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1 Introduction

1.1 Purpose of this submission

The Listed Building Consent Reference 2006/3394/L for King's Cross Station Redevelopment, dated 09 November 2007, includes Condition 6 requiring approval of all demolition or alteration by partial demolition.

The purpose of this submission is to fulfil the requirements of Condition 6 for Platforms 2/3 in the Main Train Shed.

1.2 Details of relevant Consent Conditions

The Listed Building Consent Reference 2006/3394/L Condition 6 states:

Before any work is undertaken in pursuance of this consent to demolish or alter by way of partial demolition any part of the building, structural engineer's drawings and/or method statement, indicating the proposed method of ensuring the safety and stability of the building fabric to be retained throughout the period of demolition and reconstruction, shall be submitted to and approved in writing by the Council as local planning authority, advised by English Heritage. The relevant work shall be carried out in accordance with such structural engineer's drawings and/or method statement thus approved.

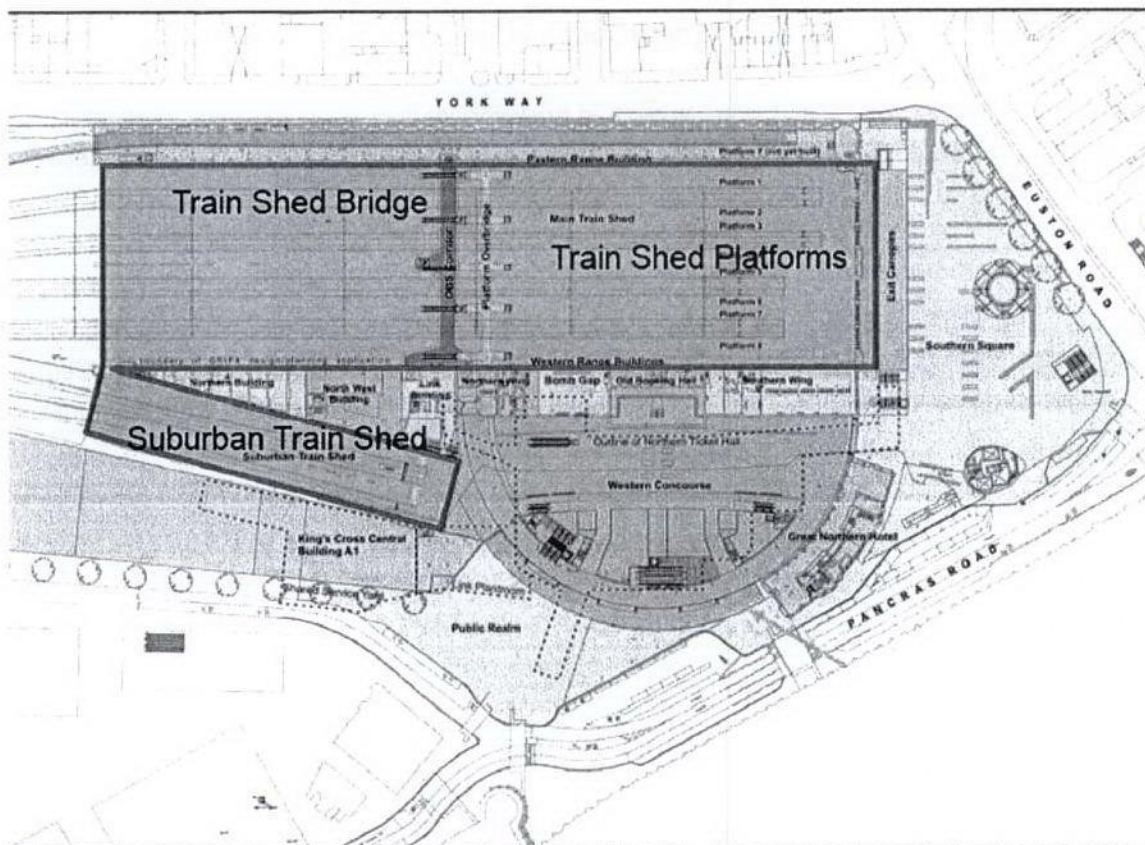
1.3 Description of Relevant Works

The demolition/excavation works and remedial works proposed to Platforms 2/3, an island platform located within the eastern bay of King's Cross Station comprise the following :

- Excavation and construction of the footbridge foundations
- Excavation and installation of new service routes
- General repairs to platform riser walls
- Replacement of riser walls in specific locations
- Resurfacing of platforms – including deck reconstruction and installation of tactiles

2 Packages Plan

Package Scope



3 Heritage considerations

When Kings Cross Station opened it had only 2 passenger platforms, one of the West of the station for departures and the other on the East, for arrivals. For this reason the Booking Hall, passenger lounges and other facilities were all accommodated within the Western Range buildings while the Cab Road was situated alongside the arrivals platform on the Eastern side.

These original platforms, now referred to as Platforms 1 and 8, were constructed of York Stone supported on brick arches. Platform 2/3 is not of uniform construction, varying throughout its length due to the different stages of construction. No arched brickwork or York stone has been discovered within the main platform, however York stone coper edges have been located along Platform 2 between arch reference 16 to 20.

In December 2009 Network Rail submitted a Method Statement in connection with removal of remaining Handyside Bridge Structure on Platform 2/3. This was approved and MOLAS recorded the process as the remains of the bridge were dismantled, and salvaged, in accordance with the approved Salvage Strategy.

MOLAS will again be in attendance to record the excavation on Platforms 2 & 3. Work will be suspended for inspection by Camden and EH, as required by the Listed Building Consent, if anything noteworthy is revealed

4 Method Statement

Network Rail

King's Cross Station

Redevelopment

Programme Package 3

GRIP 5

Listed Building Consent

Application Reference : 2006/3394/L dated 09 November 2007

Part of Condition 6

Demolition or partial demolition

Issue 1.0 April 2010

Issue for Acceptance