


Construction Management Plan Pro-forma



PRO-FORMA

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Queries: planningobligations@camden.gov.uk	
	
CONSTRUCTION MANAGEMENT PLAN	

Section 1 – Site Contacts

Q1. Please provide the full postal address of the site and the planning reference relating to the Construction works.

INTRODUCTION

A Construction Management Plan (CMP) should help developers minimise the impact of their construction on the surrounding community, both for the construction on site and the transport arrangements for servicing the site.

The completed and signed CMP should address how any impacts associated with the proposed works will be mitigated and manage the cumulative impacts of construction in the vicinity of the site. The level of detail included in a CMP will depend on the scale and kind of development. Further policy guidance is set out in Camden Planning Guidance ([CPG](#) 6: [Amenity](#) and ([CPG](#) 8: [Planning Obligations](#)

This CMP follows the best practice guidelines in [Transport for London's](#) (TfL's Standard for [Construction Logistics and Cyclist Safety](#) (CLOCS) scheme) and [Camden's Minimum Requirements for Building Construction](#) (CMRBC).

The approved contents of this CMP must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this CMP if problems arise in relation to the construction of the development. Any future revised plan must also be approved by the Council and complied with thereafter.

It should be noted that any agreed CMP does not prejudice or override the need to obtain any separate consents or approvals such as for road closures or hoarding licences.

If your scheme involves any demolition, you need to make an application to the Council's Building Control Service. Please complete the "[Demolition Notice](#)"

Please complete the questions below with additional sheets, drawings and plans as required. The boxes will expand to accommodate the information provided, so please provide as much information as is necessary.

(Note the term 'vehicles' used in this document refers to all vehicles associated with the implementation of the development, e.g. demolition, site clearance, delivery of plant & materials, construction, etc.)

Site Address: The Mulberry House School, 68 Shoot Up Hill London NW2 3XL

Planning application reference: Pre-App Ref: 2015/3390/P

Type of CMP – Draft Plan – to be updated upon the appointment of a main contractor and prior to the commencement of any relevant works on site.

Q2. Please provide contact details for the person responsible for submitting the CMP

Name: Professor Doug Clelland, Chartered Architect: AADipl, Dipl. –ing(Arch) RIAS, RIBA

Address: c/o The Mulberry House School, 68 Shoot Up Hill London NW2 3XL

Tel: 07899 66 5252

Email: profd@clelland.org.uk

Q3. Please provide the registered contact address details for the main contractor responsible for undertaking the works.

Name: Not yet appointed. Interviews being held in September 2015.

Address:

Tel:

Email:

Q4. Please provide full contact details of the site and project manager responsible for day-to-day management of the works.

Name: Not yet defined pending the appointment of a Main Contractor

Address:

Tel:

Email:

Q5. Please provide full contact details of the person responsible for dealing with any complaints from local residents and businesses, etc. In the case of [Community Investment Programme \(CIP\)](#), please provide contact details of the responsible Camden officer.

Name: Not yet appointed
Address
Tel:
Email:

Name: Zenab Haji-Ismail, BSc (Hons) MSc
Address:
SeniorPlanningOfficer
RegenerationandPlanning
CultureandEnvironment
London Borough of Camden

Tel: 020 7974 3270

Email: Zenab.Haji-Ismail@camden.gov.uk

Q6. Please provide full contact details of the person responsible for community liaison if different to above.

Name: Julie Kirwan

Address: Headteacher, The Mulberry House School, 7 Minster Lane, London NW2 3SD

Tel: 020 8452 7340

Email: head@mulberryhouseschool.com

Q7. Please provide full contact details including the address where the main contractor accepts receipt of legal documents for the person responsible for the implementation of the CMP.

Name: Not yet appointed

Address: When appointed - 68 Shoot Up Hill London, NW2 3XL

Tel: tba

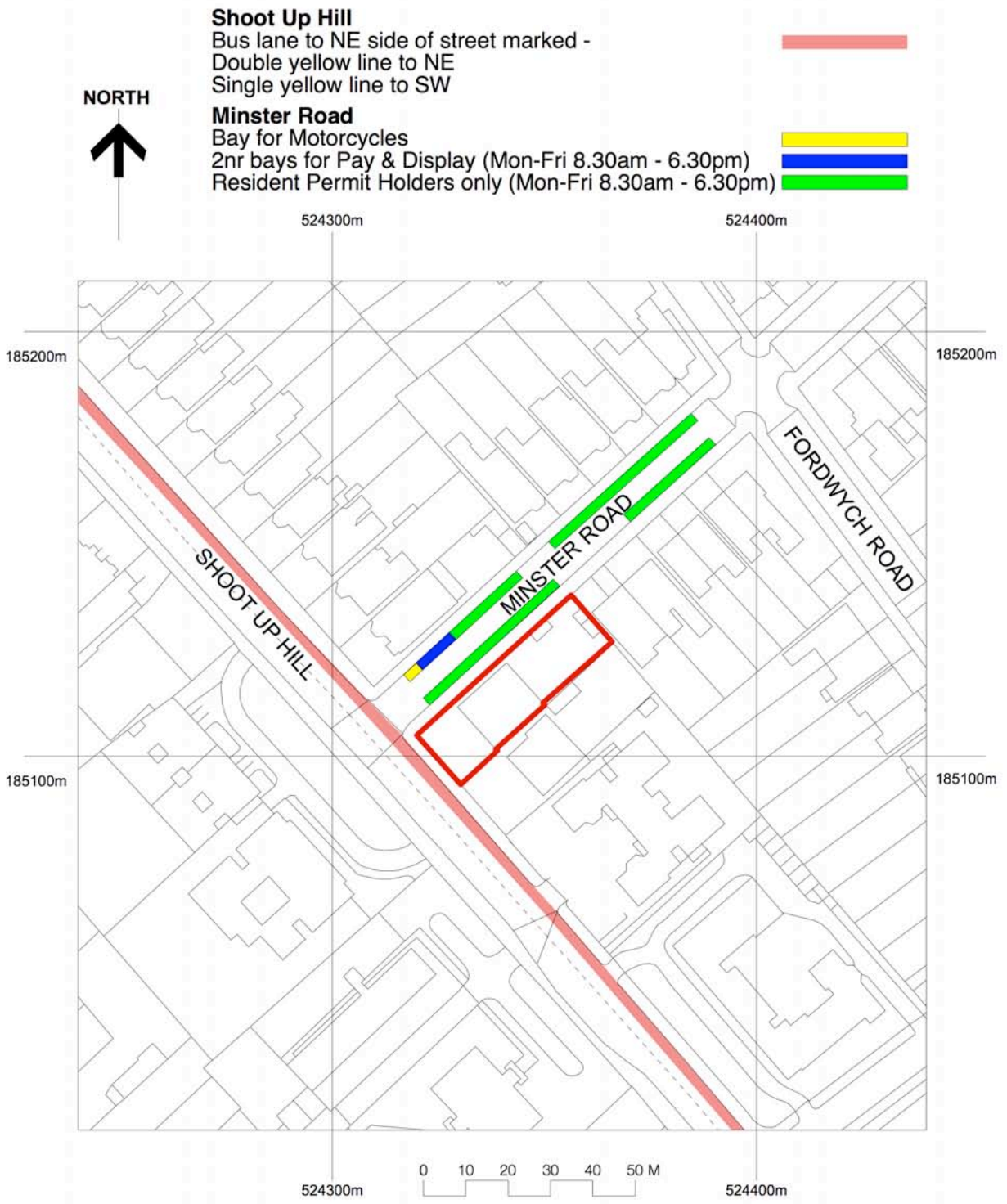
Email: tba

Section 2 – About the Site

Q8. Please provide a site location plan and a brief description of the site, surrounding area and development proposals for which the CMP applies.

The site is a Primary School located at the junction of Minster Road and Shoot Up Hill. This is a mainly residential area with a trunk road running through it. It is intended to add a new classroom/hall over the existing building with a spiral access stair and a covered roof play ground.





**Location Plan : The Mulberry House School : 68 Shoot Up Hill, London,
 Scale 1:1250**

Q9. Please provide a very brief description of the construction works including the size and nature of the development and details of the main issues and challenges (e.g. narrow streets, close proximity

to residential dwellings).

The Construction work comprises the erection of additional classroom/Hall/playground space as an independent single storey structure spanning the existing two-storey school together with an access staircase / lift linked to each floor. This will have piled foundations; steel columns; structural steel walls floors and roof with a canopy. Cladding will be either of glass, aluminium panels and photovoltaic panels.

The works will take place in three relatively brief periods when the school is closed for holidays.

Phase one will consist of four sets of four 300mm drilled – not driven – piles and pile caps. This will take place before and after Christmas 2015 and New Year 2015-2016.

Main issues and challenges: suspending of parking bays / craning of piling rigs onto the site with least damage to existing site walls and fences / removal of spoil after the boring / craning of poured concrete from delivery lorries etc. None of these issues and challenges are abnormal or inherently dangerous.

There will then be no further construction for approximately three months.

Phase two will consist of the assembly of the main structural frames, floor and roof panels, which will be manufactured off site and craned into place. This will take place before and after Easter 2016.

Main issues and challenges: suspending of parking bays / parking of delivery lorries / craning of the main four trusses and bolting / craning of floor and roof and frames to playground roof / craning of playground roof etc. None of these issues and challenges are abnormal or inherently dangerous in the context of Minster Road – a broad sub-urban street.

There will then be no significant construction for approximately three months.

Phase three will consist of the external cladding of the single-storey building around its perimeter at second floor level, and completion of the play roof and the internal fit out. This will take place during July and August 2016.

Main issues and challenges: speed of work, erection of scaffold / or temporary safe works access platforms. No issues or challenges are abnormal or inherently dangerous.

Summary: There will in all likelihood be the need to temporarily arrange for the use of some space from the width of Minster Road. As this is a wide road, such working space amendments should not result in alterations to the existing two-way flow of traffic. Footpath amendments and other alterations will be agreed with Camden at the appropriate times.

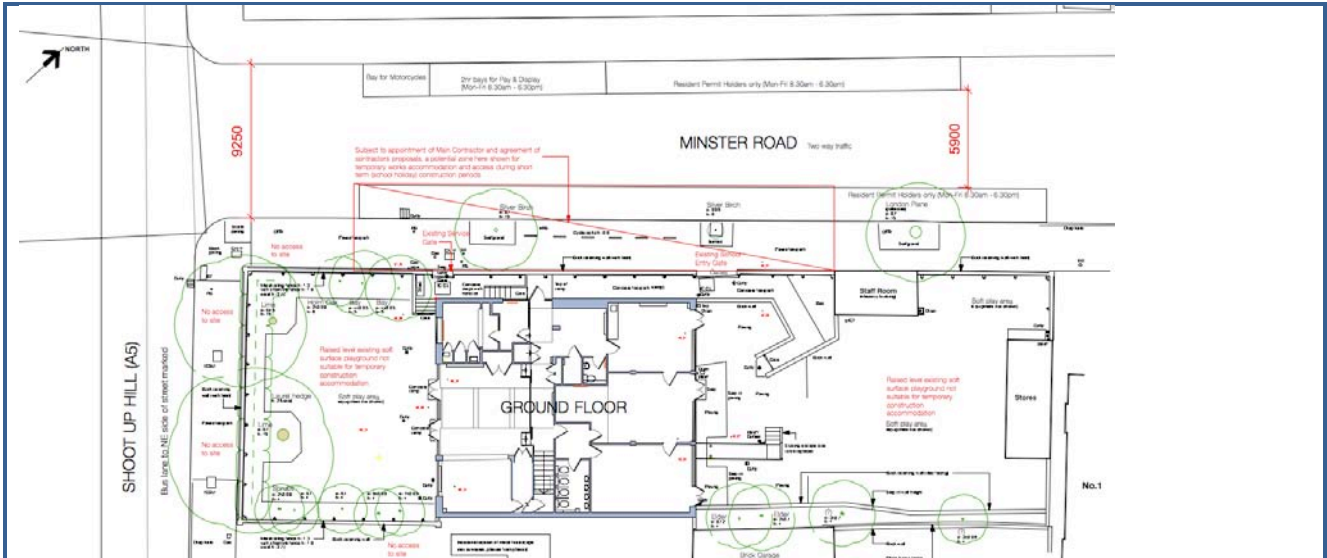
There will also in all likelihood be the need for late working and weekend working in order to meet the deadlines set by the periods of closure possible for the school. As this will be restricted to the limited periods identified above, there will be ways assessed to minimise impact on neighbours.

Q10. Please identify the nearest potential receptors (dwellings, business, etc.) likely to be affected by the activities on site (i.e. noise, vibration, dust, fumes, lighting, etc.).

Premises at 66 Shoot Up Hill London NW2 3XL; flats at 64 Shoot Up Hill; Dwellings; Nr 1 Minster Road and on the opposite side of Minster Road.

At time of writing this Draft CMP, the main activities that will affect neighbours will be noise and lighting and ways will be found to mitigate these.

Q11. Please provide a scaled plan detailing the local highway network layout in the vicinity of the site. This should include details of on-street parking bay locations, cycle lanes, footway extents and proposed site access locations.



Q13. Please confirm the standard working hours for this site, noting that the standard working hours for construction sites in Camden are as follows:

- 8.00am to 6pm on Monday to Friday
- 8.00am to 1.00pm on Saturdays
- No working on Sundays or Public Holidays

See answers to Q9

Provisional dates:

From 14 Dec 2015 – 06 Jan 2016 – Substructure works.

From 14 Mar 2016 – 07 Apr 2016 – Superstructure works.

From 13 Jul 2016 – 31 Aug 2016 – Cladding completion / Fitting out works.

These dates to coincide with school holidays when the school is vacated and there is minimum school traffic..

Q12. Please provide the proposed start and end dates for each phase of construction as well as an overall programme timescale. (A Gantt chart with key tasks, durations and milestones would be useful).

8.00am to 6pm on Monday to Friday

8.00am to 6.00pm on Saturdays

No working on Sundays or Public Holidays

except in emergency

Q14. Please indicate if any changes to services are proposed to be carried out that would be linked to the site during the works (i.e. connections to public utilities and/or statutory undertakers' plant). Larger developments may require new utility services. If so, a strategy and programme for coordinating the connection of services will be required. If new utility services are required, please confirm which utility companies have been contacted (e.g. Thames Water, National Grid, EDF Energy, BT. etc.) You must explore options for the utility companies to share the same excavations and traffic management proposals. Please supply details of your discussions.

None planned

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Q15. Please confirm when an asbestos survey was carried out at the site and include the key findings.

A full intrusive survey was carried out in August 2015. No asbestos was found.

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Section 3 – Transportation Issues Associated with the Site

Q16. Please provide a brief description of the proposed working hours within which vehicles will service the site during the construction period (Refer to the [Guide for Contractors Working in Camden](#)). Construction vehicle movements are generally acceptable between 9.30am to 4.30pm on weekdays and between 8.00am and 1.00pm on Saturdays). If there is a school in the vicinity of the site or on the proposed access and/or egress routes, then deliveries must be restricted to between 9.30am and 3pm on weekdays during term time. Construction vehicles must be managed and prevented from causing obstructions to the highway.

In general the hours in which vehicles will arrive and depart will coincide with site hours which are 8.00am to 6.00pm in the evening. However there will be occasions when heavy/wide loads will need to be delivered and removed from site outside of these hours. Such deliveries would be for piling rigs and steelwork and glazing erection and experienced site staff would be in attendance at all times. On such occasions the local neighbours will be notified some 2 weeks earlier via a Newsletter. Any parking bays that may need to be closed off will be carried out by the Contractor with full consultation with Camden/appropriate neighbours.

Q17. Please provide details of the typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction. You should estimate the average daily number of vehicles during each major phase of the work, including their dwell time at the site. High numbers of vehicles per day and/or long dwell times may require vehicle holding procedures. You will need to consider whether the roads on the route(s) to and from the site are suitable for the size of vehicles to be used. Please provide details of other known developments in the local area or on the route.

Numerous types of delivery vehicles will be used to bring materials to and from the site.

These include:

- Skip lorries. These will include roll on/roll off skips for minor demolition works/ excavations (approx size 7.5m long and 2.4m wide) and standard 8 yard skips for waste (approx size 7m long and 2.4m wide).

- Ready mix concrete lorries. (approx size 8.25m long and 2.45m wide); only for the minimal piling and ground works.
 - Flat bed delivery vehicles for the delivery of various materials including scaffolding, steelwork and general materials - timber, roofing materials, joinery etc. (approx size 8.5m long and 2.45m wide).
 - Articulated Lorries, for delivery of steel trusses and other cladding components.
- The projected vehicle movements are approximately 5 per day during the first phase and 10 – 15 per day during the second phase. Third phase movements will involve smaller vehicles.

Q18. Please provide details of any temporary structures which would overhang the public highway (e.g. scaffolding, gantries, cranes etc.)

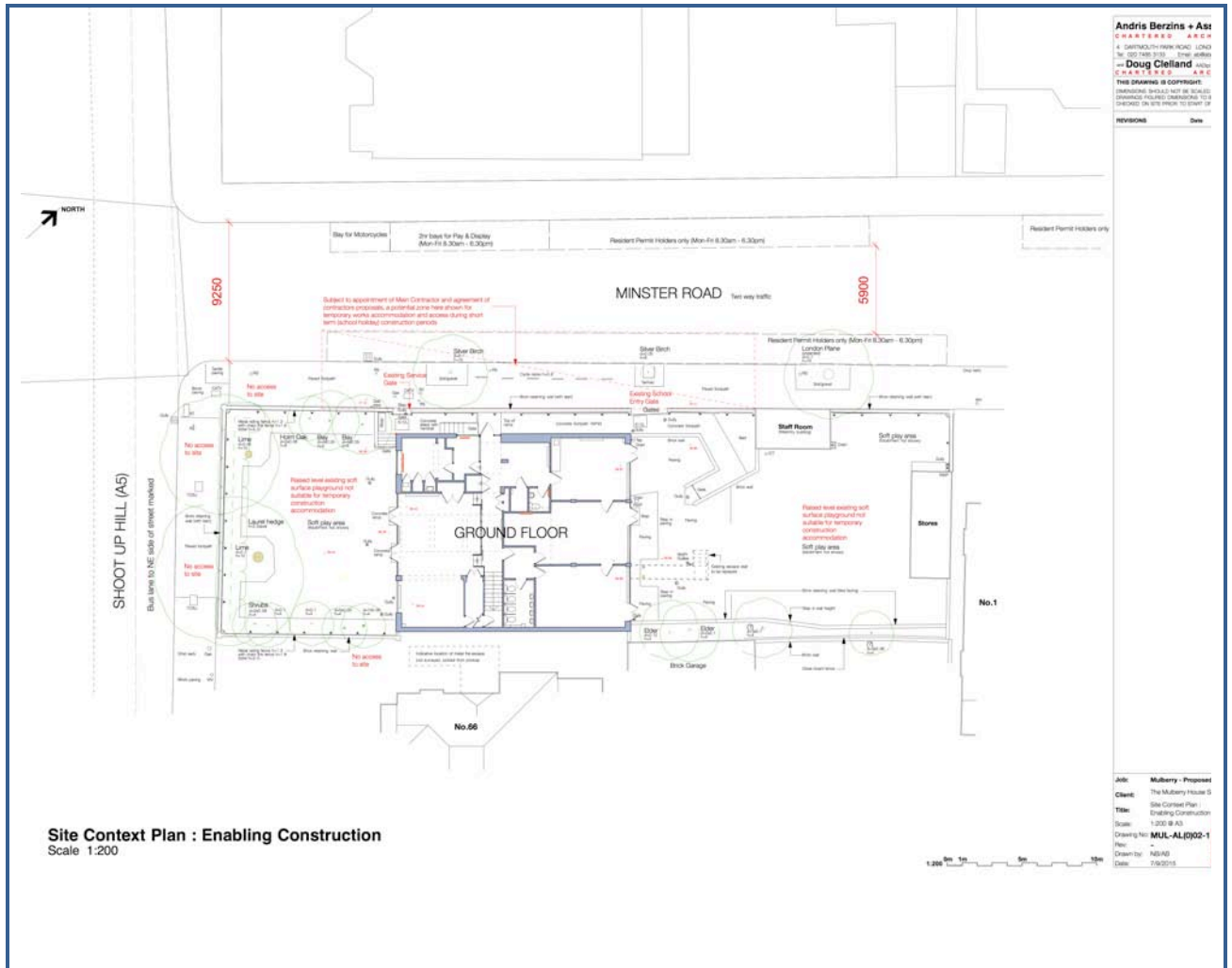
Scaffolding will be erected around the existing school and whilst it will be erected within the school boundaries it will overhang the footpath in Minster Road at second floor and above. Subject to agreement with Camden Highways Management – and once confirmed by the appointed contractor – there would in all likelihood be a desire to erect a scaffold gantry over the footpath to facilitate a safe method of moving materials.

Q19. Please provide details of hoarding requirements or any other occupation of the public highway.

The scaffolding on Minster Road will be enclosed with a hoarding and the contractor will be requested to ensure that a minimum of 1.3m of clear footpath remains open to the public. Any contractors compound, will be enclosed with a 2.4m high timber hoarding. This will be adapted as necessary, and will be painted as agreed with SCCS/LBC. Vision panels will be provided in the hoardings. Hoarding panels will be maintained and kept clean for the duration of the project.

Q20. Please provide accurate scaled drawings of any highway works necessary to enable construction to take place (e.g. construction of temporary vehicular accesses). Use of the public highway for storage, site accommodation or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose such use you must supply full justification, setting out why it is impossible to allocate space on-site. You must submit a detailed (to-scale) plan showing the impact on the public highway including; the extent of hoarding, pedestrian routes, parking bay suspensions and remaining road width for vehicle movements. We prefer not to close footways but if this is unavoidable, you should submit a scaled plan of the proposed diversion route showing key dimensions. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc.

Refer to the 1:200 scale site plan attached.
Temporary use of the Public Highway for delivery vehicles.
Further details to be advised once the contractor is appointed.



Q21. Please provide details of any proposed parking bay suspensions and temporary traffic management orders which would be required to facilitate construction. If construction vehicles cannot access the site, details are required on where they will wait to load/unload.

Suspension of resident parking bays will be kept to an absolute minimum. (Meeting will be required with LBC to discuss). During works carried out within the school during August 2015, the contractor for these works Bolt & Heeks agreed all necessary temporary suspensions with Camden.

Q22. Please provide details describing how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Banksman and/or Traffic Marshall arrangements. You should supply details of any diversion, disruption or other anticipated use of the public highway during the construction period (alternatively a plan may be submitted). Vulnerable footway users include wheelchair users, the elderly, people with walking difficulties, young children,

people with prams, blind and partially sighted people, etc. A secure hoarding will generally be required to the site boundary with a lockable access. Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage should be used on temporary structures/ skips/ hoardings, etc. Appropriate ramping must be used if cables, hoses, etc. are run across the footway.

There is no public access to the site during construction. Where vehicles are unloading on Minster Road this will be supervised by either road marshalls or crane banksman.

The general public/pedestrians will have right of way along the pathways that surround the site.

When deliveries are made to the site barriers will be put across the pavement to prevent access by pedestrians.

These barriers will be manned by site security personnel. All delivery vehicles will be supervised/controlled by a banksman. When unloading via mobile cranes in Minster Road the footpath will be closed off and pedestrians will be requested to use the pavement on the opposite side of the road.

The external perimeter of the site will be regularly patrolled (twice a day) to ensure that any debris is kept clear of the pavements.

With regard to cyclist safety any delivery vehicle parked within the loading area will be coned off to direct the cyclist around the lorry.

Work above ground floor level may require a covered walkway adjacent to the site; a licence for scaffolding and gantries. The adjoining public highway will be kept clean and free from obstructions. Lighting and signage will be provided on temporary structures/ skips/ hoardings, etc. Appropriate ramping will be used when cables, hoses, etc. are run across the footway.

- Q23. Please detail the proposed access and egress routes to and from the site, showing details of links to the [Transport for London Road Network \(TLRN\)](#). Such routes should be indicated on a drawing or diagram showing the public highway network in the vicinity of the site. Consideration should also be given to weight restrictions, low bridges and cumulative impacts of construction (including neighbouring construction sites) on the public highway network. Consideration should be given to any major trip generators (e.g. schools, offices, public buildings, museums, etc.) on the route, and how any problems can be avoided or mitigated.**

Details of agreed access/egress routes will be issued to all suppliers and subcontractors. This will be policed as far as practical but it must be recognised that we have no jurisdiction over the vehicles once they have left the site. Given the location of the site and the nature of its adjacent and surrounding network, there should not be any significant issues.

- Q24. Please describe how the access and egress arrangements for construction vehicles will be managed. Confirm how contractors, delivery companies and visitors will be made aware of the route (to and from the site) and of any on-site restrictions, prior to undertaking journeys.**

Deliveries to the site will be booked in using a Delivery Schedule so as to prevent lorry congestion to the road network that surrounds the site. Should a lorry/vehicle arrive that has not been booked in, that lorry will be turned away. In order to reduce traffic movements, full loads will be called off whenever possible; part loads will only be accepted when essential.

Sub contractors will be encouraged to use public transport to travel to site. Potential subcontractors will be informed that parking is very restricted in the local area and that residents parking bays are not to be used. The contractor will be required to monitor parking, especially on neighbouring roads, to ensure off site parking is dealt with considerately.

- Q25. Please provide details of the parking and loading arrangements for construction vehicles with regard to servicing and deliveries associated with the site (e.g. delivery of materials and plant, removal of excavated material). This is required as a scaled site plan, showing all points of access and where materials, skips and plant will be stored, and how vehicles will access and egress the site.**

Materials will be stored within the boundary of the site.

No parking will be permitted – or is available - on site and all sub contractors will be informed at the pre-order meeting that the surrounding area is for resident parking only. All subcontractors will be encouraged to use public transport. Materials will be stored within the boundary of the site.

- Q26. Please provide swept path drawings for any tight manoeuvres on vehicle routes to and from the site including proposed access and egress arrangements at the site boundary (if necessary).**

Vehicles will not enter this small site, as the site is entirely walled and fenced.

Section 5 – Environmental Issues

To answer these sections please refer to the relevant sections of **Camden's Minimum Standards for Building Construction** ([CMRBC](#)).

- Q27. Please provide details of the times of [noisy operations](#), outlining how the construction works are to be carried out.**

Noisy operations will be restricted to 9.30am - 4.30pm on weekdays
Excavation and Piling operations will be restricted to 14 Dec – 06 Jan 2016
Hoisting steelwork and glazing will be carried out between 9.30am – 4.30pm 14 Mar 2016 – 07 Apr 2016.

Any alterations to these planned times will be considered once the contractor is appointed and will be contained in the final CMP when submitted to Camden.

Q28. Please confirm when the most recent noise survey was carried out (before any works were carried out) and provide a copy. If a noise survey has not taken place please indicate the date (before any works are being carried out) that the noise survey will be taking place, and agree to provide a copy.

August 2015. Copy appended.

Q29. Please provide predictions for [noise](#) and vibration levels throughout the proposed works.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q30. Please provide details describing mitigation measures to be incorporated during the construction/[demolition](#) works to prevent noise and vibration disturbances from the activities on the site, including the actions to be taken in cases where these exceed the predicted levels.

Augured piling to be utilised to minimise vibration.

Q31. Please provide evidence that staff have been trained on BS 5228:2009

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q32. Please provide details on how dust nuisance arising from dusty activities, on site, will be prevented.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q33. Please provide details describing how any significant amounts of dirt or dust that may be spread onto the public highway will be prevented and/or cleaned.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q34. Please provide details describing arrangements for monitoring of [noise](#), vibration and dust levels.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q35. e confirm that a [Risk Assessment](#) has been undertaken in line with the [GLA's Control of Dust and Emissions Supplementary Planning Guidance \(SPG\)](#), and the risk level that has been identified, with evidence.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q36. Please confirm that all relevant mitigation measures from the [SPG](#) will be delivered onsite.

Confirmed.

Q37. If the site is a High Risk Site, 4 real time dust monitors will be required, as detailed in the [SPG](#). Please confirm that these monitors will be installed 3 months prior to the commencement of works, and that real time data and quarterly reports will be provided to the Council detailing any exceedances of the threshold and measures that were implemented to address these.

The site is not a high-risk site.

Q38. Please provide details about how rodents, including [rats](#), will be prevented from spreading out from the site. You are required to provide information about site inspections carried out and copies of receipts (if work undertaken).

School rodents are caged. Others are discouraged.

(Refer to [Tfl best practice guidance](#) and [\(CMRBC\)](#) sections: [noise operations](#), abatement techniques, noise levels, vibration levels, [dust levels](#), rodent control, community liaison, etc.)

Q39. Please provide details describing how traffic associated with the development will be managed in order to reduce/minimise traffic congestion. Deliveries should be given set times to arrive, dwell and depart. Delivery instructions should be sent to all suppliers and contractors. Trained site staff must assist when delivery vehicles are accessing the site, or parking on the public highway adjacent to the site. Banksmen must ensure the safe passage of pedestrians, cyclists and motor vehicular traffic in the street when vehicles are being loaded or unloaded. Vehicles should not wait or circulate on the public highway. An appropriate location outside the borough may need to be identified, particularly if a large number of delivery vehicles are expected.

Deliveries will be given set times to arrive, dwell and depart. Delivery instructions will be sent to all suppliers and contractors. Trained site staff will assist when delivery vehicles are accessing the site, or parking on the public highway adjacent to the site. Deliveries will be given set times to arrive, dwell and depart. Delivery instructions will be sent to all suppliers and contractors. Trained site staff will assist when delivery vehicles are accessing the site, or parking on the public highway adjacent to the site.

Q40. Please provide details of any other measures designed to reduce the impact of associated traffic (such as the use of [construction material consolidation centres](#)).

None envisaged on this small site.

Q41. Please provide details of consultation on a draft CMP with local residents, businesses, local groups (e.g. residents/tenants and business associations) and Ward Councillors. Details should include who was consulted, how the consultation was conducted and a summary of the comments received in response to the consultation. In response to the comments received, the CMP should then be amended where appropriate and where not appropriate a reason should be given. The revised CMP should also include a list of all the comments received. Developers are advised to check proposed approaches to consultation with the Council before carrying it out.

Consultation on the project as a whole is to be held on 09 September 2015 at the school from 16.00 until 20.00 involving neighbours, parents, children and staff.

Specific consultation on the CMP will follow and details will be included in the final CMP.

Q42. Please provide details of community liaison proposals including any Construction Working Group that will be set up, addressing the concerns of the community affected by the works. Please confirm how the contact details of the person responsible for community liaison will be advertised to the local community and how the community will be updated on the upcoming works i.e. in the form of a newsletter/ letter drop, or weekly drop in sessions for residents.

Consultation on the project as a whole is to be held on 09 September 2015.

Details concerning the specifics of this question will follow.

Q43. Please provide details of any schemes such as the 'Considerate Constructors Scheme', the 'Freight Operators Recognition Scheme' or 'TfLs Standard for construction logistics and cyclist safety – [CLOCS scheme](#)' that the project will be signed up to. Note, the [CLOCS standard](#) should be adhered to and detailed in response to question 46. Such details should form part of the consultation and be notified to the Council. Contractors will also be required to follow the "[Guide for Contractors Working in Camden](#)" also referred to as "[Camden's Considerate Constructors Manual](#)".

Anticipate Considerate Constructors Scheme will be implemented. This will be a requirement when the contractors are interviewed and one chosen in September 2015. Contractors will also be required to follow the [Guide for Contractors Working in Camden](#)

Q44. Complaints often arise from the conduct of builders in an area. Please confirm steps being taken to minimise this e.g. provision of suitable smoking area, tackling bad language and unnecessary shouting.

To be advised as part of the final CMP to be submitted to Camden in advance of any construction commencing.

Q45. Please provide a plan of existing or anticipated construction sites in the local area and please state how your CMP takes into consideration and mitigates the cumulative impacts of construction in the vicinity of the site.

Not exactly known. Initial research suggests that there will be limited / no significant construction works being undertaken at the time of the three phases described above for The Mulberry House School works.

Q46. Please provide details to confirm that all contractors and sub-contractors operating large vehicles over 3.5 tonnes will meet all of the following conditions, as outlined in the [CLOCS Standard](#)

OPERATIONS:

- **Quality operation:** accreditation via an approved fleet management audit scheme e.g. [Fleet Operator Recognition Scheme \(FORS\)](#) or equivalent.
- **Collision reporting and analysis:** of any collision involving injury to persons, vehicles or property, ideally including use of the [CLOCS](#) Manager collision reporting tool.
- **Traffic routing:** any route specified by the client is adhered to unless otherwise specified.

i. VEHICLES:

- **Warning signage:** warning cyclists of the dangers of passing the vehicle on the inside
- **Side under-run protection:** fitted to all vehicles over 3.5 tonnes which are currently exempt

- **Blind spot minimisation:** front, side and rear blind-spots completely eliminated or minimised as far as is practical and possible
- **Vehicle manoeuvring warnings:** enhanced audible means to warn other road users of a vehicle's left hand turn or other manoeuvres

ii. **DRIVERS:**

- **Training and development:** approved progressive training and continued progressive training especially around vulnerable road users (including for drivers excluded from Certificate of Professional Competence requirements)
- **Driver licensing:** regular checks and monitoring of driver endorsements and that drivers hold the correct licence for the correct vehicle

STANDARD FOR CONSTRUCTION CLIENTS

- **Construction logistics/management plan:** is in place and fully complied with – as per this document.
- **Suitability of site for vehicles fitted with safety equipment:** that the site is suitably prepared for vehicles fitted with safety equipment to drive across.
- **Site access and egress:** should be carefully managed, signposted, understood and be clear of obstacles.
- **Vehicle loading and unloading:** vehicles should be loaded and unloaded on-site as far as is practicable.
- **Traffic routing:** should be carefully considered, risk assessed and communicated to all contractors and drivers.
- **Control of site traffic, particularly at peak hours:** other options should be considered to plan and control traffic, to reduce traffic at peak hours.
- **Supply chain compliance:** contractors and sub-contractors throughout the supply chain should comply with requirements 3.1.1 to 3.3.2.

Contractor interviews to be held on 22 September 2015 will require compliance.

Q47. Please provide details of any other relevant information with regard to traffic and transport (if appropriate).

Not relevant for this project.

The agreed contents of this Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the development. Any future revised plan must be approved by the Council and complied with thereafter.

It should be noted that any agreed Construction Management Plan does not prejudice further agreements that may be required such as road closures or hoarding licences.

Signed: Doug Clelland **Date:** 12 September 2015

Print Name: Professor Doug Clelland **Position:** Principal Design Architect and CDM Principal Designer

Submit: planningobligations@camden.gov.uk

End of form