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6 JOHN STREET, WC1  
DESIGN AND ACCESS STATEMENT

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# CONTACTS

## SITE ADDRESS

6 John Street  
London, WC1N 2ES

## CONTACTS

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# 1.0 INTRODUCTION

DESIGN AND ACCESS STATEMENT

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# 1.1 PROJECT INTRODUCTION

## 1.1 PROJECT INTRODUCTION

This design and access statement, associated drawings and information is in support of the following re-development proposals at 6 John Street, London, WC1N 2ES:

- Conversion of existing D1 construction training centre to C3 residential use.
- Internal subdivision of the property into 7 residential units (5 x 2 bed units and 2 x 1 bed units) with associated communal and service spaces.
- Internal alterations, and rebuilding the rear facade to suit the new use and to improve the building's impact on the conservation area.
- Demolition of the existing, rear two-storey flat roof extension to form amenity space between 6 John Street and the rear adjoining consented residential scheme to King's Mews.



EXISTING JOHN STREET CONTEXT

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# 2.0 DRAWINGS

DESIGN AND ACCESS STATEMENT

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# 2.1 DRAWING LIST

NUMBER	TITLE	SCALE / SIZE
<b>SITE PLAN</b>		
798 001	Site Plan	1250/
<b>EXISTING PLANS</b>		
798 100	Site Location Plan	500/A
798 101	Lower Ground Floor	50/A1
798 102	Ground Floor	50/A1
798 103	First Floor	50/A1
798 104	Second Floor	50/A1
798 105	Third Floor	50/A1
798 106	Fourth Floor	50/A1
798 107	Roof Plan	50/A1
<b>EXISTING SECTIONS</b>		
798 111	Section AA	50/A1
798 112	Section BB	50/A1
798 113	Section CC	50/A1
<b>EXISTING ELEVATIONS</b>		
798 123	Rear Elevation	50/A1
798 124	Existing Front Elevation	50/A1
<b>DEMOLITION DRAWINGS</b>		
798 151	Lower Ground Floor	50/A1
798 152	Ground Floor	50/A1
798 153	First Floor	50/A1
798 154	Second Floor	50/A1
798 155	Third Floor	50/A1
798 156	Fourth Floor	50/A1
798 157	Roof Plan	50/A1
798 161	Section AA	50/A1
798 162	Section BB	50/A1
798 173	Rear Elevation	50/A1
798 174	Existing Front Elevation	50/A1
<b>PROPOSED PLANS</b>		
798 200	Site Location Plan	50/A1
798 201	Lower Ground Floor	50/A1
798 202	Ground Floor	50/A1
798 203	First Floor	50/A1
798 204	Second Floor	50/A1
798 205	Third Floor	50/A1
798 206	Fourth Floor	50/A1
798 207	Roof Plan	50/A1
<b>PROPOSED SECTIONS</b>		
798 211	Section AA	50/A1
798 212	Section BB	50/A1
798 213	Section CC	50/A1
<b>PROPOSED ELEVATIONS</b>		
798 223	Rear Elevation	50/A1
798 224	Front Elevation	50/A1
<b>DETAILS</b>		
798 401	Window detail	10/A3

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# 3.0 SITE INFORMATION

DESIGN AND ACCESS STATEMENT

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# 3.1 SITE INTRODUCTION

## 3.1 SITE INTRODUCTION

The site is at 6 John Street, and is located in the Bloomsbury Conservation Area within the London Borough of Camden. The orientation of the site is on a west-east axis (front to back), perpendicular to John Street. John Street is part of a grid pattern of streets, which are essentially orientated on a north-south axis between Guilford Street and Theobald's Road.

The building sits mid-terrace in a run of properties built as houses from 1754-1759. They are generally of four storeys plus basement, whereas 6 John Street has a further storey as a slate mansard to the street. The run of 2-9 John Street is Grade II listed. The property also includes an existing two storey rear extension that currently spans the rear boundary between 6 John Street and the rear King's Mews site.

Due to severe bomb damage during WWII the entire building was substantially rebuilt including a complete facade rebuild, with an internal concrete frame and no internal historic fabric or decoration. This facade of 6 John Street sits well in its context, harmonising with the adjacent Georgian façades. The continuity of the streetscape created by this rebuilt London stock brickwork facade is referred to in the property's Grade II listing with English Heritage. The existing rear facade was constructed in the 1980's and due to its unremarkable and unattractive design along with industrial scale chimney and windows, it has a negative impact on the conservation area.

The property is currently used as a construction training centre, where students would undertake practical and theoretical training in various building trades.

Existing front windows are to be retained and refurbished where possible and replaced with matching profile windows where necessary.

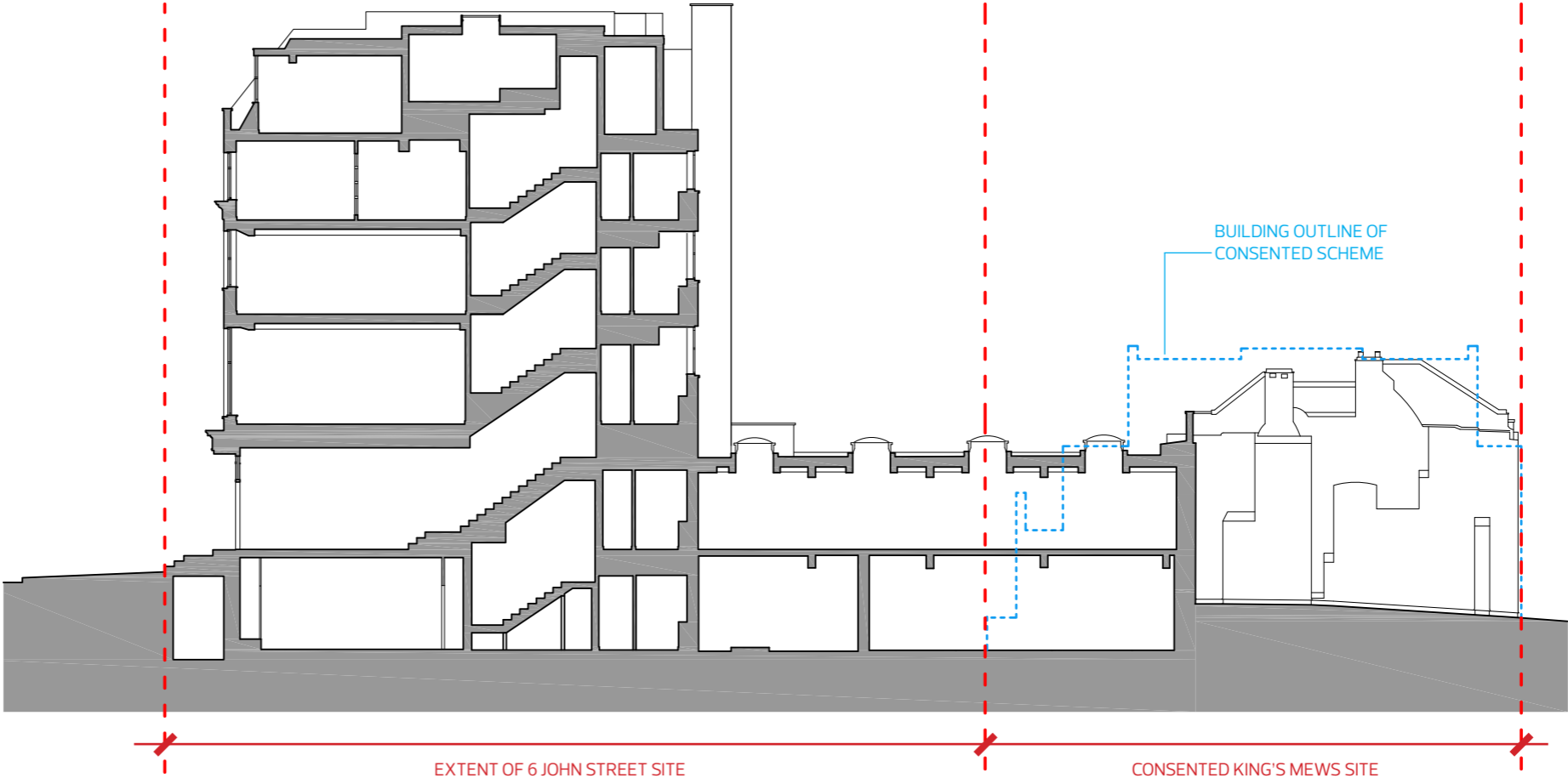


AERIAL VIEW SITE LOCATION

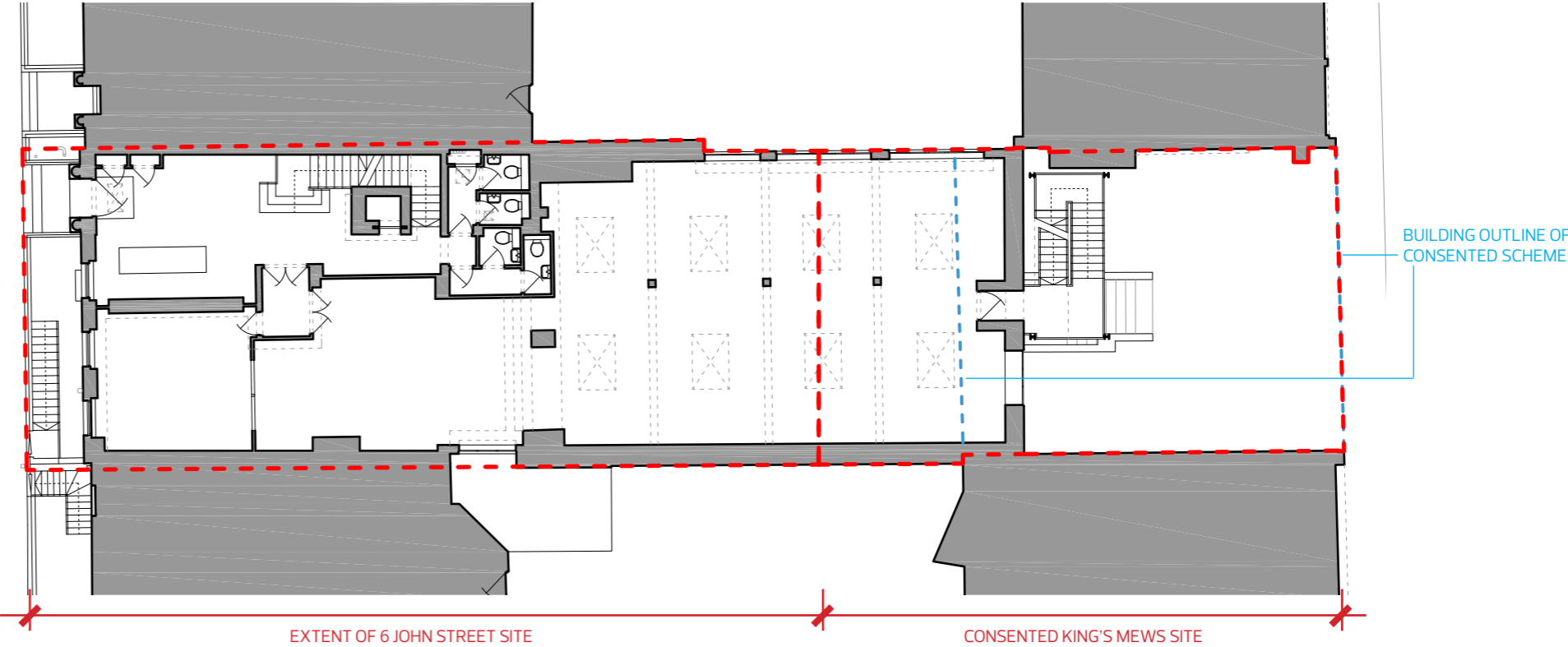
# 3.2 ADJOINING APPLICATION

## 3.2 ADJOINING APPLICATION

This planning application takes the adjoining consented King's Mews scheme (2012/6315/P) into consideration as valid. Further to the proposed demolition in this consented scheme this application proposes further demolition to the 6 John Street rear LGF / GF extension.



EXISTING SECTION SHOWING EXTENT OF SITE AND REAR ADJOINING CONSENTED APPLICATION



EXISTING GF PLAN HIGHLIGHTING SITE BOUNDARY AND REAR ADJOINING CONSENTED APPLICATION

# 3.3 SITE PHYSICAL CONTEXT



SIDE VIEW FROM JOHN STREET SHOWING MID-TERRACE LISTED FACADE

# 3.3 SITE PHYSICAL CONTEXT



PANORAMIC STREETScape VIEW FROM JOHN STREET

# 3.3 SITE PHYSICAL CONTEXT



PANORAMIC VIEW OF REAR OF JOHN STREET TERRACE

## 3.3 SITE PHYSICAL CONTEXT



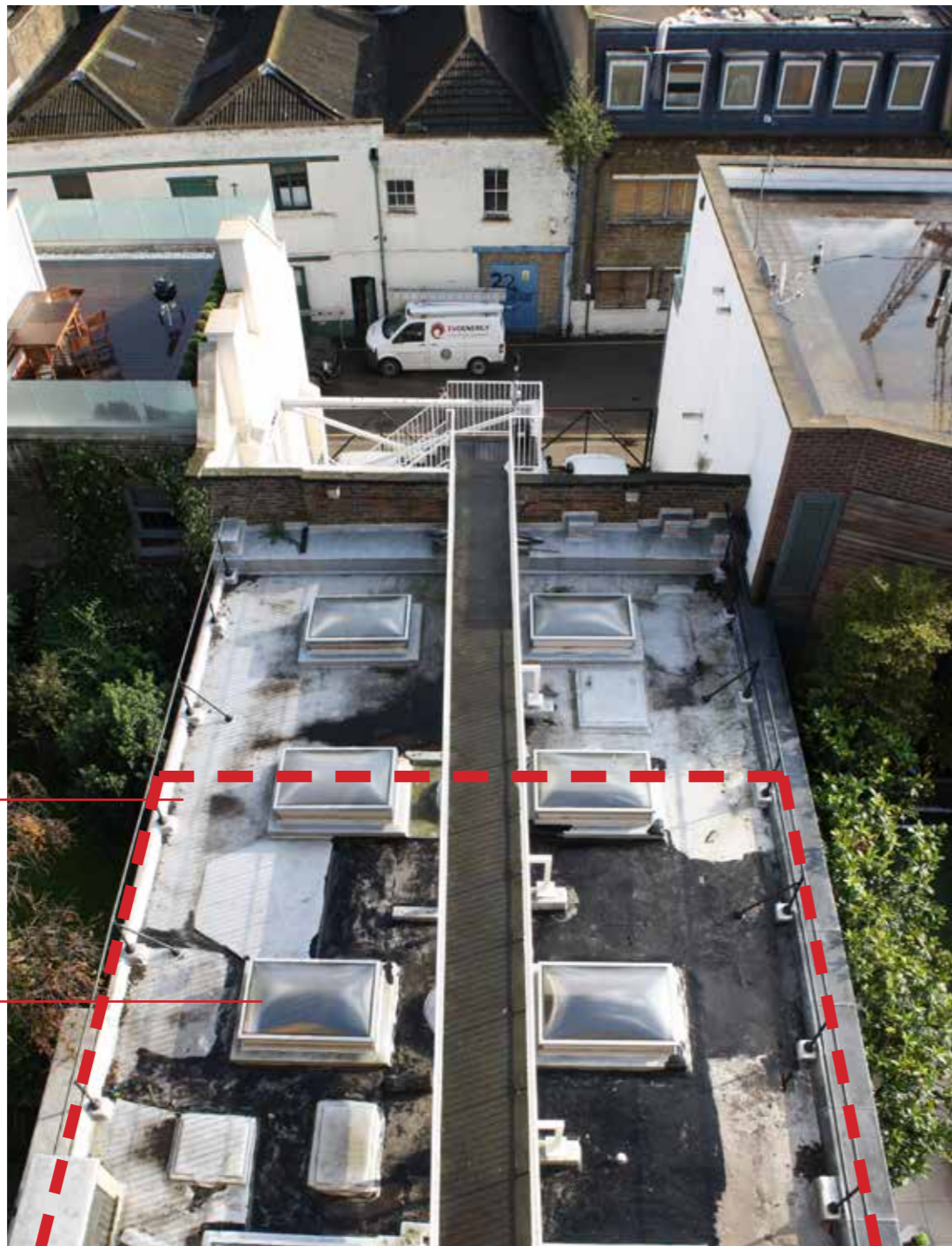
VIEW OF REAR AMENITY SPACES / GARDENS FROM EXTENSION ROOF (SOUTH FACING)

### 3.3 SITE PHYSICAL CONTEXT

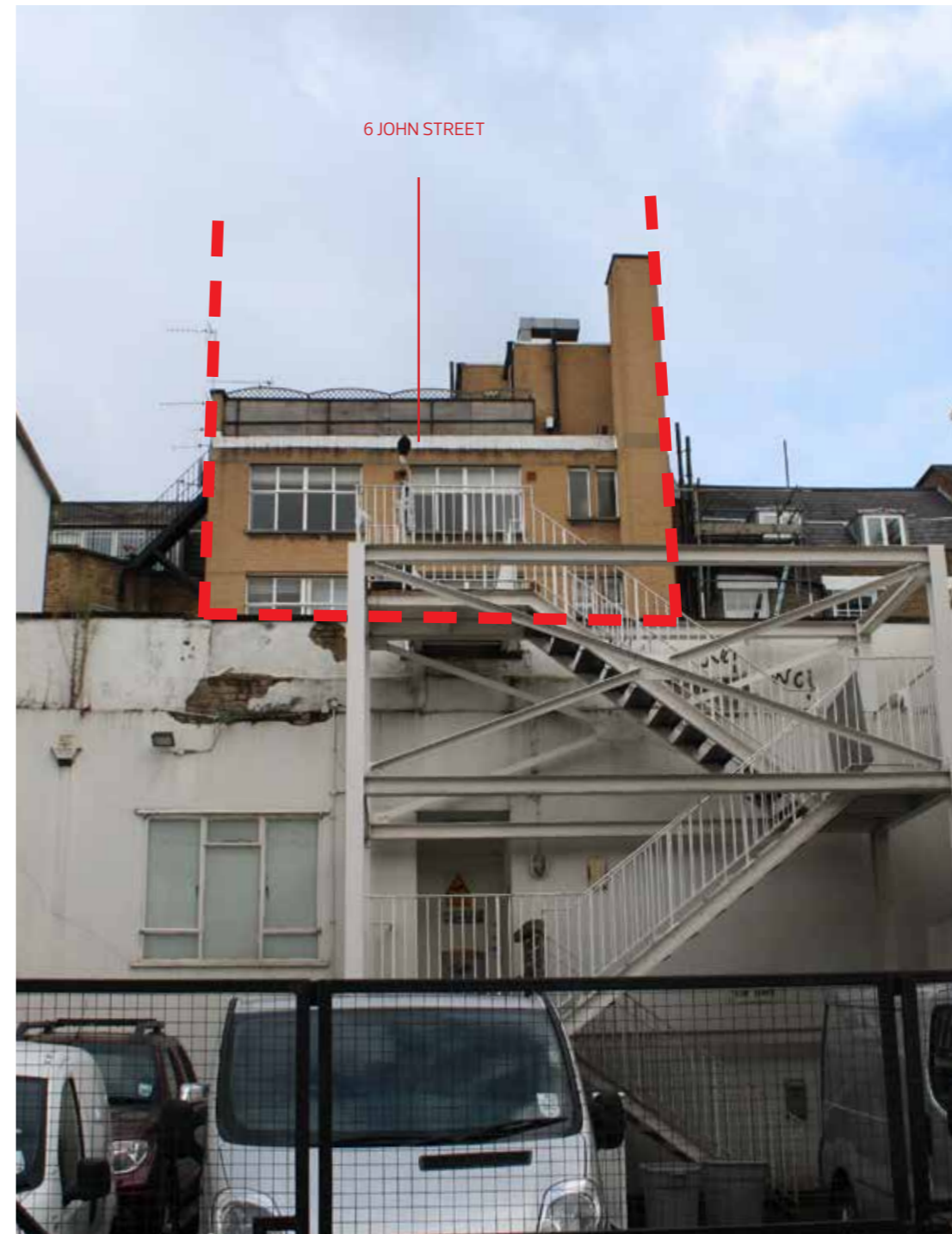


VIEW OF REAR AMENITY SPACES / GARDENS FROM EXTENSION ROOF (NORTH FACING)

### 3.3 SITE PHYSICAL CONTEXT



VIEW FROM 4F OF REAR EXTENSION HIGHLIGHTING SITE DIVISION BOUNDARY



VIEW OF 6 JOHN STREET REAR FACADE FROM KING'S MEWS



### 3.4 EXISTING BUILDING USE

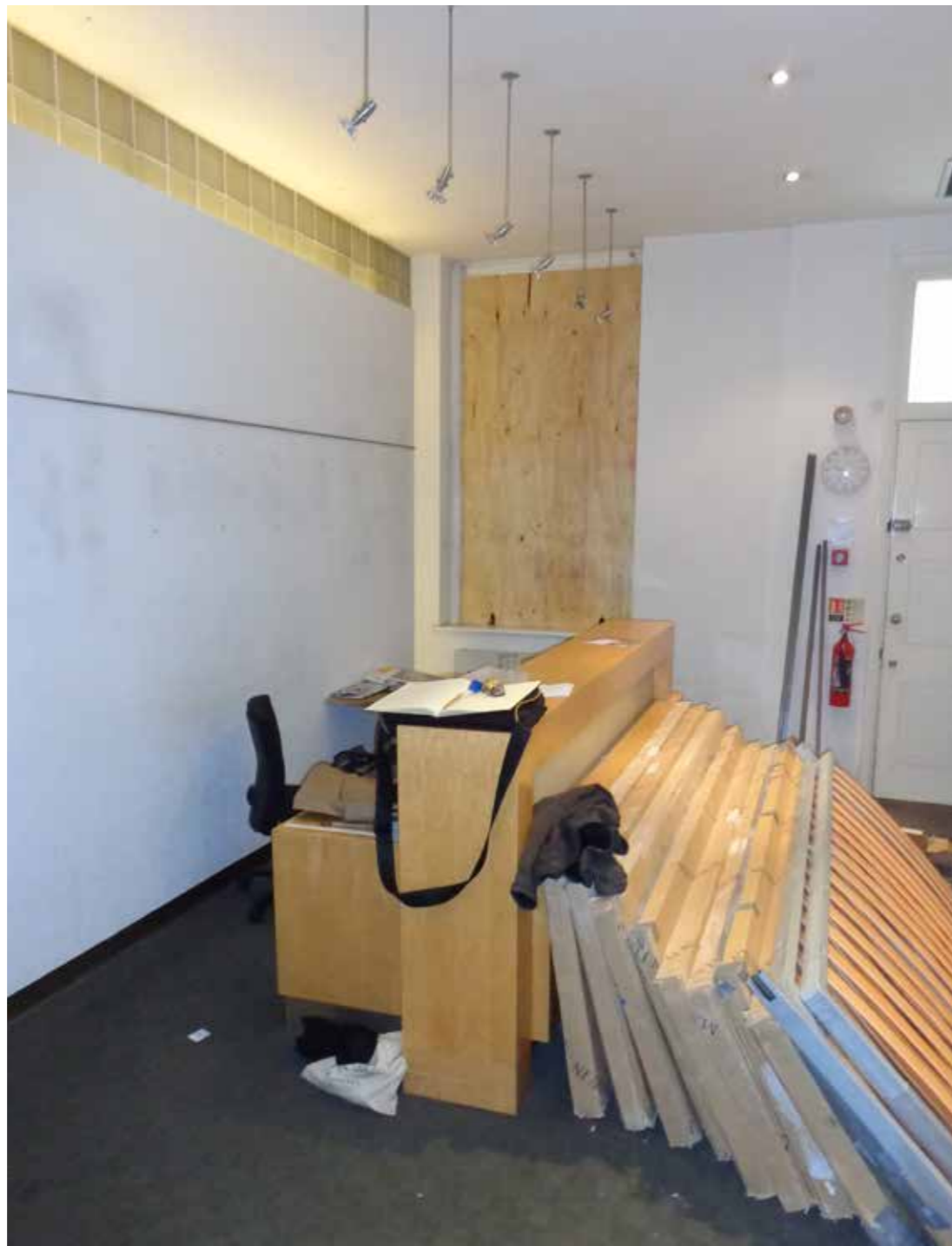


EXISTING LGF - CONSTRUCTION TRAINING AREAS



EXISTING LGF - HEAVY DUTY WORKSHOP

## 3.4 EXISTING BUILDING USE

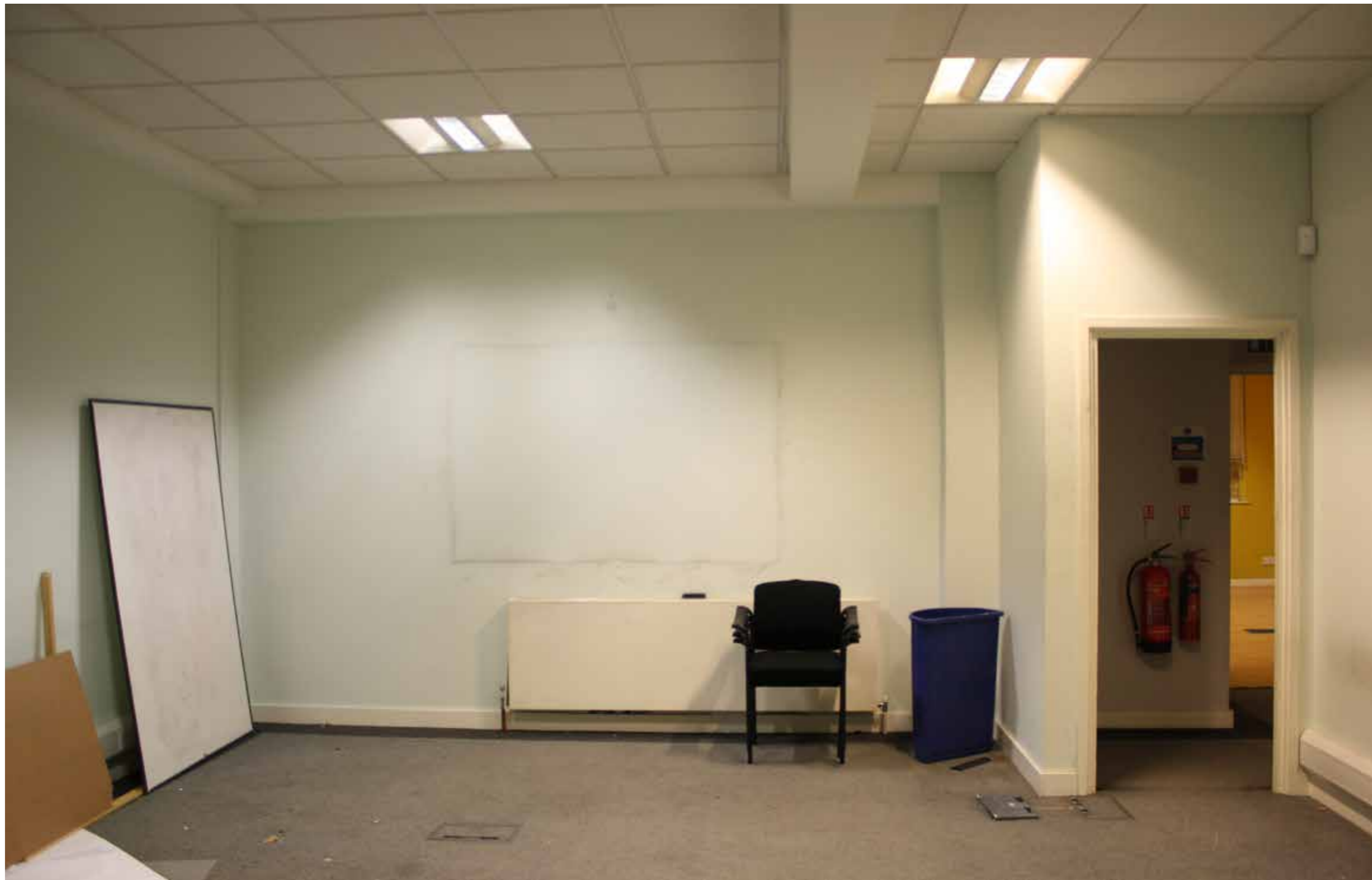


EXISTING GF - LOBBY ENTRANCE AREA



EXISTING GF - REAR OFFICES AND STUDENT LOUNGE

## 3.4 EXISTING BUILDING USE



EXISTING 1F - STUDENT CLASS ROOMS

## 3.4 EXISTING BUILDING USE



EXISTING 2F - STUDENT COMPUTER TESTING CENTRE AND OFFICES

## 3.4 EXISTING BUILDING USE



EXISTING 3F - STUDENT / TUTOR MEETING AREAS



EXISTING 3F - TUTOR OFFICES

## 3.4 EXISTING BUILDING USE



EXISTING 4F - APARTMENT KITCHEN AREA



EXISTING 3F - APARTMENT ROOM TO FRONT

## 3.4 EXISTING BUILDING USE



EXISTING 4F - EXTERNAL BALCONY SPACE

# 3.5 TRANSPORT AND LOCAL AMENITY

## 3.5 TRANSPORT AND LOCAL AMENITY

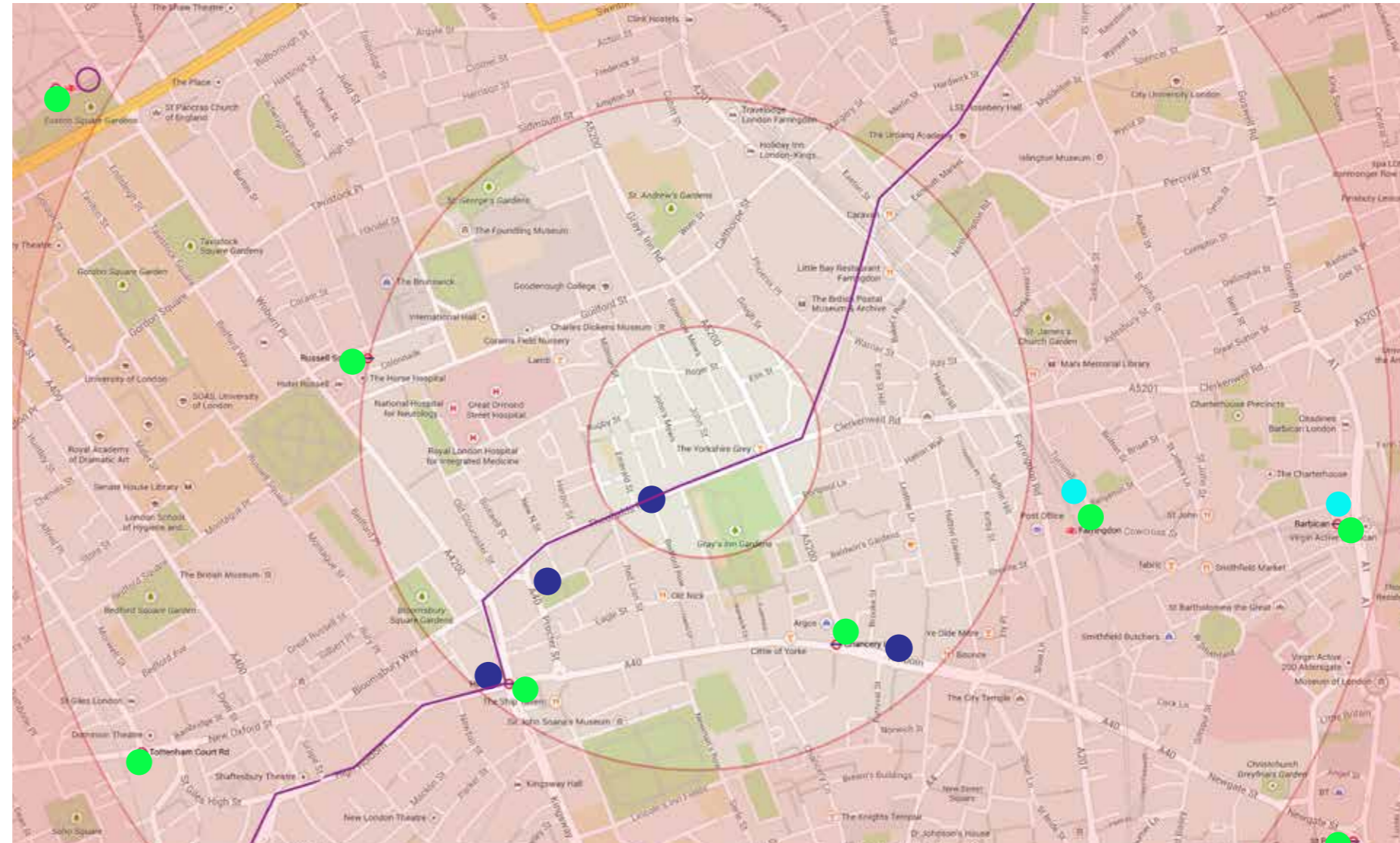
The application site has excellent access to nearby transport links. It is noted in Camden's Core Strategy CS11 that as part of its approach to minimising congestion and addressing the environmental impacts of travel, the Council will look favourably on developments that minimise the provision for private car-parking and, in particular through 'car-free' developments in the borough's most accessible locations.

Within 5 minutes walking distance are Holborn and Chancery Lane underground tube stations. Euston and Kings Cross are only 15 minutes walk away for National Rail connections – Farringdon, and City stations provide Network Rail Thameslink connections.

Just moments walk from the site are multiple main bus routes, that include routes 19, 38, 55, 243, N19, N35, N38, N41.

The access to excellent public transport links makes the development ideal for residential use and relieves need for vehicle ownership.

The site is also located a couple of minutes walk from nearby Gray's Inn Gardens – an ideal amenity space for families.



LOCAL MAP WITH TRANSPORT LINKS

- TFL CYCLE POINTS ●
- LONDON UNDERGROUND STATIONS ●
- NATIONAL RAIL STATIONS ●
- BUS STOP ROUTES —



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# 4.0 PLANNING

DESIGN AND ACCESS STATEMENT

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## 4.0 PLANNING

Please refer to Planning Statement by Indigo Planning

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# 5.0 PROPOSALS

DESIGN AND ACCESS STATEMENT

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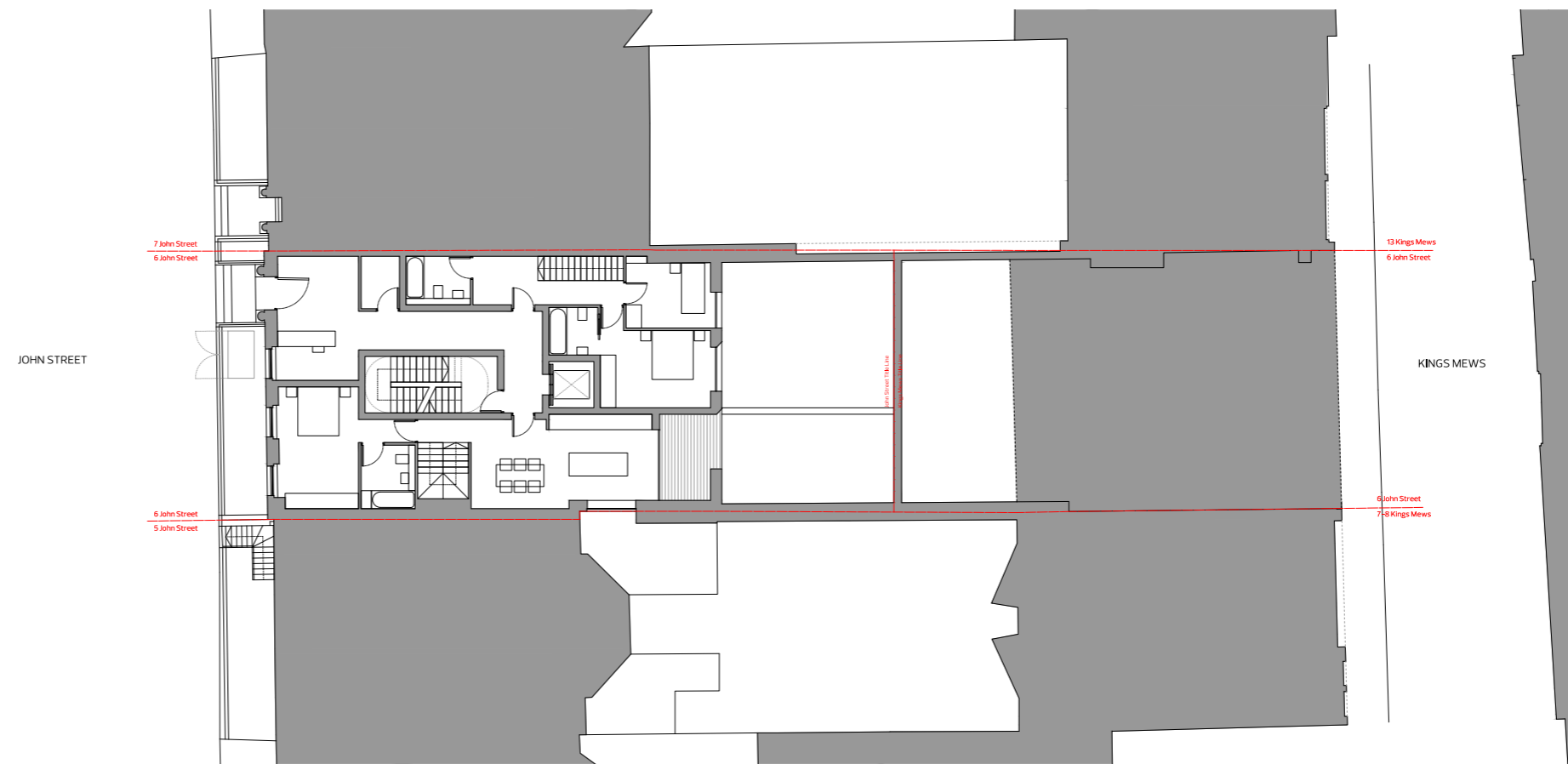
# 5.1 DESIGN OVERVIEW

## 5.1 DESIGN OVERVIEW

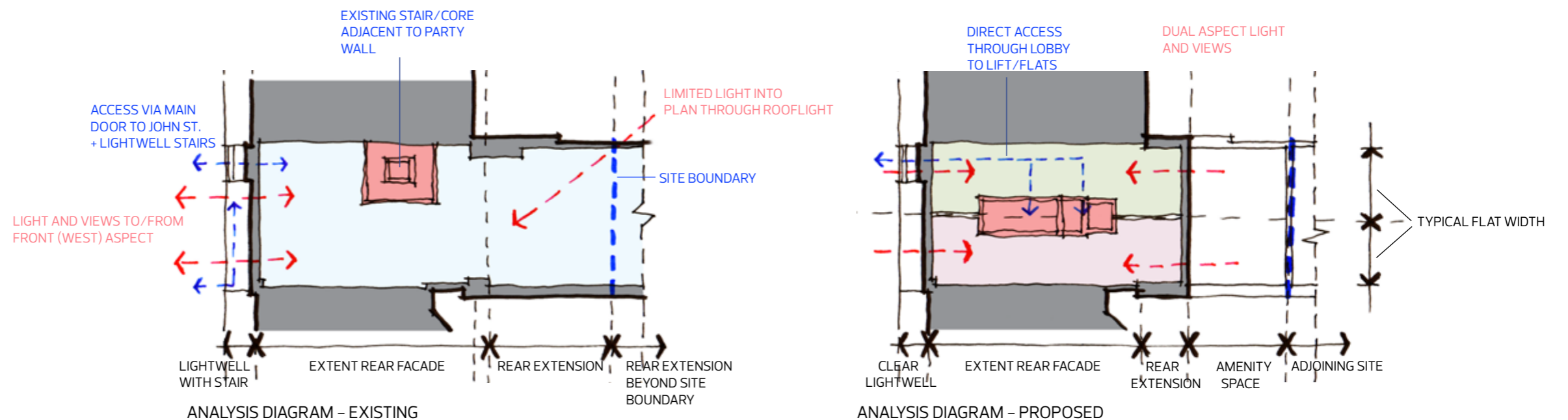
The site is at 6 John Street, and is located in the Bloomsbury Conservation Area within the London Borough of Camden. The orientation of the site is on a west-east axis (front to back), perpendicular to John Street. John Street is part of a grid pattern of streets, which are essentially orientated on a north-south axis between Guilford Street and Theobald's Road.

The building sits mid-terrace in a run of properties built as houses from 1754-1759. They are generally of four storeys plus basement, whereas 6 John Street has a further storey as a slate mansard to the street. The run of 2-9 John Street is Grade II listed. The property also includes an existing two storey rear extension that currently spans the rear boundary between 6 John Street and the rear King's Mews site.

The existing massing on the top floor of 6 John Street is uneven, with various blocks to the north and a roof terrace to the south. It is proposed to simplify the massing in this area, with a continuous roof terrace along the length of this elevation, forming a set back to the line of the top floor behind.



GROUND FLOOR PLAN OF PROPOSED SCHEME



ANALYSIS DIAGRAM - EXISTING

ANALYSIS DIAGRAM - PROPOSED

## 5.2 AREAS

The development proposes a range of housing in terms of size and type. The housing will comprise accommodation units to meet Camden's housing needs.

- Amenity Space

Amenity space has been provided to each flat that exceeds the London Housing Design Guide.

Total private amenity space(s) will be on average 16% of total flat area (external and internal) and total amenity space is 122.3m<sup>2</sup>.

### Flat Areas

Level	Flat No.	No. Beds / Person	GIA		Amenity	
			sqm	sqft	sqm	sqft
LGF / GF	FLAT 1	2/3	89.9	966.4	38	408.5
LGF / GF	FLAT 2	2 / 4	122.3	1314.7	30.6	329.0
1	FLAT 3	2/4	116.4	1251.3	24.5	263.4
2	FLAT 4	1 / 2	52.4	563.3	7.2	77.4
2	FLAT 5	1 / 2	50	537.5	6.8	73.1
3 / 4	FLAT 6	2 / 4	106.5	1144.9	7.9	84.9
3 / 4	FLAT 7	2 / 4	104.1	1119.1	7.3	78.5
<b>Total</b>			<b>641.6</b>	<b>6897.2</b>	<b>122.3</b>	<b>1314.7</b>

### Building Area

Level	GIA	
	TOTAL SQM	TOTAL SQFT
LGF	165.1	1774.8
GF	163.2	1754.4
1	140.7	1512.5
2	142.6	1533.0
3	140.2	1507.2
4	107.3	1153.5
<b>Total</b>	<b>859.1</b>	<b>9235.3</b>

# 5.3 DEMOLITION PROPOSALS

## 5.3 DEMOLITION PROPOSALS

The listed facade and concrete structural frame of the property shall be retained, with the internal partition walls, existing stair/service core, rear elevation and rear extension to be demolished. A notable element also to be demolished on the rear elevation is the unsightly industrial scale chimney (previously used to vent from the workshop areas). The existing dilapidated slate mansard roof to the roof terrace at fourth floor level will also be demolished.

As the proposed residential units are provided with a protected fire route in the proposed core, the external steel fire stair from the fourth floor terrace to 5 John Street terrace is to be removed as it has now become redundant.

Existing sash windows to the front facade will only be replaced where absolutely necessary and in such a situation, counter balanced sashes with 3mm conservation glass, putty finish and glazing bars will be installed.

For detailed demolition proposals please see drawing numbers:

- Plans - 798\_151
- 798\_152
- 798\_153
- 798\_154
- 798\_155
- 798\_156
- 798\_157
- Sections- 798\_161
- 798\_162
- Elevations- 798\_171
- 798\_172



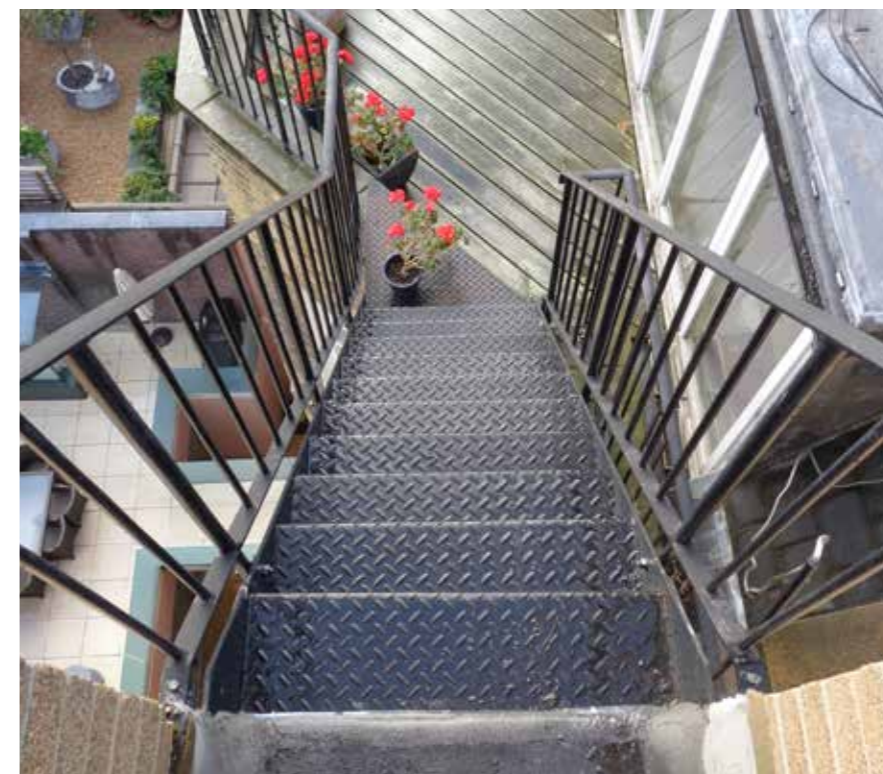
INDUSTRIAL CHIMNEY TO REAR



DILAPIDATED REAR MANSARD



TIMBER SASH WINDOWS TO FRONT IN POOR CONDITION



REAR STEEL ESCAPE STAIRS TO 5 JOHN STREET

# 5.4 DESIGN DEVELOPMENT

## 5.4.1 HOUSING MIX

The mix of units relates to the local demand and the constraints of the existing building. Camden development policy DM5 notes that in the market sector, there is a very high demand for 2 bedroom units, a medium demand for three bedroom units, and a lower demand for 1 bedroom units.

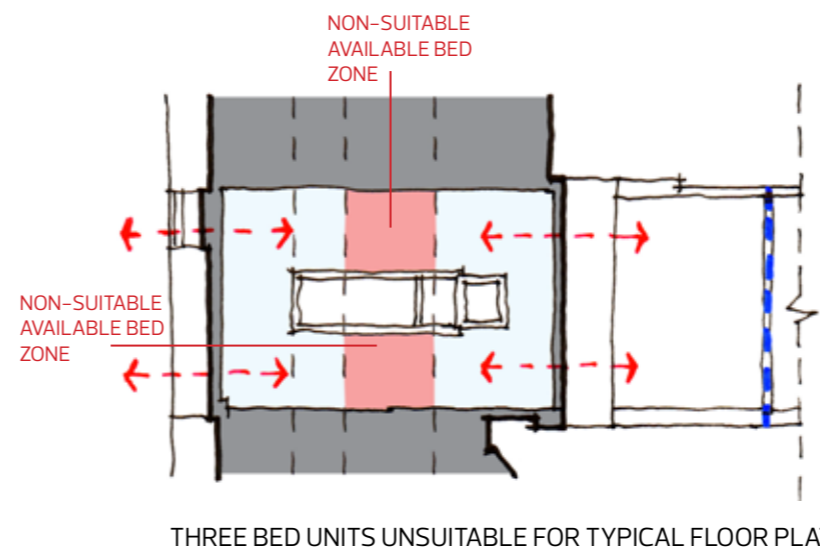
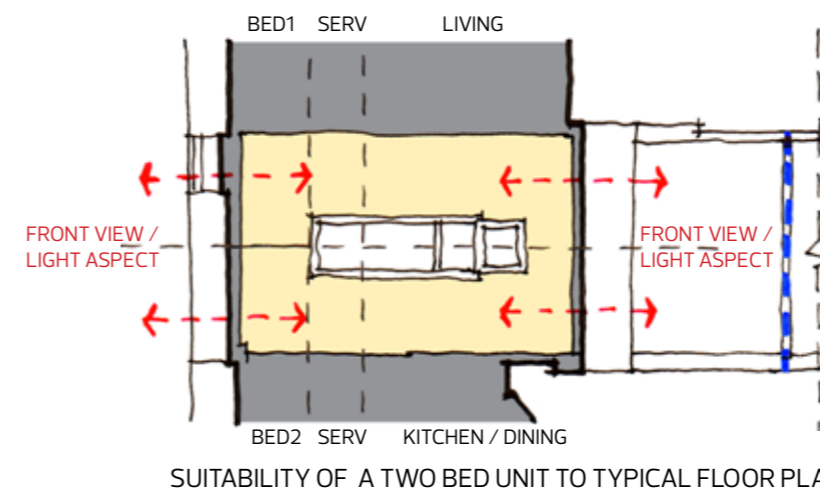
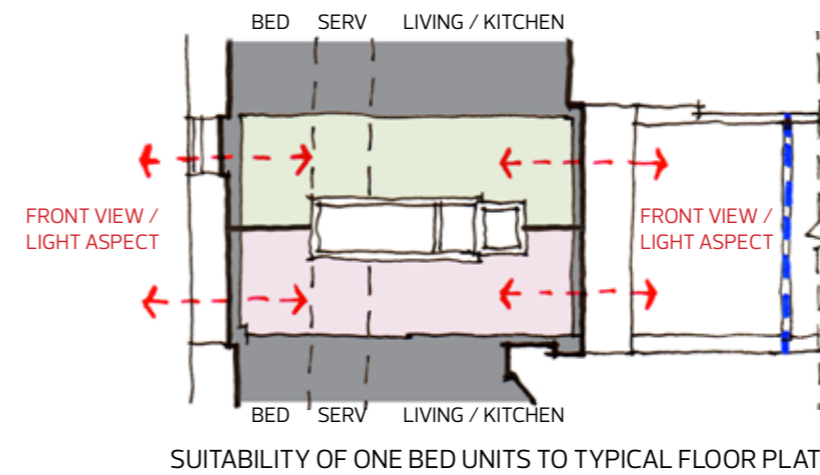
As described in the adjacent diagrams, a typical floor-plate in the property has two 9.5m wide aspects. When designed in conjunction with London Housing Design Guidelines generally results in two habitable rooms across each facade. A typical floor-plate shall therefore have 4 primary habitable rooms. Furthermore, as a result of the central core location (essential for each floor to be accessible) a three bed unit is not possible as it will require 5 habitable rooms with 5 aspects (the interior red area marked in the diagram will not be acceptable).

Note - as the lower ground and ground floor are extended the additional area behind the core affords extra depth, allowing the width to decrease and three habitable rooms to be accommodated here only. Communal lobby and service areas at GF and LGF do not allow 3 bed units here either.

Undertaking any other variation in an attempt to achieve a three bed unit becomes un-viable; potentially creating an enormous, uneconomical flat over two floors.

The proposals therefore show a total of 7 units as follows:

- 1 Bedroom Units \_ 29% (2 units)
- 2 Bedroom Units \_ 71% (5 units)



# 5.4 DESIGN DEVELOPMENT

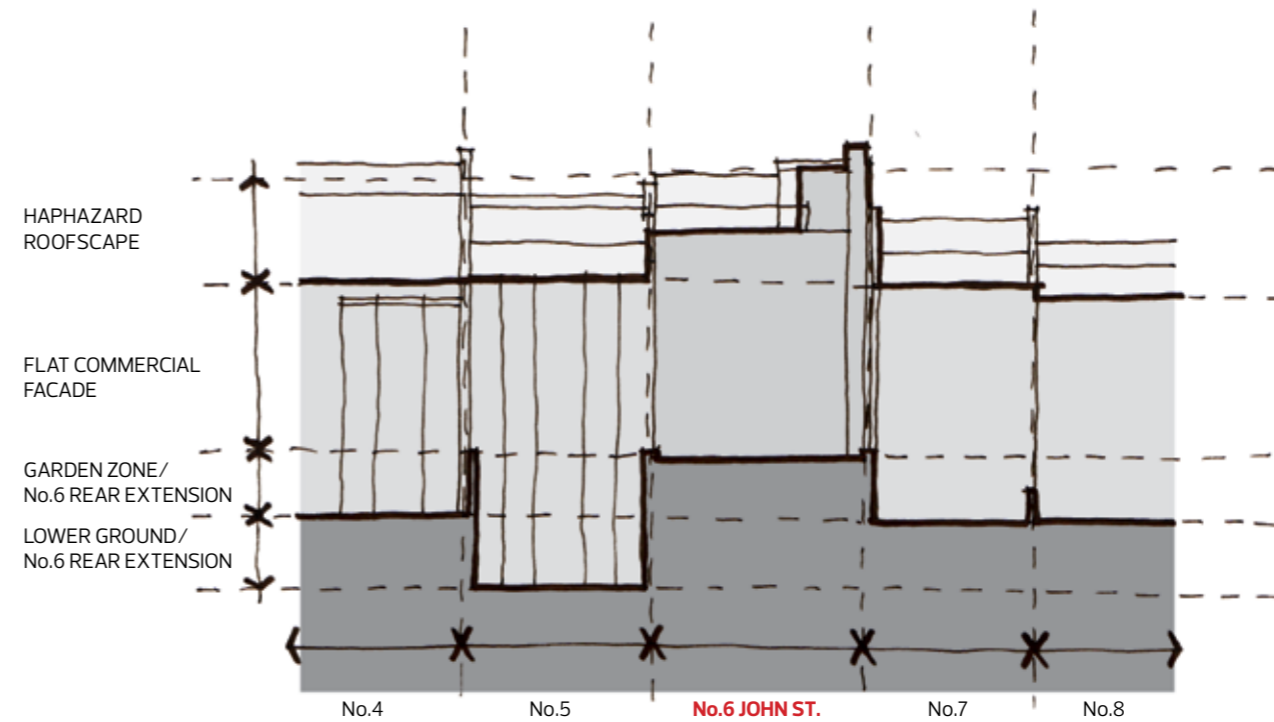
## 5.4.2 REAR FACADE

The existing rear facade of No.6 John Street was built in the 1980's and is of an industrial scale that significantly detracts from the surrounding Georgian context and character of the conservation area. Furthermore, the projection of the fourth floor external walls and industrial chimney break the obvious roofspace boundary; producing a visually jarring effect in the otherwise succinct context of the historic terrace.

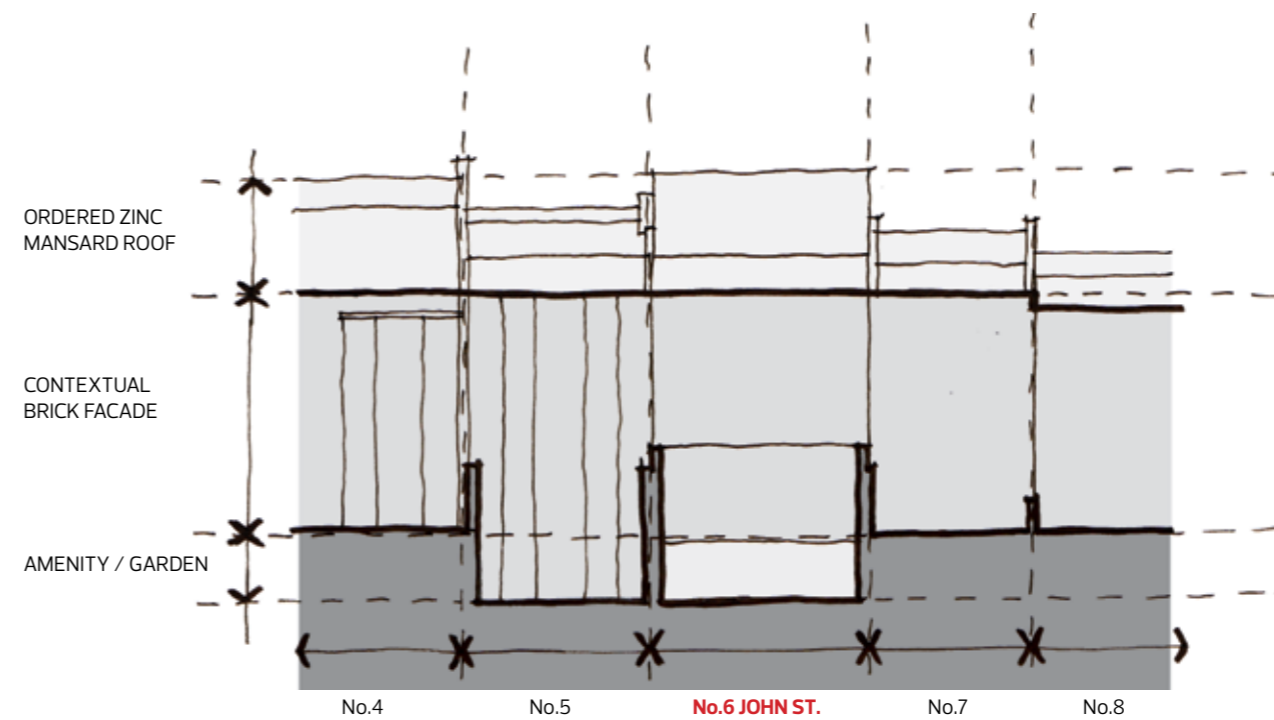
The proposal is to demolish this rear elevation and construct a contextual response which materially and visually creates a dialogue with the rear garden elevations of this urban block. The proposed facade will contrast with flat, abrupt existing building and its brick facade with vertical proportionality and ordered openings will provide depth and intrigue, clearly evident in the surrounding historic precedent.

The reconfigured brick facade will re-establish the clear distinction between roof and wall that is evident in the neighbouring buildings. The proposed double mansard roof is also consistent with neighbouring historic precedent.

Lastly, the proposed change in material to white glazed brick at lower ground floor reflects the horizontal wall finish distinction in the terrace.



EXISTING REAR ELEVATION DIAGRAM



PROPOSED REAR ELEVATION DIAGRAM



# 5.4 DESIGN DEVELOPMENT

## 5.4.3 FRONT FACADE

The proposals for the front listed facade are minimal. The existing metal access stair to the lightwell is to be removed and the access gate / external railings will be restored to original condition (as per the adjacent image). This access gate will then be moved towards the main entry door to accommodate the proposed bin lift.

The existing lightwell will be refurbished with a new access gate to the bin store – allowing for bins to be transported to the platform lift.

The existing main entry steps to GF from pavement level will be refurbished with York stone to match neighbouring properties and confirming to conservation area guidelines.

For a more detailed description of works to the front facade, please refer to the accompanying schedule of works.



IMAGE OF ORIGINAL BUILDING PRIOR TO BOMBING/DEMOLITION



EXISTING STEEL STEPS TO LIGHTWELL - TO BE REMOVED



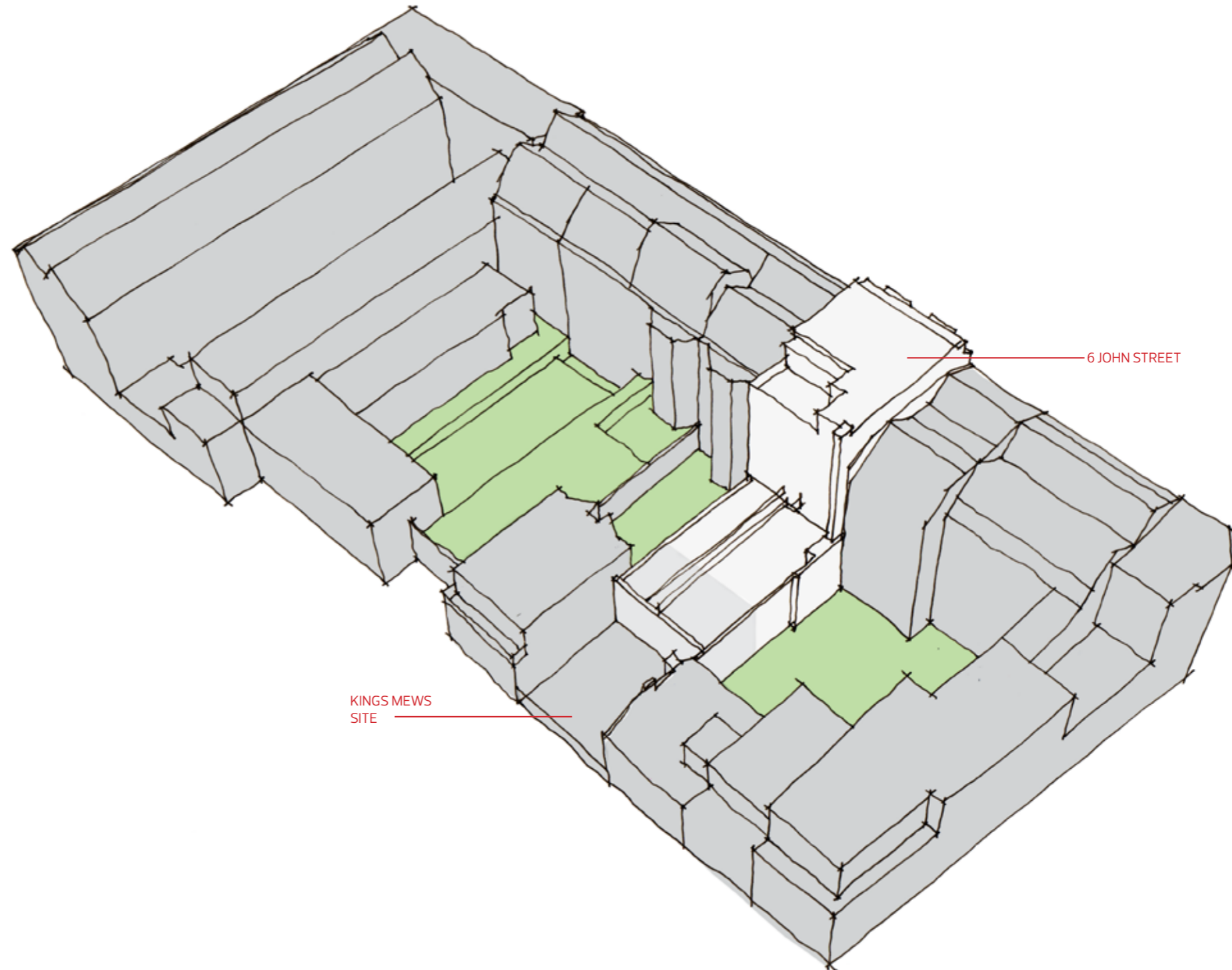
EXISTING LIGHTWELL TO BE REFURBISHED



ENTRY STEPS TO BE REFURBISHED WITH YORK STONE

# 5.4 DESIGN DEVELOPMENT

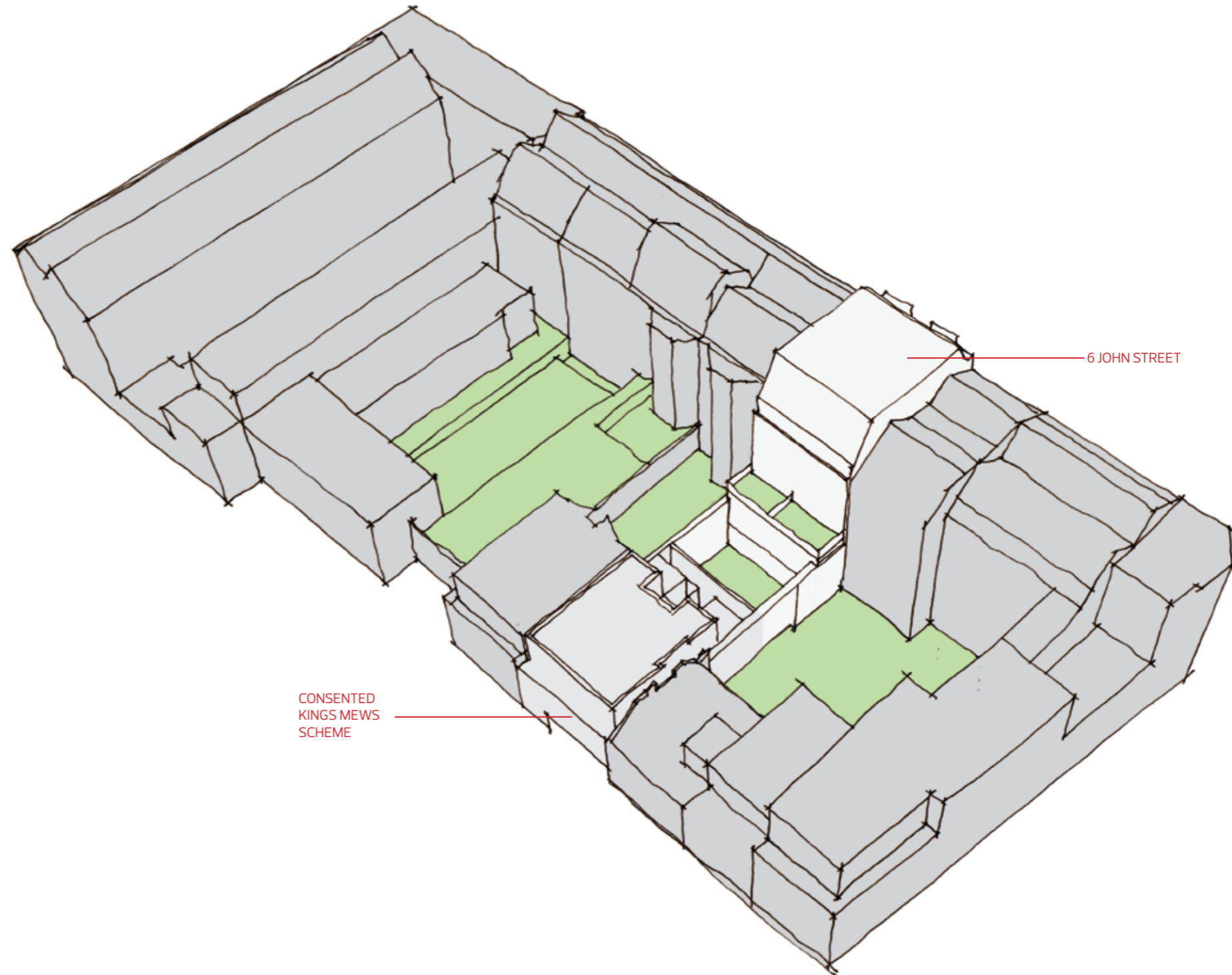
## 5.4.4 EXISTING MASSING



EXISTING MASSING SHOWING REAR EXTENSION DIVIDING THE GARDENS IN URBAN BLOCK

# 5.4 DESIGN DEVELOPMENT

## 5.4.5 PROPOSED MASSING



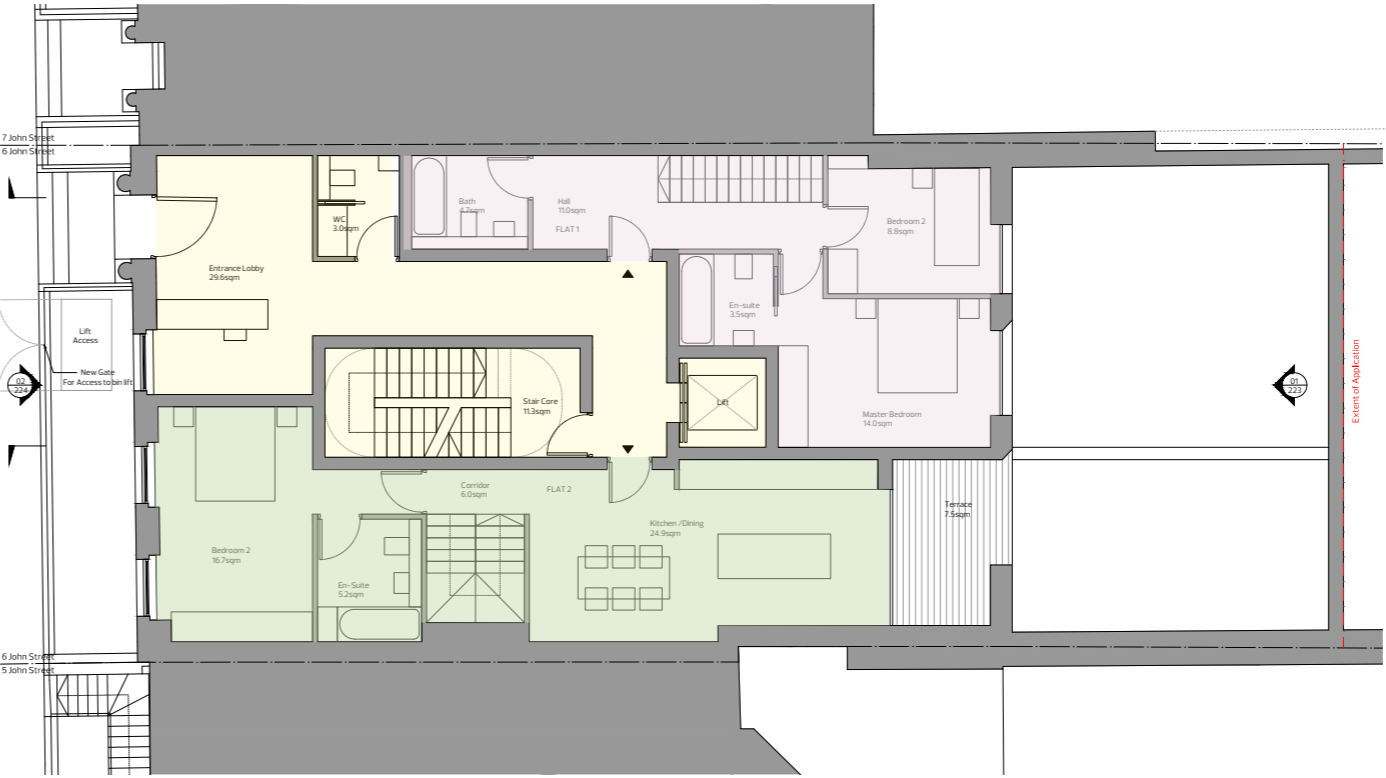
PROPOSED MASSING RE-INSTATING URBAN GARDEN ZONE

# 5.5 FLAT SIZE AND LAYOUTS

## 5.5 FLAT LAYOUT AND SIZES

The proposed development comprises a range of housing in terms of size and type. The housing comprises accommodation units to meet Camden's current housing needs and conforms to the London Housing Design Guide. All market properties have private amenity space and direct access from a shared / managed lobby, lift and stair core.

- PROPOSED 2 BED UNIT
- PROPOSED 2 BED UNIT
- COMMUNAL AREAS



PROPOSED GROUND FLOOR PLAN



PROPOSED LOWER GROUND FLOOR PLAN

# 5.5 FLAT SIZE AND LAYOUTS

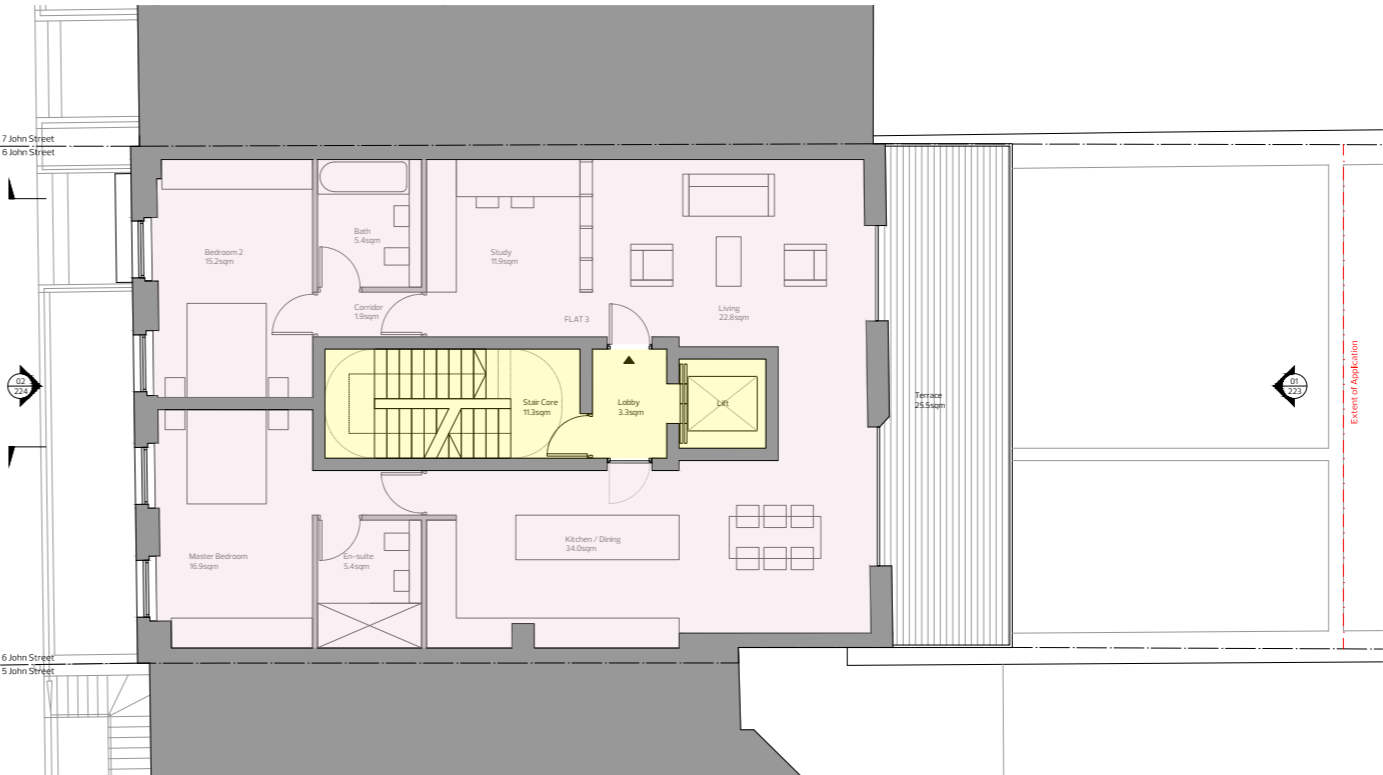
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- PROPOSED 2 BED UNIT
- PROPOSED 1 BED UNIT
- PROPOSED 1 BED UNIT
- COMMUNAL AREAS



PROPOSED SECOND FLOOR PLAN



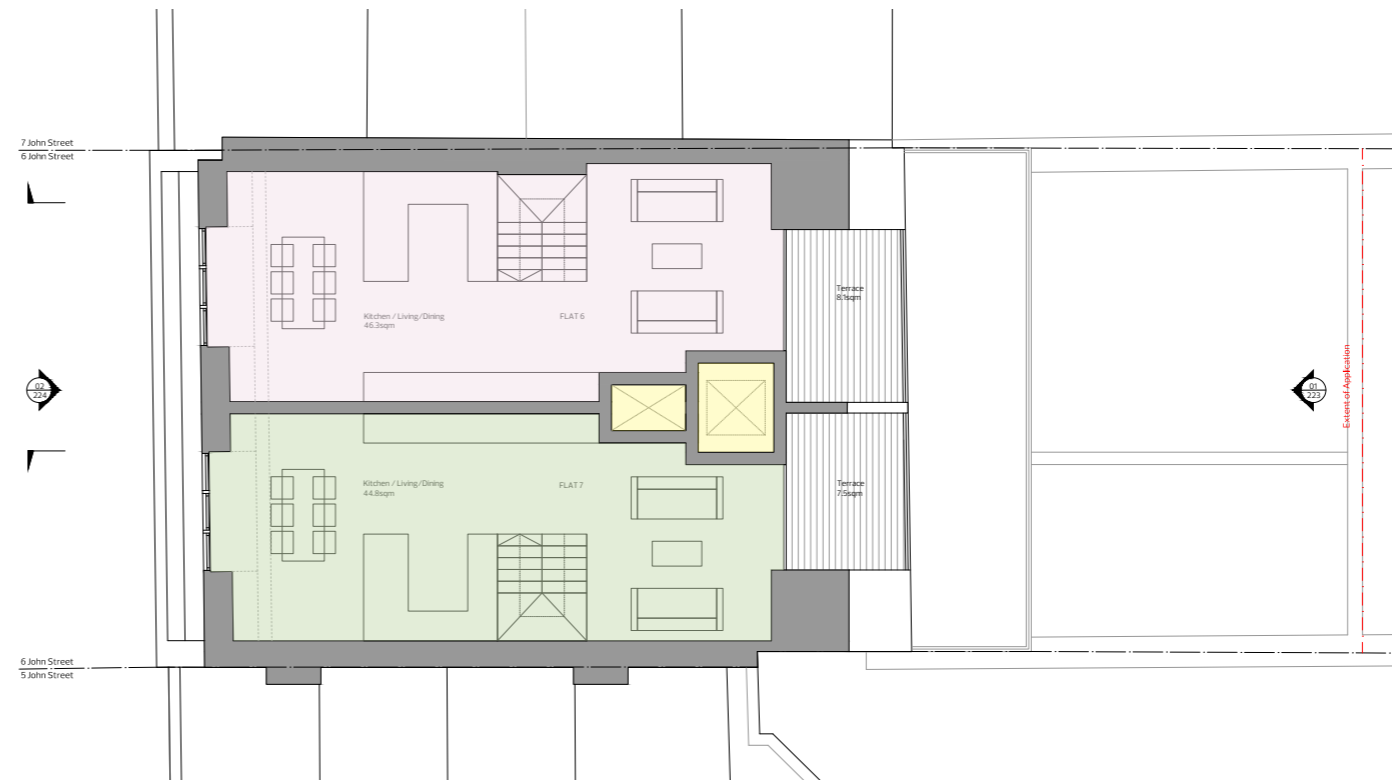
PROPOSED FIRST GROUND FLOOR PLAN

# 5.5 FLAT SIZE AND LAYOUTS

## 5.5 FLAT LAYOUT AND SIZES

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- PROPOSED 2 BED UNIT
- PROPOSED 2 BED UNIT
- COMMUNAL AREAS



PROPOSED FOURTH FLOOR PLAN



PROPOSED THIRD FLOOR PLAN

# 5.6 MATERIALS

## 5.6 MATERIALS

The design, massing and materials of the building envelope has been developed following close consideration of the site and its context. In developing the design the following principles have been adopted:

- Repair and reinforce the urban pattern and fabric;
- Integrate both traditional and contemporary materials;
- Provide a cohesive and high quality contemporary design that complements and reflects the original elevation proportions;
- Reduce the negative impact and scale of the existing rear facade on the character of the conservation area.

The existing elevations of the mid-terraced building are mostly built of London stock brick. Whilst no adverse change is proposed to the listed facade to John Street, the proposed new rear facade has been largely detailed in reclaimed London Stock brickwork. The carefully considered and vertically proportioned rear openings of the facade marries the proposal into the historic rhythm of the terrace; adding a depth evident in neighbouring Georgian properties, unlike the flat-faced, commercial appearance of the existing 1980's facade.

A delicate balance of contemporary materials shall be utilised in the rear facade, with thin framed anodised doors and windows and external white glazed brick to the lower ground floor level. This glazed brick reflects light deeper into the plan of the building and also echoes the change in material of the neighbouring buildings at LGF.

The proposed mansard roofscape has a zinc finish, a contemporary self-finished roofing material that continues the vertical emphasis and proportioning from the lower floors of the facade. This understated mansard roof shall contextually enhance the roofscape and character of the conservation area.



PROPOSED LONDON STOCK BRICK TO REAR ELEVATION



4 \_ ANODISED ALUMINIUM FRAMES TO REAR DOORS AND WINDOWS



1\_ STANDING SEAM WEATHERED ZINC TO DOUBLE MANSARD ROOF



2\_ PROPOSED LONDON STOCK



3\_ WHITE GLAZED BRICK TO REAR LOWER GROUND

# 5.7 NOISE IMPACT ASSESSMENT / ACOUSTIC REPORT

## 5.7 NOISE IMPACT ASSESSMENT / ACOUSTIC REPORT

Please refer to report from Noise and Acoustic Consultant in this application.



# 5.8 SERVICE / WASTE MANAGEMENT

## 5.8 SERVICE WASTE MANAGEMENT

Due to the limited access to and from the site, along with the listed nature of the street scene, it is proposed that the refuse store be located out of sight from the street and, in a location that can be accessed internally from all privately owned flats.

The proposal includes a carefully detailed platform lift, limiting the affect of the lift on the appearance of the street scene.

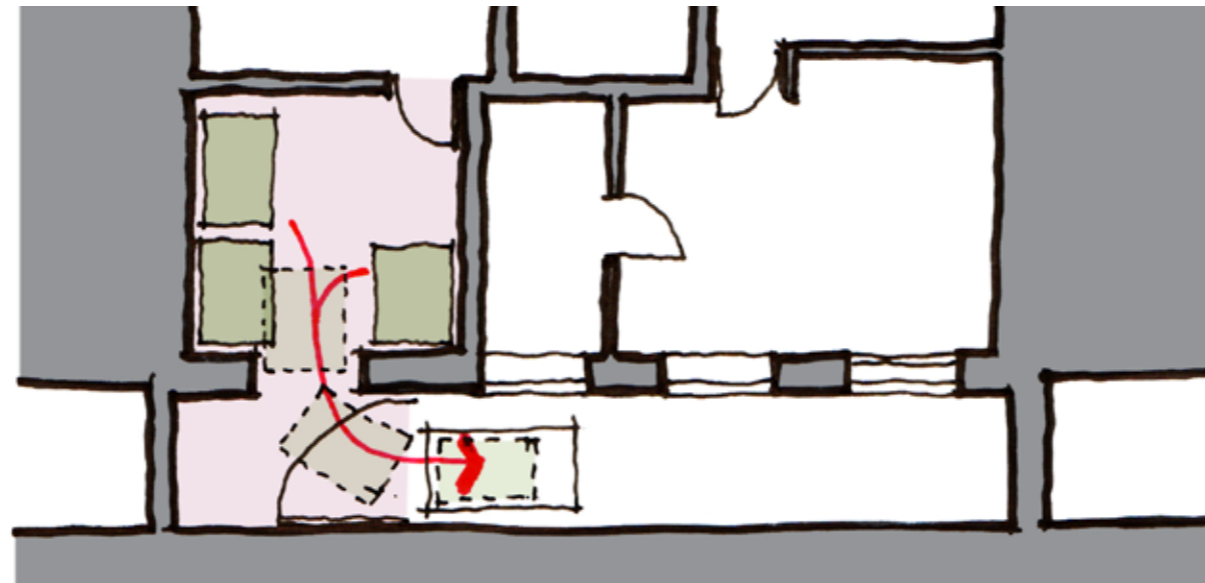
A survey of service management plans in neighbouring properties for precedent would see the following strategy for the proposal:

### Household Waste:

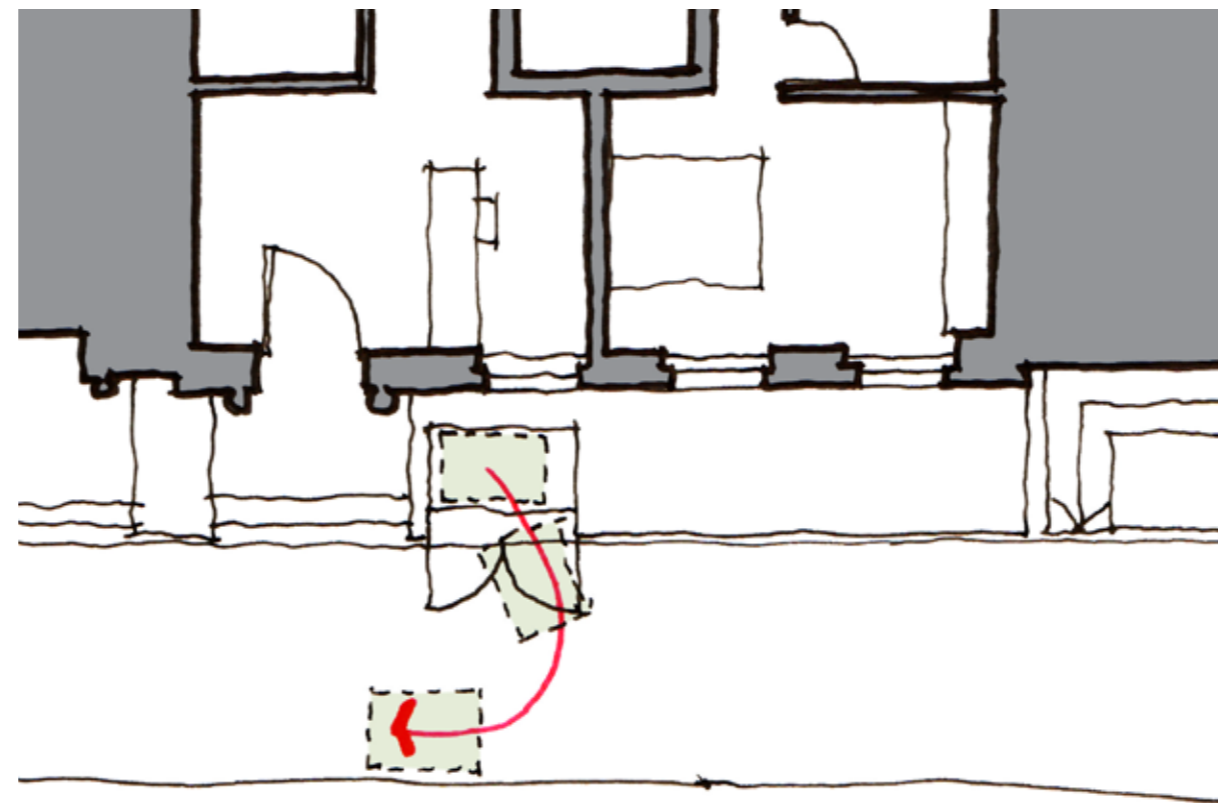
- Twice a week at 7am: taking 2no. Eurobin 1100 bins; for household waste from the basement bin store to the street level via the lightwell platform lift;
- Twice a week at 10am on the same day: returning 2no. Eurobin 1100 bins for household waste to the basement bin store via the lightwell platform lift.

### Recycling:

- Twice a week at 7am: taking 1no. Eurobin 1100 bin for recycling from the basement bin store to the street level via the lightwell platform lift;
- Twice a week at 10am on the same day: returning 1no. Eurobin 1100 bin for recycling



PROPOSED LOWER GROUND FLOOR PLAN SHOWING BIN STORE AND PLATFORM LIFT



PROPOSED GROUND FLOOR PLAN SHOWING PLATFORM LIFT AND BIN ROUTE TO STREET

# 5.9 CYCLE STORAGE

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Following Camden's Core Strategy Policy, cycling is promoted as a sustainable means of travel that provides the opportunity to relieve congestion and promote a healthy lifestyle.

The cycle store is located in the lower ground floor and access via the lift from the main lobby.

To minimise the required cycle-storage footprint, a Josta two tier storage system is proposed. Sufficient space has been allowed in this area to permit the free-access to and from the cycle stand. There is space for 8 bicycles.



TWO TIER BICYCLE STORAGE SYSTEM



'JOSTA' TWO TIER SYSTEM

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# 6.0 LIFETIME HOMES

DESIGN AND ACCESS STATEMENT

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# 6.1 LIFETIME HOMES MATRIX

## 6.1 LIFETIME HOMES MATRIX

CRITERION	FLAT 1	FLAT 2	FLAT 3	FLAT 4	FLAT 5	FLAT 6	FLAT 7	
1	PARKING							Development is car free, there is no on-site parking provision
2	PARKING ACCESS							Development is car free, there is no on-site parking provision
3	LEVEL APPROACH							Application building has a listed street elevation, no amendments are proposed to the existing, original stepped entrance.
4	EXTERNAL ENTRANCE							Application building has a listed street elevation, no amendments are proposed to the existing, original stepped entrance. All external doors within individual dwelling comply with Criterion 4
5	COMMUNAL STAIRS							The communal stairs and lift comply with criterion 5
6	DOORS & HALLWAYS	✓	✓	✓	✓	✓	✓	
7	CIRCULATION SPACE	✓	✓	✓	✓	✓	✓	
8	ENTRY LEVEL LIVING	NOTE A	✓	✓	✓	✓	NOTE B	NOTE B
9	POTENTIAL ENTRY LEVEL BED	✓	✓	Single Storey Flat	Single Storey Flat	Single Storey Flat	✓	✓
10	WC	✓	✓	✓	✓	✓	✓	✓
11	BATH/WC ADAPTABILITY	✓	✓	✓	✓	✓	✓	✓
12	STAIRLIFT CAPACITY	✓	✓	Single Storey Flat	Single Storey Flat	Single Storey Flat	✓	✓
13	BED HOIST	✓	✓	✓	✓	✓	✓	✓
14	BATHROOM	✓	✓	✓	✓	✓	✓	✓
15	WINDOWS	✓	✓	✓	✓	✓	✓	✓
16	FIXTURES	✓	✓	✓	✓	✓	✓	✓

NOTES:	
A	Flat 1 does not have entry level living space as configured in this application, however the communal lift runs down to the basement and this flat can easily be adapted so that the principal entrance is on the Lower Ground Floor, into the living area and therefore satisfying Criterion 8.
B	Flats 6/7 do not have entry level living, this would require having kitchen / dining & 1 bedroom on one floor and living and a second bedroom on another floor. After the provision of a bedroom on the upper level it is not possible to meet the required areas for either a living space or combined kitchen/dining area, therefore this configuration is the only way to provide a 2 bed/4 person dwelling over these floors.

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