

21 KIDDERPORE GARDENS
HAMPSTEAD
LONDON NW3 5PS


PROPOSED SINGLE FLOOR BASEMENT
AND ASSOCIATED WORKS



CONSTRUCTION MANAGEMENT PLAN
MARCH 2015

B&G CONSTRUCTION
48 Churchfield Road,
London W3 6DL
Bandg-construction.co.uk

B&G
CONSTRUCTION

CONTENTS	.
Introduction	
Section 1 – Site Contacts	
Section 2 – About the Site	
Section 3 – Transportation Issues Associated with the Site	
Section 4 – Traffic Management for the Site	
Section 5 – Environmental Issues	
Section 6 - Monitoring, Compliance, Reporting and Consultation about Traffic and Activities related to the Site	
Queries: planningobligations@camden.gov.uk	
 Camden	

CONSTRUCTION MANAGEMENT PLAN

Section 1 – Site Contacts

Introduction

B&G Construction have been engaged to make alterations to the single family house at 21 Kidderpore Gardens, Hampstead, London NW3 5PS as approved in 2013/0149/P. The works involve the proposed enlargement by creating a single floor basement beneath the footprint of the original house and lights wells to the front and side to allow ventilation to the new rooms and other minor alterations (all as approved under application no. 2012/0218/P) plus a new single storey minimal glazed rear extension.

The property lies within The Redington and Frognal Conservation Area. The Conservation Area is further sub-divided and Kidderpore Gardens lies within Sub Area 5. Kidderpore Gardens runs between Kidderpore Avenue and Ferncroft Avenue. Kidderpore Gardens largely comprises of semi-detached houses. St Margret's Independent Day Girls School (4 to 16 years olds) is located opposite the property, though the main entrance to the school is in Ferncroft Avenue.

B&G Construction are a well-established local contractor with substantial experience in delivering this type of project and also in working in similar central London locations. Recent projects include housing development work in Eccleston Square, Hampstead Lane, Bishops Avenue, Abercorn Place, Langford Place, Pond Square and Quickswood. Details of these developments and others can be found on our company website bandg-construction.co.uk.

This Construction Management Plan has been prepared in line with good practise and as required by the Section 106 Agreement with the London Borough of Camden dated 21st June 2013. The Section 106 Agreement states:

'A Construction Management Plan outlines how construction work will be carried out and how this work will be serviced (e.g. delivery of materials, set down and collection of skips), with the objective of minimising traffic disruption and avoiding dangerous situations and minimising the impact on local amenity. A Construction Management Plan should cover both demolition and construction phases of development. Details of the Construction Management Plan will relate to the scale and kind and location of the development and they should assess the impact on transport and on local amenity including road user amenity. Should any one of these criteria be considered not to be relevant, then specific justification, as to why that particular criterion is not relevant, will need to be provided.'

The Section 106 Agreement also contains a Schedule which sets out the issues that should be addressed in a Construction Management Plan. For clarity and ease of reference, these are set out below. However Camden have prepared a pro-forma for Construction Management Plans and this has been followed in preparing this document. However, all of the issues set out in the S106 Schedule have been addressed.

S106 Schedule

- 1 A brief description of the site, surrounding area and development proposals for which the Construction Management Plan applies
- 2 Proposed start and end dates for each phase of construction
- 3 The proposed working hours within which vehicles will arrive and depart
- 4 The access arrangements for vehicles
- 5 Proposed routes for vehicles between the site and the Transport for London Road Network (TLRN)
- 6 Typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction
- 7 Swept path drawings
- 8 Details of any highway works
- 9 Parking and loading arrangement of vehicles and delivery of materials and plant
- 10 Details of proposed parking bays suspensions and temporary traffic management orders
- 11 Proposed overhang (if any) of the public highway (scaffolding, cranes etc.)
- 12 Details of hoarding required or any other occupation of the public highway
- 13 Details of how pedestrian and cyclist safety will be maintained
- 14 Details of how traffic associated with the Development will be managed in order to reduce congestion
- 15 Other measures designed to reduce the impact of associated traffic
- 16 Details of how any significant amounts of dirt or dust that may be spread onto the public highway will be cleaned or prevented
- 17 Details of consultation
- 18 Details of any Construction Working Group
- 19 Considerate Contractors Scheme
- 20 Details of other construction sites in the local area
- 21 Operating HGVs
- 22 Any other relevant information with regard to traffic and transport.

The approved contents of this CMP must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

Q1. Please provide the full postal address of the site and the planning reference relating to the Construction works.

Site Address:
21 Kidderpore Gardens
Hampstead
London NW3

Planning application reference: 2013/0149/P

Q2. Please provide contact details for the person responsible for submitting the CMP

Name: *James Lynch*

Address: **B&G Construction Engineering Ltd**
48 Churchfield Road
W3 6DL

The site manager will be James Lynch

02087404745

Email: james@bandg.uk.com

Q3. Please provide the registered contact address details for the main contractor responsible for undertaking the works.

Name: James Lynch

Address: **B&G Construction Engineering Ltd**
48 Churchfield Road
W3 6DL

Tel: 02087404745

Email: james@bandg.uk.com

Q4. Please provide full contact details of the site and project manager responsible for day-to-day management of the works.

Name: James Lynch

Address: **B&G Construction Engineering Ltd,**
48 Churchfield Road,
W3 6DL

Tel: 02087404745

Email: james@bandg.uk.com

Q5. Please provide full contact details of the person responsible for dealing with any complaints from local residents and businesses, etc. In the case of [Community Investment Programme \(CIP\)](#), please provide contact details of the responsible Camden officer.

James Lynch
B&G Construction Engineering Ltd,
48 Churchfield Road,
W3 6DL

02087404745

Email: james@bandg.uk.com

Q6. Please provide full contact details of the person responsible for community liaison if different to above.

Name: SEE ABOVE
Address:
Tel:
Email:

Q7. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: James Lynch

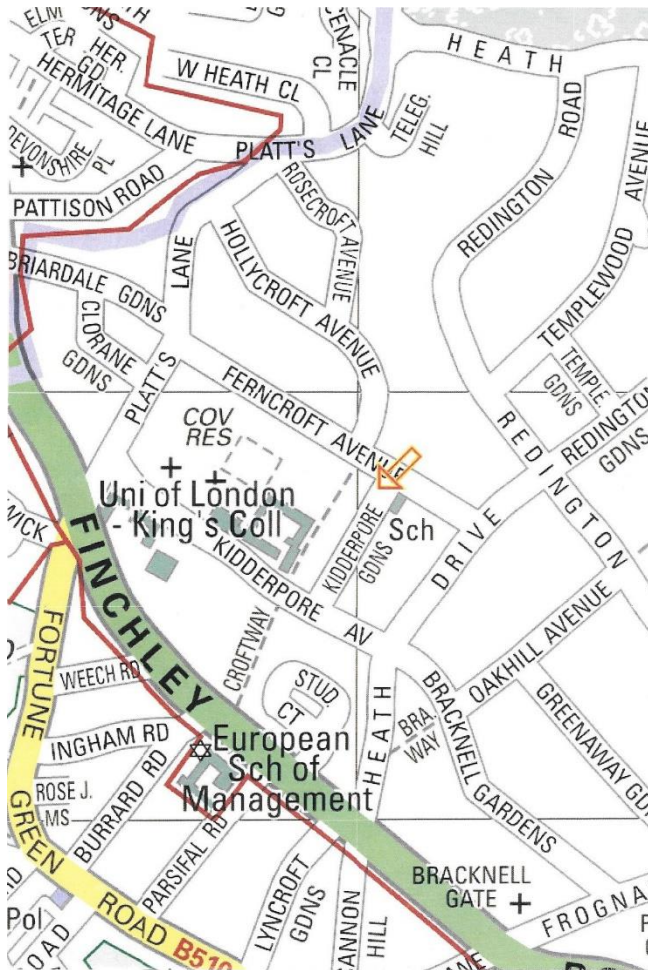
Address: **B&G Construction Engineering Ltd,**
48 Churchfield Road,
W3 6DL

Tel: 02087404745

Email: james@bandg.uk.com

Section 2 – About the Site

Q8. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.



Kidderpore Gardens lies within a controlled parking zone which operates Monday to Friday 12.30 to 2.30. The road is also subject to a 20mph speed limit. The property has a dropped crossover with a short length of single yellow line. To the north there is a P&D/Permit Holder parking bay and to the south is a Permit Holders parking space.

B&G Construction have been engaged to make alterations to the single family house at 21 Kidderpore Gardens, Hampstead, London NW3 as approved in 2013/0149/P. The works involve the proposed enlargement by creating a single floor basement beneath the footprint of the original house and lights wells to the front and side to allow ventilation to the new rooms and other minor alterations (all as approved under application no. 2012/0218/P) plus a new single storey minimal glazed rear extension.

Kidderpore Gardens is substantially residential in nature though St Margret's Independent Day Girls School (4 to 16 years olds) is located opposite the property. The main access to the school is from Ferncroft Avenue and not from Kidderpore Gardens.

Access to the site will be from the Finchley Road (A41) via Heath Drive. The junction of Finchley Road/Heath Drive is traffic signal controlled.

The agreed contents of the Construction Management Plan/Construction Management Statement will be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council and complied with thereafter.

Q9. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings).

As stated above, the works involve the proposed enlargement by creating a single floor basement beneath the footprint of the original house and lights wells to the front and side to allow ventilation to the new rooms and other minor alterations (all as approved under application no. 2012/0218/P) plus a new single storey minimal glazed rear extension.

As such the work is of a fairly routine nature. There are no particular challenges in respect to this project.

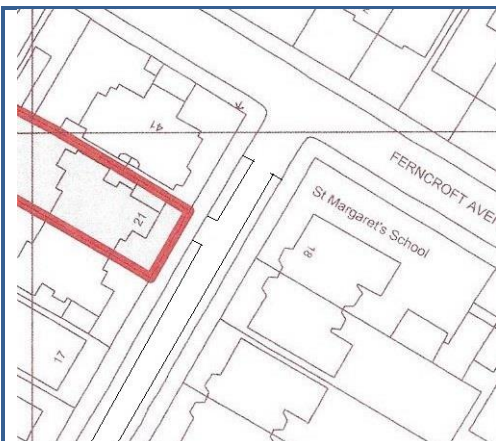
The access from the Finchley Road will be via Heath Drive and Ferncroft Avenue. The egress will be via Kidderpore Gardens, Kidderpore Avenue and Heath Drive. This circular route will mean that no construction related vehicles will need to turn around on residential roads. All the junctions on the access and egress routes are relatively wide and also protected with waiting restrictions.

It will be important to liaise with the St Margret's Independent Day Girls School to minimise any potential conflicts.

Q10. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting, etc.).

The adjacent two residential properties are the nearest potential receptors.

Q11. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.



The main to the site for materials and for the skips will be via the existing cross over. There are on-street parking bays on either side of the crossover and also on the opposite side of the road. There are no cycle lanes.

Q12. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be useful).

The overall programme of works is 9 months. This can be split into three main phases:

Work Phase	Proposed Start	Proposed End	Duration
Site set up, internal strip and preparation	June 16	June 16	1 month
Excavation and structural work	July 16	September 16	3 month
1 st /2 nd fix and fit out	Oct 16	Feb 17	5 months

The 24 hour contact will be James Lynch (TBC)

Tel: 02087404745

Email: james@bandg.uk.com

Detail of the 24 hour emergency contact details will be displayed at the site.

Q.13 Please confirm the standard working hours for this site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

The standard working hours for the site will be as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays

No working on Sundays or Public Holidays

Q14. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT. etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

No new service connections are anticipated

Q15. Please confirm when an asbestos survey was carried out at the site and include the key findings.

An asbestos survey will be carried out prior to the commencement of works, though no asbestos is anticipated.

Section 3 – Transportation Issues Associated with the Site

Q16. Please provide a brief description of the proposed working hours within which vehicles will service the site during the construction period (Refer to the [Guide for Contractors Working in Camden](#)). Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. Construction vehicles must be managed and prevented from causing obstructions to the highway.

The proposed working hours during which vehicles will arrive and depart will be 08.00am to 18.00pm Monday to Friday and between 08.00 and 13.00 hours on Saturdays. Only in exceptional circumstances will vehicles arrive or depart outside these hours. **Construction vehicle movements will be between 9.30am and 3pm on weekdays during term time.**

Q17. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures. You will need to consider whether the roads on the route(s) to and from the site are suitable for the size of vehicles to be used. Please provide details of other known developments in the local area or on the route.

It is likely a number of varying types of vehicles will be required to undertake deliveries to and from site. These will include:

Skip Lorries (approx. size 7.5m long and 2.4m wide)

Ready Mix Concrete Lorries (approx. size 8.25m long and 2.45m wide)

Flatbed delivery lorries, for items such as steelwork/timber/windows/doors etc (approx. size 8.5m long and 2.4m wide)

It is not anticipated that this scheme will require any more than 2-3 deliveries a day on average. It is envisaged that this level of delivery will be fairly constant throughout all three construction phases.

Nearly all deliveries and the skip exchanges are expected to last less than 15 minutes.

No vehicles will be entering the site and there will not be a tight manoeuvre required for the site access or servicing. As such no Autotrack swept path analysis has been undertaken.

Q18. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.)

No temporary structures will overhang the public highway (e.g. scaffolding, gantries, cranes etc.).

Q19. Please provide details of hoarding requirements or any other occupation of the public highway.

There will be no occupation of the public highway.

Q20. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses). Use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway including; the extent of hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

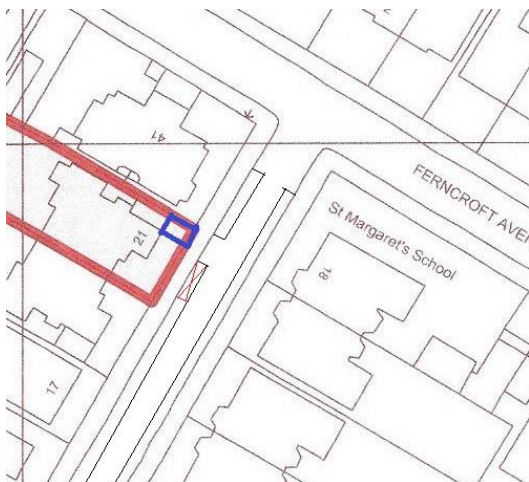
No highway works are required to enable construction to take place.

Q21. Please provide details of any proposed parking bay suspensions and temporary traffic management orders which would be required to facilitate construction. If construction vehicles cannot access the site, details are required on where they will wait to load/unload.

Details of proposed parking bays suspensions and temporary traffic management orders

It is anticipated that a parking space will be required directly in front of 21 Kidderpore Gardens, as shown in red in the plan below. This space will be required for loading and unloading and also to facilitate the skip deliveries. The yellow line in front of the property will allow for extra manoeuvring space.

A skip will be located within the site as shown in blue on the plan below.



The Council's Highways Team will be contacted regarding the above and made aware of the timescales and in terms of the required Temporary Traffic Management Order and License requirements.

Proposed overhang (if any) of the public highway (scaffolding, cranes etc.)

There will be no scaffolding and therefore no overhang to any public footpath. There will also be no gantry/cranes overhanging the public highway.

There will be no occupation of the public highway.

Section 4 - Traffic Management for the Site

- Q22.** Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Banksman and/or Traffic Marshall arrangements. You should supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted). Vulnerable footway users include wheelchair users, the elderly, people with walking difficulties, young children, people with prams, blind and partially sighted people, etc. A secure hoarding will generally be required to the site boundary with a lockable access. Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/ skips/ hoardings, etc. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

Materials will need to be moved across the footway between the loading area and the site entrance. While this footway is only lightly used, in order to ensure this is done with maximum safety for pedestrians, all delivered will be managed and controlled by site staff (banksmen) who will ensure all movement of goods/supplies is carried out safely.

Cyclists will not materially be affected by the works.

Details of how traffic associated with the Development will be managed in order to reduce congestion

As previously mentioned, deliveries to site will be closely controlled and managed. The timing of deliveries should ensure that any disruption/congestion is kept to an absolute minimum. Where possible, deliveries will be programme to take place in the off-peak period. In particular, it is envisaged that the limited concrete and steel deliveries will be carried out outside the peak period and outside of school arrival and departure times.

Whenever possible, all site deliveries will consist of full loads rather than part loads in order to reduce the overall number of trips.

All workers and subcontractors will be encouraged to travel to site by public transport. As no parking will be provided for workers, it is not anticipated that these will generate vehicular movements.

Other measures designed to reduce the impact of associated traffic

The proposed development is relatively small with a very low traffic generation and as such no further measures to reduce the impact of associated traffic have been proposed.

Details of hoarding required or any other occupation of the public highway

2.4 metre high wooden hoardings will be provided all round the site during the main works. The hoardings will be painted in the livery of B&G Construction and will be maintained in good order throughout the construction period. Gates will be provided in the hoarding for deliveries from Kidderpore Gardens. There will be no public viewing built into the hoardings as these are not felt appropriate for this project. There will be no decorative displays as these are not felt to be appropriate.

- Q23. Please detail the proposed access and egress routes to and from the site, showing details of links to the [Transport for London Road Network \(TLRN\)](#). Such routes should be indicated on a drawing or diagram showing the public highway network in the vicinity of the site. Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. Consideration should be given to any major trip generators (e.g. schools, offices, public buildings, museums, etc.) on the route, and how any problems can be avoided or mitigated.**

The access from the Finchley Road will be via Heath Drive and Ferncroft Avenue. The egress will be via Kidderpore Gardens, Kidderpore Avenue and Heath Drive. These circular route will mean that no construction related vehicles will need to turn around in residential roads. All the junctions on the access and egress routes are relatively wide and also protected with waiting restrictions.

The junction of Finchley Road/Heath Drive is traffic signal controlled where all movements are permitted.

The Finchley Road (A41) is a Red Route.

- Q24. Please describe how the access and egress arrangements for construction vehicles will be managed. Confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.**

A strict delivery procedure will be followed for all parking and loading arrangements. A designated site operative will ensure that traffic flow is maintained at all times and that any inconvenience to other road users (drivers, cyclists and pedestrians) is kept to a minimum.

All subcontractors and suppliers will be required to give 48 hours' notice of deliveries. Deliveries will be allocated time slots to ensure good control and coordination and to minimise the chance of any disruption to other road users.

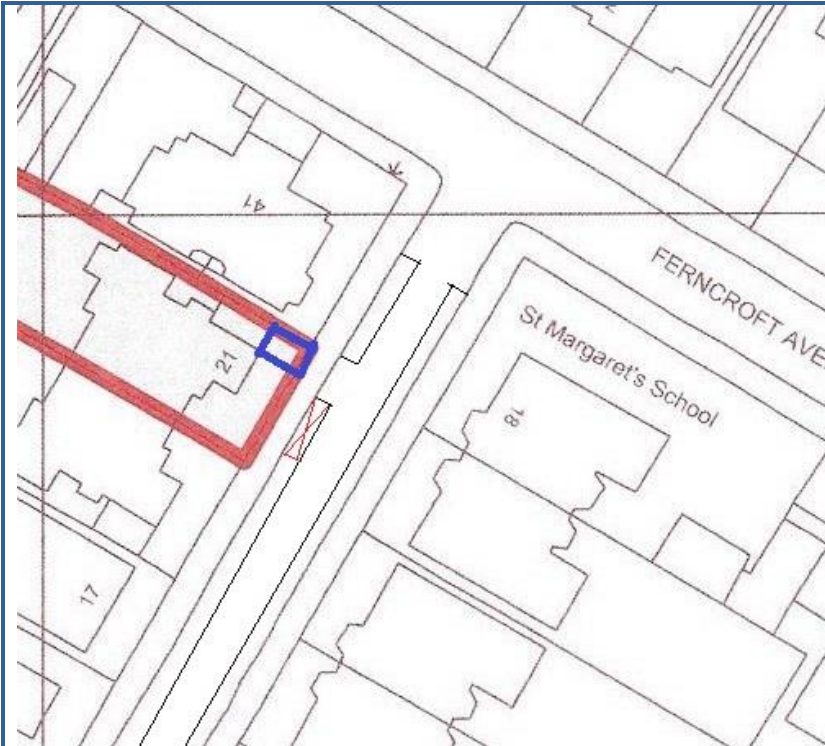
All deliveries will be made from the kerbside in front of the site.

The designated site operative will be responsible for the movement of materials from delivery vehicles to the site. Where necessary site operatives will control deliveries along the footway to ensure pedestrian safety is maintained at all times.

All materials will be stored within the site boundary. No materials will be stored on the public highway.

No parking will be permitted within the site as no spaces are available. All sub-contractors will be informed that all roads in the area are restricted by parking controls and they will be encouraged to use public transport.

- Q25. Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site.**



The above plan shows the parking bay which will need to be suspended and also the location of the skip. The existing cross over will be used to deliver the skip.

- Q26. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).**

There are no tight manoeuvres tight manoeuvres on vehicle routes to and from the site or in terms of the access and egress arrangements at the site and as such no swept path drawings have been prepared.

Section 5 – Environmental Issues

To answer these sections please refer to the relevant sections of **Camden's Minimum Standards for Building Construction** ([CMRBC](#)).

Q27. Please provide details of the times of [noisy operations](#), outlining how the construction works are to be carried out.

Noisy Operations will only be carried out between 8.00am to 6pm on Monday to Friday.

Q28. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

Noise monitoring:

Noise levels from construction during the working day will be monitored against indicative 75dB action level and in line with the recommended levels in BS 5228-1: 2009 Annex E for a residential area.

Noise levels will be monitored during construction as follows:

- Noise and Vibration monitoring will be carried out regularly, as well as in response to requests/complaints or any new activities that have the potential to generate significant noise.
- Checks will be made on method statements to ensure that the best practice described in the standards is being applied in the method and site activities.

Q29. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

It is not anticipated that noise levels will exceed indicative 75dB action level and in line with the recommended levels in BS 5228-1: 2009 Annex E for a residential area. Monitoring will be undertaken to ensure compliance with this recommendation.

Vibration is not predicted to be an issue in light of the nature of the proposed works.

Q30. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

Noise and Vibration Mitigation:

All hand operated tools and equipment shall be effectively silenced and will bear the manufacturers guaranteed maximum sound level generated. The recommendations made in BS 5228-1: 2009 "Code of Practice for Noise and Vibration control on Construction and Open Sites" will be specified for adoption by the contractor, and its sub-contractors.

- Any noise emitting equipment on site that is required to run continuously will be housed in a suitable acoustic enclosure.
- a small tracked mini-piling rig will be used with sectional flight augers used to form the bore from the existing ground level which will greatly reduce vibration in the immediate vicinity of the work area.
- Machines in intermittent use will be shut down in the intervening periods between works or throttled down to a minimum.
- The use of and noise from, percussive tools will be limited as far as reasonably possible.
- The external perimeter scaffold will be fully encapsulated in monaflex sheeting which will reduce the transfer of noise.
- The hoarding erected around site will also help to reduce noise transmission.
- Excavators will be fitted with hydraulic pulverisers and shears whenever possible in preference to hydraulic hammers.
- All plant and machinery will be fitted with silencers and where hydraulic hammers are used they will be fitted with bafflers as per BS 5228-1: 2009.
- Sound reduced compressors will be used and/or fitted within acoustic enclosures where necessary.
- The positioning of compressors will also be taken into consideration to reduce noise transfer to neighbouring properties.
- Pneumatic tools will be fitted with silencers or mufflers
- Electrically powered tools will be used as opposed to petrol/diesel powered, wherever possible.
- Care will be taken when erecting or striking scaffolds to avoid impact noise from banging steel.
- No personal audio equipment will be allowed on site e.g. radio.
- Acoustic blankets will be employed where necessary on the party wall.

Visual assessments on dust levels will be taken on a daily basis by the works manager and recorded in the site diary.

Q31. Please provide evidence that staff have been trained on BS 5228:2009

The recommendations made in BS 5228-1: 2009 "Code of Practice for Noise and Vibration control on Construction and Open Sites" will be specified for adoption by the contractor, and its sub-contractors.

Q32. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

Best Practice Means (BPM) will be used to ensure that dust does not cause nuisance. Where dust is considered to be a risk during a specific site activity, mitigation measures will be included in the task specific method statement for the work. The controls listed in the method statement will be assessed on site to ensure they are adequately carried out and effective. The controls will be briefed to the engineers and operatives to ensure they are aware of mitigation measures and controls to be employed.

During demolition the following controls will be implemented:

- Monaflex sheeting will be placed to screen the demolition where possible.
- A soft strip of materials will be carried out prior to structural demolition.
- Materials will be removed from site as soon as possible for appropriate recycling and disposal.
- Drop heights will be minimised as far as possible.
- A water spray will be used to control dust.

Mitigation measures to ensure dust is kept to a minimum will include the following:

- Large stockpiles of materials will be avoided and are not anticipated due to the nature of the project and the restrictive available area during the initial Basement excavation.
- Use dust screening where possible.
- Damping down the areas with water to suppress the dust whilst ensuring the application does not create excessive mud.
- Construction plant will be well maintained and operated to minimise emissions to air.
- Good housekeeping including the regular sweeping of floors will be maintained and debris disposed of in enclosed skips.
- Outer surfaces of skip will be washed before leaving site. Vehicles shall not enter or leave site via road in any but exceptional circumstances.
- Equipment and techniques such as dust extractors will be used to minimise dust when using cutters and saws.
- Portable knapsack dust suppressors will be employed on floors.
- The Environmental Advisor will brief operatives on good practice and will carry out regular inspections to ensure that BPM is employed across the project.
- Wind conditions will be taken account of when arranging activities that are likely to emit aerosols, fumes, odours and smoke.

Materials will be pre-fabricated and pre-cut off site where possible to minimise dust from cutting and grinding activities. If cutting and grinding cannot be mitigated off site then water suppressant systems and or local exhaust ventilation will be employed.

In terms of our Strategy we have taken due regard of the Mayor's Best Practise Guidance on Control of Dust and Emissions.

Q33. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

Dirt and dust on the public highway will be greatly restricted as no vehicles will access the site (except skip lorries and for some deliveries). Waste material will be transported directly into skip. All muck away lorries will be covered to reduce the risk of debris falling onto the highway.

If conditions require, dust will be suppressed by spraying with water. However, as there is only limited (internal) demolition and no site traffic, it is felt that this is unlikely to be required.

In the unlikely event of materials being deposited on the public highway, immediate action will be taken by site staff to safely remove the material. If a large spill did occur, an approved road sweeper will be hired to remove the material and clean the public highway.

As no vehicles will enter the site, no wheel washing facilities will be provided.

Q34. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

See above

Q35. Please confirm that a [Risk Assessment](#) has been undertaken in line with the [GLA's Control of Dust and Emissions Supplementary Planning Guidance \(SPG\)](#), and the risk level that has been identified, with evidence.

An initial Risk Assessment has been undertaken in line with GLA's Control of Dust and Emissions During Construction and Demolition 23 July 2014. During the three phases of site set up, earthworks/structural works and fit out the dust emission magnitude was deemed to be small

Q36. Please confirm that all relevant mitigation measures from the [SPG](#) will be delivered onsite.

The relevant mitigation measures from the SPG will be delivered onsite

Q37. If the site is a High Risk Site, 4 real time dust monitors will be required, as detailed in the [SPG](#). Please confirm that these monitors will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

The site is not deemed to be High Risk.

Q38. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and copies of receipts (if work undertaken).

Pest Management

Renovation

1. Renovation of buildings containing a pest infestation can result in a dispersion of these pests into the surrounding area and may also result in the same pests infesting the new buildings.
2. Six weeks prior to the commencement of renovation, individual blocks, properties and the surrounding area will be surveyed by professional pest control staff in order to identify the presence and extent of any infestations. Where infestations are identified, appropriate treatments must be implemented to eliminate infestation before demolition.
3. Unprotected exposure to the debris arising from bird infestation in disused buildings can result in disease such as ornithosis. Such debris should always be dealt with by fully trained persons wearing appropriate personal protective equipment.

Sewers and rodents

1. Rats live in sewers and move through drainage systems. They come to the surface and enter buildings through breaks and faults in the drainage system.
2. Capping of drainage systems will be carried out where appropriate to isolate old redundant sewers /drains, including those servicing properties that have been vacated and are awaiting clearance and demolition.
3. Redundant drains and sewers will be grubbed out and the connection with the sewer effectively sealed.
4. Live sewer connections will be appropriately sealed and capped while construction works are in progress to prevent rat egress from the sewers.
5. To prevent rat egress from live drains and sewers to new systems, the live systems will be temporarily sealed off with expanding drainage stoppers until connection to new drainage is completed.
6. Pest monitoring and baiting programmes will be instigated on construction and refurbishment sites, including a proactive surface monitoring baiting programme during the demolition / construction process. Exposure of construction staff to risks associated with a rodent infestation may contravene the Health and Safety at Work, Etc. Act 1974.
7. An all-encompassing sewer rat monitoring programme will be implemented in the sewer systems within the project area for the duration of the project.
8. Sewers and drains will be cleared of any remaining building debris.

Site hygiene

1. Contractors will ensure that construction sites are kept as clear and tidy as possible. Accumulations of surplus or damaged building materials can act as harbourage for pests, and should be removed and disposed of promptly and safely.
2. Construction staff will not leave food debris within buildings under construction, as this will encourage pests to become established.
3. Staff site facilities including canteens, accommodation and sanitary provision will be constructed and maintained in a clean and hygienic manner and in accordance with

relevant regulations and codes of practice. Waste must be stored safely in suitably located, pest-resistant, closable containers and removed frequently.

Compliance Monitoring

1. Information relating to site inspections, copies of receipts and details of any work carried out will be retained and provided to the Local Authority upon request.

Section 6 – Monitoring, Compliance, Reporting and Consultation about Traffic and Activities related to the Site

(Refer to [Tfl best practice guidance](#) and [\(CMRBC\)](#) sections: [noise operations](#), abatement techniques, noise levels, vibration levels, [dust levels](#), rodent control, community liaison, etc.)

- Q39. Please provide details describing how traffic associated with the development will be managed in order to reduce/minimise traffic congestion. Deliveries should be given set times to arrive, dwell and depart. Delivery instructions should be sent to all suppliers and contractors. Trained site staff must assist when delivery vehicles are accessing the site, or parking on the public highway adjacent to the site. Banksman must ensure the safe passage of pedestrians, cyclists and motor vehicular traffic in the street when vehicles are being loaded or unloaded. Vehicles should not wait or circulate on the public highway. An appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected.**

As stated above a strict delivery procedure will be followed for all parking and loading arrangements. A designated site operative will ensure that traffic flow is maintained at all times and that any inconvenience to other road users (drivers, cyclists and pedestrians) is kept to a minimum.

All subcontractors and suppliers will be required to give 48 hours' notice of deliveries. Deliveries will be allocated time slots to ensure good control and coordination and to minimise the chance of any disruption to other road users.

All deliveries will be made from the kerbside in front of the site.

The designated site operative will be responsible for the movement of materials from delivery vehicles to the site. Where necessary site operatives will control deliveries along the footway to ensure pedestrian safety is maintained at all times.

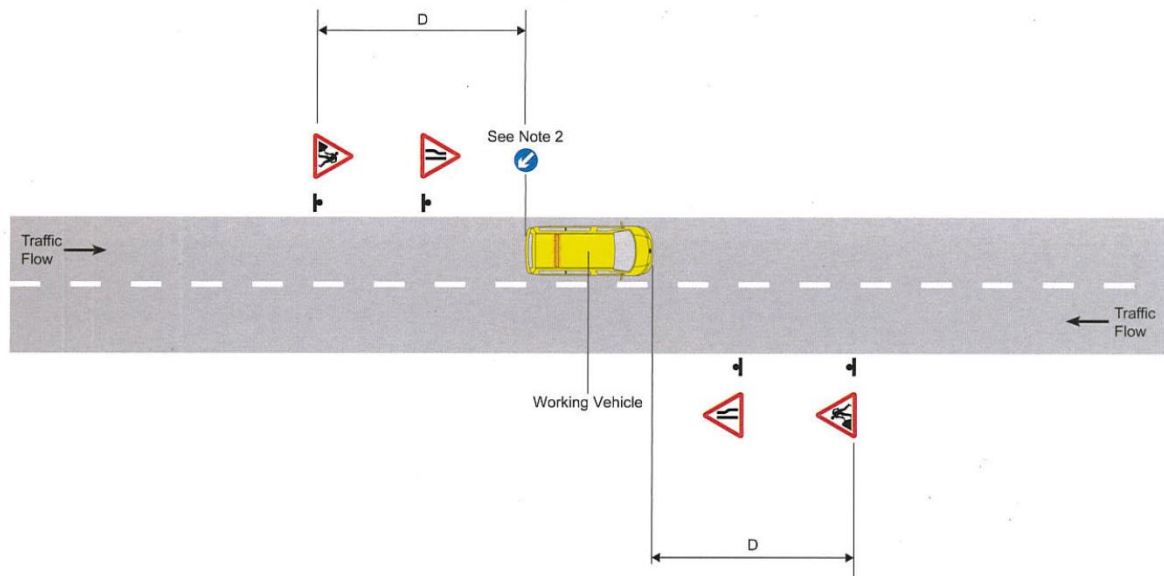
All materials will be stored within the site boundary. No materials will be stored on the public highway.

No parking will be permitted within the site as no spaces are available. All sub-contractors will be informed that all roads in the area are restricted by parking controls and they will be encouraged to use public transport.

Safety at Street Works and Roads Work – A Code of Practise October 2013.

Nearly all deliveries and the skip exchanges are expected to last less than 15 minutes. The signing for these will be as shown below (Extracted of page 87). The distance D for 30 mph speed limits or less is 20 to 45 metres.

Short duration stops less than 15 minutes on a single carriageway road

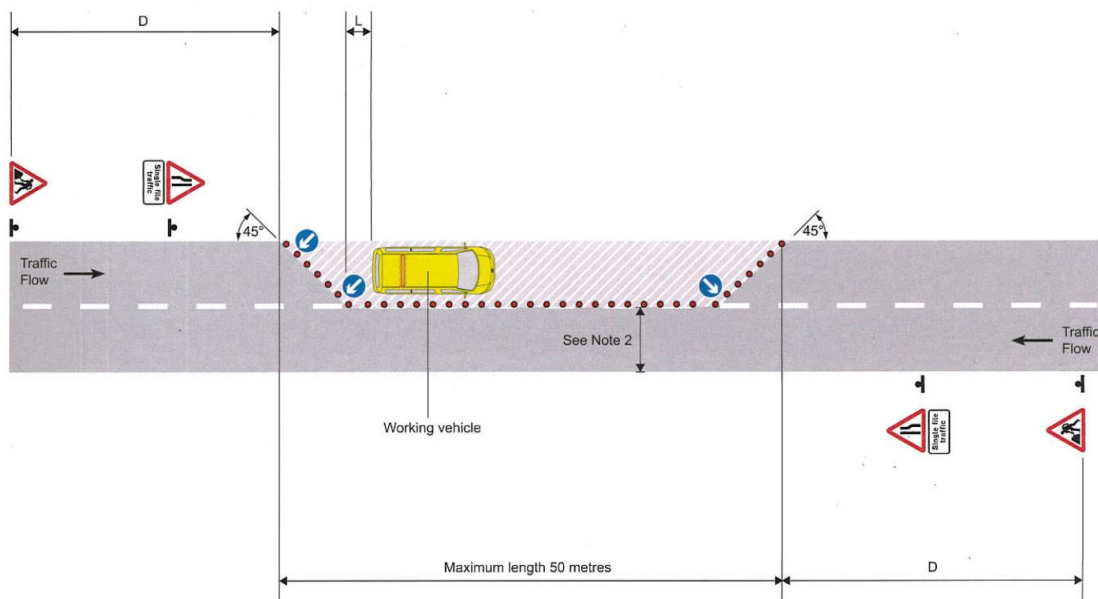


Notes

- 1 For dimension D, see table inside back cover.
- 2 The keep right arrow shown on the rear of the vehicle is only appropriate for a speed limit of 30 mph or less. For higher speed limits, a diagram 7403 sign or a light arrow is required.

A very limited number of deliveries may be between 15 and 60 minutes. The signing for these will be as shown below (Extracted of page 89). The distance D for 30 mph speed limits or less is 20 to 45 metres and distance L is 0.5 metres. The desirable restricted lane widths for shuttle working is 3.25 and 3.5m. The absolute minimum is 3m for normal traffic including buses and HGV's. However, in areas where there is generally only cars and light vehicles only and the absolute minimum can be reduced to 2.5m. Traffic control by 'give and take' is appropriate given the flows and speeds. Site observations indicate that the traffic flow on Kidderpore Gardens is low and involves very low numbers of larger vehicles.

Short duration stops more than 15 minutes on a single carriageway road



Notes

- 1 For dimensions D and L see table inside back cover.
- 2 See page 52 for guidance on unobstructed width past the works.

For deliveries or works involving stops expected to last more than 60 minutes will have signing and guarding as for a fixed site.

Banksmen will be present at all times during loading/unloading activities.

Q40. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

The works are of relatively small scale and as such use will not be made of a construction material consolidation centre. However, in order to reduce the impact of associated traffic, where possible local suppliers will be used to reduce overall traffic distance by construction related vehicles.

Site Waste Management Plans (SWMP) have been developed to ensure that site waste is managed more effectively through the course of demolition and construction and through to the operation of developments. By virtue of managing the amount of waste produced, it will help reduce the potential harm to both the environment and to human health. Although since December 2013 SWMP's are no longer compulsory for developments in England, the production of a SWMP is still a useful tool to assist in reducing the amount of waste and increasing the amount of recycling associated with the proposed development.

Any SWMP should develop throughout the various stages of the project, and responsibility for the updating of the plan can transfer between individuals and organisations during these different stages. A thorough approach to site waste management planning will produce a structured plan that can perform three different roles:

1. To set out the importance of effective waste management and how this is enshrined in legislation, policy and guidance at all levels from European down to local, and summarising those that are most important and relevant to the scheme;
2. Identify the baseline conditions and set out waste management issues at each of the three main stages – design, construction and operation, and establish a framework for further development of the Site Waste Management Plan which will continue to be a live document throughout the process of construction;
3. To analyse these waste management issues in terms of any significant environmental effects and how the scheme has been able to either avoid, minimise or mitigate against such environmental effects.

The SWMP Template that will be used has been attached as Appendix A to this report.

Q41. Please provide details of consultation on a draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors. Details should include who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. In response to the comments received, the CMP should then be amended where appropriate and where not appropriate a reason should be given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying it out.

Details of consultation

Consultation with neighbours has taken place during the extended planning application process.

Further consultations is also presently being carried out by the owners with adjacent properties and this is being reinforced with consultation in respect to the required Party Wall agreements. The contact details will be obtained during this process to ensure a good dialog can be maintained during the construction work.

No significant issues have arisen from these consultations.

Furthermore, as a “neighbourly” gesture, the contractor has agreed not to undertake “noisy” works on a Saturday.

The school will also be contacted prior to work starting on site to ensure any disruption and inconvenience is kept to a minimum.

The Principal Contractor will be :

B&G CONSTRUCTION

Bandg-construction.co.uk

Site Manager : James Lynch

48 Churchfield Road,W3 6DL

02087404745

Email: james@bandg.uk.com

The Site Manager will be the person responsible for dealing with community liaison.

- Q42. Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works. Please confirm how the contact details of the person responsible for community liaison will be advertised to the local community and how the community will be updated on the upcoming works i.e. in the form of a newsletter/ letter drop, or weekly drop in sessions for residents.**

No specific Construction Working Group will be set up. However, in order to address any concerns of surrounding neighbours, close contact will continue to be made. B&G Construction acknowledge that a key element of good community/neighbourhood relations is the contact between the site team/sub-contractors/suppliers and the local residents. As such the site manager will ensure that this ethos is reinforced at all site briefings and inductions. The site will also operate an 'open door' policy to allow any local resident to visit the site office to raise any concerns with site manager. Information boards with the contact details of key staff and head office will be on display on the hoardings.

- Q43. Please provide details of any schemes such as the 'Considerate Constructors Scheme', the 'Freight Operators Recognition Scheme' or 'TfLs Standard for construction logistics and cyclist safety – [CLOCS scheme](#)' that the project will be signed up to. Note, the [CLOCS standard](#) should be adhered to and detailed in response to question 46. Such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the "[Guide for Contractors Working in Camden](#)" also referred to as "[Camden's Considerate Contractors Manual](#)".**

B&G will register this project with Considerate Contractors Scheme.

- Q44. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of suitable smoking area, tackling bad language and unnecessary shouting.**

B&G have a strict policy in respect to the conduct of employees (both direct and sub-contractors). All complaints will be taken seriously and investigated. Action will be taken where appropriate. Repeat offenses or serious breaches will result in the removal of staff from site.

- Q45. Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site.**

Currently there are no major construction sites in the vicinity of the development. This will be reviewed prior to work starting on site. Due to the residential nature of the area it is not anticipated that the cumulative impacts from further sites in the area will create significant problems.

Contact will be made with any other sites and any other consented schemes in the immediate vicinity to ensure co-ordination where required so that any disruption is minimised.

Q46. Please provide details to confirm that all contractors and sub-contractors operating large vehicles over 3.5 tonnes will meet all of the following conditions, as outlined in the [CLOCS Standard](#)

OPERATIONS:

- **Quality operation:** accreditation via an approved fleet management audit scheme e.g. [Fleet Operator Recognition Scheme \(FORS\)](#) or equivalent.
- **Collision reporting and analysis:** of any collision involving injury to persons, vehicles or property, ideally including use of the [CLOCS](#) Manager collision reporting tool.
- **Traffic routing:** any route specified by the client is adhered to unless otherwise specified.

i. [VEHICLES:](#)

- **Warning signage:** warning cyclists of the dangers of passing the vehicle on the inside
- **Side under-run protection:** fitted to all vehicles over 3.5 tonnes which are currently exempt
- **Blind spot minimisation:** front, side and rear blind-spots completely eliminated or minimised as far as is practical and possible
- **Vehicle manoeuvring warnings:** enhanced audible means to warn other road users of a vehicle's left hand turn or other manoeuvres

ii. [DRIVERS:](#)

- **Training and development:** approved progressive training and continued progressive training especially around vulnerable road users (including for drivers excluded from Certificate of Professional Competence requirements)
- **Driver licensing:** regular checks and monitoring of driver endorsements and that drivers hold the correct licence for the correct vehicle

STANDARD FOR CONSTRUCTION CLIENTS

- **Construction logistics/management plan:** is in place and fully complied with – as per this document.
- **Suitability of site for vehicles fitted with safety equipment:** that the site is suitably prepared for vehicles fitted with safety equipment to drive across.
- **Site access and egress:** should be carefully managed, signposted, understood and be clear of obstacles.
- **Vehicle loading and unloading:** vehicles should be loaded and unloaded on-site as far as is practicable.
- **Traffic routing:** should be carefully considered, risk assessed and communicated to all contractors and drivers.
- **Control of site traffic, particularly at peak hours:** other options should be considered to plan and control traffic, to reduce traffic at peak hours.
- **Supply chain compliance:** contractors and sub-contractors throughout the supply chain should comply with requirements 3.1.1 to 3.3.2.

All contractors and sub-contractors operating large vehicles over 3.5 tonnes will meet the above conditions

Q47. Please provide details of any other relevant information with regard to traffic and transport (if appropriate).

As the construction project is relatively small, no further relevant information with regard to traffic and transport is felt to be required.

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed: James Lymch

Date: 14th March 2016.

Print Name: James Lynch

Position: Manager

Submit: planningobligations@camden.gov.uk

APPENDIX A : Site Waste Management Plan

Responsibility

Name of client	
Name of principal contractor	
Name of person who drafted plan	
Notes, amendments	

Construction Project

Location (address, postcode if appropriate)	
Estimated project cost	
Notes, amendments	

Materials Resource Efficiency

Describe here any methods adopted during the conception, design and specification phase to reduce the amount of waste arising.

Method	Resource saving (quantify if possible)

Waste Management

Declaration

The client and principal contractor will take all reasonable steps to ensure that –

- a) all waste from the site is dealt with in accordance with the waste duty of care in section 34 of the Environmental Protection Act 1990 and the Environmental Protection (Duty of Care) regulations 1991; and
- b) materials will be handled efficiently and waste managed appropriately

Signatures

Waste Type	Quantity (m ³ or tonnes)							
	Re-use on-site	Re-use off-site	Recycling on-site	Recycling off-site	Other form of recovery on-site	Other form of recovery off-site	Sent to landfill	Other disposal
Estimates								
Inert								
Non-hazardous								
Hazardous								
Totals (m ³ or tonnes)								
Actual								
Inert								
Non-hazardous								
Hazardous								
Totals (m ³ or tonnes)								
Difference between estimates and actual								

Waste Records

Date removed	Waste Type	Identity of the person removing the waste	Site the waste is being taken to and whether licensed or exempt	Waste carrier and registration number*	Confirmation of delivery

* evidence of waste carrier registration and waste transfer or hazardous waste consignment notes for each removal of waste should be provided either as part of the plan, or filed and cross-referenced

Post-Construction

[Within three months of the construction work being completed]

Confirmation

This plan has been monitored on a regular basis to ensure that work is progressing according to the plan and has been updated to record details of the actual waste management actions and waste transfers that have taken place.

Signature

Issue	Details
Explanation of any deviation from the planned arrangements	
Waste forecasts – exceeded	
Waste forecasts – not met	
Cost savings achieved	

APPENDIX B : NOISE, VIBRATION AND DUST

Noise monitoring:

Noise levels from construction during the working day will be monitored against indicative 75dB action level and in line with the recommended levels in BS 5228-1: 2009 Annex E for a residential area.

Noise levels will be monitored during construction as follows:

- Noise and Vibration monitoring will be carried out regularly, as well as in response to requests/complaints or any new activities that have the potential to generate significant noise.
- Checks will be made on method statements to ensure that the best practice described in the standards is being applied in the method and site activities.

Noise and Vibration Mitigation:

All hand operated tools and equipment shall be effectively silenced and will bear the manufacturers guaranteed maximum sound level generated. The recommendations made in BS 5228-1: 2009 "Code of Practice for Noise and Vibration control on Construction and Open Sites" will be specified for adoption by the contractor, and its sub-contractors.

- Any noise emitting equipment on site that is required to run continuously will be housed in a suitable acoustic enclosure.
- a small tracked mini-piling rig will be used with sectional flight augers used to form the bore from the existing ground level which will greatly reduce vibration in the immediate vicinity of the work area.
- Machines in intermittent use will be shut down in the intervening periods between works or throttled down to a minimum.
- The use of and noise from, percussive tools will be limited as far as reasonably possible.
- The external perimeter scaffold will be fully encapsulated in monaflex sheeting which will reduce the transfer of noise.
- The hoarding erected around site will also help to reduce noise transmission.
- Excavators will be fitted with hydraulic pulverisers and shears whenever possible in preference to hydraulic hammers.
- All plant and machinery will be fitted with silencers and where hydraulic hammers are used they will be fitted with bafflers as per BS 5228-1: 2009.
- Sound reduced compressors will be used and/or fitted within acoustic enclosures where necessary.
- The positioning of compressors will also be taken into consideration to reduce noise transfer to neighbouring properties.
- Pneumatic tools will be fitted with silencers or mufflers
- Electrically powered tools will be used as opposed to petrol/diesel powered, wherever possible.
- Care will be taken when erecting or striking scaffolds to avoid impact noise from banging steel.
- No personal audio equipment will be allowed on site e.g. radio.
- Acoustic blankets will be employed where necessary on the party wall.

Visual assessments on dust levels will be taken on a daily basis by the works manager and recorded in the site diary.

Mitigation Measures:

Best Practice Means (BPM) will be used to ensure that dust does not cause nuisance. Where dust is considered to be a risk during a specific site activity, mitigation measures will be included in the task specific method statement for the work. The controls listed in the method statement will be assessed on site to ensure they are adequately carried out and effective. The controls will be briefed to the engineers and operatives to ensure they are aware of mitigation measures and controls to be employed.

During demolition the following controls will be implemented:

- Monaflex sheeting will be placed to screen the demolition where possible.
- A soft strip of materials will be carried out prior to structural demolition.
- Materials will be removed from site as soon as possible for appropriate recycling and disposal.
- Drop heights will be minimised as far as possible.
- A water spray will be used to control dust.

Mitigation measures to ensure dust is kept to a minimum will include the following:

- Large stockpiles of materials will be avoided and are not anticipated due to the nature of the project and the restrictive available area during the initial Basement excavation.
- Use dust screening where possible.
- Damping down the areas with water to suppress the dust whilst ensuring the application does not create excessive mud.
- Construction plant will be well maintained and operated to minimise emissions to air.
- Good housekeeping including the regular sweeping of floors will be maintained and debris disposed of in enclosed skips.
- Outer surfaces of skip will be washed before leaving site. Vehicles shall not enter or leave site via road in any but exceptional circumstances.
- Equipment and techniques such as dust extractors will be used to minimise dust when using cutters and saws.
- Portable knapsack dust suppressors will be employed on floors.
- The Environmental Advisor will brief operatives on good practice and will carry out regular inspections to ensure that BPM is employed across the project.
- Wind conditions will be taken account of when arranging activities that are likely to emit aerosols, fumes, odours and smoke.

Materials will be pre-fabricated and pre-cut off site where possible to minimise dust from cutting and grinding activities. If cutting and grinding cannot be mitigated off site then water suppressant systems and or local exhaust ventilation will be employed.