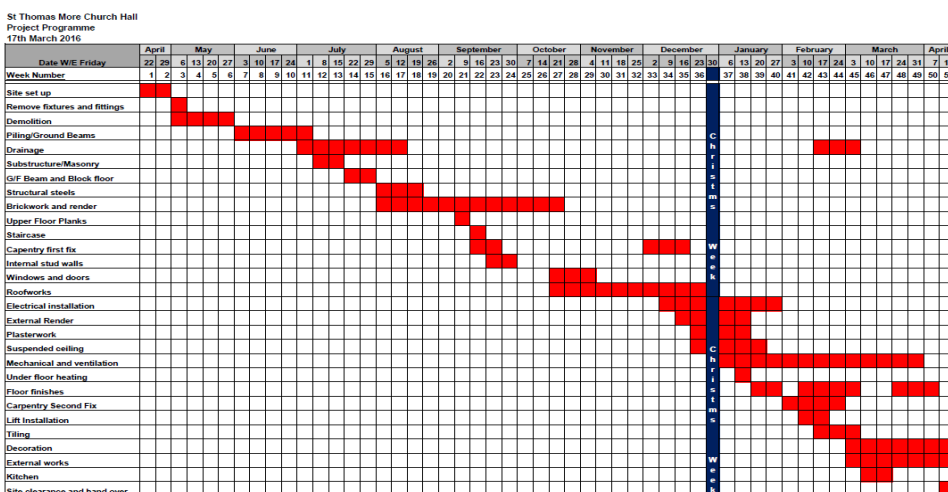
The approved contents of this CMP must be complied with unless otherwise agreed with the Council in writing. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

"The agreed contents of the Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the Development. Any future revised plan must be approved by the Council and complied with thereafter."

It should be noted that any agreed Construction Management Plan does not prejudice further agreement that may be required for things such as road closures or hoarding.

## Proposed Programme:

- Commencement 18<sup>th</sup> April 2016
- Duration 51 weeks
  - Demolition phase 4 weeks
  - Construction phase 47 Weeks
- Site operation hours 8am – 5.30 pm Monday to Friday and 8am – 1pm on Saturdays



## Contact Information:

1. Full postal address of the site and the planning reference relating to the construction works.

Address: St. Thomas More Church, Maresfield Gardens, London, NW3 5SU  
Planning ref: 2014/4774/P  
Type of CMP - Section 106 planning obligation/Major sites framework:

2. Contact details for the person responsible for submitting the CMP.

Name: Fran Matthews/RJM Luton Ltd  
Address: 25 Stockingstone Road, Luton, Beds, LU2 7ND  
Email: fran@rjmbuilders.co.uk  
Phone: 01582 616485 – 07976 925878

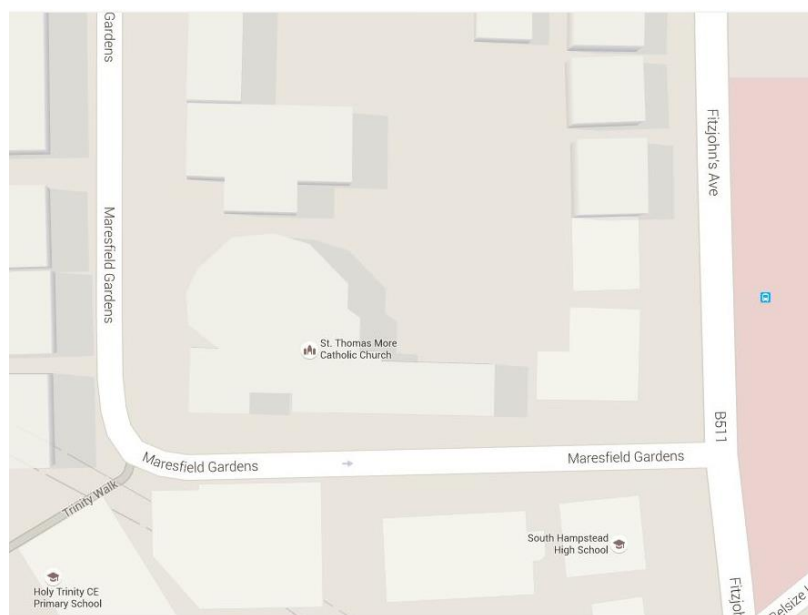
3. Full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and businesses.

Name: Richard Matthews RJM Luton LTD  
Address: : 25 Stockingstone Road, Luton, Beds, LU2 7ND  
Email: richard@rjmbuilders.co.uk  
Phone: 01582 616485 – 07976 925878

## Site

Site location plan and a brief description of the site and surrounding area.

The site located on narrow residential road with busy traffic to adjoining properties and restricted parking. The site is located opposite South Hampstead High School within a residential area. The access road joins onto a busy main road called Fitzjohn's Avenue



The proposed hours within which vehicles will arrive and depart.

In general the hours in which vehicles will arrive and depart will coincide with site working hours, however as the site is opposite a school we will try to avoid site deliveries at school drop off and pick up times.

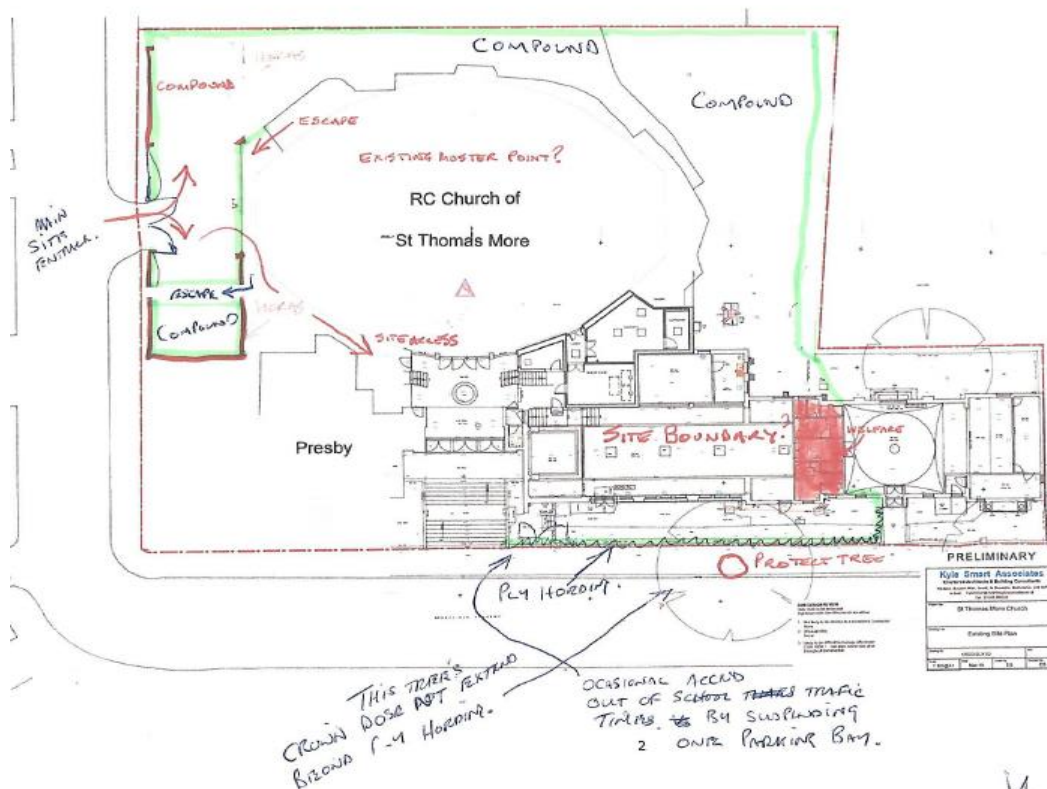
The access arrangements for vehicles:

Access to the site will generally be through the church carpark for demolition and the majority of construction materials.

For plant wider than 2metres and some heavy bulky materials (concrete staircase, hollow core floor beams, readymix concrete) these will be delivered at off peak times and pedestrians will be diverted to the opposite footpath.

A strict delivery procedure will be implemented to ensure that Maresfield Gardens is not overrun with site and delivery vehicles. Our banksmen will ensure that traffic flow on the road is maintained at all times.

All sub contractors and suppliers will be required to give 48 hours notice of deliveries.



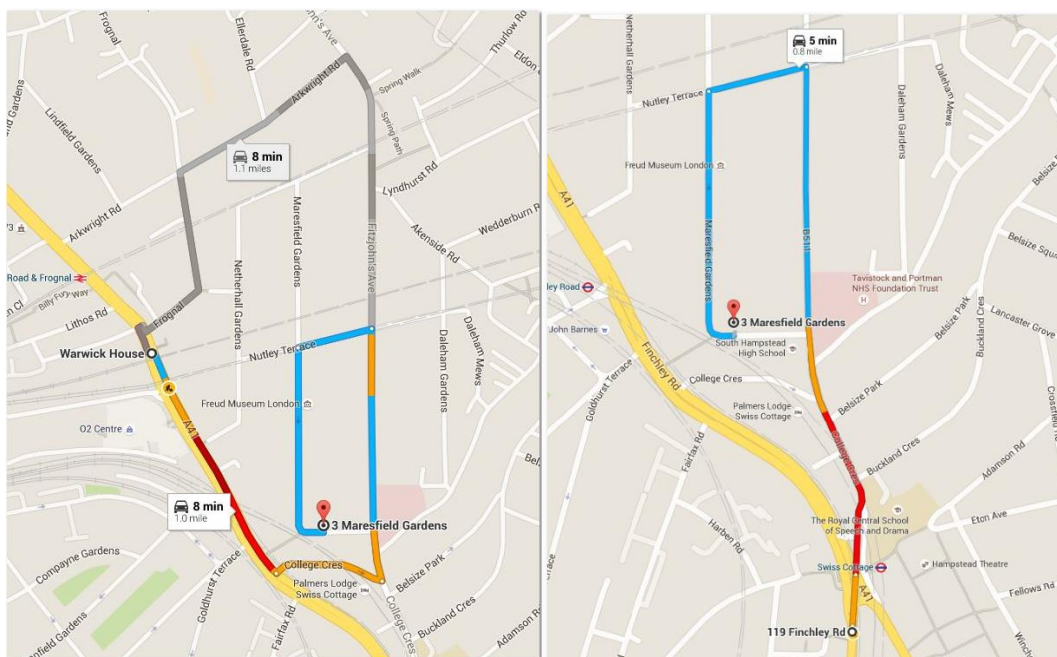
Proposed routes for vehicles between the site and the Transport for London Road Network (TLRN).

Details of agreed access/egress routes will be issued to all our suppliers and subcontractors. This will be policed as far as practical but it must be recognised that we have no jurisdiction over the vehicles once they have left our site.

Vehicles will approach the site from Finchley Rd London NW3, UK Head north-west on Finchley Rd/A41 towards Alvanley Gardens Turn right onto Froggnal Ln, Turn left onto Froggnal Turn right onto Church Row, Turn right onto Heath St/B511, Turn right onto Nutley Terrace Turn left at the 1st cross street onto Maresfield Gardens.

Or

From Swiss cottage on 151 Finchley Rd London NW3, UK Head east on College Cres/B511 towards College Cres/B511 Turn left onto Nutley Terrace ,Turn left at the 1st cross street onto Maresfield Gardens



### Size of Vehicles

Numerous types of delivery vehicles will be used to bring materials to and from the site. These include:

- Skip lorries. These will include roll on/roll off skips for major demolition works (approx size 7.5m long and 2.4m wide) and standard 8 yard skips for waste (approx size 7m long and 2.4m wide).
- Ready mix concrete lorries. ( approx size 8.25m long and 2.45m wide).
- Flat bed delivery vehicles for the delivery of various materials including scaffolding, steelwork, reinforcement, bricks/blocks, timber, roofing materials, plaster, joinery etc. (approx size 8.5m long and 2.45m wide).
- Articulated Lorries, for delivery of pre cast concrete units (only one delivery)
- The projected vehicle movements are approximately 6 per week for the first 3 weeks and 10 per week for the duration of the contract.

Swept path drawings for any tight manoeuvres on vehicle routes to the site.  
See routes above

Details of any highway works necessary to enable construction to take place: 5 metre wide cross over required, application will be made for a temporary cross over

Parking and loading arrangement of vehicles and delivery of materials and plant to the site. A strict delivery procedure will be implemented to ensure that Marsefield Gardens is not overrun with site and delivery vehicles. Parking and loading will be in the church car-park.

Details of proposed parking bays suspensions and temporary traffic management orders. Not required

Proposed overhang (if any) of the public highway (scaffolding, cranes etc.). scaffold will not overhang the public highway and all crane usage will be limited to unloading and carried out under the supervision of a trained banksman

Details of hoarding required or any other occupation of the public highway. The site area will be enclosed with a 2.4m ply hoarding, this is indicated on the site plan.

Details of how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Banksman arrangements.

When vehicles are entering or leaving the site, these will be supervised by our road banksmen. Where vehicle are unloading in Maresfield Garden, this will be supervised by banksman.

The general public/pedestrians will have right of way along the pathways that surround the site. The construction site gates will be kept closed and monitored by site security, only when deliveries are made to the site will they be opened to allow vehicles onto the site, at which time banksmen will supervise to prevent access by pedestrians. Delivery vehicles will be supervised/controlled by a banksman. When unloading via cranes in Maresfield Gardens the footpath will be closed off and pedestrians will be requested to use the pavement on the opposite side of the road.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices.

Details of how traffic associated with the Development will be managed in order to reduce congestion.

Wherever possible lorries will be brought onto site keeping the roads free for general traffic movement.

Details of how any significant amounts of dirt or dust that may be spread onto the public highway will be cleaned or prevented.

Mud and debris on the road is one of the main environmental nuisance and safety problems arising from construction sites. RJM Luton Ltd will make provision to minimise this problem.

In the early stages of the project when demolition and ground works are being carried out, wheel washers will be used to wash down all vehicles that enter/leave the construction site.

We will also make provision for cleaning of the road if required by an approved road sweeper.

We will insist on all muck away lorry's be fully sheeted to minimise the risk of any mud over-spilling onto the highway.

We will consider spraying a fine spray to suppress dust on the following:

- Structures and building during demolition.
- Unpaved areas that are subject to traffic or wind.
- Sand, spoil and aggregate stockpiles.
- During loading/unloading of dust generating materials.

Details of consultation with local residents, business, local groups:

The Parish have informed the local residents and school of the impending construction works and RJM Luton Ltd will do a letter drop to inform the neighbours of the intended start date and contact details in case there are any problems. We will ensure that any particularly sensitive works or issues are dealt with in a professional and accountable manner, with the public and local community kept informed at all times. This may include things like out of hours delivery of large items of plant such as piling rigs etc.

Details of any Construction Working Group that will be set up, addressing the concerns of surrounding residents, as well as contact details for the person responsible for community liaison on behalf of the developer, and how these contact details will be advertised to the community.

RJM Luton Company signs will be fixed to the site hoarding and the neighbours will be informed of the contact details by letter drop.

We will also maintain full and regular communications with affected neighbours regarding site activity, deliveries and traffic.

Should there be any complaints, local residents will be able to call personally to the site offices. A record will be kept of all comments/complaints.

Details of any schemes such as the “Considerate Contractors Scheme” that the project will be signed up to should form part of the consultation and be notified to the Council. Contractors will also be required to follow the “Guide for Contractors Working in Camden” also referred to as “Camden’s Considerate Contractor’s Manual”

A hard copy of Camden’s Considerate Contractor’s Manual will be available to all RJM Luton Staff and subcontractors, the hard copy will be kept in the site office and the site manager will be aware of the contents of the document and ensure that the standard is met by all operatives on site.

Details of other construction sites in the local area and how your Construction Management Plan takes into consideration the cumulative effects of construction local to your site.

There is one other building site on Maresfield Gardens this is next to the church car park, there has been no activity on this site for a few months, the site is a residential development.

All contractors and sub-contractors operating large vehicles over 3.5 tonnes must meet all of the following conditions:-

- 1) Operators must be a member of TfL’s Fleet Operator Recognition Scheme ([www.tfl.gov.uk/fors](http://www.tfl.gov.uk/fors)) or similar at the Bronze level.
- 2) All drivers must have undertake cycle awareness training such as the Safe Urban Driver module through FORS or similar.
- 3) All vehicles associated with the construction of the Development must:
  - i. Have Side Guards fitted, unless it can be demonstrated to the reasonable satisfaction of the Employer, that the Lorry will not perform the function, for which it was built, if Side Guards are fitted.
  - ii. Have a close proximity warning system fitted comprising of a front mounted, rear facing CCTV camera (or Fresnel Lens where this provides reliable alternative), a Close Proximity Sensor, an in-cab warning device (visual or audible) and an external warning device to make the road user in close proximity aware of the driver’s planned manoeuvre.
  - iii. Have a Class VI Mirror
  - iv. Bear prominent signage on the rear of the vehicle to warn cyclists of the dangers of passing the vehicle on the inside.



## Environment

To answer these sections please refer to the relevant sections of **Camden's Minimum Requirements for Building Construction (CMRBC)**.

The public access way on the pavement adjacent to the site will be separated by ply hoarding. A letter drop shall be carried out by the contractor to all surrounding properties affected by the development. The letter will advise residents of commencement and duration of the works along with contact details for the project.

Noise, dust and vibration will be controlled by employing Best Practicable Means (BPM) as prescribed in the following legislative documents and the approved code of practice BS 5228:

- The Control of Pollution Act 1972
- The Health & Safety at Work Act 1974
- The Environmental Protection Act 1990
- Construction (Design and Management) Regulations 1994
- The Clean Air Act 1993

General measures to be adopted by the Contractor to reduce noise, dust and vibration include:

- Erection of site hoarding to act as minor acoustic screen.
- Use of super silenced plant where feasible.
- Use of well-maintained modern plant.
- Site operatives to be well trained to ensure that noise minimisation and BPM's are implemented.
- Effective noise and vibration monitoring to be implemented.
- Reducing the need to adopt percussive and vibrating machinery.
- Vehicles not to be left idling.
- Vehicles to be washed and cleaned effectively before leaving site.
- All loads entering and leaving the site to be covered.
- Measures to be adopted to prevent site runoff of water or mud.
- Water to be used as a dust suppressant.
- Cutting equipment to use water as suppressant or suitable local exhaust ventilation system.
- Skips to be covered.
- Drop heights to be minimised during deconstruction.
- Use of agreed wet cleaning methods or mechanical road sweepers on all roads around site.
- Set up and monitor effective site monitoring of dust emissions.
- Working hours to be restricted as required by the Local Authority.

### **Dust**

The BRE 'Control of Dust from Construction and Demolition Activities' 2003, London Councils/GLA Best Practice Guide "Control of dust and emissions from construction and demolition" and Mayor of London's SPG on 'Control of Dust and Emissions' 2014, which gives best practice guidance on the control of dust and vehicle fumes will be implemented and followed where possible.

Stock piles will be minimised and covered/damped down. A water supply/stand pipe will be available on site for dust suppression purposes.

Vehicle movements: Any loads likely to produce dust shall be covered and wheel wash facilities where necessary will be provided at the exit to the site to prevent tracking of material off site. The contractor will monitor on a daily basis the areas immediately surrounding the site to ensure dust and dirt is minimised. All personnel working in a dusty area shall, where necessary, wear a dust mask deemed suitable by the HSE (Health and Safety Executive) General dust extraction will be used if required and local extraction used whilst wall chasing. On completion of demolition and the heavy structural works, the contractor will get a window cleaning company to attend all overlooking neighbouring properties if required.

### **Noise**

Under the Control of Pollution Act 1974, Part 3, Environmental Protection Act of 1990 and the Noise Regulation Act, noise is a recognised form of pollution and as such can be classified as a nuisance.

The Control of Noise (Codes of Practice for Construction and Open Site) Order 1984 gives legal approval for BS 5228, parts 1 & 2, 1984. This provides information on noise and noise control on Construction Sites. Every attempt shall be made to control noise at source.

On sites where construction works are in progress everyone has a responsibility to see that activities are carried out in the quietest practicable manner. Where noisy activities are unavoidable the disturbance will be minimised/attenuated by choice of technique, timing, shielding or protection as appropriate.

Where any person is liable to be exposed to noise levels greater than 80 dB (A), they will be informed and provided with suitable ear protection. The most likely protection, in ascending order attenuation is ear plugs, ear muffs and noise attenuation helmets. Noise will be kept to a minimum at all times and any further restrictions imposed under the terms of the construction contract will be strictly adhered to.

**Vibration**

All works involving vibration will be minimized, where possible eradicated by design and the use of controlled mechanical equipment. The contractor will install a monitoring system to surrounding areas and monitor levels. After discussion with party wall surveyors and depending on the activities taking place on site through the job, the monitoring levels will be set accordingly. These will activate an alarm when the limits are reached which will notify site immediately. Works can then cease and be re-assessed. Any operation involving vibration will have a HAVS risk assessment and procedures put into place to minimize the effects on personnel

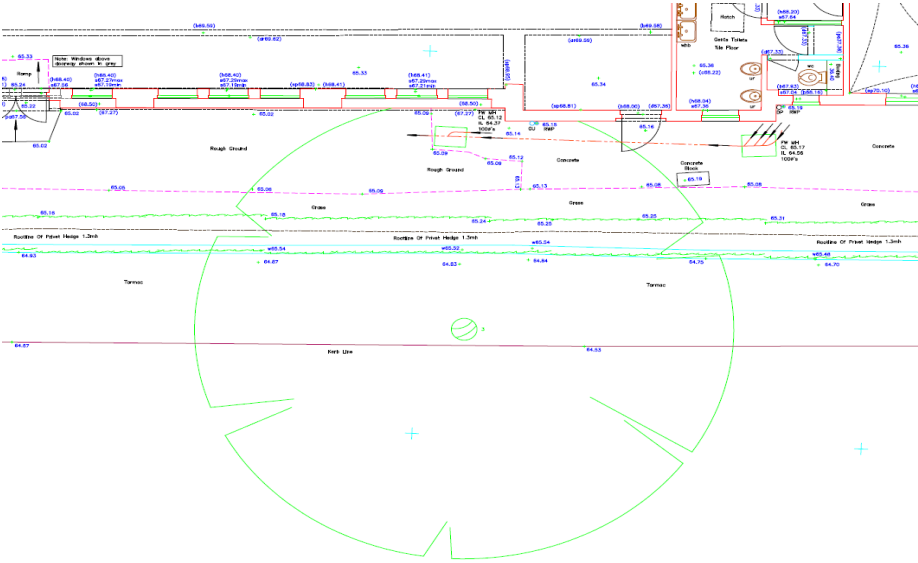
**Conclusion**

It is our opinion that the proposed works can be carried out within a safe manner by a suitable contractor and that noise, dust and vibration can be sufficiently mitigated.

**Protection of tree outside site on Maresfield Gardens:**

There is one tree close to the construction site, see plan below, the tree is on the public footpath close to the kerb edge, the site will be hoarded off with 2.4mtr. solid ply hoarding behind the existing boundary wall, approximately 2 metres from the trunk of the tree.

The excavation works are 6 metres away from the croen of the tree (4 metres inside the site compound) The tree has been cut back in previous years and will only overhang the site compound by approximately 2 metres)





# Agreement

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed in writing by the Council. This may require the CMP to be revised by the Developer and reapproved by the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council in writing and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

**Signed:** .....

**Date:** .....

**Print Name:** .....

**Position:** .....

Please submit to: [planningobligations@camden.gov.uk](mailto:planningobligations@camden.gov.uk)

End of form.