



**STABLES MARKET
LONG STABLE**

**DESIGN AND ACCESS
AND
HERITAGE STATEMENT**

**for Roof Works, Mechanical & Electrical Works and new
toilets**

August 2015

**Prepared for
Stanley Sidings**

by



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Stables Market: Long Stable – Design and Access and Heritage Statement

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1 INTRODUCTION

Executive Summary

This Design & Access and Heritage Statement has been prepared as supporting documentation for the Planning and Listed Building Consent application for the replacement of the existing roof covering with slate and the upgrading of the roof, new mechanical and electrical provision and new replacement toilets.

The building is Grade II listed and located within The Stables Market, Chalk Farm Road, London NW1 8AH.

The Long Stable is in the Heritage at Risk Register, which replaced the Buildings at Risk Register on July 8th, 2008. It is registered as Category C in the priority ranking. Priority for action is assessed on a scale of A to F, where 'A' is the highest priority for a site which is deteriorating rapidly with no solution to secure its future, and 'F' is the lowest priority. Buildings in category C are assessed as suffering 'Slow decay; no solution agreed' (English Heritage, HAR 2014, p. XVII). The description of the site in the HAR 2014 reads: *'Important complex of industrial stabling built 1883-1895 for the Camden Goods Yard of the London and North Western Railway. The Local Planning Authority is dealing with Planning and Listed Building Consent applications for the repair and sensitive adaptation of the Tack Room, with a view to works commencing on site in summer 2014. Applications for remedial works to the Provender Store are also expected in summer 2014. The LPA is hoping to work collaboratively with the building owners to avoid serving repairs and enforcement notices'*.

The Design Documents have been prepared by and with the input of the Project Team who are:

Client: Stanley Sidings Limited

Local Authority: London Borough of Camden

Planning Consultants: Gerald Eve

Heritage Architects: Stephen Levrant Heritage Architecture

Mechanical & Electrical Engineers: Optima BES Ltd

1.1 Reference to other documentation

This document should be read in conjunction with:

Information prepared by Stephen Levrant Heritage Architecture Ltd:

- **C-LP:** Location Plan
- **C-Sch REP-01:** Roof Works - existing and proposed details
- **C-Sch REP-02:** Roof Works – fleche details
- **C-Sch REP-03:** Roof Works – existing elevations
- **C-Sch REP-04:** Roof Works – proposed elevations
- **C-Sch REP-05:** Replacement Toilets
- **C-Schedule of Roof Repairs - August 2015**

Information prepared by Optima BES Ltd:

- **AP-297 E100 T2** – Existing services to be removed
- **AP-297 E200 T2** – Proposed electrical services
- **AP-297 E201 T2** – Proposed mechanical services
- **AP-297 E300 T2** – Proposed services on elevations
- **AP-297 /spec/M&E/Tender Issue** – Specification

1.2 Planning Policy Guidance and Legislation

The assessments of the listed buildings and conservation area have been prepared taking into account the information contained in:

- NPPF National Planning Policy Framework, 27 March 2012.
- Planning practice guidance for the National Planning Policy Framework and the planning system. , 12 June 2014.
- Conservation principles, policies and guidance for the sustainable management of the historic environment, EH, April 2008.
- The Setting of Heritage Assets: English Heritage Guidance, October 2011.
- BS 7913:1998 Guide to the Principles of the Conservation of Historic Buildings.
- Understanding Place: Historic Area Assessments in a Planning and Development Context, EH, June 2010;
- Camden Development Policies 2010 – 2025, Local Development Framework
- Local Development Framework - Camden Core Strategy 2010 – 2025

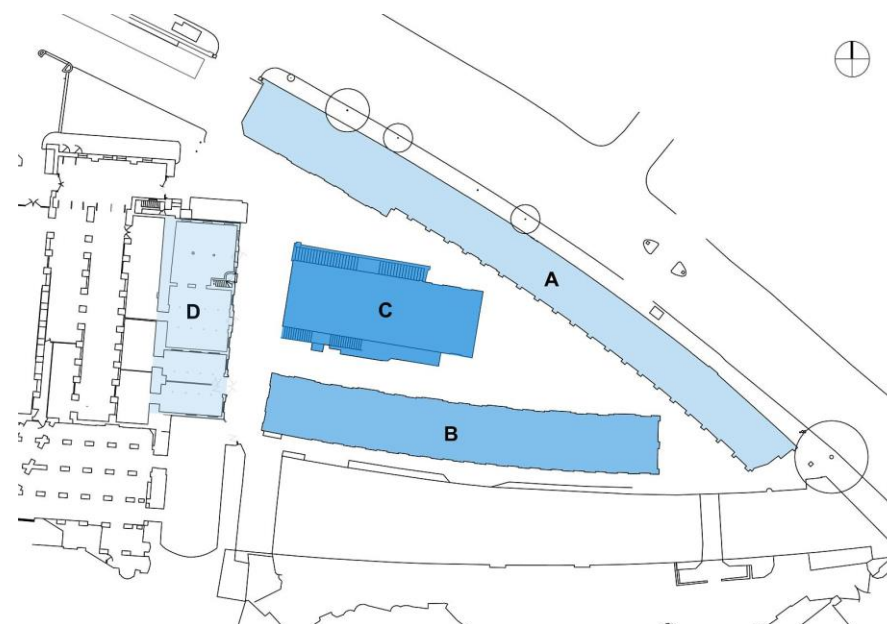


Figure 1 Stables Market - Four western ranges, subject site Building C.

2 CONTEXT

The complex of multi-storey stables was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

'The heart of Stables Market was previously a part of the Camden Goods Yard and a large area was occupied by warehousing for W. A. Gilbey's wines and spirits from the mid 19th century. The remaining buildings on the site comprise the stables for railway horses known as Stanley Sidings [...], a later block of 1883-85, and the surviving bonded

warehouse, *Gilbey's No.2 Bond, built c.1885'* (Regent's Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.17).The Long Stable, subject of this application, was built during the first phase of stabling. These four western ranges, which also include the Tack Room, the Provender Store, and the Chalk Farm Stable, form the triangular group A-D built between 1854 and 1856 as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072):

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright. Unless stated otherwise.

Location

The Long Stable (Building C) lies in the centre of the triangular site described above part of the internationally recognised Stables Market, in the northwest corner of Regents Canal Conservation Area. (Fig. 1). The area is bounded to the north by Chalk Farm Road (Hampstead Road as it was known until 1862), and is separated from the former lands of the of the goods yard to the south by the railway viaducts (Fig. 2).

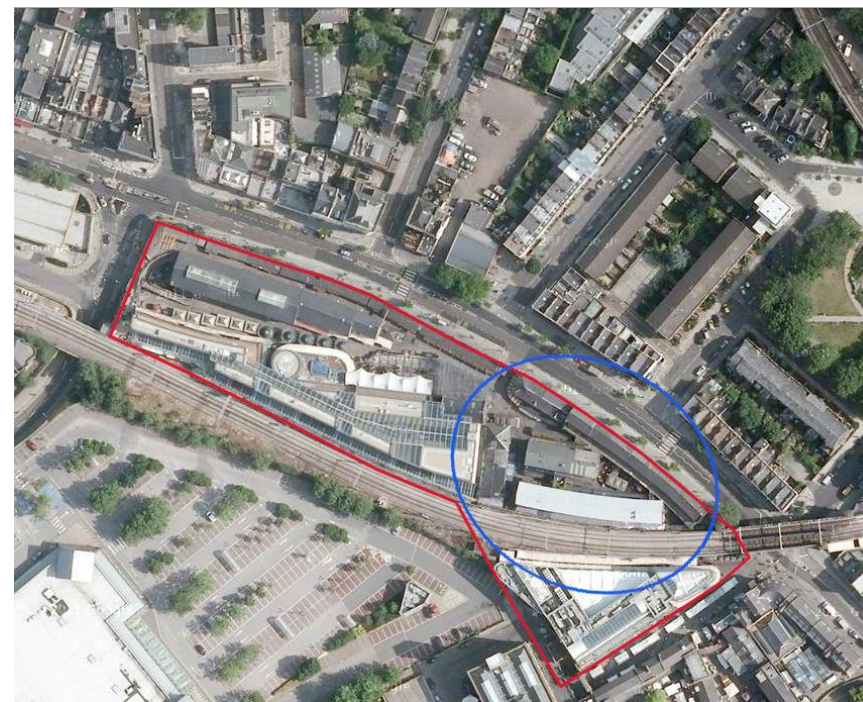


Figure 2 The Stables Market shown in red line; the stables range circled in blue.

2.1 Regents Canal Conservation Area

The Regent's Canal Conservation Area was originally designated in 1974 and subsequently extended in 1981 to include the Stanley Sidings and the Stable Buildings. The conservation area was designated due to its unique character. *"It is an important feature of historic and visual interest in the wider townscape and, following the decline of traditional canal-related commercial activities, has been increasingly recognized as a valuable resource for water-based leisure activities, for its tranquil seclusion, for its ecological value and its potential for transportation and informal recreation"* (Regent's Canal Conservation Area Appraisal and

Management Strategy, adopted 11 September 2008, pp.5).

Many of the industrial buildings and structures are fine examples of industrial brickwork, illustrating styles of engineering construction characteristics of the 19th and early 20th centuries and using various types of brick, some produced in London and others brought in by the railways from their respective regions. Cast iron and wrought iron are also represented on the site.

2.2 Setting

The National Planning Policy Framework (NPPF) defines 'setting of a heritage asset' as 'the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral' (NPPF, Annex 2: Glossary, p.52)

The Setting of Heritage Assets: English Heritage Guidance 2011, highlights para 114 of the NPPF: *'The extent and importance of setting is often expressed by reference to visual considerations. Although views of or from an asset will play an important part, the way in which we experience an asset in its setting is also influenced by other environmental factors such as noise, dust and vibration [...]'* (NPPF, para 114).

The site partly falls within the Regent's Canal Conservation Area, a heritage corridor of unique character, but the elements which contribute towards the neighbourhood identity, are not necessarily confined to the conservation area or other boundaries.

The character of the environs developed over the years and, today largely derives from the presence of four distinct but interrelated elements: the Regent's Canal, Chalk Farm Road, the Railway viaduct and the market activities

Chalk Farm Road is the principal thoroughfare of the area. The sequence of diverse views along Chalk Farm Road, defined by the built form and the curve of the road, prevents long distance views.

The built environment is defined on the East side by 19th century terraced buildings which have shops, café, and restaurants at the ground floor; and on the West side by predominant structures, such as the Roundhouse, the 'Camden Wall' and the railway bridge, which come into view on a northerly progression.

The Roundhouse is a major focus point, almost providing a gateway to the area from the north.

The 'Camden Wall' confers to Chalk Farm Road a sense of continuity and calmness with its regular progression of openings, but it is also a distinct barrier with its non-active frontage. A strong change of character occurs behind the Stables Market gate, where the original industrial stables subject of this application, still remain. The market is enclosed and separated from the public realm by the impenetrable wall.

The railway bridge, crossing diagonally across Chalk Farm Road, is not a simple barrier. It creates a visual break, emphasised by the traffic island and one-way traffic system. It also frames with its portal structure the view behind that change again in character. In fact, the buildings aligned on this stretch of the street have a narrow pavement establishing a different relationship with the street.

The immediate setting of the subject site is the larger Stables Market at the heart of the area widely known as Camden Market. The complex comprises the industrial horse stables and tightly enclosed courts leading off one another; their plan form being influenced by the horse towpath and stabling serving the vast railway goods yard and interchange traffic.

The redevelopment of the underused site after the decline of canal-related activities and the conversion of wharves for the craft markets in the 1970s, contributed greatly to the economic and physical regeneration of Camden and to the present character of the area.

The craft markets developed into one of London's top tourist attractions

giving a new iconography to Camden. Today the place is mostly known for its famous market, restaurants, café and entertainment facilities but this was never part of any planned development policies.

This market attracts large numbers of Londoners and tourists because of the character, the goods on sale and the uniqueness of the location. Boat trips, walks along the canal, and watching the barges pass through the lock gates from the Dingwall's beer garden, are important parts of the attraction of the Camden Lock area. Sunday trading was permitted on this private site while disallowed in many places elsewhere, and this also contributed to its success.

2.3 Historical Background

This complex of multi-storey stables, also known as Stanley Sidings, was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

Bounded to the north by Chalk Farm Road (known as Hampstead Road before 1862), the triangular site was separated from the goods yard to the south by the viaducts of the North London Railway but was linked to it by underground tunnels.

A 22 acre site at Chalk Farm was acquired by the London and Birmingham Railway company soon after its formation, for a terminus and depot. When development began in 1835-37 it was as a goods depot, linked to the Thames by the Regent's Canal and subsequently to the docks by the North London Railway, built in 1850-1.

In the 1840s and 1850s substantial buildings were constructed around the edge of the site, including the Southern Goods Shed, built in 1845 and rebuilt after a fire in 1857, and a goods shed north of the canal, replaced by the Interchange Warehouse in 1900-5. The Chalk Farm stables were linked to both of these buildings by underground tunnels.

Horses were required for a wide range of haulage duties in the yard as

well as for distributing goods outside; others were needed for shunting locomotives. Early provision for the horses was apparently in makeshift accommodation or below ground in the vaults of the goods station. By 1849 some horses were being stabled in the south eastern corner of the goods yard, and it seems likely that the expansion of this part of the yard in 1855 led to the construction of the stables at the triangle site.

The first phase of stabling, the four western ranges forming the triangular group A-D in the modern site plan, was built between 1854 and 1856, as indicated as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072). They were designed by the LNWR staff on the Stafford office and had a capacity of 148 horses. The ranges were mainly one and a half storeys, with ground floor stabling and haylofts above.

In the early 1880's the whole site underwent a phase of expansion and alteration with additional storeys added to the western ranges between 1880-3. During this period, a bridge connection between the Provender Store and the Tack Room was also created.

The site was vacated in 1973, and many of the buildings were demolished, tracks lifted, and various parcels of the land were sold into different ownerships. After 1973 the sidings and Camden Lock were used for various light industrial and storage uses including garaging. The market activities started at this time in a rather ad hoc fashion, commencing with small stalls located externally to the stable buildings. Gradually shop units and canopies were added to the buildings and by the 1980s it was a fully-fledged market place.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright.

2.4 Long Stable

The Long Stable building was built in 1853-5 as a single storey stable block with hay lofts. In 1881-3 it was raised (builders, Bradney and Company) to provide an upper level of stabling and a ramp was provided on the south side for horses to gain access to the upper storey (Fig.3). A second ramp to the north was added c1895 to provide access to the upper storey of the newly raised Building A. Only the latter ramp now survives (Fig.4).

The building is seven bays long, with the centre bay breaking slightly forward on both sides; there was originally an entrance and a loophole to the south. On the ground floor there are round-headed openings with high level sills to light the stalls from above, as seen elsewhere on the site. The later first-floor openings have segmental heads. The east gable end has a small upper-storey hatch over a widened ground-floor opening. The west gable end has had its upper-storey opening enlarged as a goods hatch.

The 1880s 'horse road' to the south was not strictly a ramp as it had steps with 4 ½ inches risers; its position is evident on the south wall. The ramp to the north is brick built with concrete-paved road surfaces and an intermediate landing.

Interior:

There were originally 26 stalls in a single undivided space giving rise to the naming of this block as the 'long stable', even though this is the shortest of the stable blocks built on the site. The upper floor rests on cast-iron stanchions with moulded heads along the north and south walls; these bear marks that they were made by Barton & Son, of Darlaston. They support wrought-iron I-section girders carrying rolled joists to brick jack arches. There are timber king-post roof trusses, but no traces of the original stable fittings.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables, Report by Peter Guillery, September 1995. Crown Copyright.



Figure 3 1975 photograph showing horse ramp to south side (now lost).

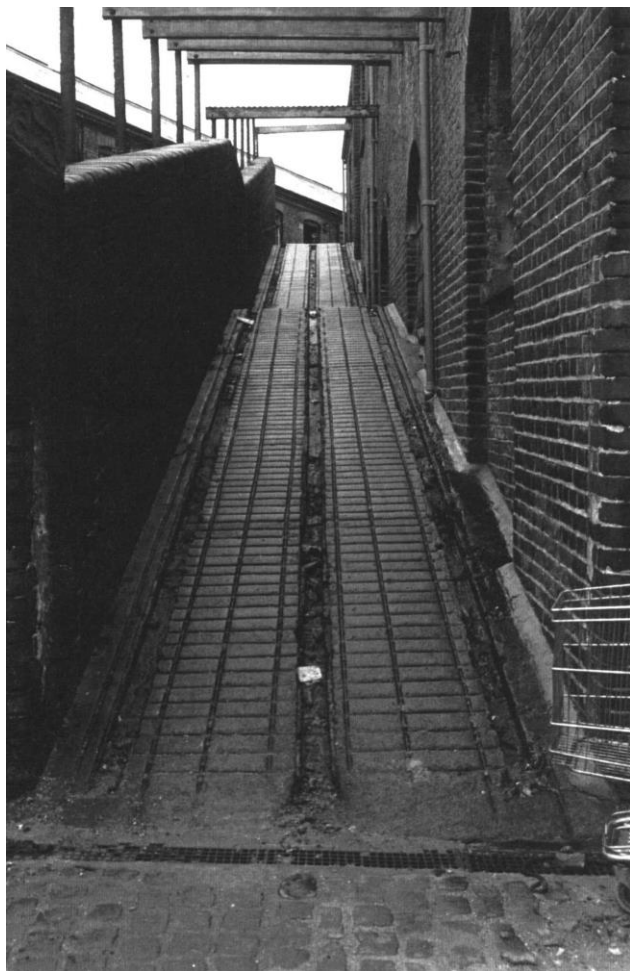


Figure 4 1975 photograph showing horse ramp to the north side (still extant).



Figure 5 1975 photograph showing interior roof structure and sarking boards.

3 Assessment of Significance

3.1 Criteria

As recommended by NPPF (March 2012) proposals for the alteration or redevelopment of listed building or buildings within a Conservation Area should be considered and be based on an understanding of the site's significance.

Paragraph 128 of NPPF states that '*In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should also be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance*'.

The criteria for the assessment of significance according to English Heritage's 'Conservation Principles' consists primarily of:

- Evidential Value – relating to the potential of a place to yield primary evidence about past human activity;
- Historical Value – relating to ways in which the present can be connected through a place to past people, events and aspects of life;
- Aesthetic Value – relating to the ways in which people derive sensory and intellectual stimulation from a place;
- Communal Value – relating to the meanings of place for the people who relate to it, and whose collective experience or memory it holds.

2.2 Long Stable

Evidential Value

There are numerous surviving features providing evidence of the original function of the building, although disturbed by many alterations. The legibility of the former use is however clear.

The extent of historic fabric preserved contributes greatly to the special interest and character of the Provender Store and to that of the group in general. **Evidential value is therefore high.**

Historical Value

The historical value of the Long Stable is recognised by its statutory Grade II listing, as part of a distinctive group of former stables.

In this case the historic value is closely associated to the evidential value, providing a significant example of stables construction of their historic period beyond the numerous alterations suffered through the years.

Historical value is therefore medium to high.

Aesthetic Value

The Long Stable, as well as the whole stables complex, is a utilitarian building purpose-made. There is no artistic 'design ethos' in these buildings; utilitarian buildings are economical: there are no added embellishments, wasted space, nor selection of materials but those easily sourced.

Although the Long Stable is an exemplary Mid-Victorian stable building, it has little artistic or architectural merit. Its **aesthetic value is therefore low.**

Communal Value

The Stables Market and its listed buildings have a **high communal value** as there is a strong sense of identity with the place, nurtured by the historical industrial use.

The original purpose of the stables has been superseded. There are no possible horse-related activities that may be carried out within the site, and there have not been since the 1950's/60's. The market use, established since the late 1970's/80's, is the reason why this area of Camden achieved worldwide recognition and attracts over 100.000 visitors per week. Furthermore, a poll carried out by NOP World in late 2003, shows that 95% of visitors agreed that the Camden markets are a valuable tourist attraction and 89% agreed the markets are a valuable amenity to the local community as well as visitors (London Borough of Camden, the role the markets play in the vitality & viability of Camden Town, 2006.). The communal values of the site can be associated to the later but firmly established market function, and as proved by the studies and polls carried out since its inception. The fact that the building is still in use brings together past and present common uses by the community, reaffirming the communal value as high.

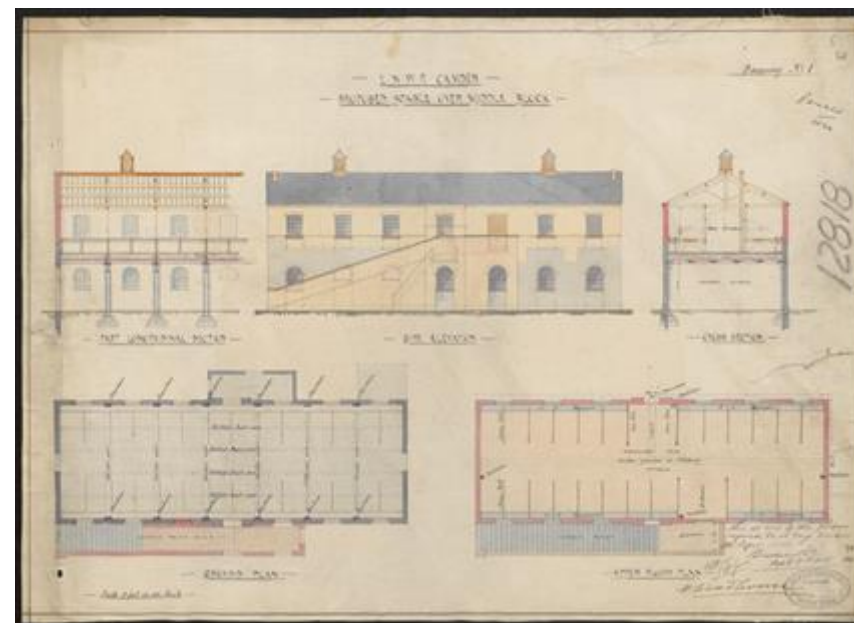


Figure 6 Drawing dated 20th September 1881, showing first floor extension to the Long Stable.

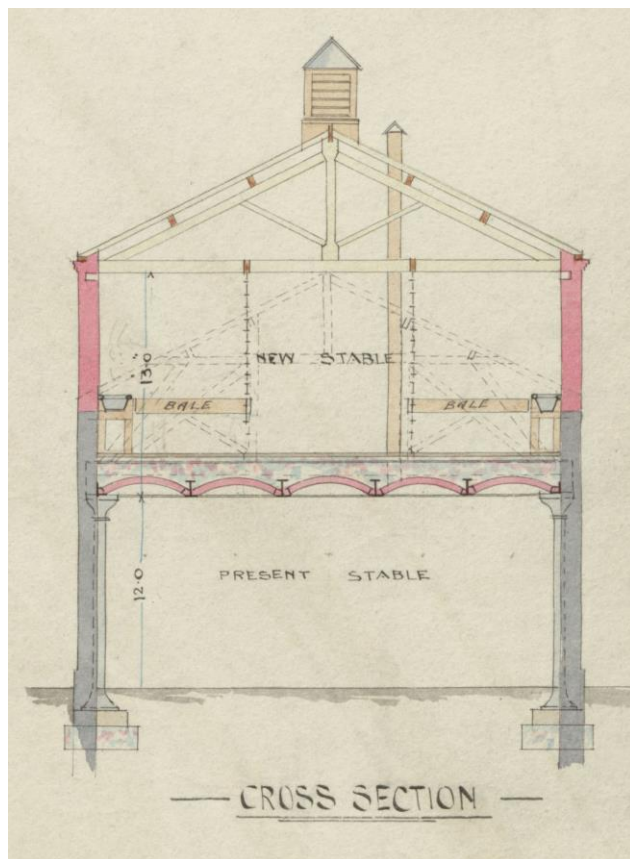


Figure 7 Detail of drawing. Long Stable - Cross Section.



Figure 8 Detail of drawing. Long Stable - Longitudinal Cross Section.

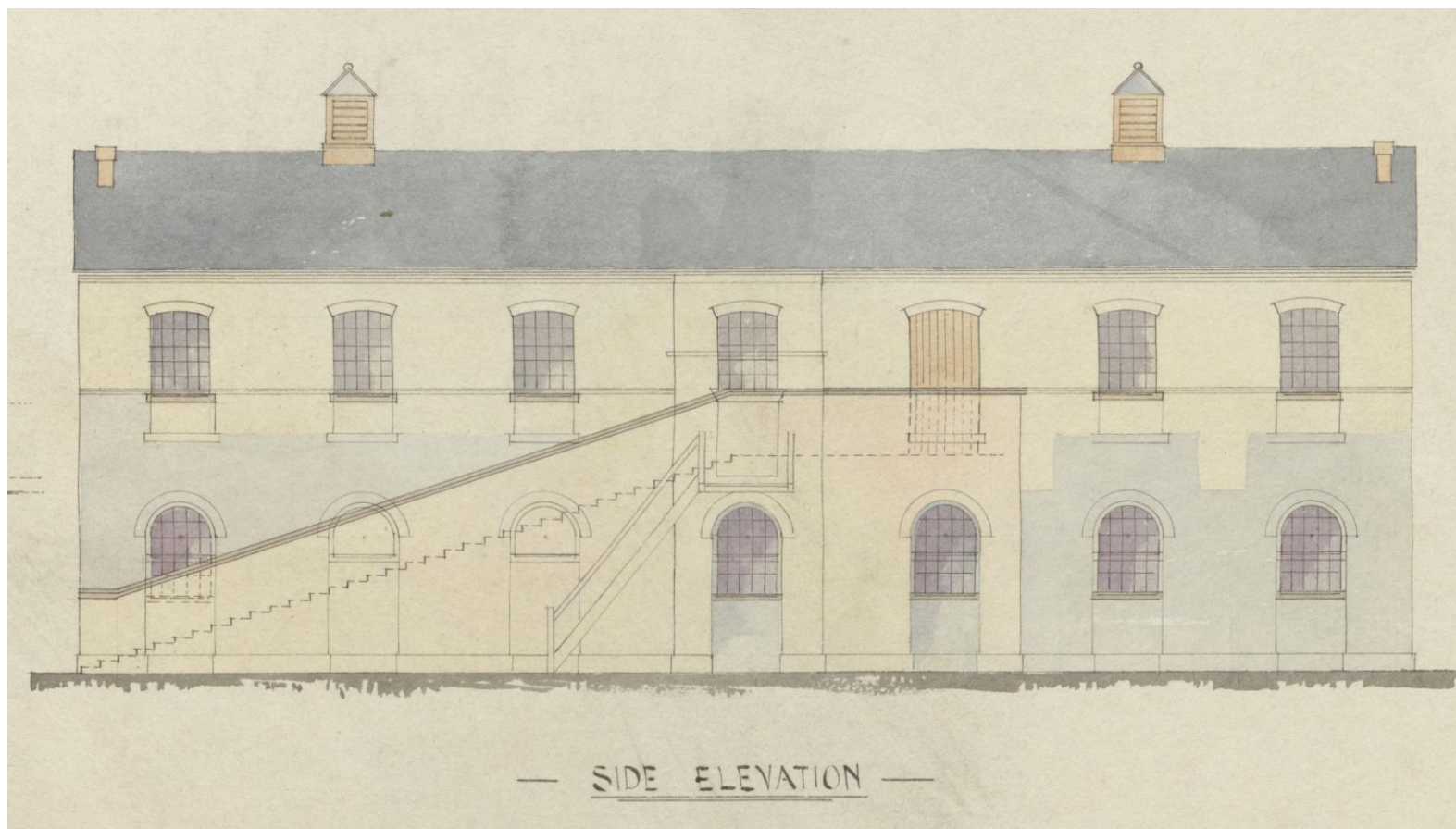


Figure 9 Detail of drawing. Long Stable Elevation.

3 PROPOSED WORKS

3.1 Description

Roof Works

This application includes for the removal of the existing roofing felt, the repair and upgrading of the underlying roof structure and the reinstatement of the original slate covering. Two number painted timber and zinc roof fleches will also be reinstated to match those now lost. The building's adhoc external drainage arrangement, including rainwater goods and soil and vent pipes will also be rationalised and items generally replaced with cast iron as originally.

The removal of the existing roofing felt will allow for the reinstatement of a historically correct slate covering and also provide the opportunity to upgrade and repair the underlying structure to include insulation. These works considered together with the other works to the building - namely the repair and replacement of windows and doors (the subject of a separate listed building consent) and the new mechanical and electrical installations - will improve the building's overall energy performance and comply with the recognised objectives of a sustainable future.

The further removal of two modern rooflights (on the north roof slope) and the reinstatement of the two roof fleches will ensure the finished roof attains its original appearance.

Following an extensive search for reclaimed slates and thorough investigation and trials for reuse, it is our belief that there would be no benefit in the use of reclaimed slates since this natural stone will eventually age to achieve the expected appearance. Furthermore, the durability of aged slates is questionable as they are often friable due to natural erosion and will have a shorter life expectancy.

It is therefore proposed to use new natural slates to match the colour and size of those used on the newly restored Tack Room (building D).

Mechanical & Electrical Works

The works further include for the new installation and distribution of mechanical and electrical services to the Long Stable building to provide much improved services throughout. All existing services are to be removed, including those cables and conduits currently fixed externally in an adhoc manner to the building's facades (figure 4 & 5).

A new electrical mains distribution system (LV & ELV) will be routed within a horizontal compartment trunking system to run around the building internally at ground floor high level. The building will also be provided with internal small power services as required including BT, data and power points. A new gas and water supply will enter the building at the north east corner at ground level and rise to the first floor Plant Room.

Externally the building's facades will be fitted with CCTV cameras, high level luminaires and emergency lighting.

The restaurant kitchen will be provided with a new extractor system which will discharge to outside air via one of the two restored roof fleches.

The existing customer toilets, located at first floor level and accessed currently via an external metal stair, will be completely replaced with new toilets within a new interior fit out. The interior will comprise a high quality proprietary contemporary washroom system complete with cubicles, Wc's, and basins set within an enclosure of traditionally tiled walls (figure 13, 14 & 15).

The works will greatly improve the external appearance of the building as well as providing a modern and safer services installation overall and, correspondingly, an improved energy performance.

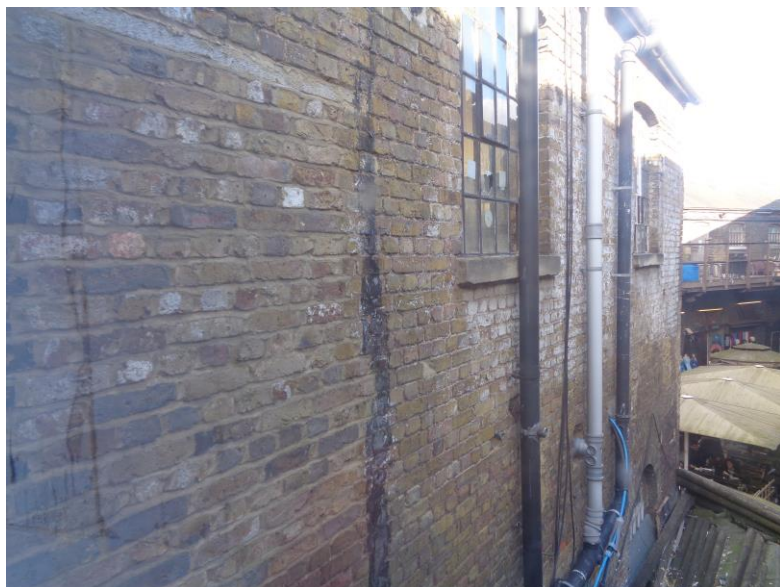


Figure 10 2014 photograph of the south façade showing the proliferation of adhoc service provision.



Figure 11 Current high level view of the Long Stable from the west. The roof fleche upstands still remain over the ridge.



Figure 12 Internal view of roof structure showing remaining roof fleche upstand.



Figure 13 Indicative ceramic wall tile types for the replacement washroom (colours to be agreed).

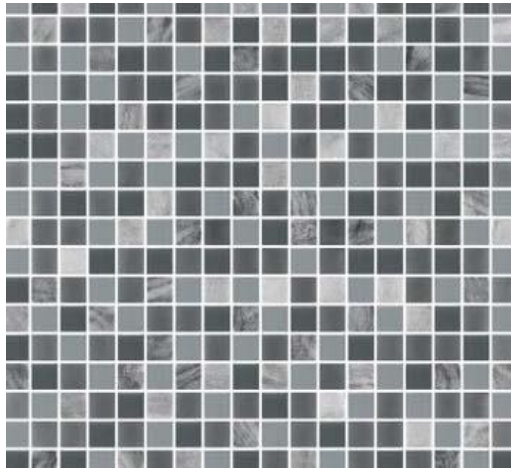


Figure 14 Indicative mosaic floor tile for the replacement washroom (colours to be agreed).



Figure 15 Indicative flush fronted washroom cubicle, vanity top and basins (colours to be agreed).

4 IMPACT ASSESSMENT

The proposed works will replace the modern roofing felt with the historically correct roofing material for the building: slate.

Furthermore, there will be no harm caused to the building's historic fabric by the wider repair and upgrading works. It is considered that the proposed works will enhance the historic character and appearance, thus having an overall highly positive impact.

The proposed works to the Long Stable roof, including the reinstatement of slate, the roof fleches and the renewed drainage goods, will therefore enhance the listed buildings significance and that of the stables complex at large.

The proposal comprises the removal of existing adhoc mechanical and electrical services from the building and the installation of new services along rational planned compartment routes. The new washroom and its plumbing is part of this and will result in much needed improved facilities for site customers.

External elements, such as lighting and CCTV have been carefully sourced and located so as to be subservient to the historic features. External luminaires and fittings are proposed in a contemporary industrial style. Internally, discretely located service risers provide routing for gas and water to the First floor Plant Room. Electrical services (LV & ELV) are distributed at Ground floor via a horizontal compartment trunking running around at high level.

The new services installation is designed to have a minimal impact on the building fabric. Compartment systems and risers will be painted to minimize their visual impact internally. Following the installation, any repairs necessary will be carried out in appropriate materials to match existing fabric.

Impact: It is considered that the removal of existing cabling and other associated detrimental features from the elevations of the Long Stable building and the rationalisation of services in general will allow for a better interpretation of the heritage asset. The proposed works are, therefore, considered necessary and highly beneficial.

5 NPPF CONSIDERATIONS

All policies in the NPPF adopted 27th March 2012 constitute the government's view of what sustainable development means in practice. The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions.

'Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy.' (NPPF Paragraph 7).

This statement deals principally with Section 12 of the NPPF, "Conserving and enhancing the historic environment", however Heritage considerations and issues are prevalent throughout the framework.

NPPF Paragraph 126 states that *"Local planning authorities should set out in their Local Plan a positive strategy for the conservation and enjoyment of the historic environment, including heritage assets most at risk through neglect, decay or other threats. In doing so, they should recognise that heritage assets are an irreplaceable resource and conserve them in a manner appropriate to their significance. In developing this strategy, local planning authorities should take into account:*

- *the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;*

NPPF Paragraph 128 states that *“In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contributions made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on that significance”*.

As recommended in Paragraph 128, an assessment of the significance of the Long Stable building has been provided earlier in this report. Appropriate and proportionate historic research and analysis was undertaken to identify the heritage values and character of the heritage assets, including the origins of the building and how it has changed over time.

The significance assessment was based on historic research and consultation of relevant historic records and was examined according to the criteria set out in English Heritage’s ‘Conservation Principles’.

It is our belief that the NPPF requirement for provision of proportionate information which will enable the assessment of the likely impacts of proposed development on the special historic and architectural interest of the Long Stable building has been therefore fulfilled.

NPPF Paragraph 131 states: *‘In determining applications, local planning authorities should take account of:*

- *The desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;*
- *The positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and*
- *The desire of new development making a positive contribution to local character and distinctiveness’.*

The proposal has been driven by the need to ensure the historic character of the listed building is preserved and enhanced and meets all

current standards in a respectful manner towards the historic environment. Overall, the proposed planning, relocation and upgrade of mechanical and electrical services will ensure the continued effective use and operation of the Long Stable building.

NPPF Paragraph 134 states: *‘Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including its optimum viable use’.*

The proposed removal of existing adhoc services and the installation of new services throughout will mitigate against any current safety risks to the heritage asset and will enhance the historic building and its setting. Ensuring its continued use together with associated maintenance is in the interest of the heritage asset’s ‘long term conservation’.

There is no harm involved in the replacement of the existing modern roofing felt with a slate covering as originally, or in the updating and rationalisation of servicing.

The established market use of the former stables is not challenged by the proposals; the works will greatly improve not only the appearance of the building but its thermal efficiency and energy consumption thus increasing comfort levels for site staff and visitors alike.

The public benefit the proposal provides is in preserving not only the listed building but also the already established "optimum viable use" for the site, as well as the enhancement of the wider historic environment in general.

The combined proposals will aid in safeguarding the continued use and long term preservation of the building. It is therefore concluded that the significance of the heritage asset will be preserved and enhanced in compliance with the NPPF requirements.

6 LOCAL PLANNING POLICY

Camden Local Development Framework, Camden Core Strategy, 2010 - 2025, Adopted Version, November 2010.

The Camden Core Strategy Policy CS14 – ‘Promoting high quality places and conserving our heritage’, sets out the requirements to safeguard Camden’s heritage. The overall strategy is to sustainably manage growth in Camden in a way that conserves and enhances the heritage and valued places that give the borough its unique character.

Camden Planning Guidance provides advice and information on how the Local Authority applies its planning policies. The guidance is consistent with the Core Strategy and the Development Policies, and forms a Supplementary Planning Document (SPD) which is an additional “material consideration” in planning decisions.

CPG 1- Design deals with heritage issues in Section 3. This section sets out further guidance on Core Strategy Policy CS14 Promoting high quality places and conserving our heritage and Development Policy DP25 Conserving Camden’s Heritage.

Paragraph 3.22 refers to the statutory requirement, when assessing applications for listed building consent, to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

It is considered that the proposal not only preserves but enhances the character of the historic environment through the repair and recovering with slate of the currently dilapidated roof and the removal and replacement of mechanical and electrical services, which in turn, retains and enhances the elements of significance within and in the vicinity of the site. The reinstatement of the two roof fleches further contributes to this objective.

Paragraph 3.23 expands on the desirability to retain original or historic

features and to carry out repairs in matching materials. The proposals should seek to respond to the special historic and architectural constraints of the listed buildings, rather than significantly change them.

As stated earlier in this report, the proposal actively seeks to restore historic finishes and features. The works will be carried out using traditional materials and techniques in order to reinforce the integrity of the listed building.

NPPF as detailed above and is consistent with the spirit of local policies and national conservation principles.

7 CONCLUSION

The Long Stable building is a listed Grade II building that is on the Heritage at Risk Register. The building is open to the public and in continuous use. It has been subject to change and in order to remain sustainable, welcoming and pleasing, it will continue to evolve. The proposed scheme is driven by a need to not only conserve and enhance the building, but also to ensure a sustainable solution that safeguards the current and future use of the market.

The proposed works, including the reinstatement of the historic slate roof covering, will enhance the historic character and architectural importance of the heritage asset safeguarding its long term conservation.

This proposal complies with policy at the heart of NPPF in respect of sustaining and enhancing not just the historic fabric but the significance of the Regent's Canal Conservation Area and the heritage assets within and in the vicinity of the site.

The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions. Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy (NPPF Para 7).

For the above reasons, it is considered that the proposal would be acceptable in the context of the setting of the heritage assets in the immediate surroundings and the conservation area. It is therefore concluded that the proposed works satisfy the relevant clauses of the

APPENDIX I LISTED BUILDING DESCRIPTION

List entry Summary

List entry Number: 1258101

Grade: II

Four blocks of industrial stabling, now workshops and warehousing. c1855-1870, with later Victorian additions. For the London and North-Western Railway Company's Camden Goods Yard. Stock brick, with hipped slate roofs, some stone lintels. Some iron columns internally, but floors and roofs generally of timber. EXTERIOR: mostly of 2 storeys.

Northernmost block (A) abutting on Chalk Farm Road, c1855, with upper storey of c1895. Long curved front road, mostly of 2 storeys with eaves cornice but western end of one and a half storeys, somewhat altered, with chimney on roof. Round-headed half windows for stabling on ground storey, segment-headed industrial windows in upper storey (eastern end only). Elevation towards yard irregular. Eastern portion has cantilevered open balcony at first-floor level retaining some concrete horse troughs and connected by bridge to Block B and separately to ramp on Block C. Ceilings of ground storey have jack-arch iron and brick construction. INTERIOR of upper storey has separate compartments and paving for horses but no stalls.

Block B immediately to north of North London railway line. 3 storeys. Ground storey c1868, originally provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A.

Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A. Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper storey 1881; horse ramp on north side of block c1895, connected with balcony on Block A. Round-headed windows on ground storey, segment-headed industrial windows above on both north and south sides. South side formerly had another horse ramp, of 1881, and covered bridge connecting with Block B, demolished in 1980s. Ground storey has iron and brick jack-arch construction and iron stanchions against walls stamped 'Norton and Son Darlaston'.

Block D at right-angles and to west of Blocks B and C. 2 storeys. Ground storey c1868, upper storey c1881. Main elevation faces eastwards, with return northwards. Round-headed half-windows for stabling at ground level, some segment-headed sash windows above. Tall brick chimneys. INTERIOR with original timber benching, one timber partition and some harness hooks. Said to have been formerly the Tack Room for the stabling. Formerly connected by a bridge at south end to Block B. Included as a rare example of substantial industrial stabling and a major surviving portion of the former Camden Goods Yard. Forms a group with the 'Horse Hospital' to north-west (qv) and with further remnants of stabling and warehouses west of Block D (qv). A tunnel (now blocked) south of the North London line connects the complex with further LNWR buildings and the Regent's Canal south of the North London Line.

Listing NGR: TQ2862684201