



# New End Square Hampstead, London

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## Construction Management Plan

December 2015

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**Overall, this document sets out to identify potential disturbance to local residents, businesses and general public and details specific mitigation measures that ensure that the impacts are kept to an absolute minimum, ensuring industry best standards are in operation at all times.**

## INTRODUCTION

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Blue Sky Building has been commissioned by Urban High Developments to produce this Construction Management Plan (CMP) for the project currently known as New End Square, Hampstead, NW3, London. The standards and procedures proposed in this CMP ensure that the interests of local residents, businesses and the general public are given special attention by the Contractor during the works duration.

This report identifies how the critical construction activities will be undertaken, and specifically covers the environmental, public health and safety aspects of the proposed development. The baseline for our analysis is Camden's Minimum Requirements for Building / Construction / Demolition Sites, the Construction Management Plan Pro-forma, and the Guide for Contractors Working in Camden, but we envisage these requirements as the minimum standards to be achieved and have identified improvements in most areas under consideration. In due course, when the Contractor for the works has been chosen and appointed, they will produce a Site Environmental Management Plan (SEMP) to demonstrate how they will comply with the requirements of Camden Council and how they will address the measures contained within this report.

This document details:

- the specific obligations on the Contractor when undertaking the works;
- the specific measures to be used during the demolition and re-building of the works; and
- the specific details of the control measures for each environmental issue.

Key outputs from this report are:

- **Noise during demolition.**

Further information on the building design will be required before more detailed information on methodology and the equipment required to undertake the works can be provided. This will be progressively added to this CMP as the construction scope develops.

- **Noise during construction.**

The objective is to control noise within recognised limits. The on-going quiet enjoyment of the existing neighbours is of paramount importance. Further information will be progressively added to this CMP as the construction scope develops.

- **Community relations.**

Proper consultation with neighbours and the local community is of paramount importance. This Construction Management Plan identifies the need for a pro-active approach to the construction activities. An initial community/public consultation has been undertaken on the 18<sup>th</sup> November 2015 and feedback from this meeting is being reviewed for inclusion in the CMP.

- **Working hours.**

To ensure that the impact of the construction is kept to a minimum on this project we have proposed a voluntary Section 61 Prior Consent is applied for.

- **Deliveries.**

An initial delivery and vehicle movement plan is included in this CMP. Further detail will be added to this CMP as further engagement with the local community progresses.

### ▪ **Waste removal.**

Demolition arisings will be removed from site. It may be possible to re-use some of the basement excavation soil in the garden element of the project. Trade contractors and suppliers will have the requirement to minimise deliveries and waste as part of their contractual obligations.

### ▪ **Programme.**

There is currently no programme for this building. Further information is required to allow a detailed programme to be developed. The current planned start date on site is summer 2016, but this will be subject to planning and utilities diversions.

### **Camden's Minimum Requirements for Building / Construction / Demolition Sites and Guide for Contractors Working in Camden**

These documents set out the standards and procedures for managing the environmental impact of constructing major projects where construction of these projects has the potential to affect the environment, amenity and safety of local residents, businesses and the general public including the surroundings in the vicinity of the proposed works.

They cover all aspects of construction work that could reasonably be anticipated to impact on the local community and the environment throughout the construction of the proposed works. This report sets out:

- the general principles to be applied during construction and the context within which mitigation measures will operate and be developed;
- the specific provisions for construction site operations; and
- the specific environmental issues that need to be considered throughout the period of construction works.

### **Planning conditions/ working hours**

Certain aspects of construction such as working hours are controlled by conditions contained in planning permission imposed under the Town and Country Planning Act. To ensure that the impact of the construction is kept to a minimum on this project we would propose a voluntary Section 61 Prior Consent.

### **Licences**

In addition to the environmental requirements highlighted above, the Contractor will be responsible for obtaining licences from Camden Council before:

- erecting any scaffolding, hoardings, gantries, temporary crossings or fences on the highway;
- depositing a skip on the highway; and operating a mobile crane, aerial platform, concrete pump lorry or any such equipment.

## 1.0 CONTACTS

**C.1. Please provide the full postal address of the site and the planning reference relating to the Construction works.**

<b>Site Address:</b>	New End Square, Hampstead
<b>Planning application reference:</b>	TBC upon submission
<b>Type of CMP – Condition discharge / Section 106 planning obligation / Major sites framework</b>	

**C.2. Please provide contact details for the person responsible for submitting the CMP.**

<b>Name:</b>	Graeme Tait
<b>Address:</b>	Blue Sky Building 35 Duke Street London W1U 1LH
<b>Tel:</b>	+44(0)20 7831 5950
<b>Email:</b>	graemetait@blueskybuilding.com

**C.3. Please provide full contact details of the site project manager responsible for day-to-day management of the works and dealing with any complaints from local residents and business**

<b>Name:</b>	TBC
<b>Address:</b>	
<b>Tel:</b>	
<b>Email:</b>	

**C.4 Please provide full contact details of the person responsible for community liaison and dealing with any complaints from local residents and business if different from question 3**

<b>Name:</b>	TBC
<b>Address:</b>	
<b>Tel:</b>	
<b>Email:</b>	

**C.5. Please provide full contact details of the person responsible for community liaison/dealing with any complaints from local residents and businesses if different from question 3. In the case of Community Investment Programme (CIP), please provide contact details of the responsible Camden officer.**

<b>Name:</b>	TBC
<b>Address:</b>	
<b>Tel:</b>	
<b>Email:</b>	

**C.6. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.**

<b>Name:</b>	TBC
<b>Address:</b>	
<b>Tel:</b>	
<b>Email:</b>	

## 2.0 SITE

### S.1 Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

The project located at the corner of New End and Flask Walk in Hampstead consists of the demolition of existing garages to create a space for a 2 bedroom family residence. The proposed scheme utilises the slope of the descending footpath of Flask Walk to create a compact 4-storey house, including a newly excavated basement and an integrated garage.

The site sits within the current residential aspect of New End and the terraced housing of Flask Walk Cottages. The proposed façade and building height is designed to blend the new house into the current residential landscape.

New End Primary School is located to the west of the site and will require additional restriction with relation to deliveries and noise considerations and awareness of pedestrian movement. The remaining local area is primarily residential, with the only non-residential local interests being the Burgh House and Hampstead Museum, the Livingstone Studios and the Old White Bear Public House, all on New End Square.

Directly opposite the site entrance is the Madeleine Trehearne; pashmina, cashmere and textile specialist. We will be liaising directly with her to ensure her needs for client access are maintained.

Hampstead station is approximately 200 metres southwest of the site. Road access to the site is via the one way system of New End Square and New End. Both these roads are narrow and have 20mph restrictions.





### **S.2 Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity to residential dwellings).**

The development consists of demolishing the existing buildings, excavating a new basement, and building the new house. The site is constrained by neighbouring buildings and gardens. The Flask Walk Cottages party line wall is likely to require propping during the basement excavation. Similarly, the garden areas to the side and rear of 26E New End Square will require a measure of propping or other retention methodology. There are two trees whose root structure will be influenced by the construction and these will require protection and treatment as required by the arboricultural consultant.

The sloping nature of the site means a progressive build of the house itself will be required, ensuring that the floor slabs and walls are built sequentially to ensure that stable ground conditions are maintained and ground and storm water conditions are managed.

During the Pre-Construction and Site Establishment phases, the construction team will mobilise, acquire all necessary approvals and licences to undertake the works. Any required adjacent building monitoring will also be installed at this stage.

Once on site, the team will upgrade the current hoarding and install secure gates. Secure on site storage and facilities for workers will be established.

The start of works to the building will begin with asbestos removal (if required) and soft strip followed by structural demolition of the garages. In order to maintain stable ground and provide access to the site, the current concrete ground slab will be retained as much as possible until construction of the house requires it's removal.

The basement excavation phase of the works will then commence, including installation of any required retention/propping systems required for the party wall to Flask Walk Cottages and the garden to the rear of 26E New End Square. The basement excavation will require sheet piling or similar retention to it's full extent. The soil will then be excavated to the upper level of the site, still within the hoarding line. From there it will be placed in vehicles for removal from site.

Once excavation is complete, the basement box concrete will be poured and the slabs at ground floor will be created, including the garage slab. The garage slab will create a stable vehicle delivery spot and this will be utilised during the construction of the higher southern element of the house structure. Only when this is complete will the northern section of the structure be built. By this time the need for larger vehicles will have been removed and deliveries will continue to the site by smaller vehicles, allowing deliveries to be managed within the garage space and adjacent garden area.

The envelope of the building will follow the superstructure sequencing. Once the building achieves watertightness, the installation of the residential fit-out will begin.

Key issues are:

1. Basement retention and excavation and ensuring the integrity of the Flask Walk Cottages and gardens to 26E New End Square
2. Maintaining an on-site vehicle hard standing for as long as possible during the project

3. Managing vehicle access and egress through the one-way system of New End Square and New End
4. Minimising disturbance to residences around the site
5. Managing the interface with New End Primary School, both from a pedestrian perspective and with regard to peak vehicle times for the school

**S.3 Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting, etc.).**

The primary receptors would be the immediately adjacent residential buildings along New End, Flask Walk and New End Square. These are identified in S.1 above.

New End Primary School is located to the west of the site and will require additional restriction with relation to deliveries and noise considerations and awareness of pedestrian movement.

**S.4 Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.**

See application drawings for details.

**S.5 Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be useful).**

There is currently no programme for this building. Further information is required to allow a detailed programme to be developed. The current planned start date on site is summer 2016, but this will be subject to planning and utilities diversions.

**S.6 Please confirm the standard working hours for this site, noting that the standard working hours for construction sites in Camden are as follows:**

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

We confirm the standard working hours for this project will be in accordance with the Camden Council guidelines as per above.

**S.7 Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT. etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions:**

To be confirmed after planning submission.

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## 3.0 COMMUNITY LIAISON

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### CL.1 Consultation

**The Council expects meaningful consultation. For large sites, this may mean two or more meetings with local residents prior to submission of the first draft CMP. Details should include who was consulted, how the consultation was conducted and a summary of the comments received. In response to the comments received, the CMP should then be amended where appropriate and, where not appropriate, a reason should be given. The revised CMP should also include a list of all the comments received. Developers are advised to check approaches to proposed consultation with the Council before carrying them out.**

**Please provide details of the consultation of draft CMP with the local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors.**

An initial consultation event was held for local residents on the evening of 18<sup>th</sup> November 2015. This draft CMP will form part of the planning application submission for general public consultation. If the Council require further consultation post-determination this will be discussed with officers and appropriate provision made.

### CL.2 Construction Working Group

**Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing concerns of the community affected by the works, the way in which the contact details of the person responsible for community liaison will be advertised in the local community, and how the community will be updated on the upcoming works i.e. in the form of newsletter/letter drop, or weekly drop-in sessions for residents.**

The Contractor will provide community relations personnel, who will be focussed on engaging with the local community. The Contractor will ensure that occupiers of nearby properties and local residents, will be informed in advance of works taking place, including the estimated duration.

The Contractor will inform local businesses and residents such as The Old White Bear Pub, Burgh House & Hampstead Museum, New End Primary School, and other neighbouring properties likely to be affected by such activities at least 14 days prior to undertaking the works, as well as applying for the appropriate permits and licences, e.g. road closures for delivery or abnormal deliveries to the site.

Whilst the Contractor will provide monthly newsletters, we propose that an additional liaison group will be set up with representatives of the adjacent properties.

The Contractor's project manager together with the nominated person (if different) will agree with these neighbours a schedule of regular review meetings. Sufficient time prior to activities will be allowed for the neighbours' reasonable concerns to be addressed. Where required and reasonable, requested ad-hoc meetings with these neighbours will be attended by the Contractor's project director and the nominated person.

In the case of work required in response to an emergency, Camden Council, and all neighbours will be advised as soon as reasonably practicable that emergency work is taking place. Potentially affected occupiers will also be notified of the 'hotline' number, which will operate during working hours.

### **CL.3 Schemes**

**Please provide details of any schemes such as the 'Considerate Constructors Scheme', such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the "Guide for Contractors Working in Camden" also referred to as "Camden's Considerate Constructors Manual".**

The Contractor will be signed up to the Considerate Constructors Scheme, TfL's Standard for construction logistics and cyclist safety (CLOCS), and will follow the "Guide for Contractors Working in Camden".

### **CL.4 Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative construction in the vicinity of the site. The Council can advise on this if necessary.**

An initial schedule of possible construction sites that may be in progress while the New End Square project is underway is listed in Appendix A. Further development of this list, together with area plan and mitigation/co-ordination strategies, will follow in the ongoing development of the CMP post-planning application. We would welcome Council input to this schedule at that time.

## 4.0 TRANSPORT

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### T.1 Name of Principle Contractor

Yet to be appointed

### T.2 Please submit the proposed method for checking operational, vehicle and driver compliance with the CLOCS Standard throughout the duration of the contract (please refer to our CLOCS Overview document in the appendix and CLOCS Standard point 3.4.7)

#### **OPERATIONS**

- Quality operation: accreditation via an approved fleet management audit scheme e.g. Fleet Operator Recognition Scheme (FORS) or equivalent.
- Collision reporting and analysis: of any collision involving injury to persons, vehicles or property, ideally including use of the CLOCS Manager collision reporting tool.
- Traffic routing: any route specified by the client is adhered to unless otherwise specified.

#### i. **VEHICLES:**

- Warning signage: warning cyclists of the dangers of passing the vehicle on the inside
- Side under-run protection
- Blind spot minimisation: front, side and rear blind-spots completely eliminated or minimised as far as is practical and possible
- Vehicle manoeuvring warnings: enhanced audible means to warn other road users of a vehicle's left hand turn or other manoeuvres

#### ii. **DRIVERS:**

- Training and development: approved progressive training and continued progressive training especially around vulnerable road users (including for drivers excluded from Certificate of Professional Competence requirements)
- Driver licensing: regular checks and monitoring of driver endorsements and that drivers hold the correct licence for the correct vehicle

#### **STANDARD FOR CONSTRUCTION CLIENTS**

- Construction logistics/management plan: is in place and fully complied with – as per this document.
- Suitability of site for vehicles fitted with safety equipment: that the site is suitably prepared for vehicles fitted with safety equipment to drive across.
- Site access and egress: should be carefully managed, signposted, understood and be clear of obstacles.
- Vehicle loading and unloading: vehicles should be loaded and unloaded on-site as far as is practicable.
- Traffic routing: should be carefully considered, risk assessed and communicated to all contractors and drivers.
- Control of site traffic, particularly at peak hours: other options should be considered to plan and control traffic, to reduce traffic at peak hours.
- Supply chain compliance: contractors and sub-contractors throughout the supply chain should comply with requirements 3.1.1 to 3.3.2. of the "Guide For Contractors Working in Camden": February 2008"

These requirements will be incorporated into the contract wording when appointing contractors to ensure compliance.

**T.3 Please confirm that you as the client/developer and your principle contractor have read and understood the CLOCS Standard and included it in your contracts. Please sign-up to join the CLOCS Community to receive up to date information on the standard by expressing an interest online.**

I confirm that I have included the requirement to abide by the CLOCS Standard in my contracts to my contractors and suppliers.

**T.4 Traffic routing**

**a. Please include routes on a drawing or diagram showing the public highway network in the vicinity of the site including details of links to the Transport for London Road Network (TLRN)**

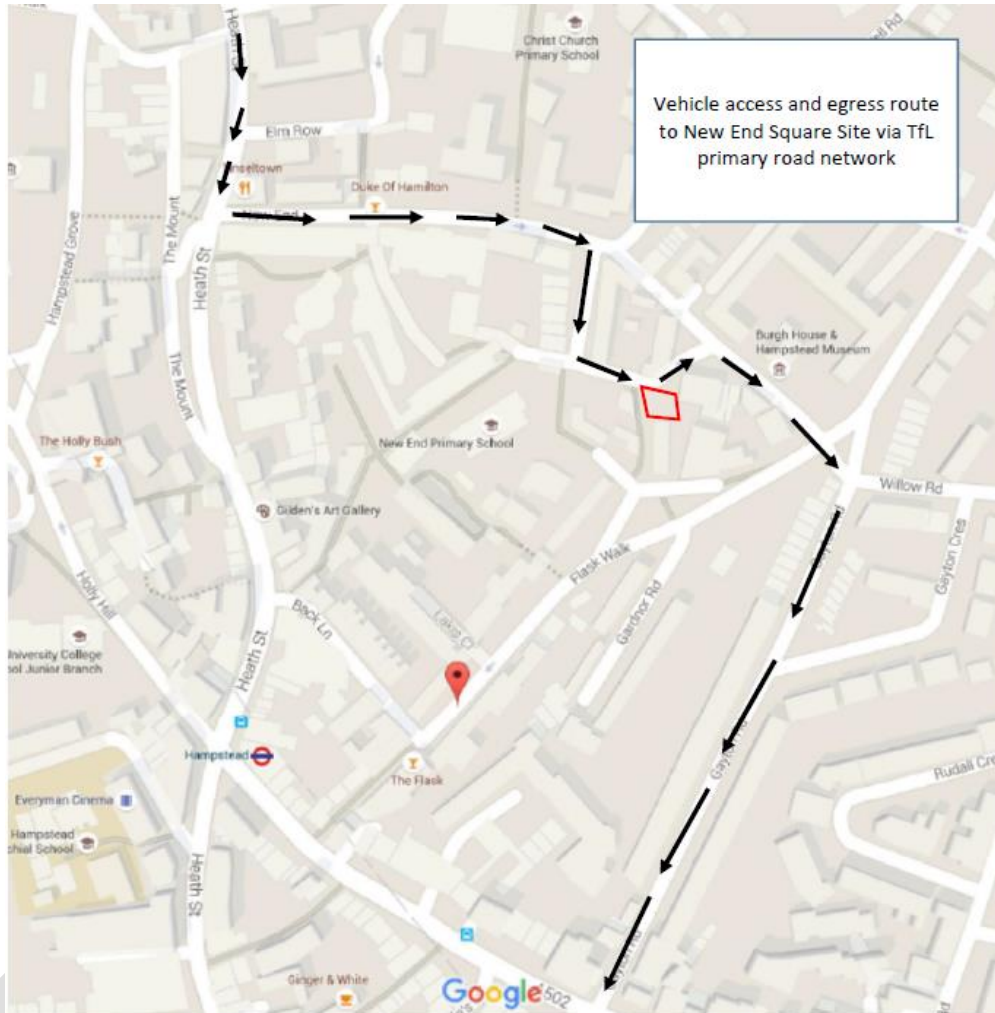
Vehicles will access the site via the TLRN along Heath Street, turning into the one-way system along New End, and will follow this to the site entrance.

Upon exiting the site, vehicles will turn right out of New End into New End Square and follow the road until the Junction of Gayton Road and New End Square.

Here they will turn right and drive south along Gayton Road until they join the A502 TLRN.

This route minimises the number of residential roads that are utilised and follows the established one way system.

There are three schools of note either along or adjacent to the route; Heathside Primary School, New End Primary School and Christ Church Primary School. Vehicle movements will be timetabled to avoid peak periods for these schools in the morning and afternoon.



**b. Please confirm how contractors, delivery companies and visitors will be made aware of the route (to and from site) and of any on-site restrictions, prior to undertaking journeys.**

A delivery/visitor journey booklet will be prepared by the Main Contractor and this will be distributed to all contractors, staff, visitors and suppliers who will attend the site.

This booklet will describe the required access and egress routes, including hazards of note and alternative routes, times that deliveries or visits will not be permitted (to avoid school times etc as noted above.) on-site rules and restrictions and public transport alternatives.



## T.5 Control of site traffic, particularly at peak hours

**a. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time on site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures.**

The overall vehicle delivery numbers will be developed once the client consultant team have provided further design information.

The current limiting factors for vehicles lengths and widths accessing the site are governed by the turning circles into and along New End and the narrow width of New End.

Weight limits of vehicles will be assessed against the overall capacity of the granite setts roadway in both New End and New End Square.

**b. Please provide details of other developments in the local area or on the route.**

Please refer to Appendix A for a schedule of currently known potential other developments, together with the response in CL.4 above.

**c. Please outline the system that is to be used to ensure that the correct vehicle attends the correct part of the site at the correct time.**

The site will have a single point of entry and a single parking bay. Deliveries will be managed on a booking basis, with strict delivery times allocated to ensure no traffic will build up on the street.

**d. Please identify the locations of any off-site holding areas and any measures that will be taken to ensure that the prompt admission of vehicles to site in light of time required for necessary compliance checks. Please refer to question 5 if any parking bay suspensions will be required for the holding area.**

Due to the small size of the project, it is not anticipated that any off-site holding areas will be required

**e. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of construction material consolidation centres).**

Additional measures have not been considered due to the small scale of the project. The focus will be on an efficient delivery booking system to prevent congestion due to vehicle deliveries on New End. Limitations on vehicle sizes will be enforced to ensure that there will be no issues with regards to turning into and out of New End.

## T.6 Site access and egress

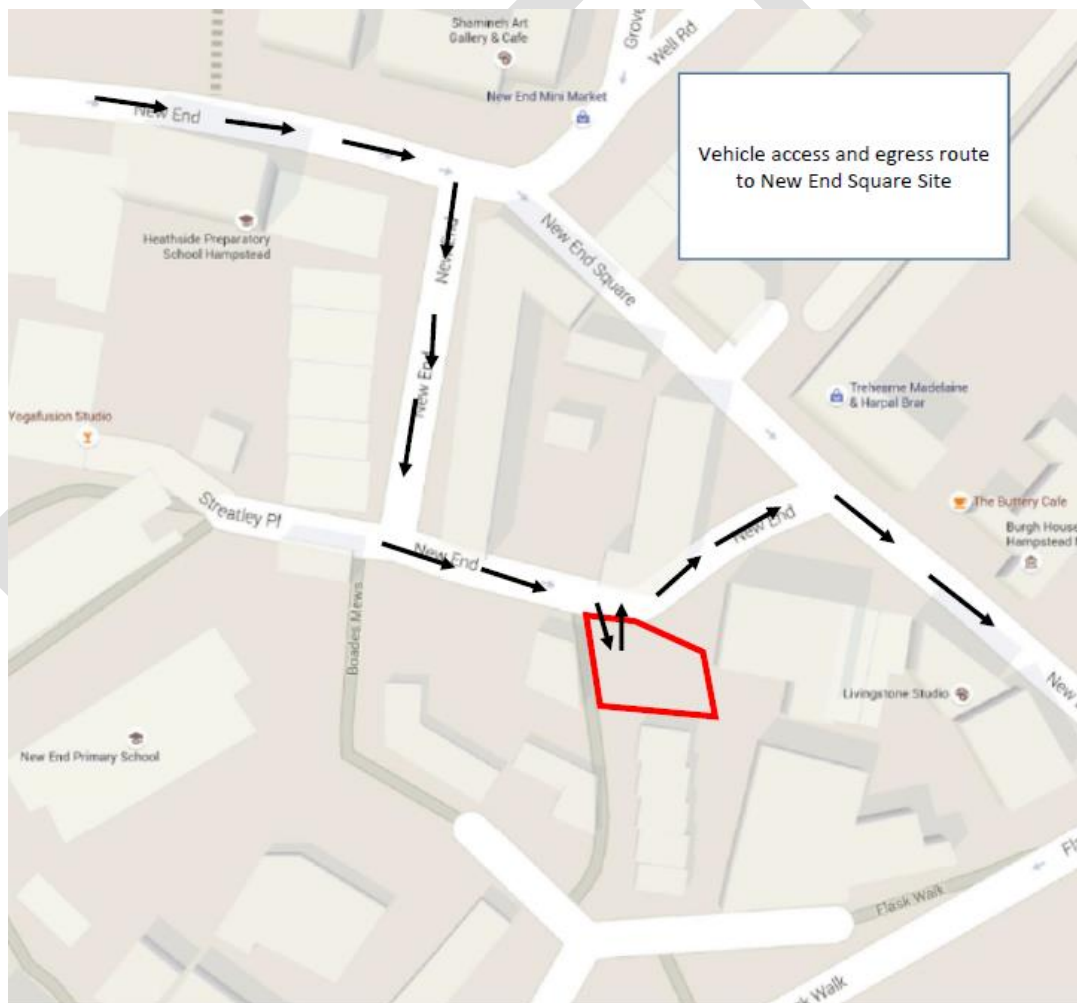
### a. Please detail the proposed access and egress routes to and from the site.

As described in T.4a above, site vehicular traffic will enter the one way system along New End. Particular care will be taken along New End due to the proximity of Heathside Preparatory School Hampstead and New End Primary School.

The traffic management team at the site entrance will manage the reversal of site vehicles through the site entrance. Offloading will take place within the site boundary.

Upon exiting the site, the vehicles will turn right and connect with the junction at New End Square.

New End has a 20mph speed limit and is very narrow close to the site, also having parking to both sides along elements of the road.



As an alternative route vehicles may access New End by driving into New End Square, with New End under traffic management by the site team, and reversing into the east end of New End.

This would require further discussion with Camden Council but may offer a safer route for specific vehicles.

### **b. Please describe how the access and egress arrangements for construction vehicles will be managed.**

The 2001 Final Audit for the Hampstead Conservation Area identified the following elements as falling under conservation requirements for the interface between New End and New End Square:

#### **NEW END west towards Streatley Place & Flask Walk**

Wall of Heathside Preparatory school, stone plaque recording former Soup Kitchen.

York stone pavement both sides, TNP both sides (and above TNP on south side a stone plaque reads Southwell Terrace)

Three 19th Century lighting columns

Narrow granite kerbstones

TNP on wall of New End House flats

#### **Left turn leading into New End Square**

Granite sett roadway

York stone pavements both sides of road with granite kerbstones.

One 19th century gas lighting column

The granite sett roadway, granite kerbs and York stone pavements both sides of the road will require protection at the access and egress point directly in front of the site. The road will also need to be assessed for overall load capacity to ensure the granite setts are not compromised.

This, together with the limiting widths of New End, will define the maximum weight of vehicle that can access the site.

Access and egress to the site will be via a strict booking system to ensure that the local roads are not clogged with construction traffic.

Traffic Marshalls at the site will have the responsibility for ensuring safe access and egress of the vehicles onto the site. They will also act as direct liaison with the local community and schools to identify any special events that will require an adjustment to planned delivery times. They will also liaise with other local developments to co-ordinate structured deliveries and, where possible, co-ordinate common supplier deliveries.

**c. Please provide swept path drawings for the tight manoeuvres on vehicle routes to and from the site boundary including proposed access and egress arrangements at the site boundary (if necessary).**

See enclose plan

**d. Provision of wheel washing facilities should be considered as necessary. If so, please provide details of how this will be managed and any run-off controlled.**

Wheel wash facilities will not be required as vehicles will be standing on concrete slabs at all times, either existing or new.

### **T.7 Vehicle Loading and Unloading.**

**“Clients shall ensure that vehicles are loaded and unloaded on-site as far as is practicable.”**

**Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site. If loading is to take place off site, please identify where this is due to take place and outline the measures you will take to ensure that the loading/unloading is carried out safely. Please outline in question 8 if any parking bay suspensions will be required.**

It is currently anticipated that there will be sufficient space on site to provide an on-site vehicle space at all times to ensure that there is no road parking for construction traffic. New End is extremely narrow and cannot accommodate any roadside parking.

The final scaled site layout will be provided following completion of the structural documentation and swept path review. An indicative site plan is included in Appendix C

### **T.8 Parking bay suspensions and temporary traffic management orders**

**Please provide details of any proposed parking bay suspensions and temporary traffic management orders which would be required to facilitate construction.**

Initial review of New End and New End Square has identified two potential parking bays that may require suspension. This will be assessed as part of the swept path assessment. Refer to Appendix B for identification of the two bays.

## T.9 Scaled drawings of highway works

**a. Please provide accurate scaled drawings of any highway works that are necessary to enable construction to take place (e.g. construction of temporary vehicular access).**

To be confirmed post determination in consultation with the highways department and to be detailed in subsequent updated CMP

**b. Please provide details of all safety signage, barriers and accessibility measures such as ramps, lighting etc.**

To be confirmed post determination in consultation with the highways department and to be detailed in subsequent updated CMP

## T.10 Diversions

**Where applicable, please supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted).**

It is not anticipated that any diversion or disruption of the public highway will be required, unless installation of associated utilities necessitate.

## T.11 VRU and pedestrian diversions, scaffolding and hoarding

**a. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Traffic Marshall arrangements.**

To be further detailed as part of next phase of CMP.

Traffic Marshalls will attend all deliveries to site along New End and will also oversee the left hand turn into the granite sett section of New End as this point has the possibility to have compromised visibility with regards to pedestrians and cyclists. Vehicles will be equipped as required by the CLOCS Guide – Vehicle Safety Equipment guidelines to maximise protection of vulnerable road users.

The positioning of the existing pavement bollards are of great assistance in segregating traffic from the pedestrian footpaths and these will be maintained throughout the project duration. Traffic Marshalls will also manage the pedestrian flow during delivery times at the site to ensure public safety.

The Traffic Marshalls will also be aware of both pedestrian and cycle use of Flask Walk, to ensure that no public are placed at risk when exiting Flask Walk into New End.

It is not anticipated that any alternative routes will be required for pedestrians or cyclists.

**b. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.) and details of hoarding requirements or any other occupation of the public highway.**

To be further detailed as part of next phase of CMP.

It is anticipated that a section of Flask Walk will require overhead protection, together with the adjacent Flask Cottages.

An external hoarding currently surrounds the site; this will need upgrading to form sufficient protection for the works. Secure doors will be established at the site entrance on New End.

## 5.0 ENVIRONMENT

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**E.1 Please list all noisy operations and the construction method used, and provide details of the times that each of these are to be carried out.**

To be detailed as part of next phase of CMP.

**E.2 Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that noise survey will be taking place, and agree to provide a copy.**

To be detailed as part of next phase of CMP.

**E.3 Please provide predictions for noise and vibration levels throughout the proposed works.**

To be detailed as part of next phase of CMP.

**E.4 Please provide details describing mitigation measures to be incorporated during the construction/demolition works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.**

To be detailed as part of next phase of CMP.

**E.5 Please provide evidence that staff have been trained on BS 5228:2009**

To be completed once Main contractor is appointed

**E.6 Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented**

Throughout the project the Contractor will ensure the following:

- Where potential dust producing activities are taking place dust screens will be utilised. This will include the demolition, piling and structural works.
- There is no burning of waste materials takes place on site.
- There is an adequate water supply on the site.
- Disposal of run-off water from dust suppression activities is in accordance with the appropriate legal requirements.
- All dust control equipment is maintained in good condition and record maintenance activities.
- Strip insides of buildings before demolition of the structure and envelope.
- Site hoarding, barriers and scaffolding are kept clean.
- The provision of clean hard standings for vehicles. Regular cleaning of hard standings using wet sweeping methods, no dry sweeping of large areas.
- Loading of material into lorries within designated bays/areas.
- If necessary, clean public roads and access routes using wet sweeping methods.
- Vehicles working on site have exhausts positioned such that the risk of re-suspension of ground dust is minimised (exhausts should preferably point upwards), where reasonably practicable.
- All vehicles carrying loose or potentially dusty material to or from the site are fully sheeted.
- Materials with the potential to produce dust are stored away from site boundaries where reasonably practicable.
- Minimise the amount of excavated material held on site.
- Sheet, seal or damp down unavoidable stockpiles of excavated material held on site, where required.
- Avoid double handling of material wherever reasonably practicable.
- Ensure water suppression is used during demolition operations.
- Sheet or otherwise enclose loaded bins and skips.
- Only use cutting, grinding or sawing equipment fitted or in conjunction with suitable dust suppression techniques such as water sprays or local extraction.
- The engines of all vehicles and plant on site are not left running unnecessarily to prevent exhaust.

- Use low emission vehicles and plant fitted with catalysts, diesel particulate filters or similar devices.
- Use ultra-low sulphur fuels in plant and vehicles.
- That plant will be well maintained, with routine servicing of plant and vehicles. On site servicing and maintenance to be carried out where possible.
- That all project vehicles, including off-road vehicles, hold current MOT certificates where required.
- Carry out site inspections regularly to monitor compliance with dust control procedures set out above and record the results of the inspections, including nil returns, in the log book detailed.
- Increase the frequency of site inspections when activities with a high potential to produce dust are being carried out and during prolonged dry or windy conditions.
- Record any exceptional incidents causing dust episodes on or off the site and the action taken to resolve the situation in the log book detailed in above.

The Contractor will ensure that dust monitoring will be carried out during potential dust producing activities. The assessment will look at the dust raising potential of construction activities proximity to potential receptors and the duration of construction activities at each location.

**E.7 Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented or cleaned.**

See response to E.6 (above)

**E.8 Please provide details describing arrangements for monitoring of noise, vibration and dust levels.**

To be detailed as part of next phase of CMP.

**E.9 Please confirm that a Risk Assessment has been undertaken at planning application stage in line with the GLA's Control of Dust and Emissions Supplementary Planning Guidance (SPG), and the risk level that has been identified, with evidence. Please attach the risk assessment as an appendix if not completed at the planning stage.**

To be detailed as part of next phase of CMP.



**E.10 Please confirm that all of the GLA's 'highly recommended' measures from the SPG document relative to the level of risk identified in question 9 have been addressed by completing the GLA mitigation measures checklist. Please attach this as an appendix.**

To be detailed as part of next phase of CMP.

**E.11 If the site is a High Risk Site, 4 real time dust monitors will be required, as detailed in the SPG. Please confirm the location, number and specification of the monitors in line with the SPG and confirm these will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.**

To be detailed as part of next phase of CMP.

**E.12 Please provide details about how rodents, including rats, will be prevented from spreading out from the site, You are required to provide information about site inspections carried out and present copies of receipts (if work undertaken).**

A vermin inspection and action plan has not yet been undertaken. This will be included as part of the demolition scope.

A method statement will be issued 28 days prior to demolition detailing the how the destruction and/or dispersion of rodents will be managed. The method statement will outline how the presence of rodents has been determined and how they will be destroyed.

The Contractor will ensure that the site is kept tidy and surplus materials are kept to a minimum on site so as not to create areas for pests to hide. Workers will not leave food debris in or around the site to eliminate food sources for rodents. Office and welfare areas will be kept clean and operated hygienically to minimise risk of rodents. Waste will be stored in pest proof containers and removed regularly. New and existing drainage will be sealed during construction.

**E.13 Please confirm when an asbestos survey was carried out at the site and include the key findings.**

There is no currently identified asbestos noted. A non-intrusive asbestos survey will be undertaken to confirm prior to demolition.

**E.3 Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of suitable smoking area, tackling the bad language and unnecessary shouting.**

The Contractor will provide a suitable smoking area within the site boundary and instructed not to smoke immediately adjacent to the site. As part of the induction the contractors will be



advised that bad language, unnecessary shouting, and wolf whistling is unacceptable on this jobsite. These issues will be monitored by the site managers and traffic marshals.

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## 6.0 AGREEMENT

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The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed: \_\_\_\_\_ Date: \_\_\_\_\_

Print Name: \_\_\_\_\_ Position: \_\_\_\_\_

## APPENDIX A

Other projects in area.

Below is a schedule of projects identified on Camden Council's Planning Applications portal that may require co-ordination with the New End Square project, or that may be able to share deliveries to reduce construction traffic volume.

A key project, New End Nurses Home, 29 New End,(Planning reference 2012/3432/NEW) has also been identified as having potential impact. This is currently listed as "Withdrawn" on the Camden Council Planning portal.

Application Number	Site Address	Development Description	Status	Date Registered	Decision
<a href="#">2015/1323/L</a>	New End Primary School Streatley Place London NW3 1HU	Refurbishment of existing WC accommodation to meet current standards for school provision and water hygiene. Conversion of store room to provide additional WCs at third floor level. Temporary siting of WC accommodation within the school's play area for the duration of the works.	FINAL DECISION	12-03-2015	Granted
<a href="#">2015/2607/P</a>	40 Gayton Road London NW3 1TU	Erection of single storey rear extension and stairs to rear garden, alterations to front boundary wall, replacement of front windows at basement level,	FINAL DECISION	28-05-2015	Granted

		and lowering of rear garden.			
2015/3968/P	3 Gardnor Road London NW3 1HA	Single storey rear extension to lower ground floor with partial roof terrace above and rebuilding of existing brick stairwell projection	FINAL DECISION	14-07-2015	Granted
2014/6564/L	35 Flask Walk London NW3 1HH	Internal and external works associated with extension to the garden annex to provide accommodation on two levels, construction of a covered walkway in the garden courtyard and internal alterations including replacement of the existing rear door to window all in association with the main single family dwelling (Class C3).	FINAL DECISION	17-10-2014	Granted
2015/3753/P	36 Flask Walk London NW3 1HE	Creation of a new basement floor with front lightwell and alterations on rear ground floor fenestration.	REGISTERED	29-07-2015	

2014/7778/P	6 Streatley Place London NW3 1HL	Erection of a part 2/part 3 storey building for short term holiday accommodation (Class Sui Generis) following the demolition of existing derelict outbuildings	FINAL DECISION	18-12- 2014	Granted Subject to a Section 106 Legal Agreement
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APPENDIX B

Site Access information images



Exit route from New End into New End Square



Approach to left hand turn of New End



New End approach to site, illustrating width and extent of granite setts





Site entry/egress point on New End

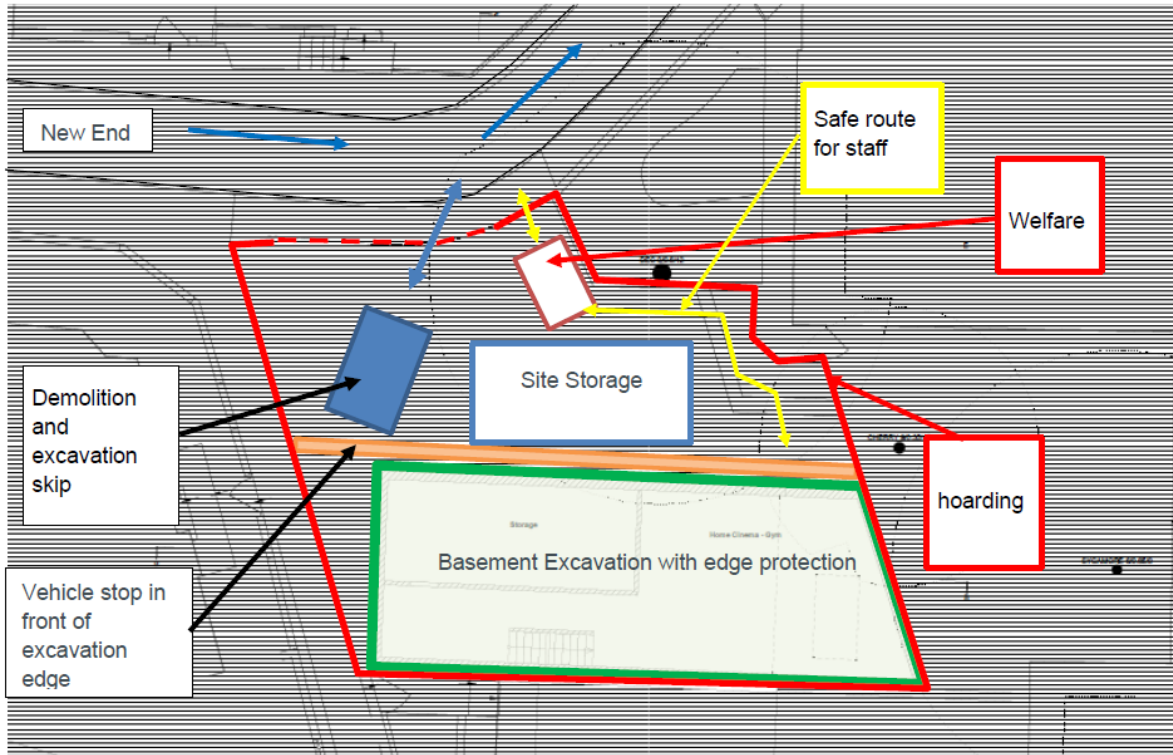
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Interface of New End with New End Square

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APPENDIX C



Indicative site layout for demolition and excavation stages

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# INTRODUCING BLUE SKY BUILDING

## FOUNDED ON EXCELLENCE

### WHO WE ARE

In 2012, Julian Daniel, our Founder and Managing Director spotted the opportunity to create a company of his own, Blue Sky Building, which would embody the enthusiasm and passion he feels for the industry. Blue Sky Building is an innovative construction management company which delivers unique solutions. Our founding directors boast a combined experience of over eight decades, uniting their background in the delivery of bespoke construction with the expertise and skills needed to manage complex engineering and construction projects, particularly in the midst of the kind of city centre environment prevalent in London and the South East. We act as a trusted collaborator, setting the kind of standards other constructors aspire to, by offering our clients quality, professionalism and innovation. We've built our reputation upon offering a bespoke service each time, tailored to meet the individual needs of each client. We know our industry and understand how the construction process works. We study our clients' business and we understand the wider business climate, bringing all three together in a pursuit of excellence which is as relentless as it is refreshing.

At Blue Sky Building, no resource is more valuable than the people charged with delivering our vision. The principles we work around are excellence, quality and safety and the values underpinning our work are intelligence, honesty, integrity and trust.

**"...WE HAVE BEEN RESPONSIBLE FOR SOME OF THE MOST COMPLEX AND CHALLENGING PROJECTS IN THE UK, CARRIED OUT FOR A NUMBER OF PRESTIGIOUS CLIENTS IN CONJUNCTION WITH THEIR PROFESSIONAL CONSULTANTS."**



### OUR PROMISE

- A focus on the client;
- Clarity of leadership and direction;
- Accessible and practical advice;
- Input and ownership up to Director level;
- Appropriate and timely communication;
- Simple solutions to complex issues;
- Advice which is independent and maintains the integrity of the clients' procurement process;
- In depth knowledge of the market and links to key trade contractors; and
- Value added throughout - from design, through procurement and on to construction

### OUR CLIENTS

STANHOPE  
GREAT PORTLAND ESTATES  
GROSVENOR  
SELFRIDGES  
LONDON & REGIONAL PROPERTIES  
BRUCE SHAW  
CLIVEDALE PROPERTIES LIMITED  
CORE  
DELOITTE  
DULWICH COLLEGE  
KAMKO LIMITED  
QUINTAIN  
GARDINER & THEOBALD

PRE- CONSTRUCTION

CONSULTANCY

PROJECT MANAGEMENT/ EMPLOYER'S  
AGENT - CONTRACT ADMINISTRATOR

CONSTRUCTION DELIVERY