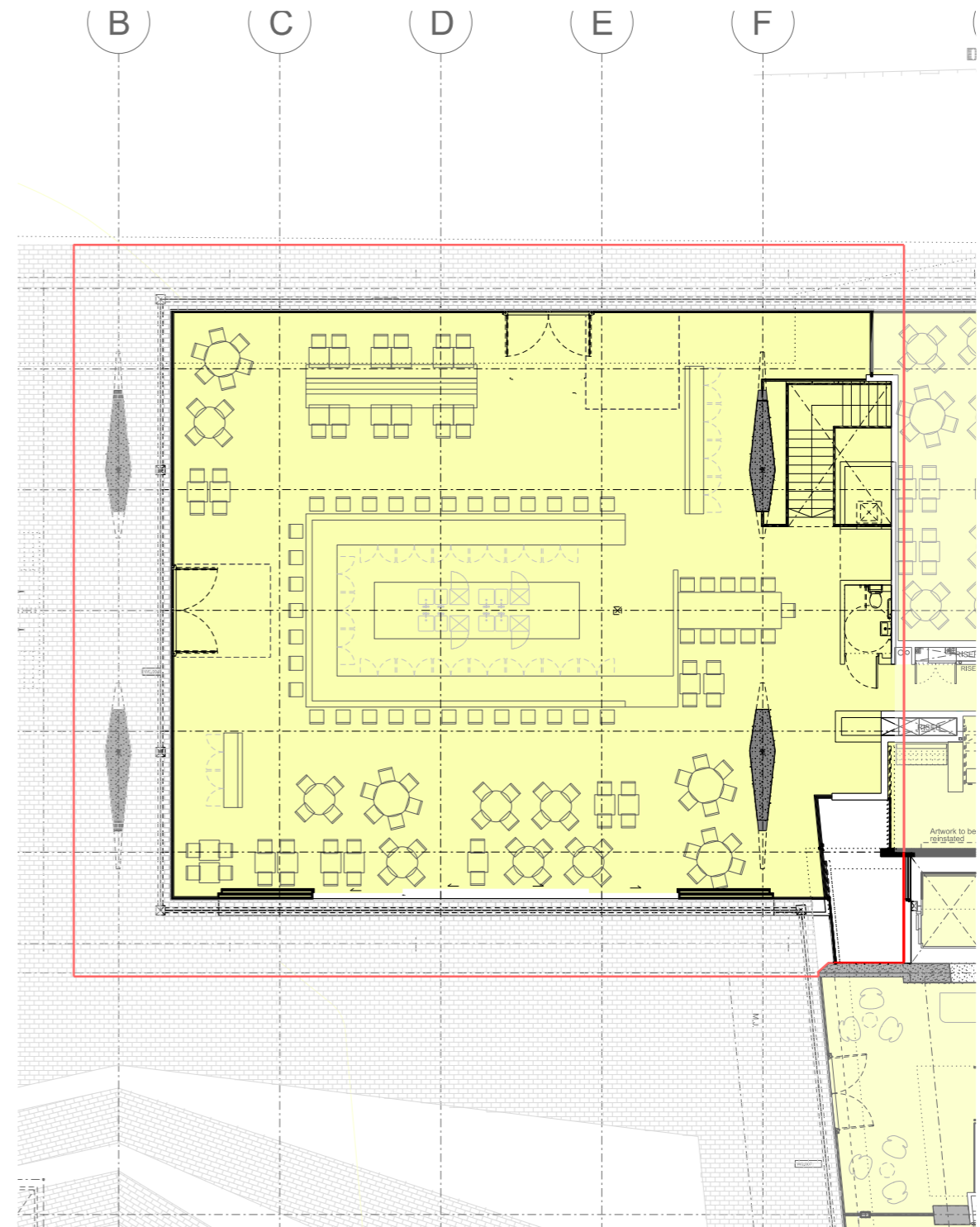


Proposed Unit Plan - Glazing Closed



Proposed Unit Plan - Glazing Opened

5.22 UNIT ACCESS + LAYOUT

Entrances

The current design assumes three potential entrances:

- Entrance from New Oxford Street
- Entrance from midway along pedestrian passageway, symmetrical between the existing columns
- Retractable glass on southern facade to public space.

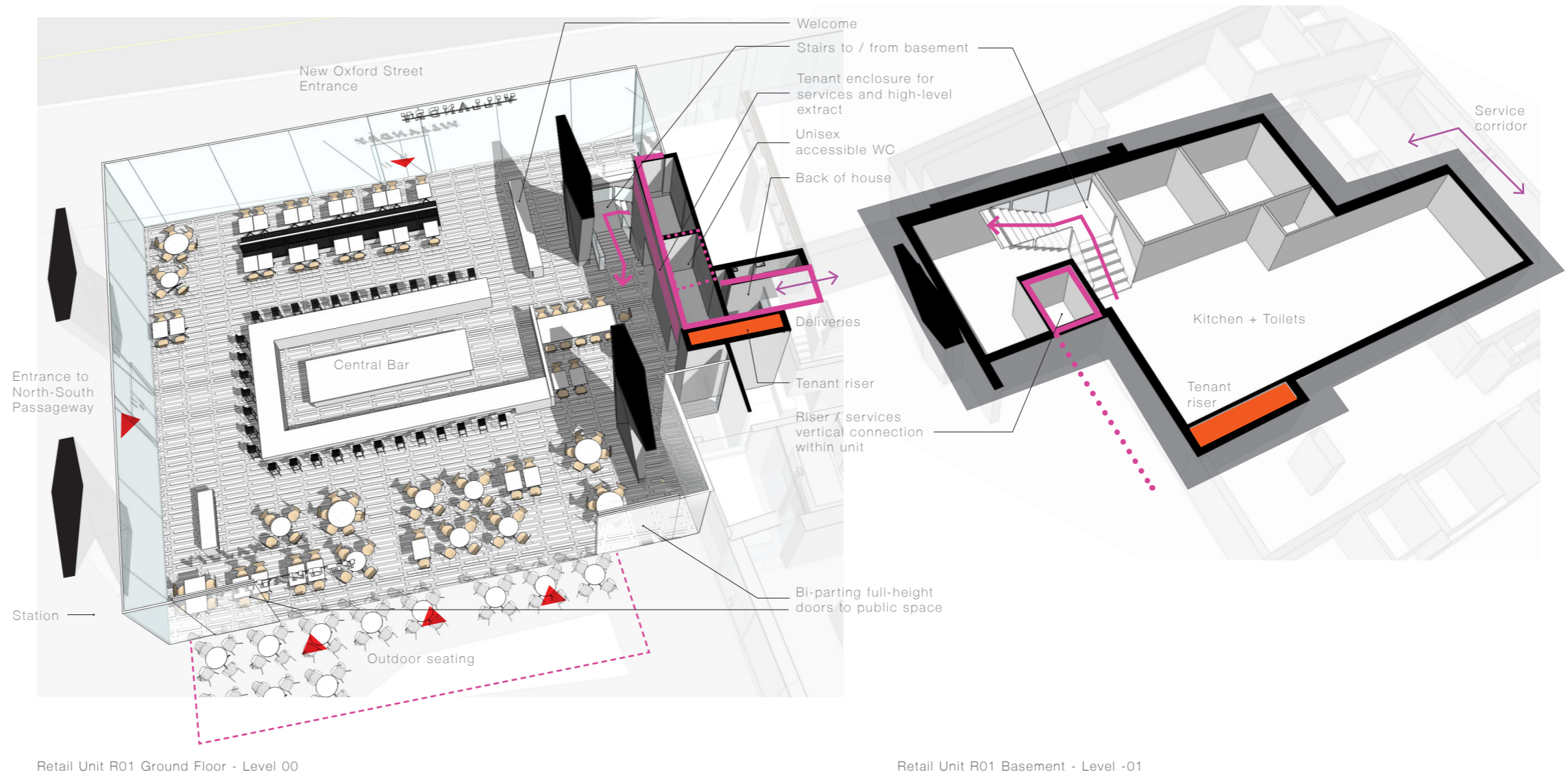
Principles

The unit is enclosed on three sides by full height glazing that lightly touch the underside of the bridge. The remaining fourth side (east) is a service wall that provides the main zone of tenant utility connections, ventilation and vertical connections to the demised basement area. The width of this zone is limited so as to ensure that the solid enclosure does not affect the transparency of the unit seen north-south and allows the sculptural designated listed columns to be read as freestanding objects within the space.

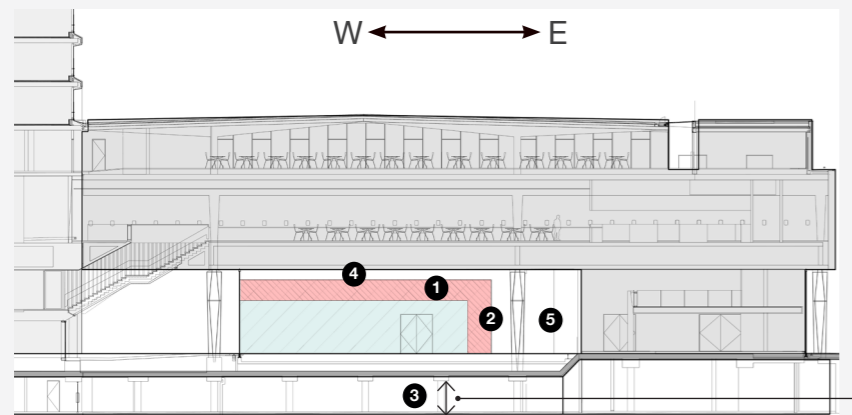
The zone is also sized for a unisex accessible WC, with remaining toilet provision to be met at basement level.

A new staircase connects to a basement area for ancillary / kitchen use.

Within the main floor area, enclosures and division of the space are envisaged to be limited to counter height to maintain sight lines through the unit, from the square to New Oxford Street.



5.23 - REASON FOR STAIR LOCATION



ELEVATION OF R01 -
Red = Service Zone
Blue = Clear View Through

DIAGRAMMATIC ELEVATION SHOWING EVOLUTION OF SERVICING STRATEGY

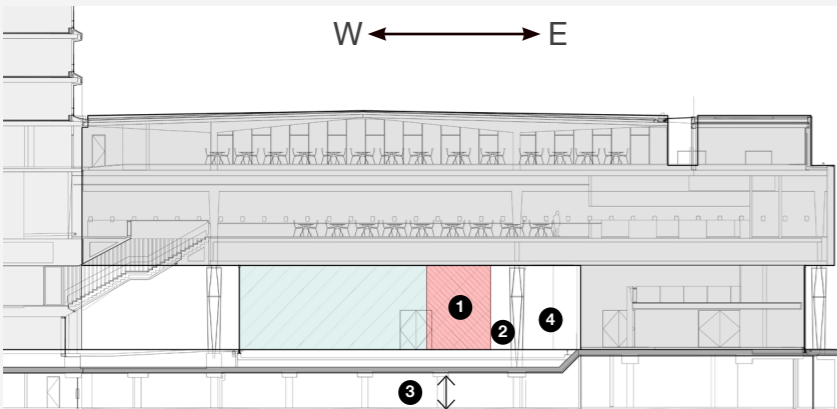
SERVICING STRATEGY - FIRST ITERATION: DISTINCT 'INSTALLATION' BELOW BRIDGE

UNSUITABLE DUE TO:

- 1 Space required for bulk of services has visual impact
- 2 View of eastern piloti blocked from interior of unit and western passage
- 3 Basement height too low to enable servicing from below
- 4 Unsightly dead space above unit servicing zone - maintenance and pest management issues
- 5 Resultant space will likely create a zone conducive to anti-social behaviour.

NOTE:

The information on this page has been already issued as part of Application 2. It is included here, alongside additional clarification, in order to restate the argument for the location & inclusion of the R01 stair.



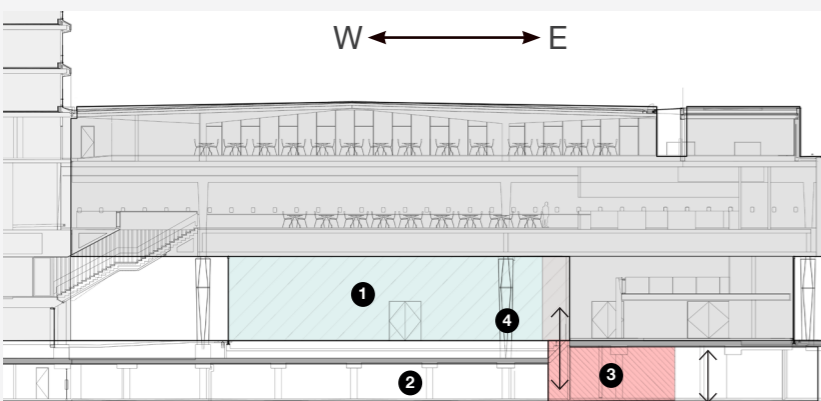
ELEVATION OF R01 -
Red = Service Zone
Blue = Clear View Through

DIAGRAMMATIC ELEVATION SHOWING EVOLUTION OF SERVICING STRATEGY

SERVICING STRATEGY - SECOND ITERATION: ABOVE GROUND SERVICING ZONE

UNSUITABLE DUE TO:

- 1 Space required for bulk of services has visual impact
- 2 View of eastern piloti blocked from interior of unit and western passage
- 3 Basement height too low to enable servicing from below
- 4 Resultant space will likely create a zone conducive to anti-social behaviour.



ELEVATION OF R01 -
Red = Service Zone
Blue = Clear View Through

DIAGRAMMATIC ELEVATION SHOWING EVOLUTION OF SERVICING STRATEGY

SERVICING STRATEGY - PROPOSED ITERATION: ABOVE GROUND SERVICING ZONE

SUITABLE DUE TO:

- 1 Clear view through unobstructed by services
Due to restrictions in headroom immediately below the proposed retail the back of house area is offset within the section and connected at the eastern edge of the unit.
- 2 Adequate space for back of house and services
- 3 Clear views through to piloti without creating an awkward space conducive to anti-social behaviour

SUMMARY

The proposed design iteration for R01 requires a stair in order to facilitate the location of back of house program and a servicing zone below ground.

The other design iterations considered R01 servicing proposals without a stair and, therefore, without basement BOH.

These iterations would increase visual obstruction of the piloti (despite it being outside the R01 envelope) and also, as an additional loss, cause a degree of harm to the appreciation of the link bridge as a whole due to a reduction in transparency of the infill and creation of awkward, dark spaces conducive to anti-social behaviour.

The stair is located at the eastern end of the R01 unit both in order to maximise clear views through but also due to restrictions in head height directly below R01 (as can be seen from the diagrams on this page).

In order to facilitate a design iteration that provides the maximum transparency through the unit, the stair should be located at the eastern end of R01. This decision has been made in order to facilitate the best outcome for appreciating both the sculptural piloti and soffit of the link bridge for both R01 users and from the public realm.