



**STABLES MARKET  
TACK ROOM & PROVENDER STORE**

**DESIGN AND ACCESS  
AND  
HERITAGE STATEMENT**

**NEW BRIDGE LINKING TACK ROOM &  
PROVENDER STORE**

**JULY 2015**

**Prepared for  
Stanley Sidings**

**by**



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**Stables Market: Provender Store and Tack Room – Design and Access and Heritage Statement**

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# 1 INTRODUCTION

## Executive Summary

This Design and Access and Heritage Statement has been prepared as supporting documentation for the application for Planning and Listed Building Consent for the reinstatement of the bridge connecting the Provender Store (Building B) and the Tack Room (Building D) at first floor level. The buildings are Grade II listed and located within The Stables Market, Chalk Farm Road, London NW1 8AH.

The Provender Store and the Tack Room are in the Heritage at Risk Register, which replaced the Buildings at Risk Register on July 8th, 2008. They are registered as Category D in the priority ranking. Priority for action is assessed on a scale of A to F, where 'A' is the highest priority for a site which is deteriorating rapidly with no solution to secure its future, and 'F' is the lowest priority. Buildings in category D are assessed as suffering 'Slow decay; solution agreed but not yet implemented' (English Heritage, HAR 2013, p. XVI).

The Design Documents have been prepared by and with the input of the Project Team who are:

Client: Stanley Sidings Limited

Local Authority: London Borough of Camden

Planning Consultants: Gerald Eve

Structural Engineers: Sinclair Johnston & partners

Heritage Architects: Stephen Levrant Heritage Architecture

In addition, we would like to thank The Regent's Canal Conservation Area Advisory Committee and especially Mr Malcolm Tucker for making available their valuable knowledge and providing advice throughout the design development of proposals.

## 1.1 Reference to other documentation

This document should be read in conjunction with design information:

- **B/D-LP:** Location Plan
- **8370/401/B:** Proposed Bridge Link (by Sinclair Johnston)
- **B/D-B-02 B:** Proposed Bridge Link Elevations
- **B/D-B-03 B:** Proposed Bridge Link - External Door Details (sheet 1 of 2) to Tack Room
- **B/D-B-04 B:** Proposed Bridge Link - External Door Details (sheet 2 of 2) to Provender Store
- **B/D-B-05 B:** Proposed Bridge Link - Window Detail to Provender Store
- **8370 JSJ July 2015 REPORT** (by Sinclair Johnston)

## 1.2 Planning Policy Guidance and Legislation

The assessments of the listed buildings and conservation area have been prepared taking into account the information contained in:

- NPPF National Planning Policy Framework, 27 March 2012.
- Planning practice guidance for the National Planning Policy Framework and the planning system. , 12 June 2014.
- Conservation principles, policies and guidance for the sustainable management of the historic environment, EH, April 2008.

- The Setting of Heritage Assets: English Heritage Guidance, October 2011.
- BS 7913:1998 Guide to the Principles of the Conservation of Historic Buildings.
- Understanding Place: Historic Area Assessments in a Planning and Development Context, EH, June 2010;
- Camden Development Policies 2010 – 2025, Local Development Framework
- Local Development Framework - Camden Core Strategy 2010 – 2025

## 2 Summary of Proposals

The proposed works to the Grade II Listed Provender Store and Tack Room comprise the reinstatement of a bridge between the two buildings at first floor level. The bridge is designed to closely match in appearance and structural arrangement the historical bridge (fig.1).



**Figure 1** The Tack Room showing the bridge in 1975 before its removal.

### 2.1 CONTEXT

This complex of multi-storey stables was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

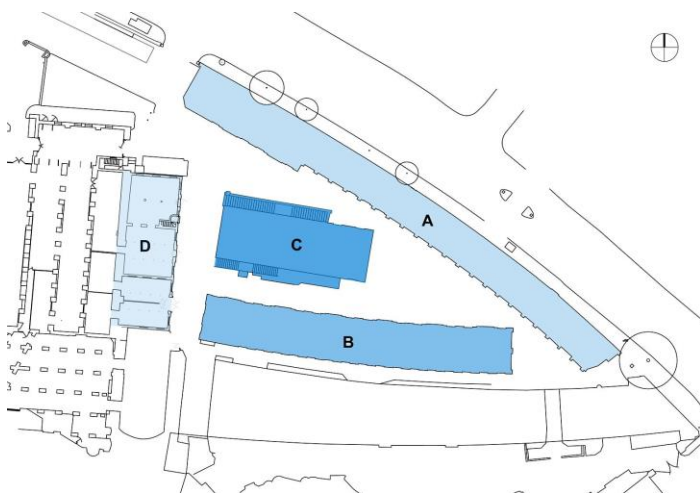
*'The heart of Stables Market was previously a part of the Camden Goods Yard and a large area was occupied by warehousing for W. A. Gilbey's wines and spirits from the mid 19th century. The remaining buildings on the site comprise the stables for railway horses known as Stanley Sidings [...], a later block of 1883-85, and the surviving bonded warehouse, Gilbey's No.2 Bond, built c.1885'* (Regent's Canal Conservation Area Appraisal and Management Strategy, adopted 11 September 2008, pp.17). The Tack Room and Provender Store, subject of this application, were built during the first phase of stabling. These four western ranges, which also include the Long Stable and the Chalk Farm Stable, form the triangular group A-D built between 1854 and 1856 as indicated on a site plan of 1856 (The National Archives, RAIL

410/2072).

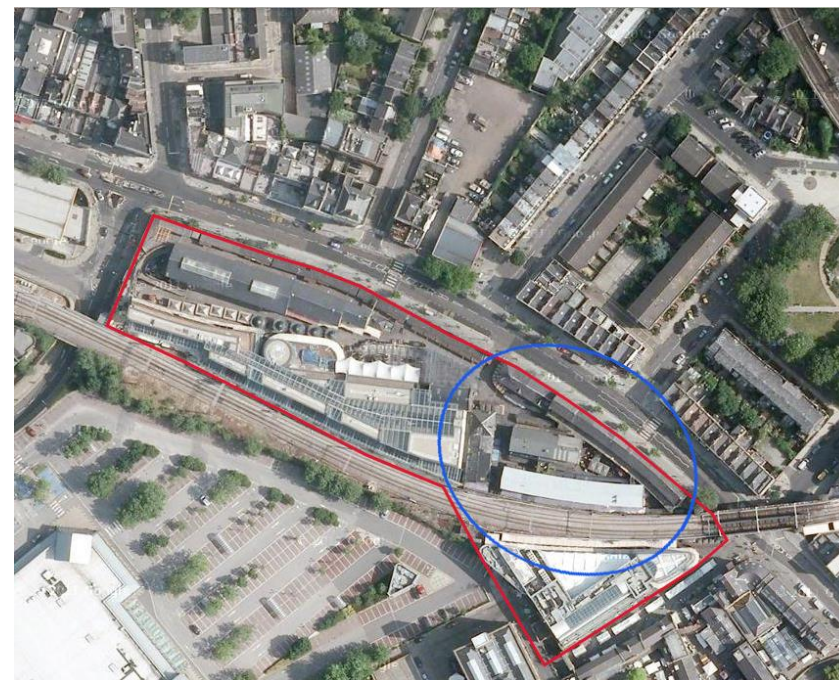
Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright. Unless stated otherwise.

**Location:**

The Tack Room (Building D) and the Provender Store (Building B) are within the triangular site described above part of the internationally recognised Stables Market, in the northwest corner of Regents Canal Conservation Area. (Fig. 2. The area is bounded to the north by Chalk Farm Road (Hampstead Road as it was known until 1862), and is separated from the former lands of the of the goods yard to the south by the railway viaducts (Fig. 3).



**Figure 2** Stables Market - Four western ranges, subject site Building B and Building D.



**Figure 3** The Stables Market shown in red line; the stables range circled in blue.

**2.2 Regents Canal Conservation Area**

The Regent's Canal Conservation Area was originally designated in 1974 and subsequently extended in 1981 to include the Stanley Sidings and the Stable Buildings. The conservation area was designated due to its unique character. *"It is an important feature of historic and visual interest in the wider townscape and, following the decline of traditional canal-related commercial activities, has been increasingly recognized as a valuable resource for water-based leisure activities, for its tranquil seclusion, for its ecological value and its potential for transportation and informal recreation"* (Regent's Canal Conservation Area Appraisal and

Management Strategy, adopted 11 September 2008, pp.5).

Many of the industrial buildings and structures are fine examples of industrial brickwork, illustrating styles of engineering construction characteristics of the 19th and early 20th centuries and using various types of brick, some produced in London and others brought in by the railways from their respective regions. Cast iron and wrought iron are also represented on the site.

## 2.3 Setting

The National Planning Policy Framework (NPPF) defines 'setting of a heritage asset' as 'the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral' (NPPF, Annex 2: Glossary, p.52)

The Setting of Heritage Assets: English Heritage Guidance 2011, highlights para 114 of the NPPF: *'The extent and importance of setting is often expressed by reference to visual considerations. Although views of or from an asset will play an important part, the way in which we experience an asset in its setting is also influenced by other environmental factors such as noise, dust and vibration [...]'* (NPPF, para 114).

The site partly falls within the Regent's Canal Conservation Area, a heritage corridor of unique character, but the elements which contribute towards the neighbourhood identity, are not necessarily confined to the conservation area or other boundaries.

The character of the environs developed over the years and, today largely derives from the presence of four distinct but interrelated elements: the Regent's Canal, Chalk Farm Road, the Railway viaduct and the market activities

Chalk Farm Road is the principal thoroughfare of the area. The sequence of diverse views along Chalk Farm Road, defined by the built form and the curve of the road, prevents long distance views.

The built environment is defined on the East side by 19th century terraced buildings which have shops, café, and restaurants at the ground floor; and on the West side by predominant structures, such as the Roundhouse, the 'Camden Wall' and the railway bridge, which come into view on a northerly progression.

The Roundhouse is a major focus point, almost providing a gateway to the area from the north.

The 'Camden Wall' confers to Chalk Farm Road a sense of continuity and calmness with its regular progression of openings, but it is also a distinct barrier with its non-active frontage. A strong change of character occurs behind the Stables Market gate, where the original industrial stables subject of this application, still remain. The market is enclosed and separated from the public realm by the impenetrable wall.

The railway bridge, crossing diagonally across Chalk Farm Road, is not a simple barrier. It creates a visual break, emphasised by the traffic island and one-way traffic system. It also frames with its portal structure the view behind that change again in character. In fact, the buildings aligned on this stretch of the street have a narrow pavement establishing a different relationship with the street.

The immediate setting of the subject site is the larger Stables Market at the heart of the area widely known as Camden Market. The complex comprises the industrial horse stables and tightly enclosed courts leading off one another; their plan form being influenced by the horse towpath and stabling serving the vast railway goods yard and interchange traffic.

The redevelopment of the underused site after the decline of canal-related activities and the conversion of wharves for the craft markets in the 1970s, contributed greatly to the economic and physical regeneration of Camden and to the present character of the area.

The craft markets developed into one of London's top tourist attractions

giving a new iconography to Camden. Today the place is mostly known for its famous market, restaurants, café and entertainment facilities but this was never part of any planned development policies.

This market attracts large numbers of Londoners and tourists because of the character, the goods on sale and the uniqueness of the location. Boat trips, walks along the canal, and watching the barges pass through the lock gates from the Dingwall's beer garden, are important parts of the attraction of the Camden Lock area. Sunday trading was permitted on this private site while disallowed in many places elsewhere, and this also contributed to its success.

## 2.4 Historical Background

This complex of multi-storey stables, also known as Stanley Sidings, was developed by the London and North Western Railway Company (LNWR) from 1854. The horses stabled here supplied much of the power for the large LNWR goods yard at Chalk Farm.

Bounded to the north by Chalk Farm Road (known as Hampstead Road before 1862), the triangular site was separated from the goods yard to the south by the viaducts of the North London Railway but was linked to it by underground tunnels.

A 22 acre site at Chalk Farm was acquired by the London and Birmingham Railway company soon after its formation, for a terminus and depot. When development began in 1835-37 it was as a goods depot, linked to the Thames by the Regent's Canal and subsequently to the docks by the North London Railway, built in 1850-1.

In the 1840s and 1850s substantial buildings were constructed around the edge of the site, including the Southern Goods Shed, built in 1845 and rebuilt after a fire in 1857, and a goods shed north of the canal, replaced by the Interchange Warehouse in 1900-5. The Chalk Farm stables were linked to both of these buildings by underground tunnels.

Horses were required for a wide range of haulage duties in the yard as

well as for distributing goods outside; others were needed for shunting locomotives. Early provision for the horses was apparently in makeshift accommodation or below ground in the vaults of the goods station. By 1849 some horses were being stabled in the south eastern corner of the goods yard, and it seems likely that the expansion of this part of the yard in 1855 led to the construction of the stables at the triangle site.

The first phase of stabling, the four western ranges forming the triangular group A-D in the modern site plan, was built between 1854 and 1856, as indicated as indicated on a site plan of 1856 (The National Archives, RAIL 410/2072). They were designed by the LNWR staff on the Stafford office and had a capacity of 148 horses. The ranges were mainly one and a half storeys, with ground floor stabling and haylofts above.

In the early 1880's the whole site underwent a phase of expansion and alteration with additional storeys added to the western ranges between 1880-3. During this period, a bridge connection between the Provender Store and the Tack Room was also created.

The site was vacated in 1973, and many of the buildings were demolished, tracks lifted, and various parcels of the land were sold into different ownerships. After 1973 the sidings and Camden Lock were used for various light industrial and storage uses including garaging. The market activities started at this time in a rather ad hoc fashion, commencing with small stalls located externally to the stable buildings. Gradually shop units and canopies were added to the buildings and by the 1980s it was a fully-fledged market place.

Note: From Royal Commission on the Historical Monuments of England; Historic Building Report; Former LNWR Stables, Chalk Farm Road, Camden, London NW1; September 1995. Crown Copyright.

## 2.5 Provender Store

This long range backing onto the line of the North London Railway and shallowly curved accordingly was built in 1853-5 as a single storey stabling with hay lofts. In 1880-3 it was raised by the addition of two storeys replacing the hay lofts to provide extended provender storage.



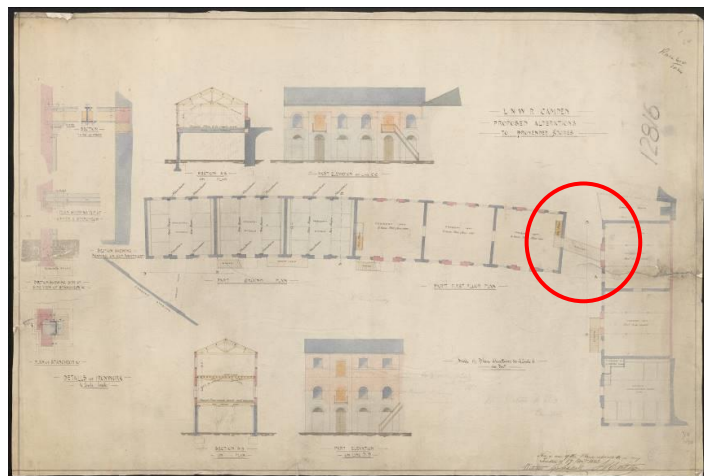
The range is 18 bays long, in yellow stock brick. The ground floor stabling matches that of the rest of stable on site; the round-arched bays open to their heads with semi-circular stable windows grouped as threes with central entrances.

The first floor has round-headed windows, the second floor segmental heads. There were formerly external timber staircases giving access to the pitching holes of the hay lofts.

Near the eastern unit there was a link at first floor level with the gallery of Building A. The east gable end wall has been rebuilt in blue brick. The west gable end had a link to Building D at first floor level; only a beam now remains.

The floors inserted in 1880-3 were evidently of iron construction with trussed beams; timber roof trusses were used. The upper floor and roof appear to have been replaced.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables, Report by Peter Guillery, September 1995. Crown Copyright.



**Figure 4** 1880 plan of Tack Room and Provender Store, showing the bridge.

## 2.6 Tack Room

The east elevation has the same arrangement to the ground floor as the other 1854-5 stabling. Each of the three stables has three arched bays with a central projecting entrance bay and the flanking bays closed up to the round-headed windows, many of which still retain their original wooden frames. The only variation to this design is the additional door opening, for access to the first floor, to the northern stable. The three northernmost bays of the first floor were probably part of the original phase of construction (1854-5) and retain three twelve-pane sash windows and chimney stacks to both the east and north sides. When built the attics to the south had two loading or loophole bays, with flat-heads projecting above the roof line, flanked by short flat-headed windows. These bays were raised to two storeys in 1881, at which time the brickwork was raised to the level of the loophole heads and the windows enlarged, with round heads and radiating glazing bars, two of which still survive. A bridge to building B (Provender Store) was built at this time, extending from the southernmost loading bay, to link the provender stores together. Its position is indicated by a wooden rail and the holes for the former beams below the raised cill of the loading door. On the evidence of the OS maps the bridge was apparently demolished within the last thirty years. The northern bay retains a simple hoist over the head of the door.

The north side, which has arching to the ground floor and two sash windows to the first floor, appears largely altered. The south side has apparently undergone various alterations. An engine room abutted to the south side of the building, apparently built between 1870 and 1880. By the 1970s it had been gone, and a lean-to covering for a vehicular entrance to the railway vaults was in its place; this now also disappeared. An opening on the ground floor, probably not original, has been subsequently blocked. The west side is abutted by the railway vaults and the 1880 bonded warehouse.

Note: From Royal Commission on Historical Monuments of England, Former London & North Western Railway Stables– Historic Building Report, September 1995 NGR No: 2858 8420. Crown Copyright

## 2.7 The bridge

The bridge appears for the first time in the 1880 plan 'L.N.W.R Proposed Alterations to Provender Stores'.

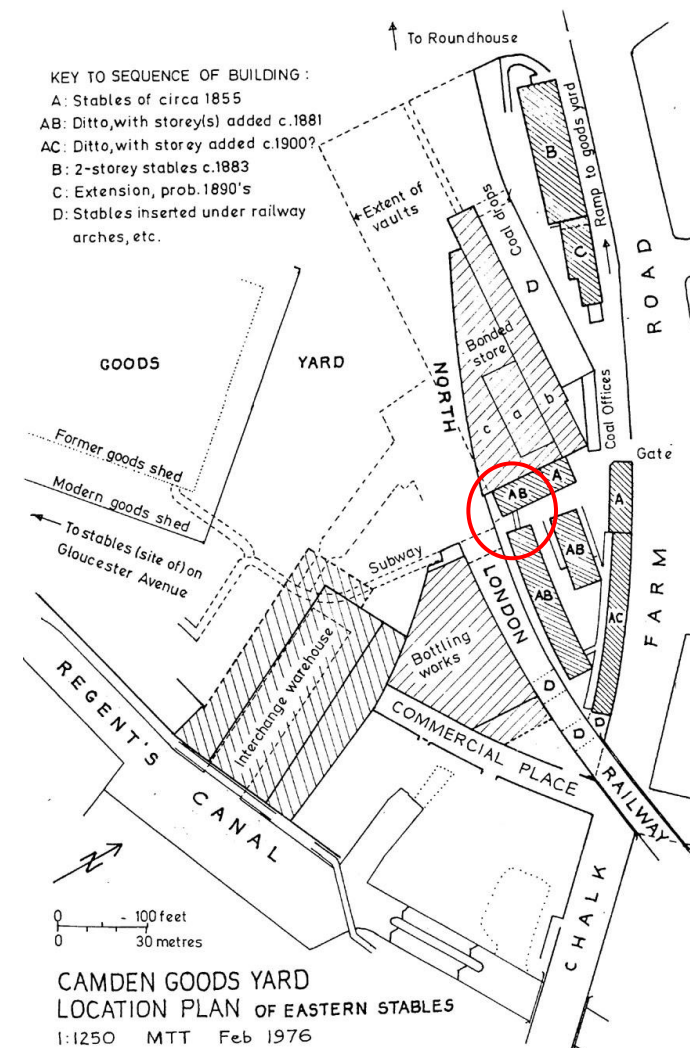
A report by Andrew Saint (English Heritage, 1991), describes the Tack Room as follows:

*'The upper storey is contemporary with the upper storeys of Block B, in other works it was added in November 1880. It was joined to Block B by a bridge at first floor level, which has since disappeared'*.

Based on the above, the photographs taken in 1975, and the drawings by Malcolm T. Tucker dated 1976, it can be assumed that the bridge was dismantled between 1976/77 and 1990/91.



**Figure 5** Close up of the 1880 plan of Tack Room showing the bridge connection between the Provender Store and the Tack Room



**Figure 6** Site plan by Malcolm T. Tucker from 1976 showing the now missing bridge and the now demolished Bonded Wine Store.



**Figure 7** 1975 photograph showing the bridge.

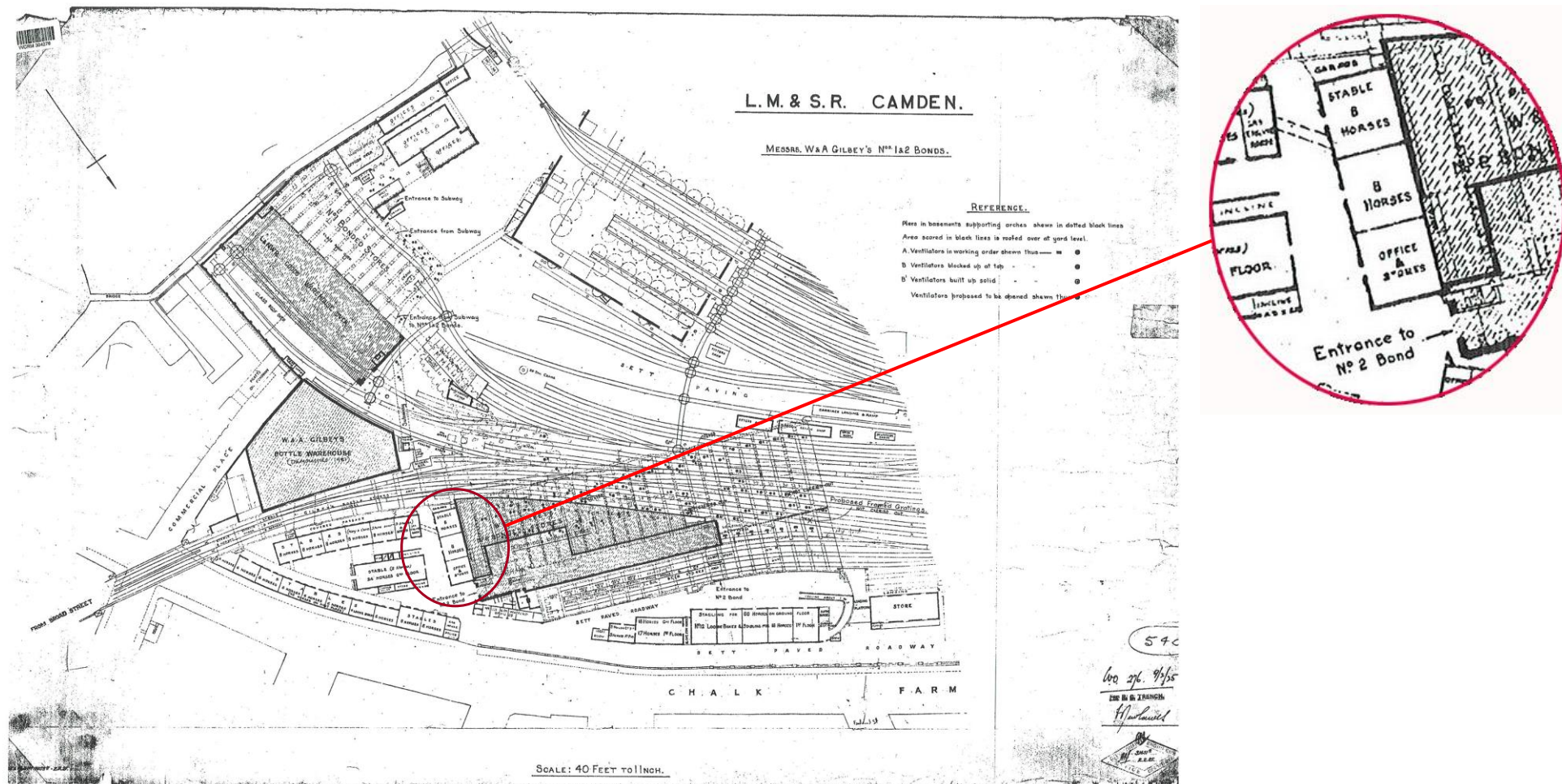


Figure 8 Stables Market – Site plan dated 9th February 1925.

### 3 ASSESSMENT OF SIGNIFICANCE

#### 3.1 Assessment of Significance Criteria

As recommended by NPPF (March 2012) proposals for the alteration or redevelopment of listed building or buildings within a Conservation Area should be considered and be based on an understanding of the site's significance.

Paragraph 128 of NPPF states that '*In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should also be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance*'.

The criteria for the assessment of significance according to English Heritage's 'Conservation Principles' consists primarily of:

- Evidential Value – relating to the potential of a place to yield primary evidence about past human activity;
- Historical Value – relating to ways in which the present can be connected through a place to past people, events and aspects of life;
- Aesthetic Value – relating to the ways in which people derive sensory and intellectual stimulation from a place;
- Communal Value – relating to the meanings of place for the people who relate to it, and whose collective experience or memory it holds.

#### 3.2 The Provender Store

The special interest of Provender Store in Camden Stables Market is established primarily its evidential, historical and communal value. With three other stable buildings, as a group, it enhances the historical significance of the site.

##### Evidential Value

Its **evidential value is high** due to the surviving features preserving much evidence of its original operation; much of the historic fabric still remains and contributes greatly to the building's special interest and character. Many original windows have been replaced or altered throughout the building, however, this does not affect the legibility of the building's use as a stable and many of the existing replaced windows were there when the building was listed in 1981.

##### Historical Value

It is a grade II listed building and has a number of features that display the historical value of the building: most important of these is the surviving original fabric in addition to the subsequent adaptations of the structure such as the additional floors to provide space for storage and hay lofts. **Historical value is therefore medium to high.**

##### Aesthetic Value

The Provender Store, as well as the whole stables complex, is a utilitarian building purpose-made. There is no artistic 'design ethos' in these buildings; utilitarian buildings are economical: there are no added embellishments, wasted space, nor selection of materials but those easily sourced.

Although Provender Store is an exemplary Mid-Victorian stable building, it has little artistic or architectural merit. Its **aesthetic value is therefore low.**

##### Communal Value

Stables Market listed buildings have a **high communal value** as there is

a strong sense of identity with the place, nurtured by the historical industrial use.

The original purpose of the stables has been largely superseded. There are no possible horse-related activities that may be carried out within the site, and there have not been since the 1950's/60's. The market use, established since the late 1970's/80's, is the reason why this area of Camden achieved worldwide recognition and attracts over 100,000 visitors per week. Furthermore, a poll carried out by NOP World in late 2003, shows that 95% of visitors agreed that the Camden markets are a valuable tourist attraction and 89% agreed the markets are a valuable amenity to the local community as well as visitors (London Borough of Camden, the role the markets play in the vitality & viability of Camden Town, 2006.). The communal values of the site can be associated to the later but firmly established market function, and as proved by the studies and polls carried out since its inception. The fact that the building is still in use brings together past and present common uses by the community, reaffirming the communal value as high.

### 3.3 The Tack Room

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#### Evidential Value

There are many surviving features preserving much evidence of its original operation; much of the historic fabric still remains and contributes greatly to the building's special interest and character. **Evidential value is therefore high.**

#### Historical Value

It is a listed building (Grade II) and has a number of features that display the historical value of the building: most important of these is the surviving original fabric in addition to the subsequent adaptations of the structure to accommodate new uses or changes in demand. **Historical value is evident but eroded.**

#### Aesthetic Value

Although the Tack Room is an exemplary Mid-Victorian stable building, it has little artistic or architectural merit; this is expected for a building of this type. It is of traditional construction with materials including yellow stock brick, timber, metal and slate. As they are common examples of a wide-spread building type, **aesthetic value is therefore low to medium.**

#### Communal Value

There is a strong sense of identity with the place, nurtured by the historical trade and industrial character of the area; and for many years now, the international recognition of the site as one of the most famous markets in London. The Stables Market is therefore considered to have high communal value since it's a vivid representation of the history and development of the area. The fact that the building is still in use brings together past and present common uses by the community. **The Stables Market listed buildings therefore have a high communal value.**

## 4 DESIGN AND ACCESS STATEMENT

### 4.1 Description

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The subject site of the application is the Provender Store west elevation and the Tack Room east elevation, which are visible from the public realm. Between 1880 and 1883 two extra floors were added to the Provender Store and an upper floor to the Tack Room. Since then, both buildings have undergone numerous alterations, including the removal of the bridge.

### 4.2 Layout

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The proposal does not involve changes in the internal layout of either

subject buildings. The circulation layout of the market will however, be enhanced by the improved accessibility between the Provender Store and the Tack Room at first floor level.

#### **4.3 Use**

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The proposal does not involve any changes in the existing use.

#### **4.4 Scale**

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The proposal does not involve any changes in the existing scale.

#### **4.5 Landscape and Context**

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The proposal does not involve any changes in the existing landscape and context.

#### **4.6 Appearance**

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The historical character of the Provender Store and Tack Room's exterior will be enhanced by the proposal. Works will be carried out using traditional materials and techniques in order to restore a historical feature, the bridge. It is therefore considered that the appearance will be improved.

#### **4.7 Access**

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The proposal does not involve any changes in the existing access into the subject buildings, however, additional access between the buildings is created through the reinstatement of the bridge.

#### **4.8 Neighbour Amenity Issues**

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The proposed alterations will have minor impact on daylight reaching the ground floor levels of the Provender Store west elevation and the Tack

Room east elevation. The retail units already use awnings to shelter their items on display, and therefore there will be little difference to light penetration if the proposed bridge is reinstated.

## **5 IMPACT ASSESSMENT OF THE PROPOSALS**

The following paragraphs briefly discuss the potential impact of the proposal, which is subject of this application, on the special interest of the heritage assets: Grade II listed Provender Store (Block B) and Grade II listed Tack Room (Building D) at Stables Market, Camden.

#### **Proposal:**

The subject buildings of this application are Mid-Victorian industrial buildings once used for horse stables and storage. The proposal is for the reinstatement of the bridge connecting the Provender Store to the Tack Room at first floor level. The proposed width of the bridge is 1505mm between balustrade posts and supported by 290x180mm oak carriage beams housed in the existing walls of the subject buildings. Thick oak boards with small gaps between form the deck and soffit boards are used on the underside to conceal a services route connecting the two buildings. The design closely follows the structural principles of the original bridge but modified on account of its now intended pedestrian use. The open balustrade of the original bridge is a characteristic clearly not acceptable for the new bridge and so an oak balustrade of vertical boards with gaps between is proposed. This allows for a holistic and contiguous use of timber rather than an uneasy mix of different materials.

In place of the existing window on the Provender Store west elevation, which was subsequently reduced once the bridge was removed, there will be a new framed ledged and braced timber double door that leads onto the bridge. The existing loading door in the Tack Room, currently not in use, will also be replaced by a new framed ledged and braced

timber double door. Further to this proposal, there is a small contemporary window adjacent to the existing window in the Provender Store west elevation. This will be replaced with a traditional timber two light casement window.

**Impact:**

The Stables Market attracts both local and international visitors for more than just shopping; there are other vital elements which contribute to the success of the market. Its economic vibrancy arises from a combination of the special historic character of the site and the wide range of attractions and entertainment on offer. This combination gives the Stables Market a unique edge: it is the reason it differs from other markets in London and the UK. In addition, it is one of the few markets that combines both stalls and individual shops, contributing to the fluidity of the layout and intertwining indoor and outdoor spaces.

The proposed bridge and alterations to the openings on the Provender west elevation and loading door in the Tack Room east elevation are visible from public realm. It is considered that the proposal will enhance the buildings' historic character and appearance while also improving the interrelationship between buildings and indoor/outdoor spaces, thus having an overall positive impact.

**6 NPPF CONSIDERATIONS**

This section discusses the impact of the proposals according to the National Planning Policy Framework (NPPF). The NPPF contains a presumption in favour of sustainable development that meets the objectives and policies of the NPPF (which has the conservation of heritage assets as one of its 12 core principles).

The policies contained within the NPPF seek to attain the Government's aim of achieving sustainable development.

Paragraph 6 of the NPPF states: *'The purpose of the planning system is to contribute to the achievement of sustainable development. The policies in paragraphs 18 to 219, taken as a whole, constitute the Government's view of what sustainable development in England means in practice for the planning system.'* The entire Framework constitutes a definition of "sustainable development" with no one part assuming greater weight than any other.

*'Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy.'* (NPPF Paragraph 7).

This statement deals principally with Section 12 of the NPPF, "Conserving and enhancing the historic environment", however Heritage considerations and issues are prevalent throughout the framework.

NPPF Paragraph 128 states that *'In determining applications local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contributions made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on that significance'*.

As recommended in Paragraph 128, an assessment of the significance of the Provender Store and the Tack Room has been provided in Chapter 3: Assessment of Significance. To identify the heritage values and character of the heritage assets an appropriate and proportionate historic research and analysis was undertaken to include the origins of the buildings and how they have changed over time.

This report aims at fulfilling the National Policy requirement for provision of proportionate information which will enable the assessment of the likely impacts of proposed development on the special historic and architectural interest of the Provender Store and the Tack Room by the Local Planning Authority. The significance assessment was based on historic research and consultation of relevant historic records and was examined according to the criteria set out in English Heritage's



‘Conservation Principles’.

‘Conservation’ is defined in the NPPF only for heritage policy as: *‘the process of maintaining and managing change to a heritage asset in a way that sustains and, where appropriate, enhances its significance’*.

By reinstating the 1880’s timber bridge at first floor level, the buildings’ significance is enhanced. The proposal will aid in safeguarding the buildings’ continued use and long term conservation. It is therefore argued that the significance of the heritage assets will be preserved and enhanced in compliance with the NPPF requirements.

## 7 LOCAL PLANNING POLICY

Camden Local Development Framework, Camden Core Strategy, 2010 - 2025, Adopted Version, November 2010

The Camden Core Strategy Policy CS14 – ‘Promoting high quality places and conserving our heritage’, sets out the requirements to safeguard Camden’s heritage. The overall strategy is to sustainably manage growth in Camden in a way that conserves and enhances the heritage and valued places that give the borough its unique character.

Camden Planning Guidance provides advice and information on how the Local Authority applies its planning policies. The guidance is consistent with the Core Strategy and the Development Policies, and forms a Supplementary Planning Document (SPD) which is an additional “material consideration” in planning decisions.

CPG 1- Design deals with heritage issues in Section 3. This section sets out further guidance on Core Strategy Policy CS14 Promoting high quality places and conserving our heritage and Development Policy DP25 Conserving Camden’s Heritage.

Paragraph 3.22 refers to the statutory requirement, when assessing applications for listed building consent, to have special regard to the desirability of preserving the building or its setting or any features of

special architectural or historic interest which it possesses.

It is considered that the proposal not only preserves but enhances the character of the historic environment through the reinstatement of historic features, namely the bridge and openings of the Provender Store and the Tack Room, which retain and enhance the elements of significance within and in the vicinity of the site.

Paragraph 3.23 expands on the desirability to retain original or historic features and to carry out repairs in matching materials. The proposals should seek to respond to the special historic and architectural constraints of the listed buildings, rather than significantly change them.

As stated earlier in this report, the proposal actively seeks to restore historic features. The works will be carried out using traditional materials and techniques in order to maintain and enhance the integrity of the listed buildings.

## 8 CONCLUSION

The Provender Store and the Tack Room are listed Grade II buildings that are both on the Heritage at Risk Register. The listed buildings are open to the public and in a continuous state of flux. They have been subject to change and in order to remain sustainable, welcoming and pleasing, they will continue to change. The proposed scheme is driven by a need to not only conserve and enhance the buildings, but also to ensure a sustainable solution that safeguards their current and future use within the market.

The proposed works aim to reinstate the bridge, originally added in the 1880’s, thereby enhancing the listed buildings’ historical character. Furthermore, improving the circulation between buildings will benefit the market function while adding to the architectural detailing, therefore safeguarding their long term conservation.

This proposal complies with policy at the heart of NPPF in respect of sustaining and enhancing not just the historic fabric but the significance

of the Regent's Canal Conservation Area and the heritage assets within and in the vicinity of the site.

The NPPF contains an express presumption in favour of sustainable development which should be taken into account when making planning decisions. Change is at the heart of sustainable development. The three dimensions of sustainability: economic, social and environmental, are not static; neither is the built environment. Buildings need to change in order to adapt to climate change and move towards a low carbon economy (NPPF Para 7).

In light of the above, it is considered that the relevant criteria policies of NPPF have all been complied with, and thereby must also satisfy the relevant local plan policies.

For the above reasons, it is considered that the proposal would be acceptable in the context of the setting of the heritage assets in the immediate surroundings and the conservation area. It is therefore concluded that the proposed works satisfy the relevant clauses of the NPPF as detailed above and is consistent with the spirit of the local policies and national conservation principles and therefore ought to be granted Listed Building Consent and Planning Permission, with conditions as appropriate.

## APPENDIX I LISTED BUILDING DESCRIPTION

### List entry Summary

List entry Number: 1258101

Grade: II

Four blocks of industrial stabling, now workshops and warehousing. c1855-1870, with later Victorian additions. For the London and North-Western Railway Company's Camden Goods Yard. Stock brick, with hipped slate roofs, some stone lintels. Some iron columns internally, but floors and roofs generally of timber. EXTERIOR: mostly of 2 storeys.

Northernmost block (A) abutting on Chalk Farm Road, c1855, with upper storey of c1895. Long curved front road, mostly of 2 storeys with eaves cornice but western end of one and a half storeys, somewhat altered, with chimney on roof. Round-headed half windows for stabling on ground storey, segment-headed industrial windows in upper storey (eastern end only). Elevation towards yard irregular. Eastern portion has cantilevered open balcony at first-floor level retaining some concrete horse troughs and connected by bridge to Block B and separately to ramp on Block C. Ceilings of ground storey have jack-arch iron and brick construction. INTERIOR of upper storey has separate compartments and paving for horses but no stalls.

**Block B immediately to north of North London railway line. 3 storeys. Ground storey c1868, originally provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A.**

Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper provender store, with round-headed half-windows for stabling on long elevations; upper storeys c1881, with round-headed windows at first-floor level and segment-headed windows above. Tiers of loft openings for hoists, much altered and renewed. Connected by narrow bridge at east end to Block A. Block C between Blocks A and B and parallel to Block B. 2 storeys. Ground storey c1868, upper storey 1881; horse ramp on north side of block c1895, connected with balcony on Block A. Round-headed windows on ground storey, segment-headed industrial windows above on both north and south sides. South side formerly had another horse ramp, of 1881, and covered bridge connecting with Block B, demolished in 1980s. Ground storey has iron and brick jack-arch construction and iron stanchions against walls stamped 'Norton and Son Darlaston'.

Block D at right-angles and to west of Blocks B and C. 2 storeys. Ground storey c1868, upper storey c1881. Main elevation faces eastwards, with return northwards. Round-headed half-windows for stabling at ground level, some segment-headed sash windows above. Tall brick chimneys. INTERIOR with original timber benching, one timber partition and some harness hooks. Said to have been formerly the Tack Room for the stabling. Formerly connected by a bridge at south end to Block B. Included as a rare example of substantial industrial stabling and a major surviving portion of the former Camden Goods Yard. Forms a group with the 'Horse Hospital' to north-west (qv) and with further remnants of stabling and warehouses west of Block D (qv). A tunnel (now blocked) south of the North London line connects the complex with further LNWR buildings and the Regent's Canal south of the North London Line.

Listing NGR: TQ2862684201