

King's Cross Station

**Listed Building Consent - Design and Access
Statement**

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Version 1.00



Temporary Monitoring Equipment – Listed Building Consent

Design and Access Statement

1.0 Introduction

While the steel structure for the Main Concourse is being erected it is necessary to monitor the positions of the new steels from a position on the Great Northern Hotel. This involves installing monitoring equipment from a bracket in two positions on the building. As the Hotel is Listed Grade II this work requires Listed Building Consent. Permission is also being obtained from the owners of the building for fixing this equipment to it.

Structural monitoring equipment is also needed along the parapets of the Western Range Building but these can be clamped in place without any damage to the masonry so it was agreed with the Conservation Officer that it is not necessary to apply for Listed Building Consent for this part of the work. (Email correspondence dated 15/03/10.)

2.0 Reasons for requirement

In order to enable the installation of the Western Concourse Roof structure, equipment is needed to position the individual pieces of steel precisely to the tolerances specified within the contract documents. The equipment has to be located at high level to obtain a view not obscured by the support scaffold and on a stable surface that does not move.

At high level, the Great Northern Hotel is an ideal location from which to set out the steelwork due to the fact that a clear view is provided over a large section of the Western Concourse roof.

3.0 Details of Equipment and fixings

The equipment consists of an EDM (electronic distomat measurement unit), a bracket to support and mount the EDM to the wall of the Great Northern Hotel and a power supply cable. The brackets will be installed using stainless steel bolts fixed into the mortar joints on the brickwork using Hilti Hit chemical resin. The bolts will be 8mm diameter which will enable them to fit into the mortar joint. (See *photograph 1 and drawing no. SUR-01-001.*)

4.0 Location

Two locations have been chosen at high level on the Great Northern Hotel, above the “GR” of Great and above the “L” of Hotel on its North East elevation. (See *photograph 2.*) The brackets will be put in place in both locations for the duration of the works. The EDM will be moved between the brackets as necessary for positioning the steel members and a final check of all members will be carried out prior to dismantling the equipment.

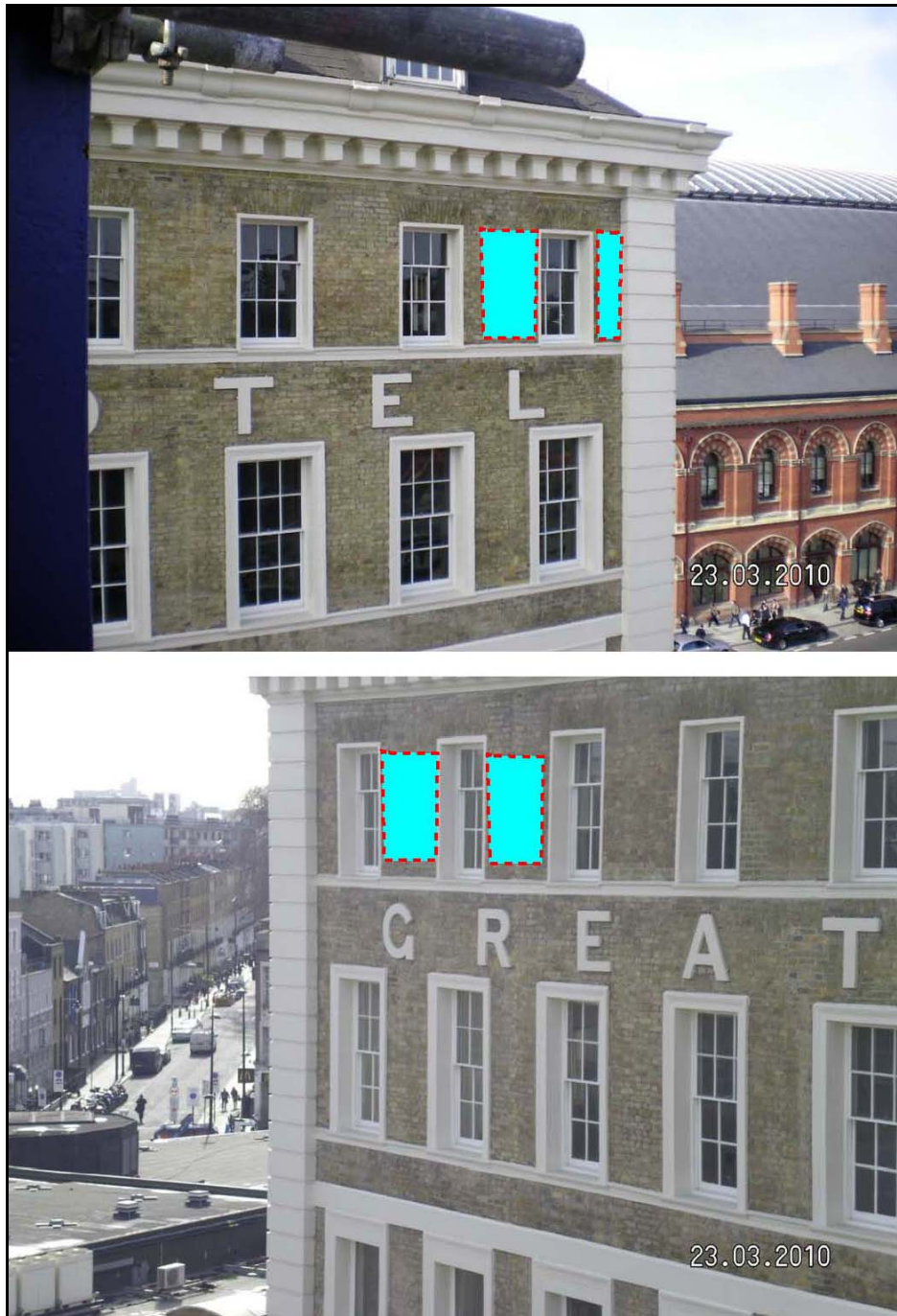


Figure 1: Proposed locations of monitoring equipment

5.0 Mounting the equipment in place / protection

The brackets will be fixed to the wall from a cherry picker or from the crane. During the installation the exterior surface of the man riding basket will be covered in a compressible/protective material to avoid any damage to the brickwork caused by the basket accidentally rubbing against the building.

Due to the difficulty of access to the locations on the Great Northern Hotel, remotely controlled equipment is proposed to set out and survey the steelwork. It will be controlled by a hand held remote by the engineer responsible for the installation of the roof steel. Access to the equipment is only required for its removal or maintenance.



Figure 2: Similar bracket supporting monitoring equipment elsewhere on the Concourse

6.0 Repair following removal

When the bracket is no longer required it will be removed using the access method described above. The exposed stainless steel bolts will be cut back flush with the mortar joint and recessed if possible. A repair mortar will then be applied over the locations where the bolts have been cut back.

7.0 Time Frame

Consent is needed to keep this equipment in place for the duration of the erection of the steel structure, until 01/09/2011. This date allows for some delay to the roof steel erection.

8.0 Policy Context

8.1 National Policy

Planning Policy Guidance 15: Planning and the Historic Environment sets out government policy and guidance to local authorities on the operation of the planning system and the identification and protection of historic buildings, conservation areas, and other elements of the historic environment.

The proposed alterations to the listed building need to be considered against PPG15. The general guidance applicable to listed building applications (PPG15, paragraph 3.5) is used:

The importance of the building, its intrinsic architectural and historic interest and rarity

The proposal is required in order to monitor the listed structures during the construction works and therefore looks to protect the buildings by alerting the project team to work that is affecting the structural integrity of the building.

Work can then be stopped and mitigation measures put in place. It is therefore considered that the proposal will in fact protect the building.

It is considered by virtue of the limited structural intervention required and the temporary nature of the installations the impact on the architectural and historic interest of the station is minimal and the station will be fully restored to its original form upon completion of the works.

Particular physical features which justify listing.

The proposed alterations do not significantly affect the main features that justify the listing of the main station building.

The building's setting and contribution to street scene

The main station building has a significant positive contribution on the character and appearance of the street scene, particularly on Euston Road. The proposed development will not detract from that contribution, particularly as the installations and supports will only be visible from limited viewpoints in and around the station.

Again, it is considered that by facilitating the enhancement works to the main train shed roof the proposed monitoring equipment will result in a significant improvement in the station's contribution to the character and appearance of the area.

The extent to which the proposed works would bring substantial benefits for the community, in particular by contributing to the economic regeneration of the area or the enhancement of its environment.

The benefits to the community are the enhanced appearance of the station building and this proposal should be considered in the context of the whole station enhancement package that Network Rail are currently progressing that will significantly improve both the functionality and appearance of the station.

8.2 London Plan

The London Plan (February 2004) is the most applicable regional planning document. The adoption of Camden's UDP post dates that of the London Plan and it is considered that the policies in the UDP relevant to this proposal contain the objectives of those of the London Plan.

8.3 Camden Unitary Development Plan

The Camden Replacement Unitary Development Plan (June 2006) proposals map shows the site affected by the following specific policy notations:

King's Cross Opportunity Area (Section 9, Proposal site 24)

- Transport Proposal area (TP1/TP4/TP5)
- Conservation Area 22
- Strategic View – Viewing Corridor
- Strategic View – Wider Viewing Corridor

Section 9 of the Replacement UDP contains both strategic (SKC1–SKC4) and more specific area development policies (KC1-KC12) relating to the King’s Cross Opportunity Area. Policies SKC1-4 distil, to the local level, the overarching policies contained in the national guidance and seek sustainable, well integrated and comprehensive development - all to a high standard of design (both built and landscape).

Policy B6 – Listed Buildings states that ‘...the Council will only grant listed building consent for alterations and extensions to a listed building where it considers this would not cause harm to the special interest of the building.’

It is considered that the proposal will not cause any significant harm to the special interest of the listed building as the only structural interventions into the fabric of the building are drilled holes to allow for the attachment of the monitoring equipment, which will be filled in and made-good once work has been completed.

For these reasons it is considered that the proposal does not cause any harm to the special architectural and historic interest of the building.

Policies within the Local Plan actively support the principles of a high standard and quality of design, as well as promoting proposals that enhance the special character and appearance of Conservation Areas and the buildings within those areas. King’s Cross Station is located within a Conservation Area and the station significantly contributes to the special character and appearance of the Conservation Area street scene.

As stated, the proposal facilitates the improvement works to the station and help ensure its protection during the works. These works will significantly improve the appearance of the station and subsequently along with the other station proposals will contribute to the enhancement of the station environment and local area. The proposal therefore meets the requirements of policies B1 – General Design Principles, B7 – Conservation Areas, KC8 – Design and KC11 – Heritage.

9.0 Conclusion

The proposal, for which Listed Building Consent is sought, is for the installation of structural monitoring equipment which will ultimately ensure the structural integrity of the buildings during the construction works and are therefore required in order to carry out the necessary refurbishment work. Those works are subject to the listed building consents 2006/3394/L and 2007/2611/L.

The size and extent of the monitoring equipment is as small and limited to as few as possible in order to ensure that all the necessary areas of the station are monitored. The equipment is required for just over a year time period and any impact on the historic fabric of the station will be minimal and will not permanently injure the special interest of the building.

The proposal accords with national, regional and local development plan policies and is considered to be a necessary element of the wider station renewal and enhancement programme.

