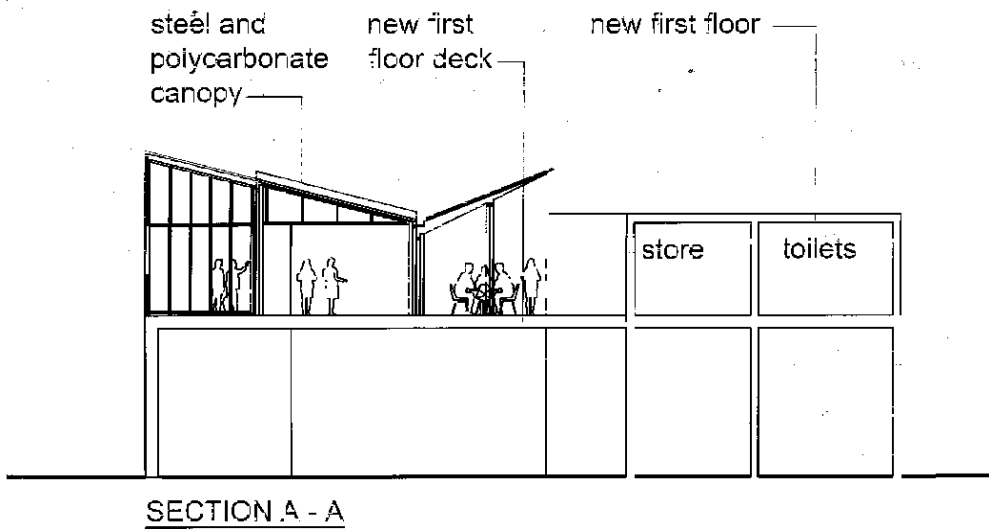


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Design & Access Statement

The Brondes Age
328, Kilburn High Road,
London NW6 2QN



1. INTRODUCTION.

This design and access statement has been prepared in support of a planning application for the construction of a roof top bar and restaurant, together with ancillary accommodation above an existing ground floor bar and restaurant at 328, Kilburn High Road, London, in the Borough of Camden.

2. SITE AND SURROUNDINGS

Kilburn High Road consists of a mixture of building uses from a variety of ages and of various sizes. The majority are from the 19th and 20th centuries. Notably there are a considerable number of bridges dating back to the industrial revolution which become immediately visible upon exiting the nearby Kilburn underground station. These include an impressive girder bridge that once belonged to the Metropolitan Railway.

Kilburn High Road consists of a variety of shops, including fast food, pubs, hair dressers, cafe/restaurants, furniture and carpet stores, as well as a clinic, a dentist, a medical centre and residential accommodation.

Adjacent to 328 is a 3 storey building containing on the ground floor a hair dressers and a clinic and above this are 2 storeys of residential accommodation, whilst on the other side is a railway bridge and embankment.



metropolitan railway bridge



Streetscape

3. THE BRONDES AGE

Number 328, "The Brondes Age" is a single storey building formed from four distinct shops which have been converted into a single bar/restaurant and includes a kitchen, cellar and toilet accommodation. The frontage to Kilburn High Road has four distinct roofs which are supported on what remains of the party walls that originally divided the shops. These roofs consist of three tiled hipped roofs and one flat roof. Projecting out onto the footpath is a full length retractable canopy for outdoor drinking and eating. The rear of the building consists of a series of single storey flat roofed modest attachments. Internally, floor to ceiling heights exceed four metres, except in the flat roofed bay. It is understood that the original ground floor restaurant was granted "change of use" retrospectively. It is apparent that the premises are looking dated and "run down" which together with its close proximity to the railway embankment, appears to have encouraged a number of "break ins" and vandalism.



railway embankment



railway bridge adjacent to 328



full length canopy and neighbours



mixture of pitched and flat roofs at rear

4. PROPOSED IMPROVEMENTS

The proposed improvements will increase the floor area by 260 sq m and the number of covers by 64, with additional bar stool space. To cope with the additional numbers new ladies and gentlemen's toilets will be available on the first floor terrace, together with increased storage space.

The new floor is a continuation of the buildings existing usage, however, it will be open to the elements, except where a canopy is provided over the bar and a number of tables.

Access to the first floor terrace will be by stairs located on the building's frontage and there is an additional means of escape on the side of the building adjacent to the railway bridge.

It is intended that music and entertainment will be available on the terrace on certain evenings and a licensing document with approved hours of use and an acoustic report has been attached to the application document.

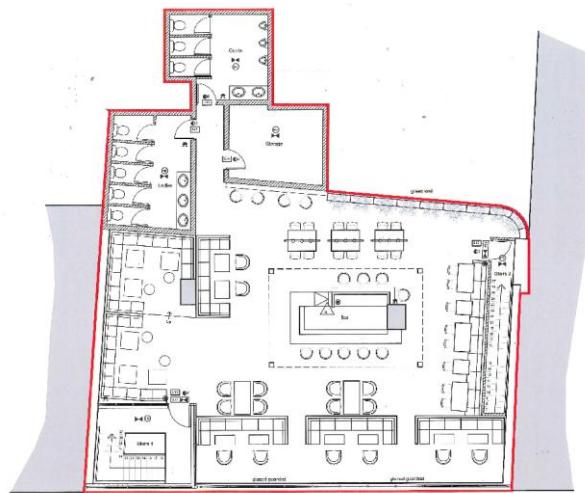
The layout of the roof terrace (and the position of the ancillary accommodation) means that the rear of the building will help prevent sound escaping in that direction and the railway bridge and embankment should also help provide a barrier.

It is considered likely that the fall in custom, currently being experienced, will be reversed with an upgrade to the ground floor interior layout and the further attraction of a roof top bar / restaurant.

It is anticipated that an increase in height at the rear of the building is also likely to deter trespassers and reduce crime.



roof layout



roof terrace furniture layout

5. DESIGN AND ACCESS

It is evident when walking along Kilburn High Road that number 328 as a single storey building appears incongruous alongside the adjacent buildings and railway bridge. In other words its scale and proportions do not relate well to its neighbours.

Careful consideration has been given to the design of the frontage to Kilburn High Road where the increase in height provided by the stairwell and canopy will help to address this rift in the streetscape. A walk along Kilburn High Road and its feeder roads reveals a network of railway bridges constructed in iron and steel from the age of the industrial revolution. These structures employ various examples of riveted plates and steel girder sections. It is the intention to introduce similar elements into the design of the outdoor canopy and stairwell by designing these structures in such a way as to complement the neighbouring structures.

Good effective lighting will also enhance the scheme by picking out the details.

6. ACCESS

The building currently acts as a bar and restaurant and is served well by public transport in the form of bus, underground and over ground railway.

Access to the ground floor from the pavement is level as is the ground floor itself and all principal areas of the building including the new stairwell will have well lit signage providing directions.

All door furniture and stair details will comply with part m of the building regulations in all aspects.

The new roof terrace will have no level changes, including the circulation route to the toilets, which will include arrangements for the ambulant disabled.

The roof terrace will have well lit signage designed to direct people safely around and to fire exits.