

MY Construction and Carpentry Ltd.

PROPOSED ALTERATIONS AND REFURBISHMENTS

79 FORTRESS ROAD
CAMDEN, NW5 1AG

CONSTRUCTION MANAGEMENT PLAN

CONTENTS

1.0 INTRODUCTION

2.0 SITE SET-UP AND GENERAL CONDITIONS

2.1 ERECTION AND MAINTENANCE OF SECURITY HOARDING

3.0 ROUTING OF DEMOLITION, EXCAVATION AND CONSTRUCTION VEHICLES

4.0 ACCESS ARRANGEMENTS TO THE SITE

4.6 MEASURES TO CONTROL EMISSION OF DUST AND DIRT

4.10 LOADING AND UNLOADING OF MATERIALS

4.12 STORAGE OF PLANT AND MATERIALS

5.0 ESTIMATED NUMBER OF VEHICLES

6.0 DETAILS OF ANY VEHICLE HOLDING AREA AND CALL-UP PROCEDURE

7.0 ESTIMATED NUMBER AND TYPE OF PARKING SUSPENSIONS

8.0 DETAILS OF ANY DIVERSION OR ABNORMAL DISRUPTION OF THE HIGHWAY

9.0 STRATEGY FOR CO-ORDINATING THE CONNECTION OF SERVICES

10.0 TIMESCALE

Appendix A

HIGHWAY SITE PLAN

Appendix B

TRAFFIC ROUTING PLAN

1.0 Introduction

- 1.1 This document details the Construction Management Plan for the alterations and refurbishments of the existing property at 79 FORTRESS ROAD,
- 1.2 The document contains a brief description of the project and sequencing of the construction operations. It also outlines how the immediate area surrounding the project will be protected as well as the local environment and amenities.
- 1.3 Safety of all working on site, of the inhabitants of the adjoining properties and pedestrians in the vicinity of the site is considered to be of the highest importance.
- 1.4 Liaison between the Contractor's staff and local residents will be undertaken throughout the project with a view to maintaining the current activities of residents with minimal disruption from the construction works.
- 1.5 The site is located in a town centre location, adjacent to a busy traffic corridor. Construction vehicle movements will be scheduled to avoid peak periods in order to minimise traffic congestion and road safety issues. Construction movements will take place between 0930 and 1630 hours on Monday to Friday and between 0800 and 1300 hours on Saturdays. It is possible that abnormal loads may need to be delivered outside these times (e.g. crane delivery, concrete deliveries) in this instance adequate notice will be given to all concerned.

2.0 SITE SET-UP AND GENERAL CONDITIONS

2.1 ERECTION AND MAINTENANCE OF SECURITY HOARDING

Site hoarding will be Herras Fencing erected to the front boundary of the site, with statutory signage and project details displayed as required by planning permission and current legislation. The hoarding will be erected approximately 2.3m in front of the building line within Bellina Mews only. Scaffolding will be erected to all four elevations of the building confined to Bellina Mews only and subject to Temporary Structure Notice, to enable excavation work to the Basement extension occupying the privately-owned front area. There will be no scaffolding or hoarding erected along Fortress Road.

The hoarding will be maintained in good condition throughout the occupation of the site.

- 2.2 Site access will be via a lockable gate. The gate will be kept securely closed during the construction phase.

Entry for deliveries, staff and visitors will be closely controlled. Provision of Banksmen or other suitably qualified person(s) will be maintained while the gate is unlocked, to prevent access to unauthorised persons. All vehicle movements will be supervised by the banksman

There is no footway in Bellina Mews. Where operatives are carrying materials along the public highway during deliveries and waste management activities, Banksmen

may also be allocated to alert pedestrians to these activities and any potential hazards that may arise.

- 2.3 No building work which can be heard at the boundary of the site will be permitted outside the hours of 8:00 to 18:00 hours Monday to Friday, and 8:00 until 13:00 on Saturday. None will be permitted on Sundays and bank holidays. The Contractor will consult with the Council if any difficulty is anticipated in complying with these restrictions.

3.0 ROUTING OF DEMOLITION, EXCAVATION AND CONSTRUCTION VEHICLES

- 3.1 We anticipate that a large proportion of deliveries and waste removal vehicles will approach via FORTRESS ROAD and they will reverse into Bellina Mews.

Traffic leaving the site would be expected to drive out and exit Bellina Mews either left or right onto Fortress Road the A400.

4.0 ACCESS ARRANGEMENTS TO THE SITE

- 4.1 There is no vehicular access on to the site, and consequently no off-street parking.

Site operatives and visitors will be advised that there is little available on-street parking in the vicinity, and they will be advised to use public transport. Alternatively arrangements will be made for operatives and their portable equipment to be bussed to site each morning, and picked up on their return each evening.

- 4.2 79 Fortress Road has a yellowed line 2m away from the front of the building for use of the occupiers of the building and the contractor during working hours.

Delivery drivers will be advised to park in areas where there are no loading/unloading restrictions. Deliveries will not be accepted by Site Staff where suppliers' vehicles have parked illegally or in a manner inconsiderate to other road users. Site Staff will immediately support any requests from Parking Officers without objection, and will expect delivery drivers to comply with the same

- 4.3 Any contractor vehicle which is used by a tradesman working on the site will wait sufficient time for tools and materials to be delivered or collected for that days works. The vehicle will then park at a suitable point within a recognised parking bay or car park.

- 4.4 Deliveries and removal of material to and from site will be on a 'Just in time' basis. All suppliers will be made aware that Bellina Mews is not readily accessible to articulated vehicles and will be advised to send deliveries on small rigid vehicles. Strict delivery procedures will be maintained to minimise the inconvenience to other residents.

- 4.5 A banksman or other suitably qualified or competent person will be present during the loading of vehicles to ensure the safety of pedestrians. All vehicles will travel and stay on the hard tarmac highway, there will be no site material contamination which would require wheel washing however concrete lorries will not be allowed to wash down on the site or on the highway.
- 4.6 MEASURES TO CONTROL EMISSION OF DUST AND DIRT
- 4.7 The work place will be kept as clean as possible and extra cleaning will be carried out where large amounts of dust are created. Wet suppression methods may be used if appropriate.
- 4.8 Scaffolding will be wrapped in suitable close mesh 'debris netting' to minimise dust pollution to adjacent structures and the immediate area around the site. As the work progresses a more impermeable cover for the scaffold may be required to prevent dust pollution from cutting grinding and sawing activities on site. Strict adherence to (Control of Dust and emissions from Construction and Demolition Best Practice' London Councils 2006) will be maintained.
- 4.9 Dismantling of existing structures on site and excavation of subsoil will be by hand, using hand or small power tools to minimise the potential for damage to adjacent structures. The potential for airborne dust will be minimised by wetting of structures and of demolished material where required. Noise levels will be kept to a minimum by careful selection of plant and machinery in order to comply with planning requirements.
- 4.10 LOADING AND UNLOADING OF MATERIALS
- 4.11 Dismantled and excavated material will be removed from the site to skips or waiting vehicles positioned directly outside the property, or temporarily deposited within the site hoarding pending collection by grab lorry. Removal of material to skips will be by wheelbarrow and hoist, conveyor belt, refuse chute, or small powered skip-loaders etc. Any waste resulting from demolition and suitable for recycling will be separated and taken to the appropriate local authority recycling depot.
- 4.12 STORAGE OF PLANT AND MATERIALS
- 4.13 Owing to the nature of the works - the initial excavation in the basement - and the layout of the building, the site is very restricted for space. We will initially use the existing Ground Floor for site offices and accommodation. The transit and temporary storage of materials will make use of the front 'apron' area.
As the works progress, following completion of the Basement alterations and the new Ground Floor reconstruction, these latter areas will be used for the re-location of storage areas and further site accommodation, welfare areas etc.
- 4.14 The site manager will designate suitable storage areas for plant, materials, waste, flammable substances and hazardous substances as appropriate:

- 4.15 The Highway will be brushed clean after each delivery / collection, and no waste material will be left in Bellina Mews overnight.
- 4.16 Public rights of way along Fortess Road and Bellina Mews will be maintained at all times during the construction of the project and the cleanliness of the immediate vicinity will be maintained to a good standard at all times.
- 4.17 Scaffolding will be provided with alarms and appropriate security lighting.
- 4.18 Task lighting will be turned off at night outside working hours to accommodate residents of adjoining properties.
- 4.19 No food waste will be left on site and canteen facilities will be carefully controlled.
- 4.20 Specialist suitably qualified commercial pest control companies will be employed to deal with any observed infestation.
- 4.21 All plant and machinery, including generators and other noise making equipment, shall be located within the boundaries of the agreed site perimeter and not placed at any time on the public footpath or highway.

5.0 ESTIMATED NUMBER OF VEHICLES

- 5.1 A maximum of six visits per day by delivery/waste management vehicles is anticipated during the excavation phase.

During the general construction stage, no more than three deliveries per day are anticipated.

48 hours' notice will be required from all suppliers to prevent vehicles congregating in the vicinity. Neighbours will be advised of deliveries by large vehicles and given 24 hours' notice of such occurrences.

6.0 DETAILS OF ANY VEHICLE HOLDING AREA AND CALL-UP PROCEDURE

- 6.1 Owing to the size and nature of the project, we do not consider that a formal holding area will need to be designated. Any multiple long-distance deliveries by large vehicles will be advised to wait in a suitable place in the outer suburbs until called. Any such vehicles will be called up by mobile phone and advised when to precede, depending on the progress of the unloading and the prevailing traffic conditions and estimated travel time.

7.0 ESTIMATED NUMBER AND TYPE OF PARKING SUSPENSIONS

- 7.1 It is not envisaged that a substantial number of parking suspensions will be required, as most deliveries and waste removal will be by grab lorry.

Application for skip licences and single yellow line dispensations will be made in accordance with the procedures of CAMDEN Highways Department.

8.0 DETAILS OF ANY DIVERSION OR ABNORMAL DISRUPTION OF THE HIGHWAY

- 8.1 It is not envisaged that any diversions of the highway will be required, however a limited number of partial closures may be required for crane offloading and erection [eg steel erection, roof skylights or windows].
MY Construction will apply for any necessary partial closure in accordance with the procedures of CAMDEN Highways Department.
- 8.2 Highway service connections required by the Utilities will be applied for by the relevant utility or their approved contractor.

9.0 STRATEGY FOR CO-ORDINATING THE CONNECTION OF SERVICES

- 9.1 MY Construction will apply for any necessary service connections at the beginning of the works so that the relevant utilities are aware of what is required, and their connection dates can be co-ordinated as far as possible, to avoid successive disruptions of the Highway. A brief newsletter will be circulated at regular intervals and a project information board will be provided on the site hoarding. The project should be registered with the considerate Constructors Scheme.
<http://www.ccscheme.org.uk/>

10.0 TIMESCALE

- 10.1 The Contract Start Date is 2nd November 2015 [subject to discharge of Planning Conditions];

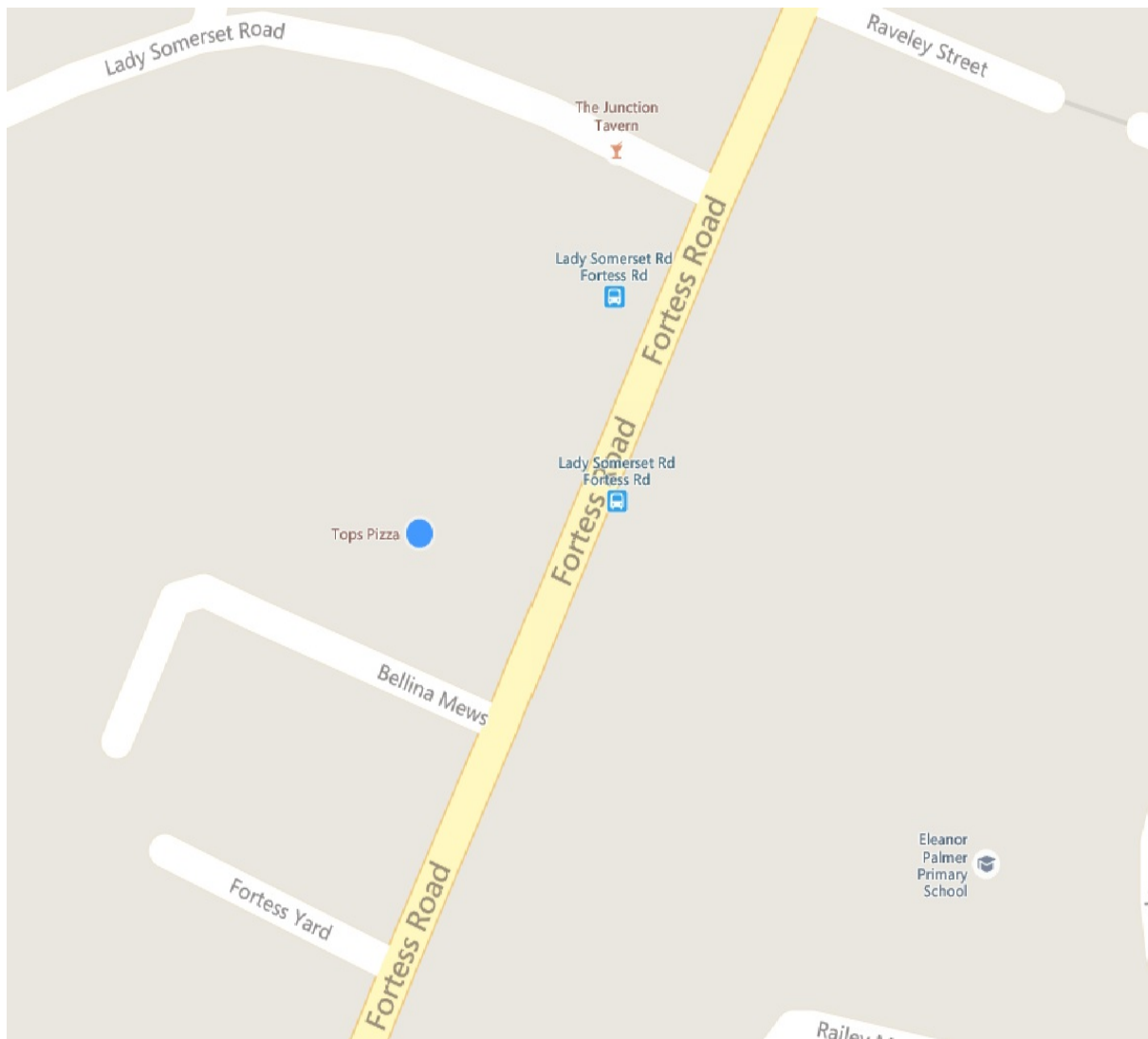
The Contract Period is 40 weeks, with Practical Completion estimated in May 2016;

Within this period, the Excavation Phase is anticipated from approximately November 2015 to December 2015, with the General Construction Phase running from approximately December 2015 to the Practical Completion date in approximately May 2016/June 2016

- 10.2 The agreed contents of the Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the Development. Any future revised plan must be approved by the Council and complied with thereafter.

Appendix A

HIGHWAY SITE PLAN



Appendix B

TRAFFIC ROUTING PLAN

Vehicles will reverse into Bellina Mews to make deliveries and drive forward onto Fortess Road on leaving site

