

SECTION 04.

ACCESS

4. ACCESS

4.1 VEHICULAR AND TRANSPORT LINKS

Introduction

A detailed Transport Statement has been prepared to support this planning application by Steer Davies Gleave and has been submitted separately.

The Transport Statement considers all aspects of movement by existing and future residents, employees and visitors. It also considers delivery and servicing trips associated with the site.

The methodology adopted in the Transport Statement was agreed, in principle, with officers from London Borough of Camden at a Pre-Application meeting on 17 November 2015.

The site is bounded by three roads: A40 Holborn to the south; A5200 Gray's Inn Road to the west; and Brooke Street to the east.

The existing site comprises a part nine storey and part four storey building (including basement), with a total gross external floor area (GEA) of 12,798m² (excluding plant).

The main land use is Class B1 office with some retail units occupying the ground and basement levels of the development. The existing building also contains a three bedroom residential unit on the second floor. The offices are currently vacant.

Local Highway Network

Brooke Street falls under the control of LBC. Gray's Inn Road and the A40 Holborn form part of Transport for London's (TfL) Strategic Road Network.

The site falls within Controlled Parking Zone (CPZ) CA-D Kings Cross Area, which operates Monday to Friday 08:30 to 18:30, and Saturday 08:30 to 13:30. Parking in the surrounding streets is primarily residential permit parking, with one disabled bay located on Brooke Street to the north-east of the site.

The site falls within the London Congestion Charging Zone.

As noted above, the roads which provide

access to the development site include:

- A40 Holborn
- A5200 Gray's Inn Road
- Brooke Street

A40 Holborn runs east/west to the south of the site, running towards St Paul's and Bank in the west and Oxford Street and Marble Arch in the east. At 150 Holborn, the eastbound carriageway has one lane for general traffic, a bus lane and a cycle lane. There are double yellow lines (24 hour no waiting) and no kerb marks (40 minutes maximum loading). The westbound carriageway has two lanes for general traffic and a cycle lane. There are double yellow lines (24 hour no waiting) and double kerb marks (24 hour no loading). In the middle of the road, there is a median strip which includes a traffic island for the north-south pedestrian crossing.

A5200 Gray's Inn Road is a two-way street, terminating 2.10 at A40 Holborn to the south and running northbound towards A501 Euston Road at Kings Cross. At 150 Holborn, the southbound carriageway has a left turn lane and a right turn lane adjacent to a signalised junction with A40 Holborn. There are double yellow lines and double kerb marks. The northbound carriageway has a single lane of traffic. There is a single yellow line (with controlled hours Monday-Friday 08:30-18:30 and Saturday 08:30-13:30) and single kerb marks (with unlimited loading outside of controlled hours).

Brooke Street is a two-way street, terminating at the A40 Holborn to the south and running northbound towards a close with no through vehicle access. On both sides of the road, there is a single yellow line and no kerb marks (no loading restrictions).

Vehicle Access

All vehicular access to the site is from Brooke Street. An off-street service yard is located at the north-eastern corner of the site and is accessed via a crossover from Brooke Street. The crossover to the site is shared by a vehicular access to the basement of the

adjacent Fox Court office building.

The service yard currently contains two servicing bays. All deliveries at the site take place within the site's service yard.

Household waste and recycling collection from 150 Holborn occurs five times per week on Monday, Tuesday and Wednesday.

Pedestrian and Cycle Access

There are good quality footways on both sides of A40 Holborn, A5200 Gray's Inn Road, and Brooke Street. The footways provide access to the local bus services and Chancery Lane Underground Station. The footways are well-lit and in an excellent state of repair.

Dropped kerbs and tactile paving are provided at the signalised junction of Gray's Inn Road and Holborn, and at the priority junction of Brooke Street and Holborn.

There are three Santander Cycle Hire docking stations within 300m of the site, which accommodate a total of 89 docks.

Public Transport

Public Transport Accessibility

The site is highly accessible by public transport and has a Public Transport Accessibility Level rating of 6b which is classified as "Excellent".

London Underground - Chancery Lane Underground Station is immediately on the south-west of the site. The station is served by the Central Line, providing direct access or journeys with only one interchange to the major railway stations at Waterloo, London Bridge, Blackfriars, Liverpool Street, Kings Cross/St Pancras and London Paddington.

Farringdon Underground Station is a 600m (eight minute) walk north-east of the site. The station is served by the Metropolitan, Hammersmith & City and Circle Lines, providing direct access or journeys with only one interchange to the major railway stations.

National Rail - Farringdon National Rail Station is a 600m (eight minute) walk north-east of the

site. The station is served by Thameslink trains from Brighton to Bedford, via Gatwick Airport, or from Luton to Sutton or Wimbledon. The station is also served by trains from Bedford/Luton/St Albans to Sevenoaks in peak hours, and West Hampstead and Kentish Town to Sevenoaks in off-peak hours. From 2018, Farringdon station will also be served by Crossrail 1 (Elizabeth Line) services, which will eventually run from Reading/Heathrow via Central London to Shenfield/Abbey Wood.

Bus

There are six bus routes with stops located within a 300m walk of the site. The closest bus stop is located at the north-western

edge of the site on Gray's Inn Road.

Public Realm

The site is at a busy junction and has a high level of footfall during the working week, especially given its close proximity to one of the main entrances to Chancery Lane tube station.

At present the pavements around the site are rather cluttered and unkempt with street furniture, litter bins, a retail kiosk, phone box, newspaper stand, and the majestic plane trees on Gray's Inn Road and a single Rowan tree on the Holborn Road frontage.

The rather scruffy appearance is exacerbated by the empty retail units, broken paving and litter.

The proposed redevelopment of the site presents a major opportunity to re-invigorate the public realm around the new building and to create a new "stepping stone" in the public realm along Holborn.

In pre-application discussions, the narrowness of the footpaths along the Holborn and Brooke

Street frontages in relation to the amount of pedestrian traffic and the proximity of the tube station entrance and pedestrian crossings was raised. To address this issue the following design response will be incorporated into the design of the new building:

- The building will be set-back further from the kerb edge on the Holborn frontage to create more space for pedestrians;
- The building will be set-back further from the kerb edge on the Brooke Street frontage to create more and safer space for pedestrians;
- The entrance to the new building will be in the middle of the Holborn frontage rather than at the corner of Gray's Inn Road to reduce the density of pedestrian traffic close to the existing pedestrian crossing.
- To further enhance pedestrian connectivity and permeability the site layout will incorporate a new east-west footpath connection between Brooke Street and Gray's Inn Road to recreate the historic route of Fox Court.



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Inclusive Design Statement

The redevelopment at 150 Holborn will provide a mix of office accommodation (Class B1), retail floor space (Class A1-A3), residential units (Class C3) and public realm improvements. It has been designed to take into consideration the requirements of the London Plan and London Borough of Camden planning policies to create an accessible and inclusive environment.

The scheme has been designed to meet the needs of all potential users and will incorporate:

- Suitable means of access for all people from the entrance points
- Sufficiently wide routes allowing people to pass each other with a minimum of level changes
- Principal entrances and lobbies that are identifiable and accessible
- Independent horizontal and vertical movement that is convenient and ensures that people can make use of all relevant facilities
- Dwellings that address LB Camden Policy CS6 and DP6, providing 100% accessible units with 90% of the units conforming to AD M Vol 1. M(4)2 and 10% conforming to M(4)3.

Consultation

David Dropkin FRSA NRAC Consultant, Cabe Built Environment Expert has reviewed the scheme against the legislation, guidance and standards set out in section 2 and found the principles of inclusive design to have been incorporated into the scheme and objectives of the Building Regulations in regards to Part M generally met, subject to further design development.

The design team met the LB Camden's Access Officer on 04 November 2015 to discuss the proposals. Camden's Access Officer confirmed that generally the design team's approach towards inclusive and accessible design was reflected in the scheme. The principle item the design team was asked to consider was the provision of a traditional revolving door with pass door at the principal entry to 150 Holborn, as these were no longer considered acceptable by LB Camden. As requested by the Access Officer, the design now incorporates a single leaf revolving door.



Holborn facade elevation looking east to The Prudential Building

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Planning policy, legislation and standards

The National Planning Policy Framework (2012)

At the heart of the National Planning Policy Framework is a presumption in favour of sustainable development, which should be seen as a thread running through both plan-making and decision-taking. Good design is a key aspect of sustainable development, and is stated to be indivisible from good planning. It should contribute positively to making places better for people. The NPPF sets out that it is important to plan positively for the achievement of high quality and inclusive design for all development, including individual buildings, public and private spaces and wider area development schemes.

The Principles of Inclusive Design (Commission for Architecture and the Built Environment 2006)

Inclusive design is a process that delivers an environment where everyone can access and benefit from the full range of opportunities available to members of society. It aims to remove barriers that create undue effort, separation or special treatment, and enables everyone regardless of disability, age or gender to participate equally, confidently and independently in mainstream activities with choice and dignity.

The adoption of inclusive design principles will ensure the scheme is:

- Inclusive – so everyone can use it safely, easily and with dignity
- Responsive – taking account of what people say they need and want
- Flexible – so different people can use them in different ways
- Convenient – so everyone can use them without too much effort or separation
- Accommodating for all people, regardless of their age, gender, mobility, ethnicity or circumstances
- Welcoming – with no disabling barriers that might exclude some people
- Realistic – offering more than one solution to help balance everyone's needs and recognising that one solution may not work for all.

The London Plan (2014 - FALP/ 2015 - MALP)

London Plan Policies recognise that disabled people continue to be excluded from many mainstream activities which other people take for granted and are frequently denied the opportunity to participate fully as equal citizens.

On 25 March 2015, through a written ministerial statement, the Government introduced new technical housing standards in England and detailed how these would be applied thorough planning policy. These new standards came into effect on the 1 October 2015.

The draft Minor Alterations to the London Plan (MALP) proposed updating Policies 3.5 and 3.8 by adopting the new national technical standards. In the MALP, the Greater London Authority (GLA) set out how the existing policies relating to Housing Standards in the London Plan should be applied from October 2015. The revised Policy requirements for 3.5 and 3.8 are indicated below.

London Plan Policy 3.5 Quality and design of housing developments

Housing developments should be of the highest quality internally, externally and in relation to their context and to the wider environment, taking account of strategic policies in this Plan to protect and enhance London's residential environment and attractiveness as a place to live.

The design of all new housing developments should enhance the quality of local places, taking into account physical context; local character; density; tenure and land use mix; and relationships with, and provision of, public, communal and open spaces, taking particular account of the needs of children and older people.

New development should reflect these standards. The design of all new dwellings should also take account of factors relating to 'arrival' at the building and the 'home as a place of retreat', have adequately sized rooms and convenient and efficient room layouts, which are functional and fit for purpose, meet the changing needs of Londoners over their lifetimes, address climate change adaptation and mitigation and social inclusion objectives and should be conceived and developed through an effective design process.

London Plan Policy 3.8 Housing choice

Londoners should have a genuine choice of homes that they can afford and which meet their requirements for different sizes and types of dwellings in the highest quality environments.

From the 1 October 2015 the Policy requirement is that ninety percent of new housing meets Building Regulation requirement M4(2) 'accessible and adaptable dwellings' and ten per cent of new housing meets Building Regulation requirement M4(3) 'wheelchair user dwellings', i.e. is designed to be wheelchair accessible, or easily adaptable for residents who are wheelchair users.

London Plan Policy 7.2: An inclusive environment

The Mayor will require all new development to achieve the highest standards of accessible and inclusive design and supports the principles of inclusive design which seek to ensure that developments [adopt Cabe's principles of inclusive design]. DPD policies should integrate and adopt the principles of inclusive design.

Design and access statements submitted with development proposals should explain how, following engagement with relevant user groups, the principles of inclusive design, including the specific needs of older and disabled people, have been integrated into the proposed development, whether relevant best practice standards have been complied with, and how inclusion will be maintained and managed.

GLA supplementary planning guidance: Accessible London (2014)

The Accessible London SPG provides advice to boroughs, developers, designers and planning applicants on implementing inclusive design principles effectively and on creating an accessible environment in London, with particular emphasis on the access needs of disabled and older people. It does not introduce new policy or add any additional burdens on developers. It does not form part of the development plan, but will be taken into account as a further material consideration when considering planning applications so has weight as a formal supplement to the London Plan.

GLA supplementary planning guidance: Draft Interim Housing SPG 2015

The SPG provides guidance on how to implement the housing policies in the London Plan informed by the Government's National Planning Policy Framework and by its Housing Strategy for England. In particular, it provides detail on how to carry forward the Mayor's view that 'providing good homes for Londoners is not just about numbers'. The challenge of accelerated housing delivery cannot be met at the expense of housing quality. In fact, the scale of growth to be accommodated in London, an already dense city, underlines the importance of securing well-designed new housing. It reinforces the need for new development to provide accessible, adaptable and attractive homes and neighbourhoods that stand the test of time. The draft SPG carries forward the Mayor's housing design standards for London in the context of Government's new national housing standards.

LB Camden

Camden Core Strategy 2010 Policy CS 6 Providing Quality Homes

Accessibility issues affect most households at some point. Lifetime Homes are dwellings specifically designed to address a wide-range of mobility difficulties without being tailored to the specific circumstances of a household. The Council expects all dwellings developed

in the borough to meet Lifetime Homes standards.

Guidance on application of the standards is included in Camden Development Policies (see policy DP6) and our Camden Planning Guidance supplementary document 6.41.

The Council therefore expects that 10% of homes developed in the borough should either be designed to be wheelchair accessible, or easily adaptable for residents who are wheelchair users, in accordance with the London Plan. Guidance on the 10% policy is set out in Camden Development Policies (see policy DP6) and our Camden Planning Guidance supplementary document

Policy CS 14 Promoting high quality places and conserving our heritage

The Council will ensure that Camden's places and buildings are attractive, safe and easy to use by: requiring development of the highest standard of design that respects local context and character ... seeking the highest standards of access in all buildings.

Good design is safe and accessible, and responds flexibly to the needs of its users. Good access benefits everyone. The Council requires new buildings and spaces to be inclusive and accessible to all.

Development Policies 2010 Policy DP6 Lifetime Homes and Wheelchair Housing

All housing development should meet lifetime homes standards. 10% of homes developed should either meet wheelchair housing standards, or be easily adapted to meet them.

Camden Planning Guidance CPG6 Housing 2015

All residential development should meet the 16 criteria that form the Lifetime Homes standards. • The standards will be applied flexibly to existing buildings, but applicants should justify failure to meet any of the criteria. 10% of market housing development should meet wheelchair housing standards, or should meet the 13 key Habinteg wheelchair housing criteria so that they can be easily adapted to meet wheelchair housing standards.

CPG6 Amenity: Section 9 Access for All

Well designed, accessible buildings and spaces ensure that local services and facilities are accessible to everyone and increase equality of opportunity and social inclusion. We will seek to ensure the highest standards of access and inclusion in Camden's built environment and public realm. • We expect all development of buildings and places, including changes of use and alterations to or refurbishment of existing buildings where practical and reasonable, to be designed to be accessible and useable by all to promote equality of opportunity. • Access should be considered at the beginning of the design process.

Inclusive Design Guidance and Standards

Our approach is to design beyond the minimum standards set out in the building regulations, taking into account good practice guidance. Inclusive design issues have been reviewed accordance with the objectives of the following:

1. Building Regulations 2000 (England and Wales) and associated Approved Documents
2. LB Camden Planning Policy
3. Equality Act 2010
4. Planning and Compulsory Purchase Act, 2004
5. National Planning and Policy Framework, 2012

Reference has been and will continue to be made to the following documents during design development for this project:

- Accessible London, Supplementary Planning Guidance, GLA 2014
- BS 8300:2009+A1:2010 Design of buildings and their approaches to meet the needs of disabled people – Code of practice, British Standards Institution
- Building Regulation Approved Document K – Protection from falling, collision and impact 2013 (AD K)
- Building Regulation Approved Document M – Access to and use of Buildings Vol 1: Dwellings, 2015 (AD M)
- Building Regulation Approved Document M – Access to and use of Buildings Vol 2: Buildings Other Than Dwellings, 2015 (AD M)
- Draft Interim Housing, Supplementary Planning Guidance, GLA 2015

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The scheme

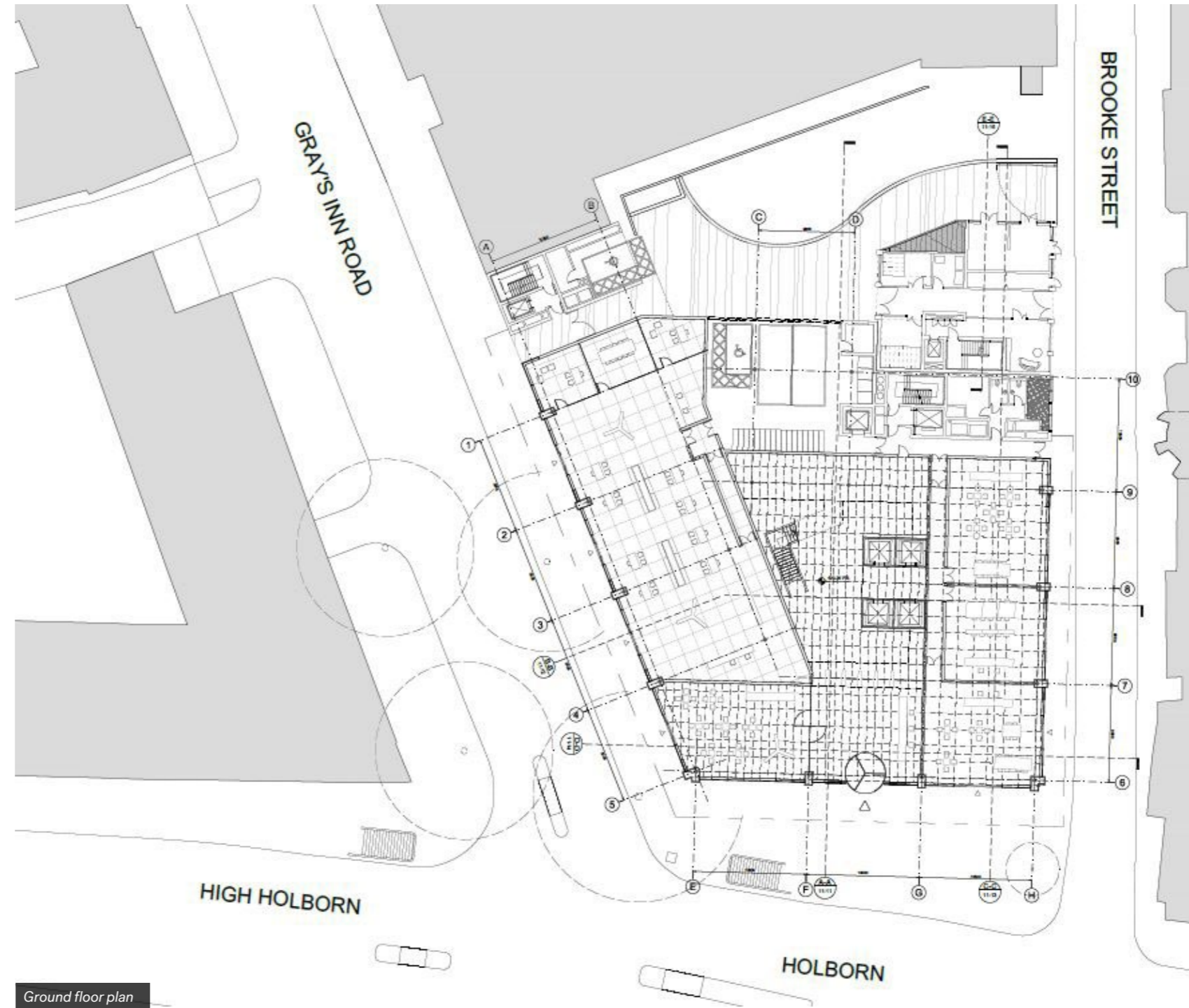
Approach

150 Holborn has been designed to be an inclusive and accessible environment to meet the needs of all potential users. In removing barriers that create undue effort, separation or special treatment, everyone, regardless of disability, age or gender will be able to use the facilities of the scheme offers equally, confidently and independently with choice and dignity.

The mixed-use scheme is located at northeast corner of the intersection Holborn and Gray's Inn Road with Brooke Street running along the eastern edge of the site. The scheme will incorporate net internal areas of 10,622sqm of B1 office space (Ground plus 8 stories), 1,353sqm A1/A3 retail (Ground and Basement levels) plus 866sqm of C3 residential accommodation at the rear of the site.

The scheme will restore a degree of permeability between Brooke Street and Gray's Inn Road, re-establishing Fox Court, which will link the Prudential Building with Gray's Inn Road.

In creating Fox Court, the public realm will have firm, slip-resistant and reasonably smooth surface treatments. Where there are different materials along the access routes they will have similar frictional characteristics and joints will conform to the provisions of AD M. Fox Court will be a shared space, but the level of traffic will be very low and controlled, as the vehicular traffic into the Court will be limited to refuse collection, deliveries and to the two designated parking bays for disabled people. The landscaping of Fox Court is described in greater detail in Section 4.6 Landscape of this submission.



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Addressing external areas during design development

- Materials used in the external environment will contrast visually against the backgrounds which they are seen
- If seating is provided, then consideration will be made in the landscaping to provide a variety of seating heights, with and without backrests and arm and will allow space for wheelchair users to sit with other people in a dignified manner.

Parking

The scheme is a 'car-free scheme'. Two parking bays will be provided both conforming to AD M providing a 1200mm access zone to one side of the with a 1200mm rear safety zone. The parking bay adjacent to the loading area will be allocated to the office; the second bay will be allocated to the wheelchair adaptable residential unit. It will have 1200mm to both sides and the rear and thus meet the requirement of M4(3) 3.12.

Both parking bays will provide charging points.

Cycle storage for both the office and residential units will be secure and adequately lit.

Office cycle parking will be accommodated within the basement. The two goods lifts serving the basement will provide for access to basement cycle stores. Showers, lockers, changing spaces and drying rooms for clothes will be provided.

There will be associated male/female showering, changing and drying facilities; there will be a wheelchair accessible shower/WC co-located with these facilities.

The residential cycle store will be located at with its entrance off Fox Court.

Addressing car and cycle parking during design development

- A car parking management plan will be put in place to assist in securing parking that is suitable for tenants when required
- Provision of storage and charging facilities for scooter users will be incorporated
- To promote accessible cycling, provision of suitably spaced stands will be incorporated for non-standard cycles such as tricycles or recumbent cycles.

Retail Units

The ground floor retail unit(s) will be accessed from Gray's Inn Road. Principle entrance doors will meet or exceed the effective clear width requirements of AD M Table 2 and provide at least 1000mm effective clear width.

For the purposes of this submission, the ground and basement level retail unit will be shell and core and will be subject to tenant fit-out.

150 Holborn Offices

The principal entrance to the office block (Ground plus 8 stories) will face Holborn. The entrance will be comprised of a single leaf revolving door (available from Boon Edam and other manufacturers). LB Camden's Access Officer confirmed that this form of revolving door was acceptable to the Local Authority. The atrium will run to the full height of the office building.

At ground level there will be reception facilities and a security gateline. The security gateline will provide at least one opening of at least 800mm effective clear width.

Staff amenity facilities will be located at ground and basement levels.

Movement between levels will be either stepped (stairs will encircle the atrium) or be via lift.

A central atrium accommodates vertical circulation in the form of four lifts and an accommodation stair that links all levels of the building. The intention of the central atrium and associated stair is intended to promote health and well-being, encouraging staff members to move between levels without using the lifts. However, with four adjacent lifts, any staff member (or visitor) who requires step-free access between levels will be accommodated.

The accommodation stair will conform to the provisions of AD K, AD M and BS 8300. The atrium stairs will be approximately 1500mm wide, have indicative nosings and closed risers. There will be two stair cores for egress located at the north east and north west of the building. These stairs will be designed to Part B requirements for fire egress and Part K as utility stairs, as their primary rationale is for egress from the building.

The four type 2 lifts (lift cars approximately 1800mm x 1800mm) serving the office will exceed the minimum requirements of AD M.

Sanitary accommodation will be adjacent to each of the core stairs. At each core at Levels 01 to 07 there will be unisex wheelchair accessible WCs co-located with separate sex accommodation. Where unisex wheelchair accessible WCs is provided in two locations on the same level, the accommodation will be 'handed' to allow right-hand and left-hand transfer. Wheelchair accessible WCs will be within 40-45m of the most distant parts of the floorplates.

Level 08 will function as a meeting space and entertainment space: no sanitary accommodation is provided at this level; however, sanitary accommodation will be one floor below at Level 07.

The design parameters set out in Section 4 below will be adopted as part of the office fit-out.



View of atrium



Roof terrace plan

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Inclusive Design Parameters

The following high-level parameters outline a recommended approach when delivering an inclusive environment and in going beyond the minimum standards of the building regulations should be considered in design development and detailed design for non-residential development.

Access

- All doors are to be clearly identifiable within their immediate environment
- Door widths and clearances are to meet the provisions of AD M and AD M Table 2
- The force of operation of doors should not exceed 30N or the doors are to be automated
- Ironmongery is to meet the provisions of AD M or the guidance of BS 8300
- Where glazing is used within the facade and entrances; manifestations are to be provided to meet the provisions of AD K or guidance of BS 8300.

Circulation

- Internal stairs are to be designed to accord with the provisions of AD K and AD M
- Stepped routes and associated handrails are to be designed to meet the provisions of AD K and AD M
- Visually contrasting nosings are to be included on all stepped routes, to meet the requirements of AD K and the recommendations of BS 8300
- All passenger lifts are to be specified to meet AD M, BS 8300 and BS EN 81-70.

Sanitary Accommodation

All accommodation should be designed to be inclusive and to the same specification rather than wheelchair user accessible provision being designed to be 'clinical' or institutional. Designs should incorporate:

- Facilities that provide a choice of heights
- Fixtures and fittings will contrast visually from their backgrounds
- In addition to 'standard' accommodation, enlarged and ambulant cubicles should be included in separate sex accommodation
- Wheelchair accessible toilets are to be 'handed' for left-hand and right-hand transfer when there are more than one unisex wheelchair accessible WC serving a facility.

Internal finishes, fixtures and fittings

- Surface materials are to be firm, durable and slip resistant
- Particular attention will be paid to visual contrast, lighting and materials for features such as walls, ceilings and floors
- In areas where clear communication is required particular attention will be paid to the reverberant nature of materials
- Fixtures and fittings, including ironmongery and controls are to visually contrast with their surrounds, be usable by people with limited dexterity and reachable by those who are seated or standing

Lighting

Lighting at changes of level in stairwells will be provided to ensure they are safe to use and will not shadow risers, and will clearly illuminate treads and ramp surfaces.

Communication Aids and Information Systems

Communication aids such as audio reinforcement systems and other sound reinforcement systems to assist hearing aid users, as well as the hard-of-hearing and deafened staff and visitors should be provided as appropriate.

Signage and Wayfinding Strategies

An inclusive wayfinding and signage strategy is to be developed. Key considerations are:

- Location of signage, ensuring that signage is placed in logical positions and will be easily identifiable
- The strategy will take into account sight lines, lighting, visual contrast and legibility are appropriate.

Emergency Egress

Evacuation for disabled people is a combination of physical and management procedures plus a liaison between the building managers and occupants. The fire-fighting lifts will be used as evacuation lifts.

The evacuation strategy for disabled people including those with sensory and cognitive impairments should be developed in conjunction with the fire strategy and management policies with reference made to AD B and good practice recommendations within BS 9999:2008.

- All refuge areas should be to be fitted

with a means of two-way communication between the person waiting in the refuge and a central monitored location from where evacuation assistance will be coordinated

- If appropriately managed, the lobbies may permit more than one disabled person to wait safely for assistance; although only one official refuge space is indicated
- Management strategies can also be set in place to provide assisted escape via the steps if required
- Staff may need to be suitably trained in evacuation features and procedures including 'carry-up' and 'carrydown' procedures for evacuation of wheelchair users
- A Personal Emergency Egress Plan (PEEP) should be developed by disabled staff members in liaison with their employers and the building management.

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Residential units

The proposed scheme creates 13 new dwellings of which all are single level units, all of which are market housing. Details of residential accommodation can be found in this submission. In meeting the provisions of AD M Vol 1 M4(2) and M4(3) the design of the units satisfies the requirements of LB Camden Development Policy DP6.

A dedicated residential reception area with the principle entrance to residential core will be from Brooke Street or Fox Court. The principle communal entrances will conform to the provisions of AD M and include weather protection to the depth of at least 900mm. The principal entrance door will provide at least 850mm clear opening width.

A single lift will provide step-free access to all dwellings in the scheme. Lift car doors will have a clear width of 800mm or greater and a clear internal area of at least 1100mm x 1400mm.

Circulation

The scheme will incorporate features making reasonable provision for disabled people to visit occupants who live on any storey. For many people, suitable means of access means independent lift access; for others it means having stairs that are designed to be suitable for ambulant disabled people. Circulation corridors will be approximately 1500mm.

The lifts will have, as a minimum, a 1500mm x 1500mm clear landing in front of entrances.

Addressing circulation during design development

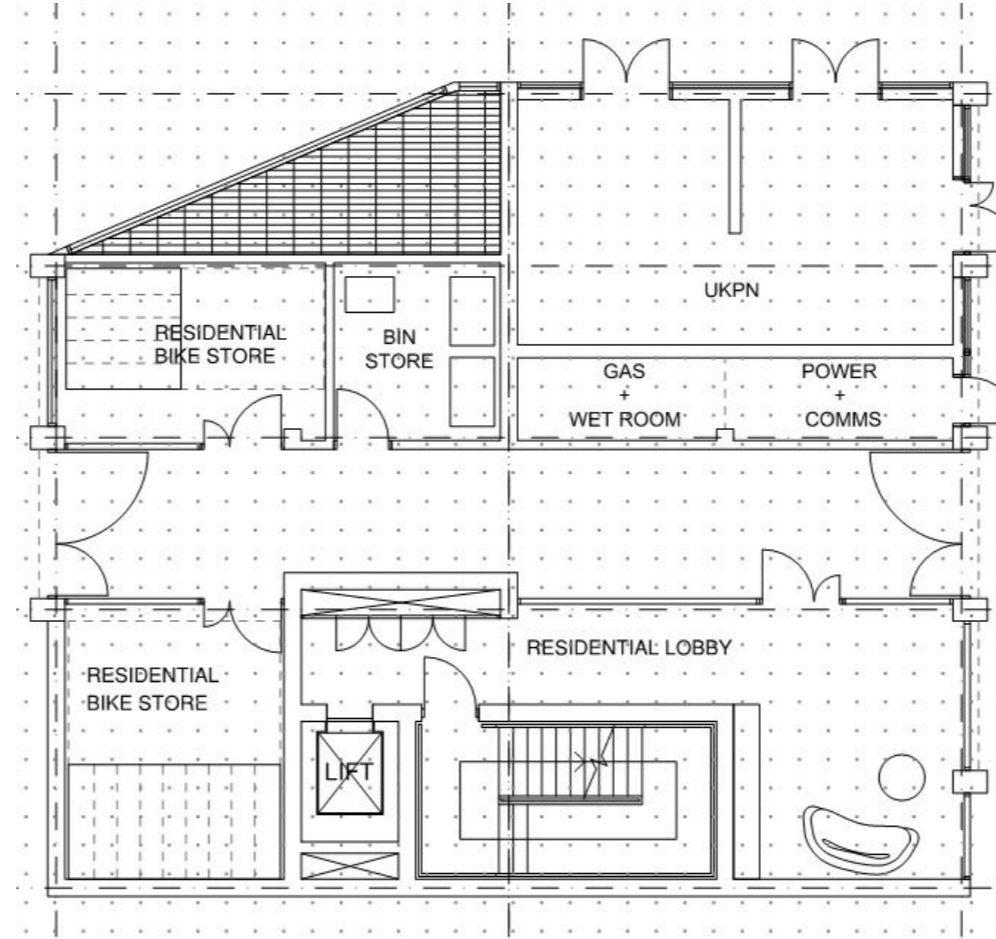
- Lifts and their controls will be specified to comply with AD M, and BS EN 81-70
- Stair nosings will contrast visually as per AD K requirements
- Continuous handrails to both sides of the internal stairs will conform to AD K/BS 8300 and be 900-1000mm above the pitch line; and 900-1100mm above the level landings. They will contrast visually with against the background which they are seen.

Residents' refuse disposal

Routes to communal bin enclosures and refuse stores and the internal layouts will be accessible to all residents including children and wheelchair users.

Residential Emergency Egress

Evacuation of disabled people is a combination of physical and management procedures plus a liaison between building managers and occupants. The evacuation strategy for disabled people will be developed in conjunction with the fire strategy and management policies with reference made to AD B and good practice recommendations within BS 9991:2015.



Residential ground floor plan

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Draft interim Housing SPG

Annex 1 Checklist Draft Interim Housing SPG

Table 6 1: Annex 1 Checklist is an extract from the GLA's Interim Draft Housing SPG Annex 1 demonstrating that the design conforms to the standards that came into force on 1 October 2015. The extracted design standard references from the Interim Draft Housing SPG Annex 1 relate only to those issues that have a crossover with accessible and adaptable dwellings, wheelchair user housing or have an impact on the accessibility or inclusiveness of the scheme's housing components.

Interim Draft Housing SPG, 2015	Met/ Comments		
Entrance and Approach			
All main entrances to houses, ground floor flats and communal entrance lobbies should be visible, clearly identifiable, and directly accessible from the public realm.	X	Where parking is provided in communal bays, at least one space should be provided per block entrance or access core that complies with Part M4(2) in addition to spaces designated for wheelchair user dwellings.	Car-free scheme; only one bay car parking bay provided to satisfy 3.3.2 above
The distance from the accessible car parking space of standard 3.3.4 to the home or to the relevant block entrance or lift core should be kept to a minimum.	X	Refuse, post and deliveries	
Approach routes should comply with the requirements of Part M4(2) unless they also serve Wheelchair User Dwellings, where they should comply with the requirements of Part M4(3).	X	Communal refuse and recycling containers, communal bin enclosures and refuse and recycling stores should be easily accessible to all residents including children and wheelchair users, and located on a hard, level surface. The location should satisfy local requirements for waste collection. Refuse and recycling stores within buildings should be located to limit the nuisance caused by noise and smells and maintained to a high hygiene standard.	X
All entrances should comply with the requirements of Part M4(2), unless they also serve Wheelchair User Dwellings, where they should comply with the requirements of Part M4(3).	X	Circulation in the home	
Active frontages should be maximised and inactive frontages minimised on the ground floor of buildings facing publically accessible space, in order to provide natural surveillance and activity. Active frontages are defined as development frontage on the ground floor where inhabited residential or non-residential uses are located, with a visually permeable elevation (eg windows or glazing) and a generous distribution of entrances.	X	The minimum width of hallways and other circulation spaces inside the home should comply with Part M4(2).	X
Shared Circulation			
Each core should be accessible to generally no more than eight units on each floor.	X	The design of dwellings of more than one storey should incorporate potential for a future stair lift to be installed by providing stairs that complies with M4(2).	X
Communal circulation should comply with Part M4(2), unless they also serve Wheelchair User Dwellings, where they should comply with the requirements of Part M4(3).	X	There should be space for turning a wheelchair in dining areas and living rooms and basic circulation space for wheelchairs elsewhere.	X
Every M4(2) dwelling should be provided with step-free access. All dwellings entered at the seventh floor (eighth storey) and above should be served by at least two lifts.	X	A living area that complies with M4(2) should be at entrance level.	X
Every designated wheelchair accessible dwelling above the ground floor should be served by at least one wheelchair accessible lift. It is desirable that every wheelchair accessible dwelling is served by more than one lift.	X	Glazing to the principal window should comply with Part M4(2). At least one opening window in the principal living area should comply with Part M4(2).	X
Principal access stairs should provide easy access regardless of whether a lift is provided. Where homes are reached by a lift, it should be fully wheelchair accessible.	X	Bedrooms	
Car Parking			
All developments should conform to LP policy on car parking provision (see Annex 3 of the Housing SPG for guidance on implementation of relevant policy including LP Policy 6.13 and associated standards below). In areas of good public transport accessibility and/or town centres the aim should be to provide no more than one space per dwelling. Elsewhere parking provision should be broadly as follows, depending on location as indicated in Annex 3:	Scheme is 'car-free'; one designated parking bay will be provided	The minimum area of a single bedroom should be 7.5sqm. The minimum area of a double or twin bedroom should be 11.5sqm to comply with the nationally described space standard.	X
Each designated wheelchair accessible dwelling should have a car parking space that complies with Part M4(3).	X	One double or twin bedroom should be at least 2.75m wide and every other double or twin should be at least 2.55m wide, in most of the length of the room, to comply with the nationally described space standard.	X
Bathrooms and WCs			
To provide step-free access to a WC that is suitable and convenient to some wheelchair users and, where reasonable, to make provision for showing, dwellings should comply with the requirements of Part M4(2).			
An accessible bathroom that complies with Part M4(2) should be provided in every dwelling on the same storey as a double (or twin) bedroom.			
Walls in the bathrooms and WCs should be capable of taking adaptations and comply with Part M4(2).			
To be addressed in design development/ detailed design			
Study and work			
Service controls should be within easy reach and comply with Part M4(2).			
To be addressed in design development/ detail design			

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Accessible and adaptable dwellings

The table below demonstrates the level of compliance with key elements of the optional Building Regulation M4(2) provision. The following is a key to the table adjacent.

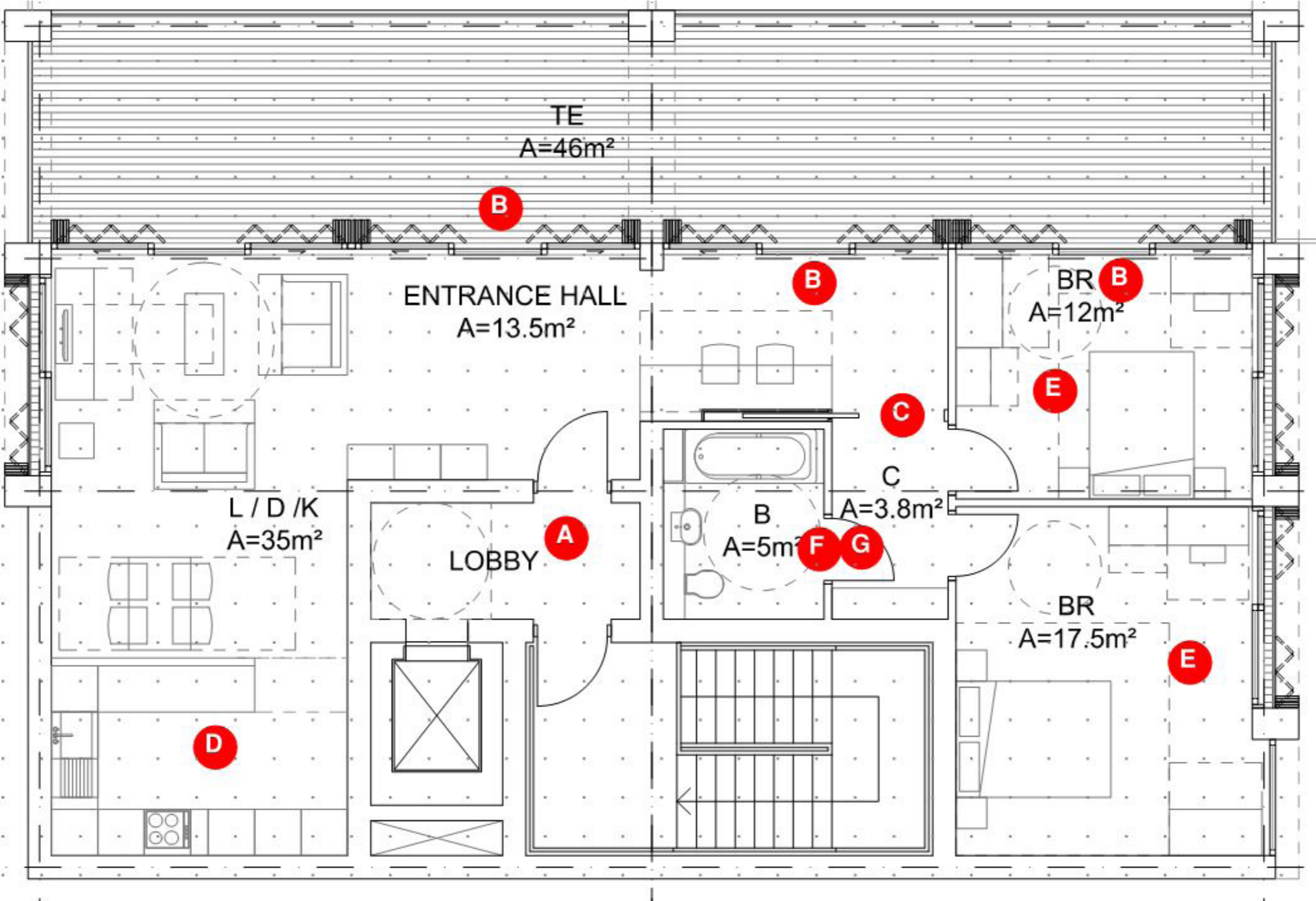
- **Compliant**
- **To be addressed in design development/detailed design**

M4(2)	Diagram key	Description	Summary of design provisions	Status	Comments
2A Approach to Dwelling					
2.6-2.9		Approach routes: general	The approach route should be safe convenient, adopt the shallowest gradient that can be achieved and be step-free, irrespective of the storey on which the dwelling is located.		
2.10		External and internal ramps forming part of an approach route	Where ramps are provided the gradient should be between 1:12 and 1:21 and the length of each flight comply with Diagram 2.1 of AD M Vol 1.	N/A	No route is steeper than 1:21
2.11		External steps forming part of an additional route	Where required steps are uniform with rises between 150-170mm and goings between 280-425mm, a minimum clear width of 900mm and provided with suitable handrail.	N/A	No external stepped access
2.12		Parking Space	Where provided at least one standard parking bay is provide close to the communal entrance of each core block (or to the lift core where the parking bay is internal) the parking bay should have a minimum clear access zone of 900mm to one side and a dropped kerb.		No standard bays provided; this provision can be addressed by the parking management strategy.
2.13		Drop-off Space	Where provided it is to be located close to the principal communal entrance of the building.		No formal drop off space provided; however, residents could be dropped off in the service area if required.
2.14	A covered level landing that is lit by fully diffused luminaires Accessible entrance door, with suitable clear opening and manoeuvring space Accessible door entry controls Level and covered entry are provided for. The remaining elements will be addressed during design development and detailed design.	Principal communal entrance	The principal communal entrance is to be accessible and to have:		
2.15	Accessible entrance door, with suitable clear opening and manoeuvring space Accessible door entry controls	Other communal doors	Other communal entrance is to be accessible and to have:		
2.16		Communal Lifts	The lift is equivalent to or meets the requirements of BS EN 81 70		Lift car size conforms; details to be addressed during design development and detailed design stages.
2.17		Communal Stairs	Stairs should meet the requirement of Part K for general access stairs		All circulation routes are step free; stair core stairs will conform to Part K

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2B: Private entrances and spaces within the dwelling					
2.20	A	<p>The principal private entrance is to be accessible and to have:</p> <ul style="list-style-type: none"> Principle private entrance and alternative entrance A covered level landing that is lit by fully diffused luminaires Accessible entrance door, with suitable clear opening and manoeuvring space 	Entrances covered, spatial requirements met; other elements to be addressed in design development and detailed design stages.		
2.21	B	<p>Other external doors are to be accessible and to have:</p> <ul style="list-style-type: none"> Accessible entrance door, with suitable clear opening and manoeuvring space 			
2.22	C	Doors and hall widths	Doors and corridors within the dwelling should comply with Diagram 2.3 and Table 2.1		
2.23		Private stairs and changes of level within the dwelling	The stair should allow for the installation of a stair-lift at a future date and comply with the provisions of Part K for private stairs	N/A	All single level units
2.24	D	Living, kitchen and eating areas	<p>Within the entrance storey there is a living area (living room, dining room or a combined living and kitchen and dining room):</p> <p>Glazing to the principal window starts at a maximum of 850mm above floor level.</p>		
2.25	E	Bedrooms	<p>Every bedroom provides a clear access route of 750mm from the door to the window.</p> <p>750mm access zone is provided to all sides of the bed in the principal bedroom and to one side and the foot in the other double bedrooms and to one side for single beds</p>		
2.26		Sanitary facilities: general provisions	All walls, ducts and boxings to the WC/cloakroom, bathroom and shower room should be strong enough to support grab rails, seats and other adaptations.		To be addressed in design development and detail design stages.
2.27 -2.28	F	WC facilities on the entrance level	Every dwelling has a room within the entrance storey that provides a WC and basin.		
2.29	G	Bathrooms	Every dwelling has an accessible bathroom with a WC on the same storey as the principal bedrooms		
2.30		Services and controls	Services and controls within the dwelling are to be accessible to people who have reduced reach.		To be addressed in design development and detail design stages.

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Residential sixth floor plan

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Wheelchair user dwellings

In accordance with the MALP and LB Camden Development Policy DP6 a single unit (representing 10%) will meet optional Building Regulation M4(3) Wheelchair User Dwellings. M4 (3) 'wheelchair user dwellings' distinguishes between 'wheelchair adaptable' and 'wheelchair accessible'. Wheelchair adaptable homes are homes that can be easily adapted to meet the needs of a household including wheelchair users and adopt provisions for M4(3) 2(a). Wheelchair accessible homes are homes readily useable by a wheelchair user at the point of completion and adopt provisions for M4(3) 2(b). The single market sale unit will be provided as an easily adaptable wheelchair housing unit M4(3)2(a).

Table 8 1 below demonstrates the level of compliance with key elements of the M4(3) 2(a) which allows for simple adaptation of the dwelling to meet the needs of occupants who are wheelchair users.

The following is a key to Table 8 1 below.

- **Compliant**
- **To be addressed in design development/detailed design**

M4(3) 2(a)	Diagram key	Description	Summary of design provisions	Status	Comments
3A Approach to Dwelling					
3.7-3.9		Approach routes: general	The approach route should be safe convenient, adopt the shallowest gradient that can be achieved and be step-free, irrespective of the storey on which the dwelling is located. Approach routes to dedicated mobility storage (where provided) should also be step-free		Step-free access is provided from Brooke Street, Fox Court or from the designated car parking bay and via internal step-free routes.
3.10		External and internal ramps forming part of an approach route	Where ramps are provided the gradient should be between 1:15 and 1:20 and the length of each flight comply with Diagram 3.1 of AD M Volume 1.	N/A	No gradient steeper than 1:21
3.11		External steps forming part of an additional route	Where required steps are uniform with rises between 150-170mm and goings between 280-425mm, a mini-mum clear width of 900mm and provided with suitable handrail.	N/A	All external routes are step-free
3.12		Parking Space	Where provided within a communal parking area, it is a standard parking bay with an additional minimum clear access zone of 1200mm to both sides		1 bay provided
3.13		Drop-off Space	Where provided it is to be located close to the principal communal entrance of the building.		No formal drop off space provided; however, residents could be dropped off in Fox Court, if required.
3.14		Principal communal entrance	The principal communal entrance is to be accessible and to have: <ul style="list-style-type: none"> A covered level landing that is lit by fully diffused luminaires Accessible entrance door, with suitable clear opening and manoeuvring space Power assisted opening is provided where the opening force exceeds 30N Accessible door entry controls 		Level and covered entry are provided for. Door width is suitable. The remaining elements will be addressed during design development and detailed design.
3.15		Other communal doors	Other communal entrance is to be accessible and to have: <ul style="list-style-type: none"> Accessible entrance door, with suitable clear opening and manoeuvring space Power assisted opening is provided where the opening force exceeds 30N Accessible door entry controls 		Other communal doors provide suitable clear widths; remaining elements addressed during design development and detailed design.
3.16		Communal Lifts	The lift is equivalent to or meets the requirements of BS EN 81:70		
3.17		Communal Stairs	Stairs should meet the requirement of Part K for general access stairs		



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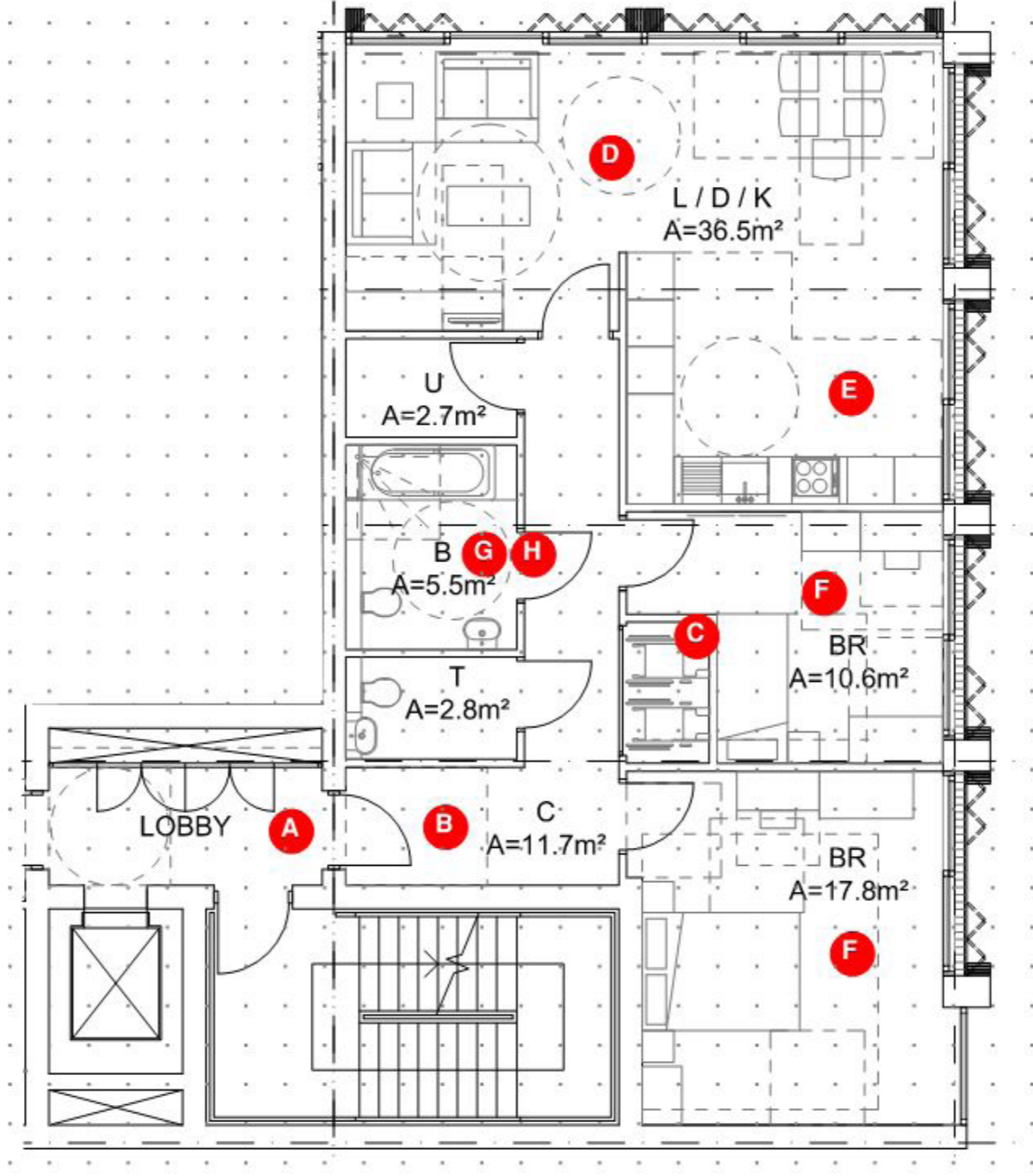
3B: Private entrances and spaces within, and connected to, the dwelling

3.22	A	Principle private entrance	<p>The principal private entrance is to be accessible and to have:</p> <ul style="list-style-type: none"> • A covered level landing that is lit by fully diffused luminaires • Accessible entrance door, with suitable clear opening and manoeuvring space • Accessible door entry controls • A fused spur, suitable for fitting a powered to door opener, is provided 		All principal private entrances conform with the clear opening width requirements; other elements will be addressed through design development and specification
3.23		Other external doors	<p>Other external doors are to be accessible and have:</p> <ul style="list-style-type: none"> • Accessible entrance door, with suitable clear opening and manoeuvring space • Accessible door entry controls 		All other external doors conform with the clear opening width requirements; other elements will be addressed through design development and specification
3.24	B	Hall and door widths	Complaint doors width to be provided and corridors within the dwelling should comply with Diagrams 3.4 and 3.5		
3.25	C	Wheelchair storage and transfer space	A dwelling should have a storage and transfer space		
3.26		General storage space	The dwelling built in storage space should comply with Table 3.1		
3.28		Through floor lifting device provision	It should be easy to install a lift. The space for a liftway can be used for another purpose (such as storage) providing it can be demonstrated that a liftway can be provided.	N/A	Wheelchair adaptable unit will be single storey
3.30		Private stairs and changes of level within the dwelling	<p>Access to all rooms and facilities on the entrance storey are step-free.</p> <p>The stair should allow for the installation of a stair-lift at a future date and comply with the provisions of Part K for private stairs</p>	N/A	Wheelchair adaptable unit will be single storey
3.31	D	Living areas	<p>Within the entrance storey there is a living area (living room, dining room or a combined living and kitchen and dining room)</p> <p>The minimum combined internal floor area of living, dining and kitchen space meets the provisions of table 3.2.</p> <p>Glazing to the principal window starts at a maximum of 850mm above floor level.</p>		
3.32-3.34	E	Kitchen and eating areas	<p>The kitchen should be located on the ground floor.</p> <p>A clear access zone of 1500mm in front of the kitchen units is provided and a minimum length of worktop to comply with Table 3.3</p>		
3.35	F	Bedrooms	<p>One bedroom should be close to an accessible bathroom and suitable for a wheelchair user. All other bedrooms are to be accessible to a wheelchair users.</p> <p>The principal bedroom has a minimum floor area of 13.5m² and is a minimum of 3m wide, clear of obstructions. Every other bedroom is 12.5m² and a minimum of 3m wide</p>		
3.36		Sanitary facilities: general provisions	<p>All walls, ducts and boxings to the WC/cloakroom, bathroom and shower room should be strong enough to support grab rails, seats and other adaptations.</p> <p>The ceiling structure to accessible bathrooms and WC/cloak rooms is strong enough to allow the fitting of an overhead hoist.</p>		To be addressed in design development and detail design stages.
3.37 – 3.40	G	WC facilities on the entrance level	Every dwelling has on the entrance storey a wet room, which contains a WC, basin and installed level access shower.		

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3.41 – 3.43	H	Bathroom facilities	<p>Dwellings with up to four bedspaces should be provided with a bathroom that contains a WC, basin and an installed level access shower with the potential for a bath to be installed above it. The bathroom with the level access shower should be on the same storey as the principal bedroom.</p> <p>Dwellings with 5 or more bedspaces require an accessible bathroom and shower room.</p>	
3.44		Services and controls	<p>Services and controls within the dwelling are to be accessible to people who have reduced reach.</p>	
3.45		Private outdoor space	<p>Every private outdoor space provided, whether a private garden, balcony or roof terrace is to be accessible to a wheelchair user.</p>	N/A

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Residential first floor plan