



HERITAGE STATEMENT

In respect of

**2 DUMPTON PLACE AND
LAND TO THE REAR OF
68-88 GLOUCESTER
AVENUE, NW1 8JD**

On behalf of

SARENA LTD

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Fig. 1: Visualisation of the proposed office building, echoing the prevailing solid-void ratios and the material palette of properties on Gloucester Avenue in the use of stock brick and light coloured render. (PMA Architects)

1.0 INTRODUCTION

- 1.1 This Heritage Statement has been prepared by Edward Kitchen of CgMs Historic Buildings on behalf of Sarena Ltd, in support of applications seeking planning permission and conservation area consent for the redevelopment of property at 2 Dumpton Place and Land to the Rear of 68-88 Gloucester Avenue, NW1 8JD.
- 1.2 The Heritage Statement is intended to assist those involved in the determination of the applications to make an informed assessment of the proposals, based on a clear understanding of the site within the context of the Primrose Hill Conservation Area.
- 1.3 Conservation areas are defined as 'designated heritage assets' under *Planning Policy Statement 5: Planning for the Historic Environment* (2010), and consent applications relating to such assets are required by virtue of Policy HE6 to include information which identifies and explains any potential impact upon the significance of such assets. This report meets that requirement by providing an account of the history of the site, assessing the impact of development upon that significance, and considering the merits of the proposals with reference to relevant national and local planning policy.
- 1.4 The report should be read alongside the submission drawings and supporting documents accompanying the applications, including the Planning Statement prepared by CgMs, and the Design and Access Statement prepared by the architects, PMA.



Fig. 2: Map of the area dating from the 1860s, showing the development of residential properties along Gloucester Avenue (then Gloucester Road) and Dumpton Place (then Fitzroy Place) with a large Engine Shed located immediately to the north east of the application site.

2.0 LEGISLATIVE AND POLICY FRAMEWORK

2.1 This section sets out the national and local planning policies and guidance relevant to the development proposals, within the legislative framework concerning areas of special architectural or historic interest provided by the Planning (Listed Building and Conservation Areas) Act 1990 (the 1990 Act). Section 72 of the 1990 Act requires that in the exercise of all planning functions, local authorities pay special regard to the desirability of preserving or enhancing the character or appearance of a conservation area.

National Planning Policy

PPS1: Delivering Sustainable Development (2005)

2.2 PPS1 sets out the Government's overarching objectives for delivering sustainable development through the planning system. The policy statement emphasises the need for good design in all new development and confirms that 'design which is inappropriate in its context, or which fails to take the opportunities available for improving the character and quality of an area and the way it functions, should not be accepted.'

PPS 5: Planning for the Historic Environment (2010)

2.3 PPS5 sets out the Government's objectives for the historic environment and establishes the need for decision-making to be based on a clear understanding of the particular significance of any heritage assets affected by proposed development. This understanding should be used by the local planning authority to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposals.

2.4 PPS 5 also emphasises the need for the historic environment to remain a living and integral part of the local scene through pro-active and intelligent management of heritage assets. It is acknowledged that change will sometimes be desirable to facilitate viable uses that can provide for their long term conservation.

2.5 In relation to design, applicants are encouraged to consider how existing valued heritage assets can inform high quality design that is inspired by its local context

and how best the contemporary design can fit comfortably into its surroundings. This should deliver sustainable communities and places that residents value highly. The development is also likely to last longer and thereby make a greater contribution to sustainability. Local planning authorities are encouraged to seek well-considered and inspirational design that is founded on a full understanding of local context (para 44).

- 2.6 The associated Practice Guide to PPS 5 encourages applicants to have pre-application discussions with local authorities, particularly where development proposals may impact on heritage assets or their setting (para 63). The proposed development has been the subject of pre-application discussions with the Council's planning, design and conservation officers.

Conservation Principles, Policies and Guidance - English Heritage, 2008

- 2.7 Conservation Principles outlines English Heritage's approach to the sustainable management of the historic environment. In line with PPS5, the document emphasises the importance of understanding significance as a means to properly assess the effects of change to heritage assets. The English Heritage guidance describes a range of heritage values, which enable the significance of assets to be established systematically. The four main 'heritage values' are: evidential, historical, aesthetic and communal.

The London Plan (2008)

- 2.8 The broad principles established in national policy and guidance on the historic environment are reflected in the Mayor's 2008 London Plan. Policy 4B.12 seeks to ensure that the protection and enhancement of historic assets in London are based on an understanding of their special character, and form part of the wider design and urban improvement agenda. The London Plan also seeks to promote design principles for a compact city which maximises the potential of sites and respects local context, history, built heritage, character and communities (Policy 4B.1).

Camden Core Strategy Pre-Adoption Version (2010)

- 2.9 Camden's Unitary Development Plan (2006) is soon to be superseded by the adoption of the Camden Core Strategy and Development Policies. The Core Strategy and Development Policies have been the subject of examination with the

Inspector's report now received. These documents will be adopted by the time of consideration of these applications and will supersede the policies of the adopted UDP.

- 2.10 The following Core Strategy Policy has been identified as being of particular relevance to the consideration of the proposals in terms of design and conservation:

CS14 - Promoting high quality places and conserving our heritage

Core Strategy policy CS14 - sets out the Council's overall strategy on promoting high quality places, seeking to ensure that Camden's places and buildings are attractive, safe, healthy and easy to use and requiring development to be of the highest standard of design that respects local context and character. Camden has a unique and rich built and natural heritage, with many areas with their own distinct character, created by a variety of elements including building style and layout, history, natural environment including open spaces and gardens, and mix of uses. We have a duty to respect these areas and buildings and, where possible, enhance them, when constructing new buildings and in alterations and extensions

- 2.11 The following Development Policies have also been identified as being of particular relevance to the consideration of the proposals in terms of design and conservation:

DP24 Securing high quality design

Policy DP24 contributes to implementing the Core Strategy by setting out our detailed approach to the design of new developments and alterations and extensions. These principles will ensure that all parts of Camden's environment are designed to the highest possible standards and contribute to providing a healthy, safe and attractive environment.

DP25 Conserving Camden's heritage

Policy DP25 provides further guidance on the preservation and enhancement of the historic environment.

Primrose Hill Conservation Area

2.12 The site lies along the north eastern boundary of the Primrose Hill Conservation Area, as designated by the London Borough of Camden on 1 October 1971, and extended on 18 June 1985. This designation is supported by a Conservation Area Statement (published in 2001), setting out the Council's rationale for designation as well as policies and guidance for the area's ongoing management.

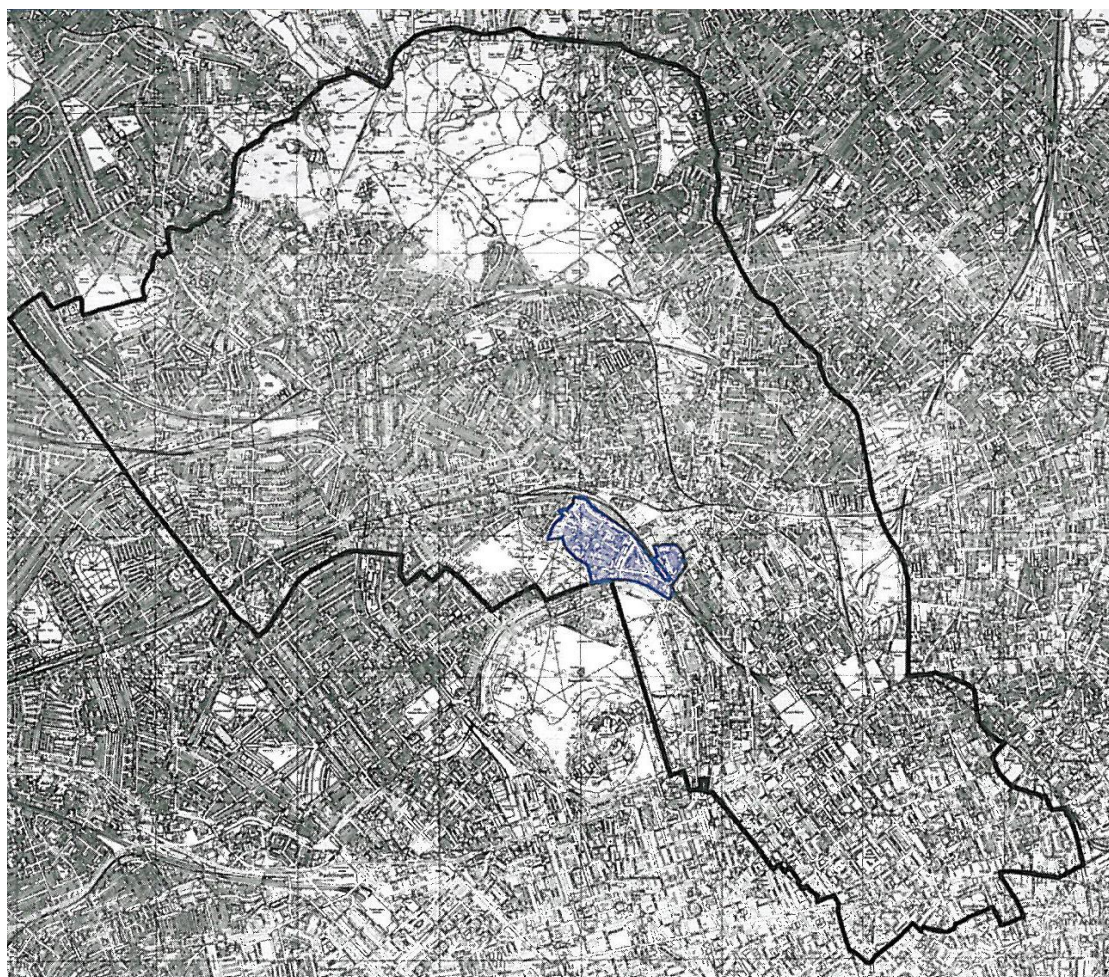


Fig. 3: Map showing the location of the Primrose Hill Conservation Area within the London Borough of Camden

2.13 The Conservation Area Statement describes the development of the area from the early nineteenth century when Primrose Hill began to develop following completion of John Nash's Regent's Park to the south. The area's development would also be heavily influenced by two new transport routes - the Regent's Canal running from Regent's Park towards Camden Lock in the east, and the new railway line connecting London and Birmingham.

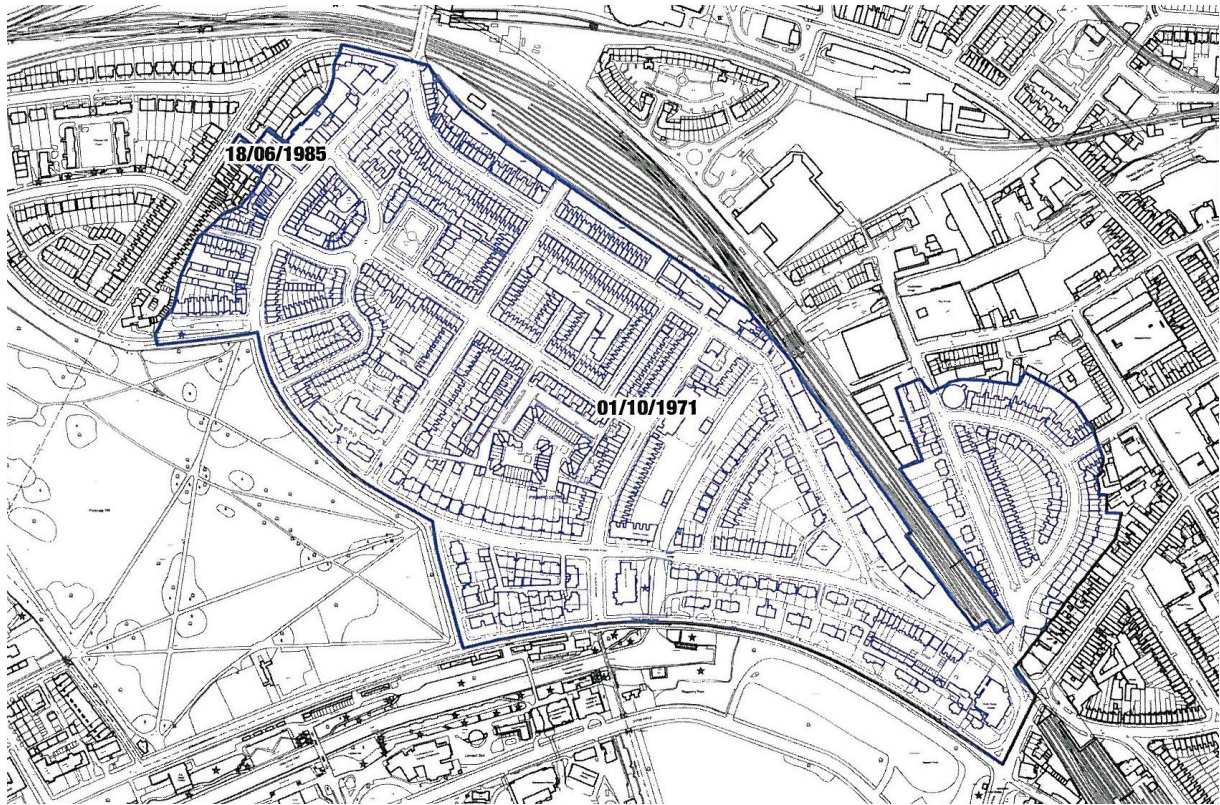


Fig. 4: Map showing the Primrose Hill Conservation Area boundary with the extension of the designation to include the north part of Erskine Road in 1985.

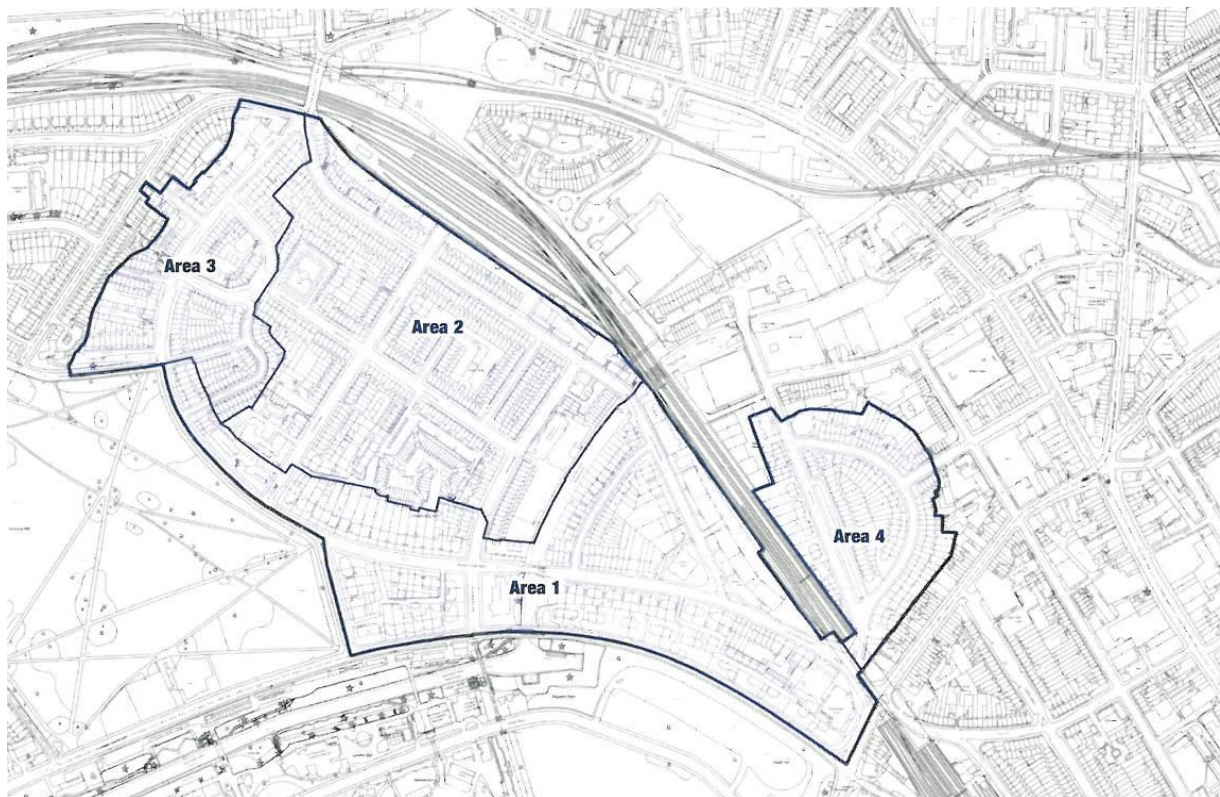


Fig. 5: Map showing the Primrose Hill Conservation Area with the four defined Sub Areas identified in the Conservation Area Statement. The application site lies in 'Sub Area 2: Central Area'.

2.14 The Conservation Area Statement identifies four distinct sub areas, with the development site being located within Sub Area Two: Central Area. This part of the Conservation Area is identified as possessing an urban character with a high density of development, and only a few areas of greenery. The Statement notes that the area is 'dominated by long terraces of mid 19th century houses that are set back from the pavement with small lightwells and railings', with relatively few buildings dating from earlier or later periods.

2.15 While the building on the application site is not identified as making a contribution to the area's character, the terraces of three storey residential properties on Gloucester Avenue are identified, and new development should be considered in the context of their setting as heritage assets.

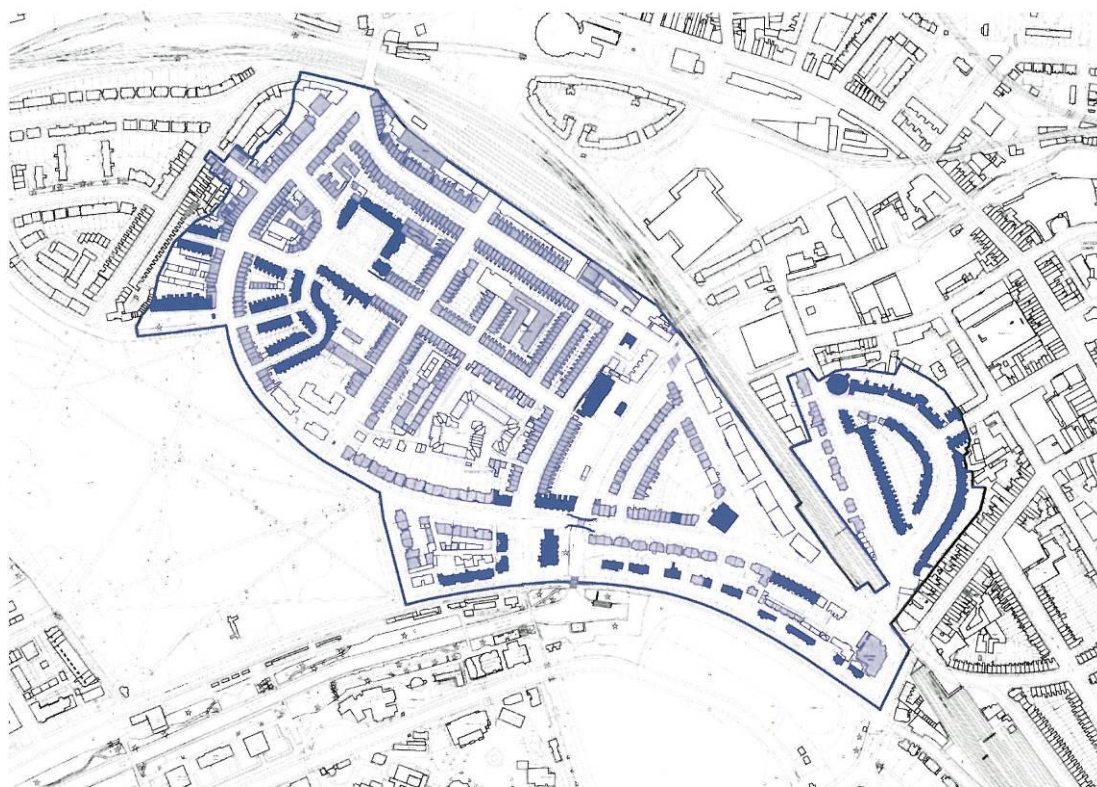


Fig. 6: Map showing listed buildings (dark blue) and buildings identified as making a positive contribution to the Primrose Hill Conservation Area (light blue).

2.16 The Conservation Area Statement also makes clear that all 'new development should be seen as an opportunity to enhance the Conservation Area. All development should respect existing features such as building lines, roof lines, elevational design, and where appropriate, architectural characteristics, detailing, profile, and materials of adjoining buildings.'

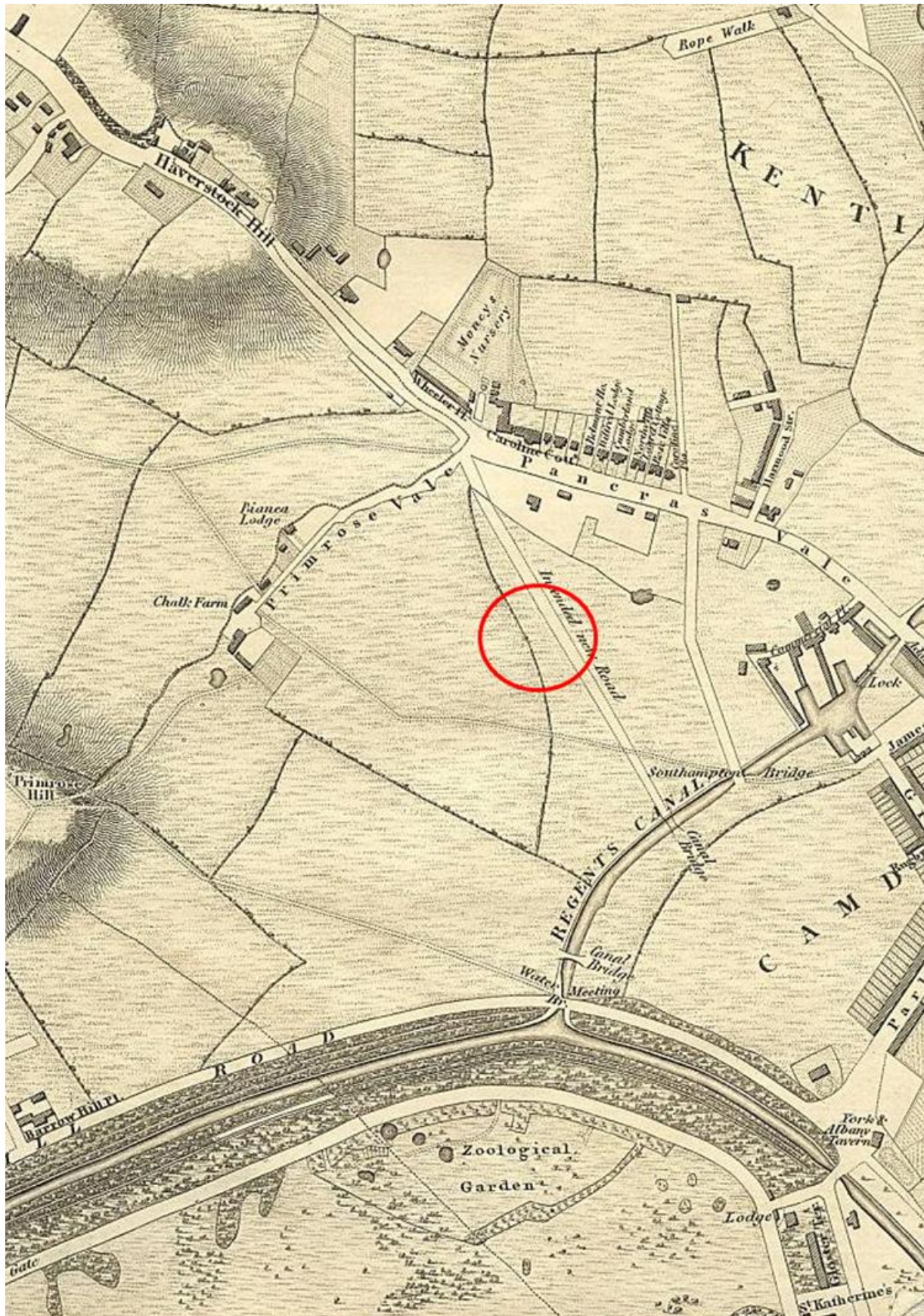


Fig 7. Greenwood's Map of London, 1827, with the 'Intended new Road' broadly following the line of Gloucester Avenue north towards Haverstock Hill.

3.0 HISTORICAL AND ARCHITECTURAL APPRAISAL

3.1 The application site lies along the north eastern boundary of the Primrose Hill Conservation Area, forming a narrow strip of land between the railway lands to the east and a terrace of three-storey properties on Gloucester Avenue, dating from the 1850s.

3.2 Primrose Hill began to attract development interest in the first quarter of the nineteenth century, when the completion of John Nash's Regent's Park showed the potential to develop the open land in the area to provide detached villas and terraces within easy reach of central London. Greenwood's map of 1827 (fig. 7) shows the course of the Regent's Canal to the south of the site, and plans for an 'Intended new Road' broadly following the course of Gloucester Avenue towards Haverstock Hill.



Fig. 8: Map of London dating from the 1860s, showing the development of residential properties along Gloucester Avenue, and the road now known as Dumpton Place.

3.3 At the same time, plans began to emerge for a new railway line to connect London and Birmingham, which according to an Act of parliament would terminate 'on the West side of the High Road leading from London to Hampstead, at or near to the first bridge Westward of the Lock on the Regent's Canal at Camden Town.' The Act, dated 6 May 1833, allowed the London to Birmingham Railway Company to construct the railway and acquire land to the north east of the current

application site to be used as railway lands for the various industrial buildings necessary at the termination of the new line. The work on the new line was undertaken by Messrs. Stephenson and Son, who only later presented proposals to extend the line further into London, to Euston, initially by means of stationery engines housed in a large vaulted complex under the railway lands.

3.4 Despite the extension of the line south, Camden Goods Yard retained its overall importance in the operation of the new line by supporting the management of good and engines in close proximity to central London.

3.5 Through the same period of the 1830s and 1840s, land to the west of the railway lands, owned by Lord Southampton, was divided and sold in lots to a number of speculative developers. Early plans for the area show that the initial intention was to develop the area with a number of large detached villas in their own grounds, some of which are shown overlooking the new railway lands. As development occurred, however, both the density and layout of the plans changed, with Gloucester Avenue eventually being developed with terraces turning their back against the railway lands, and a small number of similar properties extending down Dumpton Place (then known as Fitzroy Place), terminating at the boundary of the railway lands and the site of a large Engine Shed.

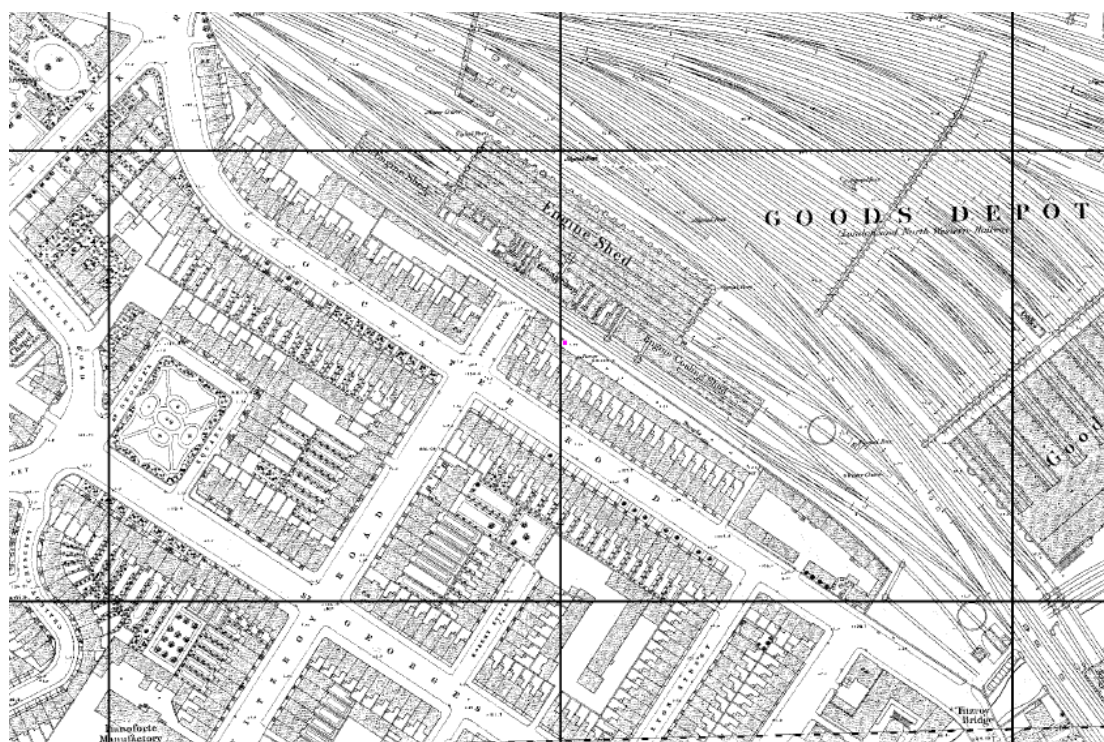


Fig. 9: Map dating from 1872-3 showing the development of residential properties on Dumpton Place

- 3.6 The history and development of the application site over the following two hundred years can be set within the context of an ongoing interplay between the area's attractive residential character on the one hand, and the close proximity of the railway lands on the other, as 'peaceful' tree-lined avenues sat cheek-by-jowl with the throng and smoke of countless engine sheds, coal stores and commercial warehouses.
- 3.7 As the railway's importance grew through the Victorian era, so too did the impact of the railway lands on the wider character and fortunes of Primrose Hill. Levels of noise, vibration, steam and smoke pollution increased, and the industrial and commercial activities associated with the railway began to bleed into the surrounding streets, with one example of this phenomenon being found at 44 and 44A Gloucester Avenue, where the Electric Telegraph Company occupied a house to support their work trialling new telegraph signalling for trains travelling between Euston and Camden.

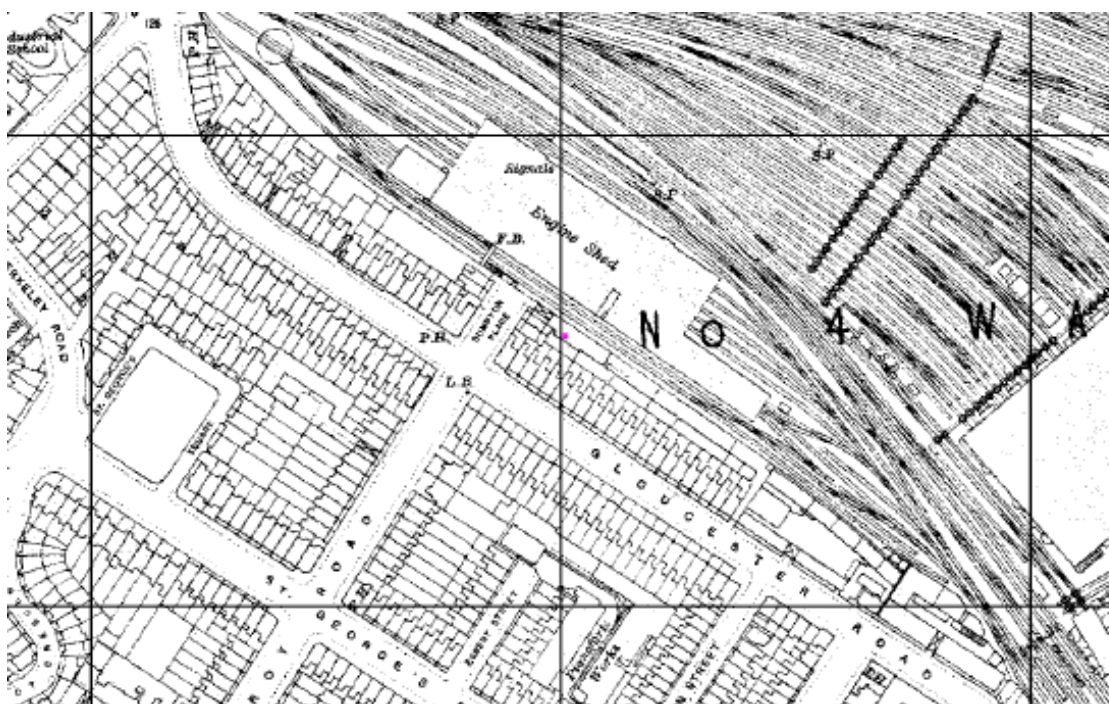


Fig. 10: Ordnance Survey Map of 1916, with what appear to be terraced properties shown to the rear of 88 Gloucester Avenue.

- 3.8 The application site also saw development, seemingly becoming more attractive for industrial (rather than residential occupiers), owing to its proximity to the railway lands. Aerial photographs of the site taken during the Second World War, show the extent of railway activity by that date, and the number of sheds and warehouses which had grown up to the rear of Gloucester Avenue (see fig. 11).



Fig. 11: Aerial photograph of the site published in 1946, with the large engine shed still visible immediately to the north east of the application site.

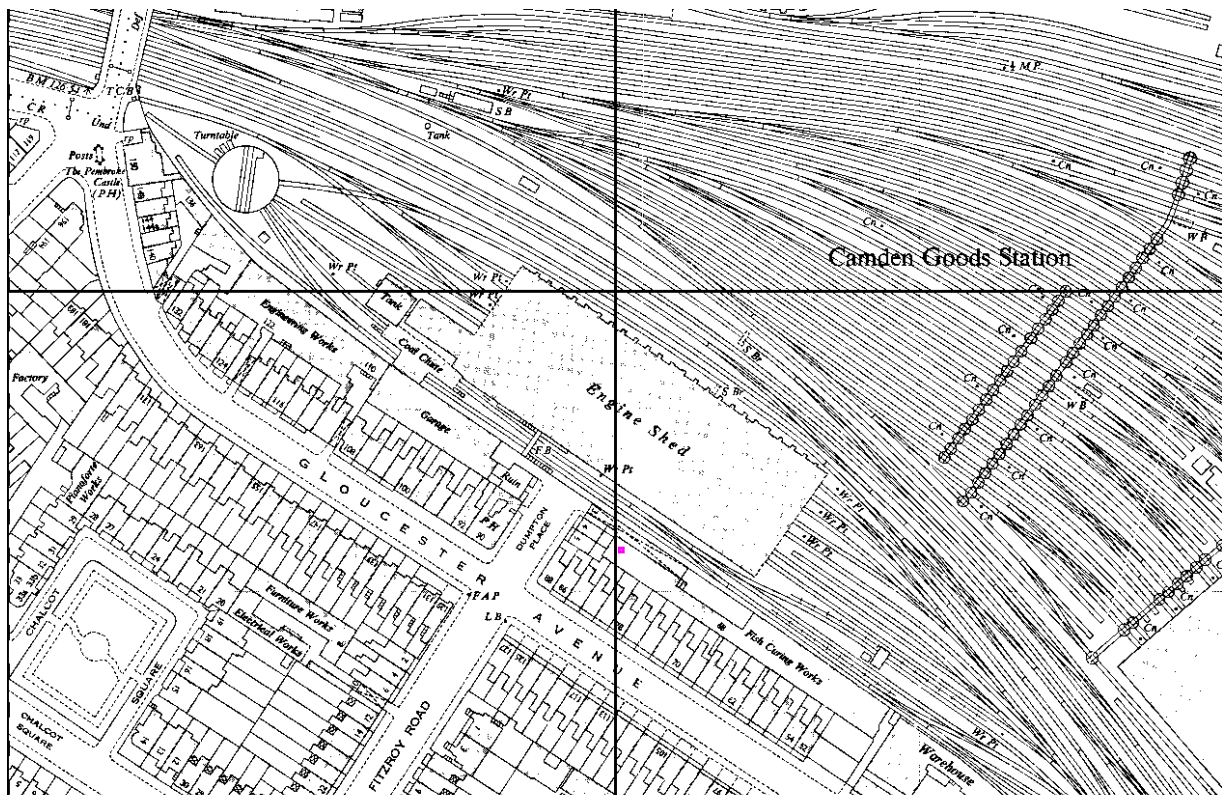


Fig. 12: Ordnance Survey Map from 1953-5, when a 'fish curing works' is shown running through the site. Property on the site of the current Volvo office at 1 Dumpton Place is marked as a ruin.

- 3.9 The electrification of the line in the 1960s - by then known as the West Cost Main Line - saw a marked shift in the character of the area, and improvements in environmental quality coinciding with a more general decline in levels of industrial activity on the Camden Good Yard site.
- 3.10 Against this background, Primrose Hill began to emerge as one of the most attractive residential areas in London, reinforced, in 1971, in the designation of the Primrose Hill Conservation Area, deliberately excluding the railway lands to the east to focus instead on the area's 'series of well laid out Victorian terraces'. While small pockets of industry and commercial activity are identified in the Conservation Area Statement, it was evidently the special architectural and historic interest of the residential area which formed the primary basis for designation.
- 3.11 The existing commercial properties on Dumpton Place do not sit comfortably within this context, nor do they contribute to the character or appearance of the area.



Fig. 13: The west elevation of the existing single-storey workshop building at 2 Dumpton Place. Notwithstanding its stock brick construction, the building is not considered to make a positive contribution to the character and appearance of the conservation area.

- 3.12 The application site is occupied by a simple, single-storey workshop building dating from the second half of the twentieth century, with its gable end elevation facing Dumpton Place and the remaining part of the building extending back through the site parallel to the railway lines. The workshop building was previously used as part of the Volvo Servicing Centre based at No. 1 Dumpton Place, directly opposite the site (fig. 16).

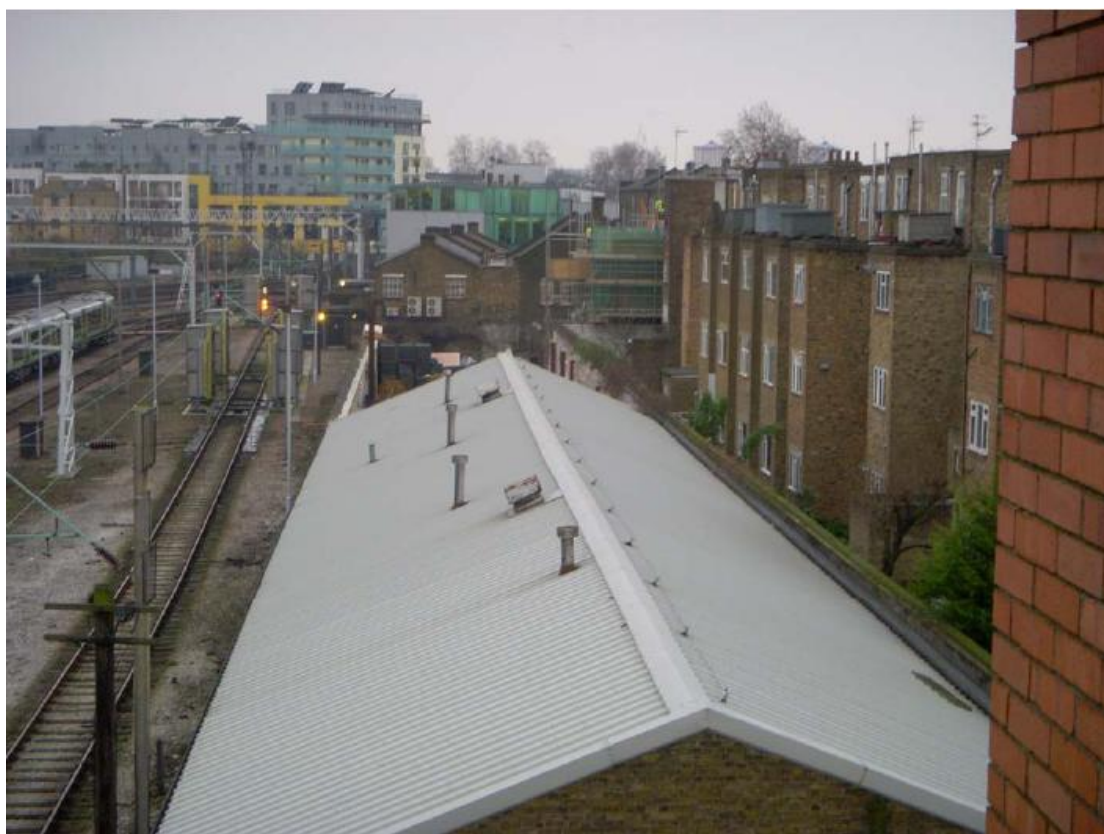


Fig. 14: View of the application site from 1 Dumpton Place, showing the extent of the remaining structure following partial demolition.

- 3.13 The site's southern boundary is marked by a high brick wall, once part of the workshop building (exposed following partial demolition and visible in fig. 15). This wall is between 5m and 6m in height above the level of the rear gardens of the properties on Gloucester Avenue.
- 3.14 The larger red brick building at 1 Dumpton Place dates from the 1980s, and is identified as having a negative impact on the character and appearance of the conservation area.



Fig. 15: View of the retained wall on the southern boundary of the site, with the terrace of mid nineteenth century properties beyond.



Fig. 16: View of 1 Dumpton Place from the application site. The Conservation Areas Statement identifies the building as having a negative impact on the character and appearance of the conservation area.

4.0 DEVELOPMENT PROPOSALS

4.1 The proposals submitted envisage demolition of the remaining part of the single storey workshop building, retaining only the southern boundary wall between the application site and the rear of the terraced properties at 68-88 Gloucester Avenue. Proposals for the demolition of this workshop building have been considered on previous occasions, including in 2004, when proposals for the redevelopment of a larger area of land to the rear of Gloucester Avenue were presented (PEX0200634 and CEX0200635).



Fig. 17: View of 2 Dumpton Place looking north from the junction with Gloucester Avenue

4.2 Conservation Area Consent for the demolition of the single-storey workshop building on the application site was also considered in relation to an alternative scheme of redevelopment in 2009, under application references 2009/1296/P and 2009/1299/C, when the officer report noted that:

'A 20th century light industrial building occupies the entire site: it is a low-rise single storey brick structure. The building has a gable end with 3 windows and a loading bay onto Dumpton Place, and a number of multi-pane metal-framed windows onto the railway tracks. The building is not identified as making a

positive contribution to the character and appearance of the Conservation Area so consent for its demolition does not require a PPG15 test.

Due to the location of the site at the rear of the terrace 52-88 Gloucester Avenue, it is not very prominent within the Conservation Area; the breadth of the railway cutting results in the site having little prominence when viewed from across the tracks.'

- 4.3 In light of the previous consent, this recent assessment by officers, and our own appraisal of the existing building, the proposed demolition of the remaining part of the single-storey workshop is not considered contentious subject to there being an appropriate scheme for the site's redevelopment.
- 4.4 The current proposals are therefore presented as an opportunity to enhance the Primrose Hill Conservation Area and to strengthen its character in a mixed use development, comprising four new houses on land to the rear of Gloucester Avenue, and a new office building facing Dumpton Place. This is also an opportunity to introduce a quality of architectural design that has previously been absent on the site.

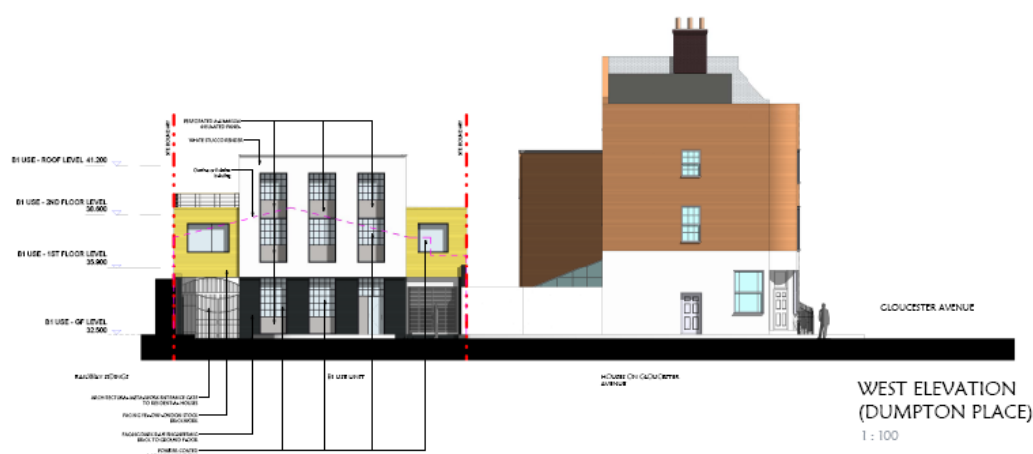


Fig. 18: Scheme drawing showing the west elevation of the new office building in context with the property at 88 Gloucester Avenue. (PMA Architects)

- 4.5 Owing to the site's backland location, the proposed development's most prominent elevation in the streetscape is that to Dumpton Place, where, following pre-application discussions with the Council's officers in planning, design and conservation, the new building has been designed to respond to the surrounding

townscape in a contemporary architectural idiom. This elevation clearly announces itself as a commercial building, but otherwise remains subservient to the property at 88 Gloucester Avenue in terms of its scale, and in its use of stock brick and lightly coloured render to respect the established material palette of historic buildings in the area.



Fig. 19: Visualisation showing the proposed development from the north, against the backdrop of properties facing Gloucester Avenue. (PMA Architects)

- 4.6 Though the site is discreetly located to the rear of the properties, efforts have also been made to ensure that all elevations add visual interest to the townscape, including the north elevation, which is afforded limited distant views from the railway lands. Here the linear site would be developed with a scale and rhythm which reflects that found along Gloucester Avenue, using a range of coloured renders and glass atriums to lend the scheme its own contemporary architectural identity.
- 4.7 The aim throughout has been to deliver a scheme which strengthens and reinforces the area's established character and appearance in a contemporary architectural idiom. The scheme would also create a range of high quality outdoor spaces for private and communal use, with an entrance courtyard accessed from Dumpton Place.
- 4.8 Further details on the design rationale behind the proposed development are provided in the Design and Access Statement prepared by PMA Architects.

5.0 CONCLUSIONS

5.1 The national planning policy framework concerning the management of the historic built environment is based on a clear understanding of conservation as the process of managing change to places in ways that will best sustain their heritage values, while recognising opportunities to reveal or reinforce those values for present and future generations.



Fig. 20: Visualisation of the proposed office building, echoing the material palette of properties on Gloucester Avenue in the use of stock brick and light coloured render. (PMA Architects)

5.2 This report has outlined the history and development of the area surrounding the application site, up to and including the Council's decision in 1971, to draw a clear distinction between the industrial railway lands to the east, and the area of residential streets to the west, with the inclusion of the latter in the Primrose Hill Conservation Area.

5.3 The existing building on site is not of architectural or historic interest, nor does it make a positive contribution to the character or appearance of the wider area. This assessment of the building is supported by the Primrose Hill Conservation Area Statement, and by officer reports which have been prepared in respect of previous applications for the site. Accordingly, there is no basis to resist

demolition and redevelopment of the site in principle, providing a satisfactory scheme is presented to preserve or enhance the character and appearance of the conservation area.

- 5.4 The submitted scheme has been developed following pre-application discussion with officers, to ensure a high standard of design in a contemporary architectural idiom, which takes reference from the existing townscape in the rhythm and scale of the new development and the material palette displayed. Both the new commercial building on Dumpton Place and the four residential properties to the rear of the site would enhance the character and appearance of the Primrose Hill Conservation Area in views from Dumpton Place and Gloucester Avenue, and in the limited distant views of the site across the railway lands to the north.
- 5.5 Overall, the scheme would add architectural quality and visual interest to the townscape, with the range of stock brick, rendered and glass elements serving to reinforce qualities in the existing townscape while also delivering a scheme with its own contemporary architectural identity.
- 5.6 In light of the conclusions of this report, it is respectfully submitted that planning permission and conservation area consent should be granted.