



3317 – 82 Guilford Street

Construction Management Plan

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Date: 28th July, 2014

Checked: James Bailey

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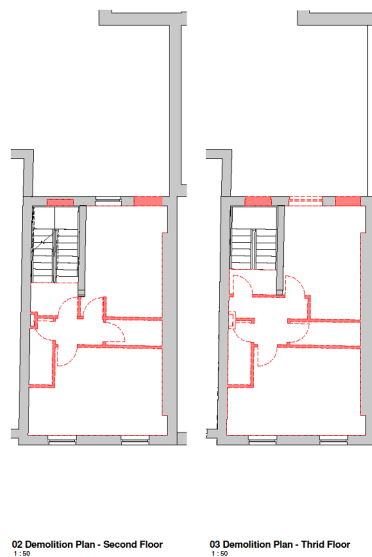
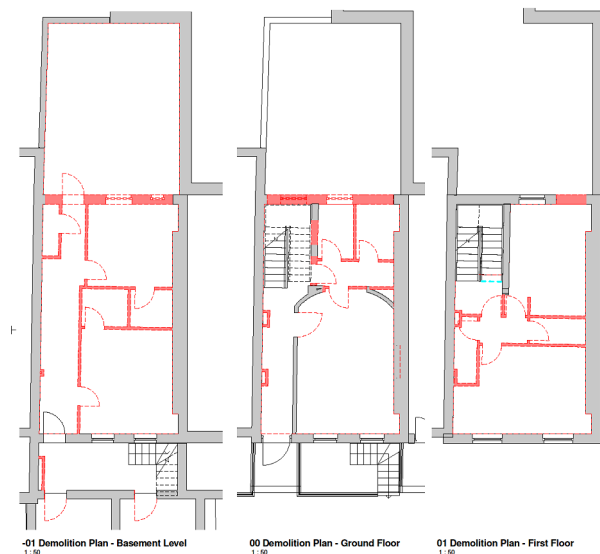
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Introduction

The owner of 82 Guilford Street recognises the importance of timely and proper attention being paid to the impact that construction projects have on their neighbours. They are aware that construction activity may be an inconvenience at times. This statement outlines the process and systems that will be put in place to ensure good neighbourliness.

For clarity this document should be read in conjunction with the full detailed planning application.

This Construction Management Plan is illustrative pending appointment of a Contractor for the works. It will form the basis of Employer's Requirements for the main Contractor for the works and illustrates key considerations for inclusion within the Contractor's own Method Statements for the construction period.



The Proposal

AWW have been appointed by the client to obtain planning permission and listed building consent for the internal alterations and external renovations to the existing grade II listed Victorian townhouse to create 4 apartments.

The approach has been to create high quality, sympathetic apartments with a mix of 1,2 and 3 bed dwellings that will contribute to the restoration of the existing terrace and to reinstate high quality of accommodation suitable for the Bloomsbury area.

The site lies within the London Borough of Camden in inner North London and is currently occupied by a 4 storey Georgian townhouse with basement that has been converted to nurses' accommodation under use class Sui Generis.

The building has most recently been run as hostel/ bedsit accommodation without the correct HMO licences in place and without planning permission for the change of use from its lawful use as nurses' accommodation.

The importance of retaining as many of the property's original features has been noted and where possible, these will be repaired and reinstated to maintain the building's heritage.

Requirements of Contractors

A written Construction Management Plan in relation to the proposed development will be produced. The plan must include the following details

1. A construction programme for all activities
2. Site management organisation chart including a 24 hour emergency contact number;
3. Parking of vehicles of site operatives and visitors (including measures taken to ensure satisfactory access and movement for existing occupiers of neighbouring properties during construction);
4. Locations for loading/unloading and storage of plant and materials used in constructing the development;
5. Measures to control the emission of dust and debris during construction; and
6. a scheme for recycling/disposing of waste resulting from demolition and construction works, including wheel-washing facilities.
7. Details of Site accommodation and welfare facilities.

Proposed start and end dates for each phase of construction

The target dates for the project are:

Main Contract Works

Start date: 1st September, 2014

Completion date: 1st September, 2015

Basic Hours of Working

Construction work on the site will only be carried out during the time periods as agreed with the Local Authority.

Out of Hours Working

Should events arise which require work to be undertaken outside of agreed normal working hours, a system will be put in place that allows advice and approval to be sought from the Local Authority.

The working vehicles will be arriving and departing the site between the agreed working hours.

Approach to construction noise, vibration and dust

The client appreciates that during the construction stage the potential exists for a negative impact upon the neighbourhood in which the development is situated.

The Contractor will be required to organise activities to ensure the adjacent residential units and businesses are able to continue with minimal disruption or inconvenience.

A method statement will be produced by the Contractor outlining the measures that will be put in place to control construction noise, vibration and dust. The procedure to seek Local Authority approval for unplanned events, which may require works to be carried out outside of agreed normal working hours, will also be included in the method statement.

Dust arising from construction activities on the site will be controlled by Best Practicable Means. The control of dust shall be considered at the design stage and appropriate mitigation measures will ensure that the levels of dust generated by construction activities are minimised.

Road brushes will be employed while activities producing spoil are being undertaken. This approach ensures all parties are kept informed of the contractor's intentions and that there is a clear mechanism for dealing with any problems that may arise.

Construction Traffic

Recognising the potential for congestion / disruption all construction deliveries will be required to telephone in advance of arrival and will be away from Guilford Street until it is confirmed that access is clear. There will be a dedicated site operative who will

manage the whole delivery process. Access is to be maintained at all times along Guilford Street.

Recognising the constraints of the site, the design will promote the use of off-site manufactured components where practicable, to mitigate the quantity of deliveries necessary.

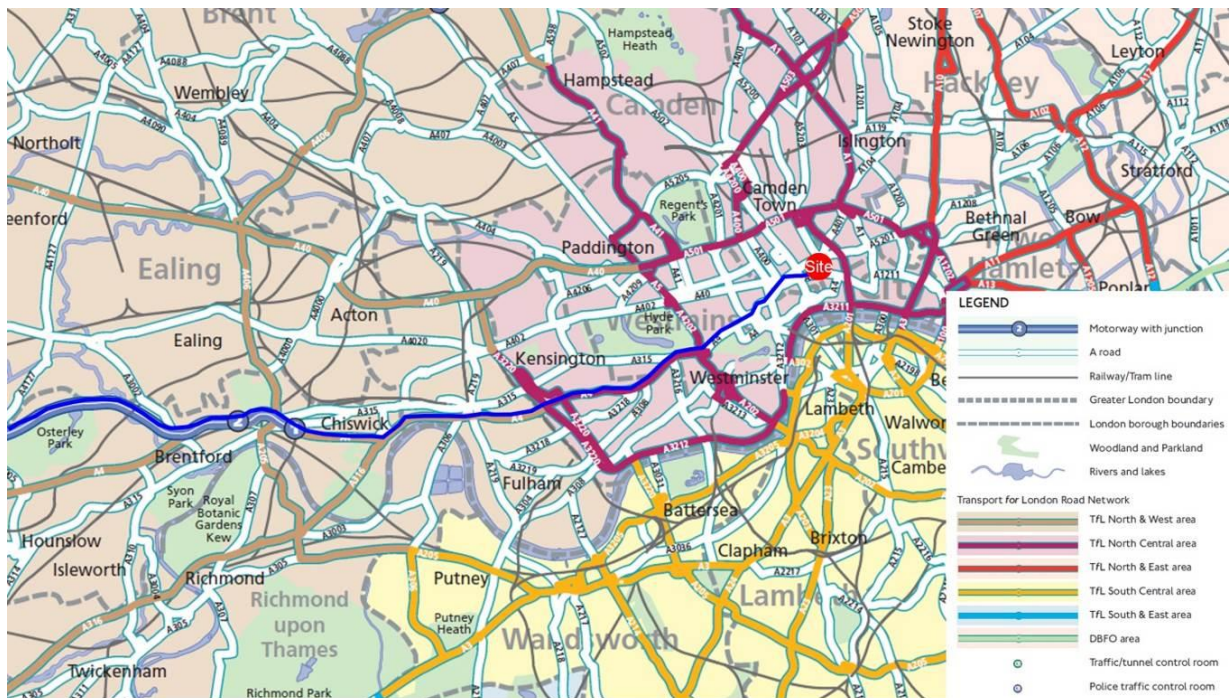
Limited vehicle parking will be available on site. Where parking is necessary on the surrounding roads, it will be monitored by the Site Manager and measures taken to encourage the workforce to travel by other means. Initiatives such as car sharing, travel by public transport, cycling will be promoted with site personnel to mitigate impact on the local community.

Proposed routes for vehicles between the site and the Transport for London Road Network (TLRN)

The site will be accessed mainly from the TFL North Central Area Road Network. Guilford Street is located on a 'B' road which branches from the A40.

The access routes to site won't affect any highways majorly and there are no weight restrictions or low bridges on the proposed access routes.

The map below shows the proposed access route from the M4.



Typical Sizes of all vehicles and the frequency and times for each phase of construction

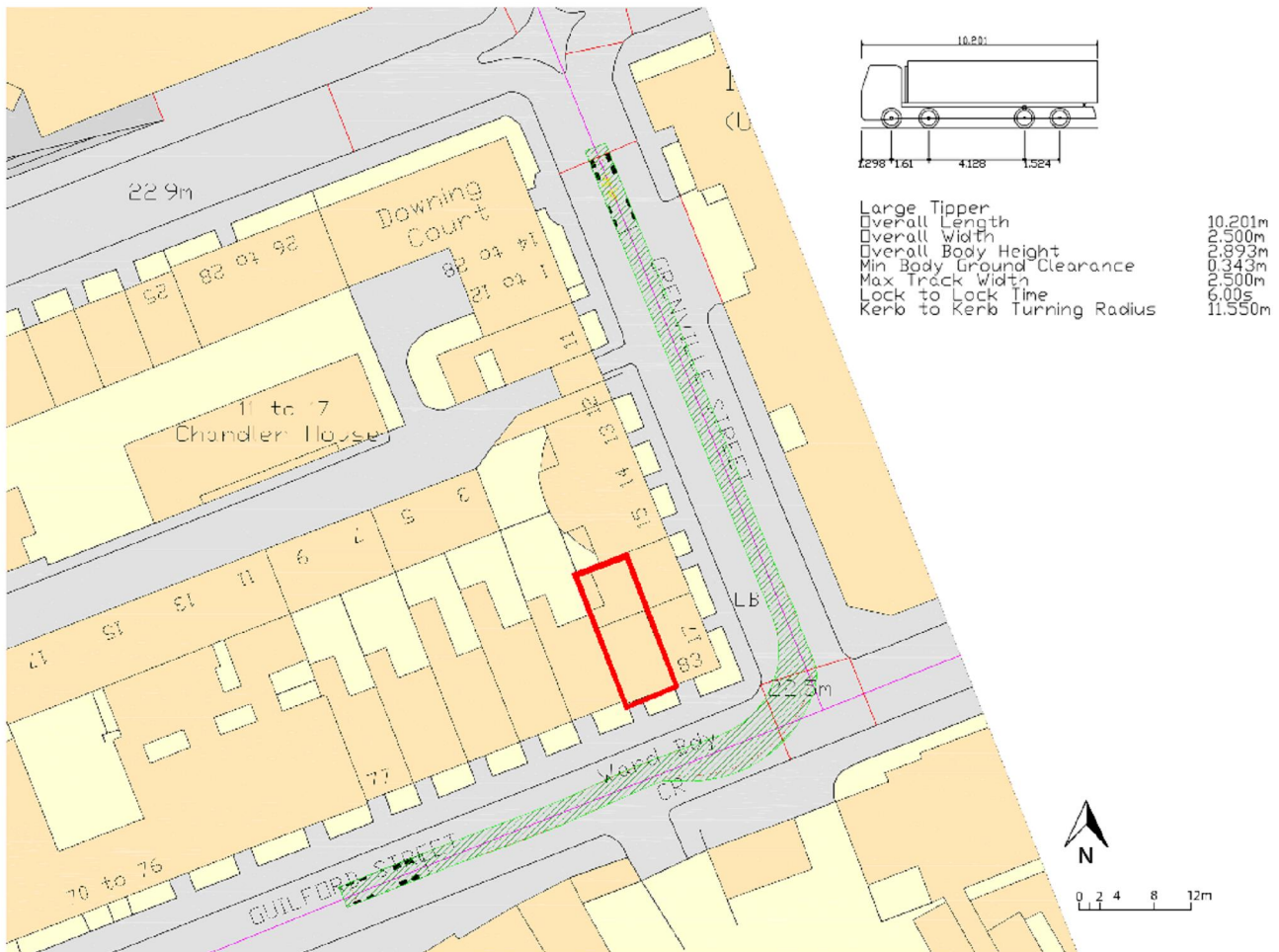
Numerous types of delivery vehicles will be used to bring materials to and from the site. These include:

- Skip lorries. These will include roll on/roll off skips for minor demolition works (approx. size 7.5m long and 2.4m wide) and standard 8 yards skips for waste (approx. size 7m long and 2.4m wide).
- Flatbed delivery vehicles for the delivery of various materials including scaffolding, steelwork, reinforcement, bricks/blocks, timber, roofing materials, plaster, joinery etc. (approx.. size 8.5m long and 2.45m wide).

The projected vehicle movements are approximately 0.5 per day during the enabling works and 1 per day during the main contract works period.

Swept path drawings for any tight manoeuvres on vehicle routes to the site

The diagram below shows a large tipper swept path:



Details of any highway works necessary to enable construction to take place

We are not proposing to change the public highways therefore no changes will be made and the existing route is sufficient.

Parking and loading arrangement of vehicles and delivery of materials and plant to the site

A strict delivery procedure will be implemented to ensure that Guilford Street is not overrun with site and delivery vehicles.

All subcontractors and suppliers will be required to give 48 hours' notice of deliveries. The movement of materials, particularly in the main contract works stage, will also be controlled by our road marshals. He will be responsible for the control and coordination of all aspects of material deliveries and movement.

Vehicles will pull into the site for unloading wherever possible.

Materials will be stored within the boundary of the site.

No parking will be permitted on site and all sub-contractors will be informed at the pre order meeting that the surrounding area is for resident parking only. All sub-contractors will be encouraged to use public transport.

Details of Proposed parking bays suspensions and temporary traffic management orders

Suspension of resident parking bays will be kept to an absolute minimum.

Scaffolding relationship with the highway

Scaffolding will be erected around the building and whilst it will be erected it will overhang the footpath in Guilford Street.

The scaffolding will be enclosed with a hoarding and we will ensure that the full width of the footpath remains open to the public at all times.

Details of hoarding required or any other occupation of the public highway

The site area will be enclosed with a 2.4m high timber hoarding. This will be adapted as necessary, and will be painted as agreed.

We will ensure that hoarding panels are maintained and kept clean for the duration of the project.



Example of typical hoarding

The hoarding will generally be fixed to the scaffolding and where the building sets back; scaffolding will be fixed to an independent scaffold framework.

Construction management

The Main contractor will provide a clear organisational chart describing the management team who will have responsibility for the delivery of Project. The chart will identify other resources allocated to responsibilities including and not limited to

- Health and Safety,
- strategic Planning,
- procurement, Design and
- System Compliance;
- technical Construction organisation and implementation of Health, Safety and Environmental and Strategic Objectives;
- On-site supervision of the works, trade contractor management and document control.

The Contractor will develop detailed Project Specific Health and Safety, Environmental and Construction Plans integrated to ensure mutual compatibility.

A Neighbour Liaison Policy will be developed and a senior member of the Project Team should be responsible for implementing the Policy throughout the duration of the construction operations

Health and Safety

The client's overriding concern for the site will be the health and safety of everyone involved, including the general public and our staff.

A project specific construction-phase plan will be prepared together with method statements, risk assessments and Control of Substances Hazardous to Health (COSHH) assessments for all aspects of the works.

The Contractor shall provide regular training both on and off site. All site personnel including trade contractors must receive induction training before being allowed onsite. Regular tool box talks shall be held to keep personnel up to date with site conditions and hazards.

The Main contractor is required to be a member of the Considerate Contractor's Scheme.

Considerate Contractors Scheme

The Considerate Constructors Scheme is a voluntary scheme aiming to present a more positive image of the construction industry. Participating constructors commit themselves to higher standards of site cleanliness and tidiness, improved site safety, better site housekeeping and traffic management leading to a reduction in their impact on the local community and the wider environment.

The Scheme gives advice on:

- Minimising any disturbance or negative impact (in terms of noise, dirt and inconvenience) sometimes caused by construction sites to the immediate neighbourhood
- Eradicating offensive behaviour and language from construction sites
- Recognising and rewarding the contractor's commitment to raise standards of site management, safety and environmental awareness beyond statutory requirements.

The Scheme commits the contractor to be considerate good neighbours, as well as clean, respectful, safe, environmentally conscious, responsible and accountable. Posters are displayed around the construction site advertising the Scheme detailing the Code to which the constructor is committed. If passers-by wish to make a comment, the name and telephone number of the Site Manager and Scheme Administrator is clearly displayed. Those contacted will respond immediately and implement the corrective action required.

The site will be monitored by experienced site monitors drawn from senior positions of every discipline within the Industry. Monitoring consists of an impression of the site from the point of view of the neighbour and/or the general public. The monitor may talk to site neighbours if it seems that it would be informative to do so. The Scheme requires constructors to adhere to the Code of Considerate Constructors.

Considerate Constructors Code of Practice

Consideration:

- All work is to be carried out with positive consideration to the needs of traders and businesses, site personnel and visitors, pedestrians, shoppers, the general public and the environment in general.

- Special attention is to be given to the needs of those with sight, hearing and mobility difficulties.

Environment:

- Noise from construction operatives and other sources is to be kept to a minimum at all times.
- Consideration should be given in the selection and use of resources - local resources should be used wherever possible.
- Attention should be paid to waste management and the avoidance of pollution - recycling of surplus materials is to be encouraged.
- All operatives to be inducted on the issues specific to the site.

Cleanliness:

- The working site is to be kept clean and in good order at all times.
- Temporary safety barriers, lights and warning signs are to be maintained in a clean and safe condition.
- Surplus materials, rubbish etc. shall not be allowed to accumulate on the site or spill over to the surrounding environment.
- Dust etc. from construction operations shall be kept to a minimum.

Neighbourliness:

- Full and regular consultation with neighbours including adjacent traders and business regarding programming and site activities shall be maintained from pre-start to completion.
- General information regarding the Scheme for these neighbours using the area shall be provided.

Respect:

- Respectable and safe standards of dress, appropriate to the weather conditions, shall be maintained at all times.
- Lewd or derogatory behaviour and language should not be tolerated under threat of severe disciplinary action.
- Pride in the management and appearance of the site and the surrounding environment is to be shown at all times.
- Operatives shall be instructed in dealing with the general public.

Safety:

- Construction operatives and site vehicle movements are to be carried out with great care and consideration for the safety of the general public, school population, and site personnel.
- No building activity shall be a safety or security risk to others.

Responsibility:

- Considerate Constructors will ensure that all site personnel, specialist sub - contractors, drivers and any other persons working on the site understand and implement the obligations of this Code and monitor their compliance with it.

Accountability:

- Posters relating to the Scheme will be displayed around the site, giving names and telephone numbers of staff who can be contacted in response to issues raised by the general public, school population, and others affected by the site operation.

Details of how pedestrian and cyclist safety will be maintained

When vehicles are entering or leaving the site, these will be supervised by the road marshall. Where vehicles are unloading in Guilford Street, this will be supervised by the road marshall.

The general public/pedestrians will have right of way along the pathway. The construction site gates will be kept closed and monitored, only when deliveries are made to the site will they be opened to allow vehicles onto the site, at which time barriers will be put across the pavement to prevent access by pedestrians.

With regard to cyclist safety any delivery vehicle parked on the road will be coned off to direct the cyclist around the lorry.

Should there be any complaints arising from the works, local residents will be able to call personally to the site offices.

Details of how traffic associated with the Development will be managed in order to reduce congestion

In order to reduce traffic movements, we shall call off full loads whenever possible and only accept part loads when essential.

We shall encourage our sub-contractors to use public transport to travel to site. We shall also inform potential subcontractors that parking is very restricted in the local area and that residents parking bays are not to be used. We will monitor parking, especially on neighbouring roads, to ensure offsite parking is dealt with considerately.

Details of how any significant amounts of dirt or dust that may be spread onto the public highway will be cleaned or prevented

Mud and debris on the road is one of the main environmental nuisance and safety problems arising from construction sites.

We will also make provision for cleaning of the road if required by an approved road sweeper.

We will insist that any skip lorries are fully sheeted to minimise the risk of any mud over-spilling onto the highway.

We will consider spraying a fine spray to suppress dust on the following:

- Structures and building during demolition.
- Unpaved areas that are subject to traffic or wind.
- Sand, soil and aggregate stockpiles.
- During loading/unloading of dust generating materials.

All contractors operating HGVs

All contractors operating HGV's must meet all the following conditions:

- 1.) Operators must be a member of the TFL's Fleet Operator Recognition Scheme or similar at the Bronze level.
- 2.) All drivers must have to undertake cycle awareness training through FORS or similar.
- 3.) All vehicles must:
 - Have Side Guards fitted, unless it can be demonstrated to the reasonable satisfaction of the Employer, that the lorry will not perform the function, for which it was built, if Side Guards are fitted.
 - Have a close proximity warning system fitted comprising of a front mounted, rear facing CCTV camera, a Close Proximity Sensor, an in-cab warning device and an external warning device to make the road user in close proximity aware of the driver's planned manoeuvre.
 - Have a Class VI Mirror
 - Bear prominent signage on the rear of the vehicle to warn cyclists of the dangers of passing the vehicle on the inside.

Any other relevant information with regard to traffic and transport

i.) Targeting zero non-hazardous waste to landfill

As part of our environmental approach we seek to source materials from local companies provided that specification requirements and costs are met.

ii.) Energy usage

Where practicable, we seek to source green energy providers for the construction phase. Meters will be supplied for the site enabling energy consumption levels to be monitored.

iii.) Fuel consumption

We strive to procure local contractors for the project therefore minimising transport costs and impact on the local environment.

iv.) 23.0 Waste Management

Our approach to the treatment of waste is to employ a specialist waste management contractor as a trade package. This contractor is responsible for:

- Ensuring the site is kept clean and safe.
- The collection of waste from a central point.
- Segregation of waste on site.

The waste management contractor will ensure that all access routes, fire escapes and staircases are swept and kept clear of debris on a regular basis to maintain high standards of health and safety on the project. All general areas of the project will be swept clean on a weekly basis. Sub-contractors will be responsible for removing waste emanating from their works to a central point on site.

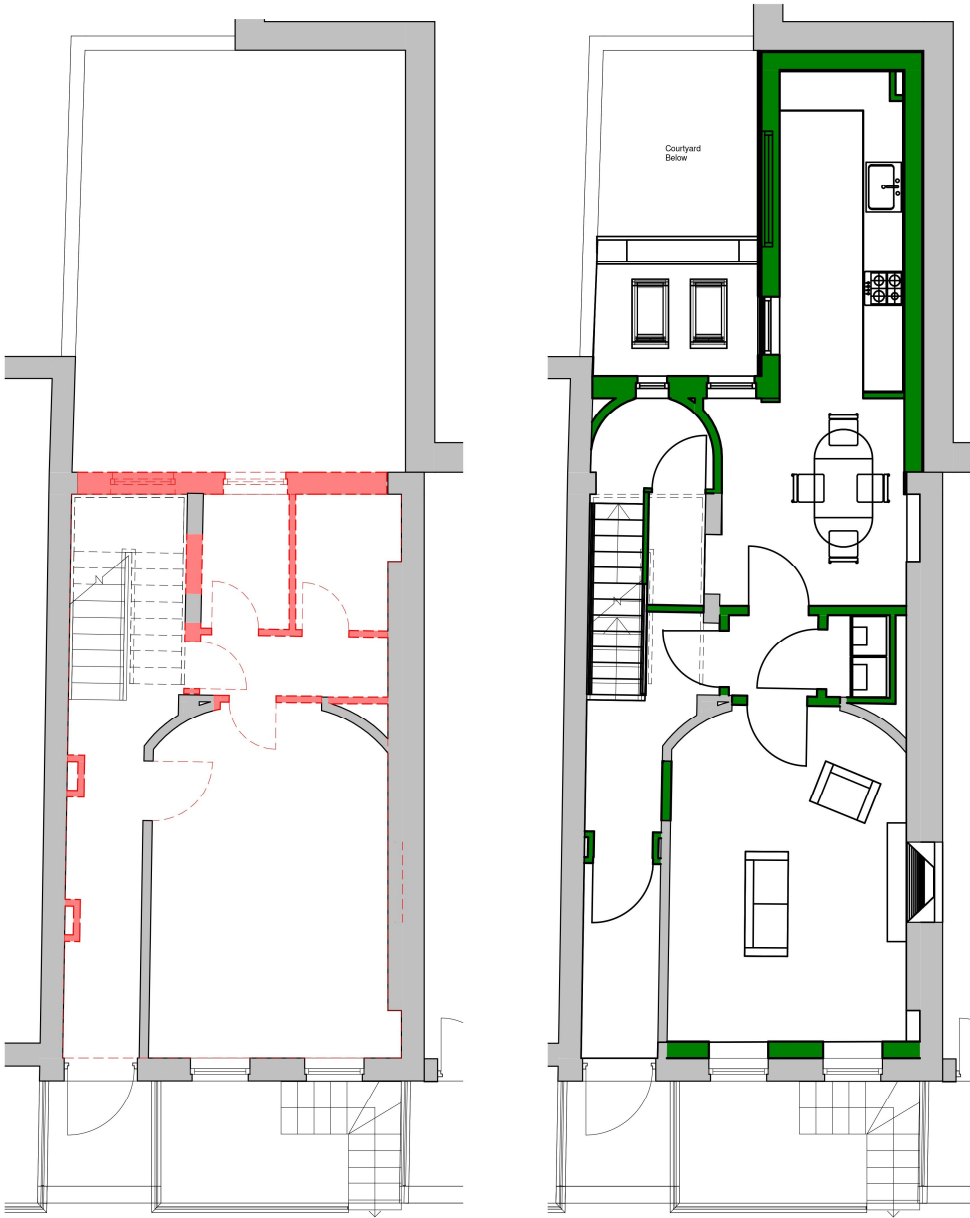
APPENDIX 1: Proposed and Demolition Plan

Key:
- - - Removed Elements
■ Proposed Elements



BASEMENT

Key:
- - - - - Removed Elements
█ Proposed Elements



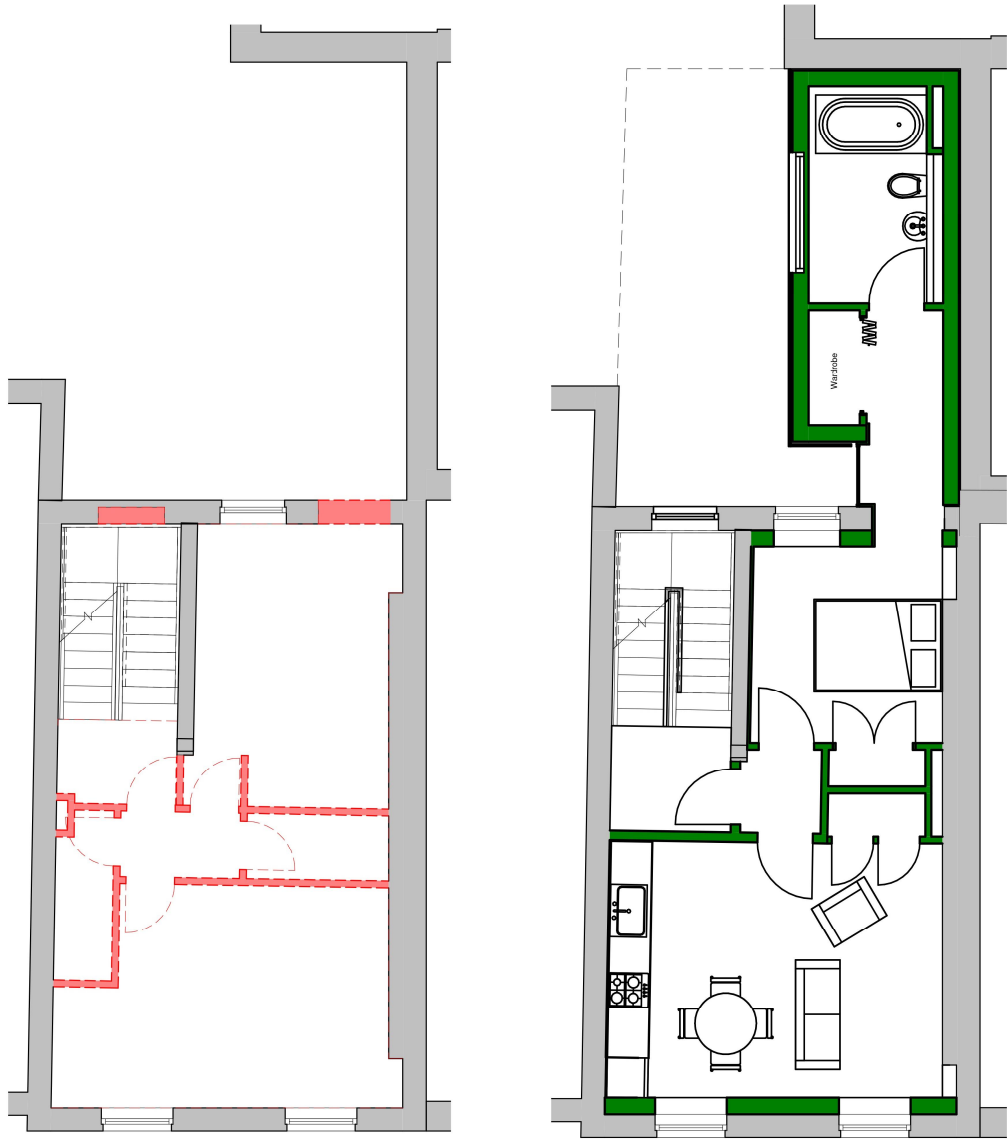
GROUND

Key:
- - - Removed Elements
█ Proposed Elements



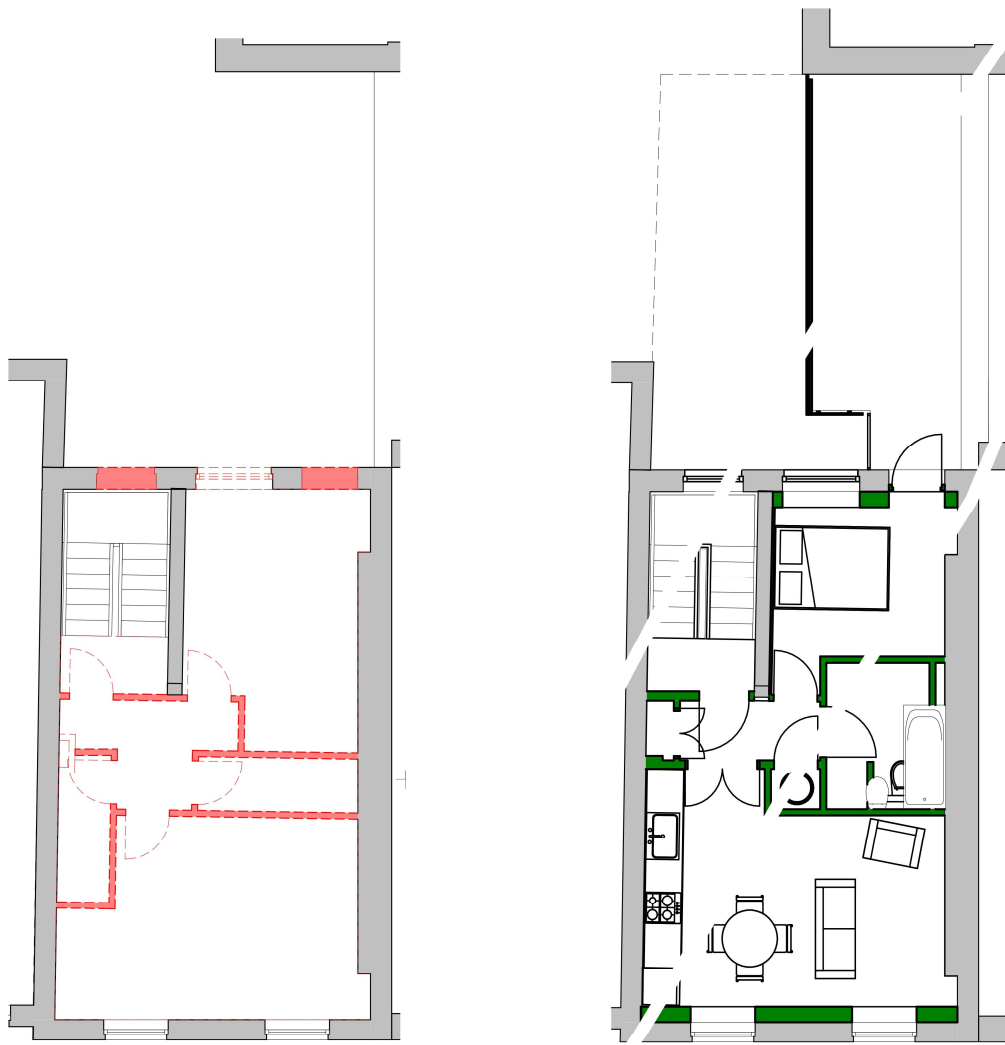
FIRST

Key:
- - - Removed Elements
█ Proposed Elements



SECOND

Key:
- - - Removed Elements
█ Proposed Elements



THIRD

APPENDIX 2: Asbestos Report

An invasive Asbestos Survey is currently being undertaken.

APPENDIX 3: All Drawings