



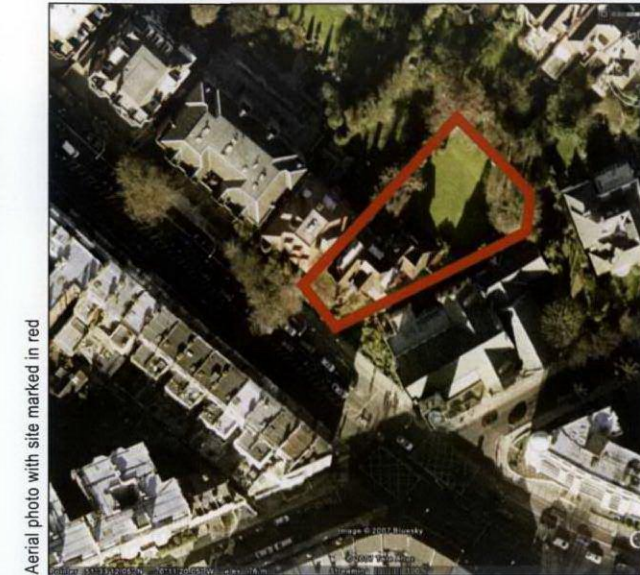
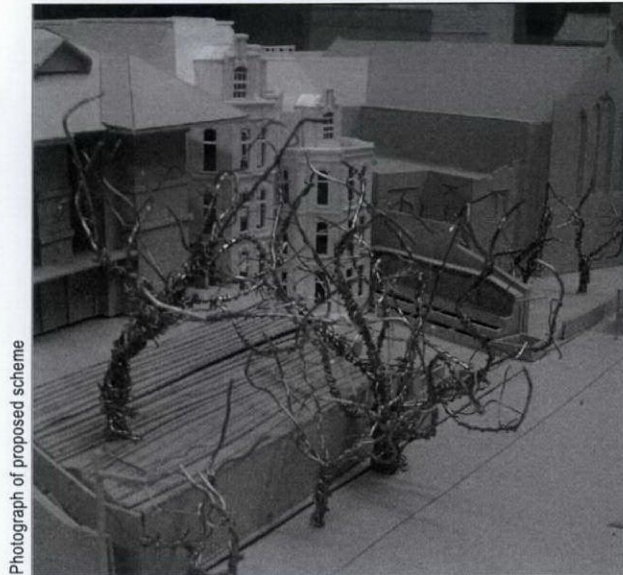
## “252 Finchley Road” Historic Scheme

PARRITT LENG

Planning Application with supporting  
Design and Access statement for discussion.

Providing 14 new apartments. Designed in a  
historic style to complement the Redington /  
Frognaal Conservation Area and Finchley Road /  
Swiss Cottage Town Centre in the London  
Borough of Camden.

December 2010

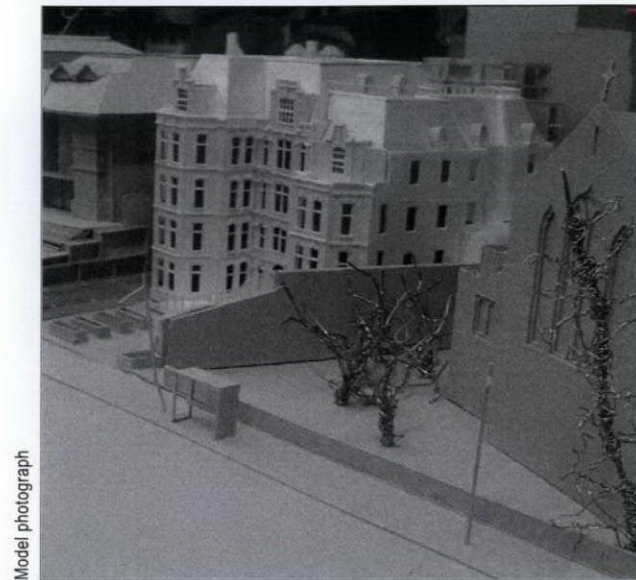


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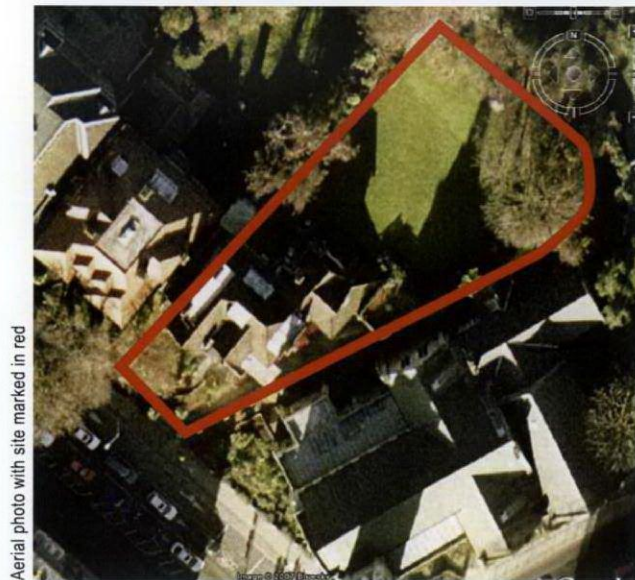
NB This A3 report contains a design, access, and sustainability statement. A set of A3 reduced prints of the A1 application drawings are included for reference.

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Model photograph



Aerial photo with site marked in red

## Introduction

This report is the supporting statement for **252 Finchley Road** planning application. Design, access and sustainability issues are categorised to support the proposed new build apartment building on Finchley Road in the London Borough of Camden.

The scheme proposes the demolition of a dwelling house which makes a neutral contribution to the character and appearance of Finchley Road and the erection of a new landmark apartment building of timeless quality, which will enhance the character and appearance of the setting.

This document has been prepared to explain and justify the design approach that has been taken, and how this solution responds to the site context. This statement shows the principle of inclusive design, including the specific needs of disabled people, have been integrated into the proposed development.

Although the site is not in the Redington / Frognal Conservation Area, the Redington / Frognal Conservation Area statement has been analysed and the proposed alterations aim to enhance the character of the area.

Parritt Leng was appointed in August 2007 to undertake the architectural and urban design for this project in collaboration with the following design team.

Client	Investland PLC.
Architect	Parritt Leng
Urban Design	Parritt Leng



## 2.0 Executive Summary

View of the existing site from Finchley Road



### General Assessment

The site is located on Finchley Road. It is important to make the distinction that the site is not in the Redington/ Frogna Conservation Area, however, its immediate boundaries share Sub Area Six (Bracknell, Greenaway and Chesterford Gardens) and Sub Area Seven (The "Triangle"- Frogna Lane, 214-248 Finchley Road and Langland Gardens) of the Redington/ Frogna Conservation Area. The site is next to St. Andrew's United Reform Church, a Grade II Listed Building and sits just on the outskirts of the Finchley / Swiss Cottage Town Centre. The proposal recognises these unique boundaries and sensitively matches the context with a new landmark building of timeless quality aimed to complement the neighbouring listed building as well as enhance the surrounding setting.

The properties lining Finchley Road are generally of mixed periods. Mostly Victorian with predominantly London stock bricks, they are characterised as medium rise buildings between 4 to 6 storeys with a strong road frontage of some townscape value leading to St. John's Wood and Golders Green. Being a highly trafficked and busy road, Finchley Road is in some respects disconnected from the rest of the Conservation Area and at odds with the quiet green nature of Redington and Frogna Lane. There are a few buildings that are of architectural merit such as No. 324 and 326, a pair of Edwardian houses with tall mansard roofs and rounded corners to their street frontages.

View of the existing site



**252 Finchley Road** or White Lodge is a substantial size dwelling. Architecturally, it is an eclectic mix of styles and it is quite difficult to ascertain the true period of its construction due to the oddities. It appears to be a building built in the 1930s with new extensions through the ages. It is not of any particular merit and would be described as a neutral building, neither contributing nor detrimental to the setting.

The new proposal aims to replace this existing building with a 14 unit apartment sensitively designed to enrich the townscape. The proposal recognises the unique setting of the church and is subservient to the building. Through its site arrangements, the dominant features of the church are enhanced and making it a positive contribution to the area. The new proposal has been designed in a historical style that echoes existing buildings on Finchley Road.

View of the entrance to the existing site

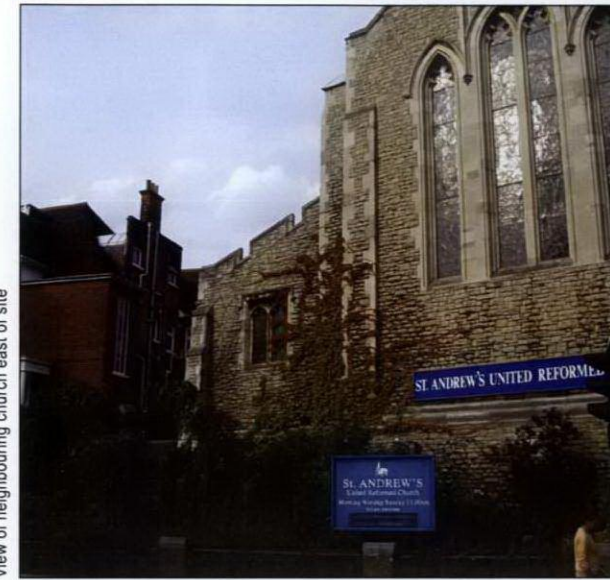


### Setting

Frogna Lane is of a generally uniform and consistent character on its southern side. To the northern side, there is a wide mix and quality and styles of architecture. The three storey late Victorian red brick houses towards the eastern end of the road (Nos. 15-19), and the neo-Georgian houses at Nos. 9-13 Frogna Lane, contribute to the street. Nos. 1 and 3 Frogna Lane and Bracknell Gate are well set back from the road and their main contribution to the character and appearance of the street is the original boundary wall which is partly constructed from "lava" bricks- mis-fired bricks from the local brickfields. At the junction with Finchley Road is the listed United Reform St Andrew's Church c1902-4, by Pite and Balfour. In coursed, snecked rusticated rubble with Bath stone dressings and slated roofs, it forms an entrance to the Conservation Area.

Access to the site is via Finchley Road. It is a busy through route into central London and this is where the front of the building faces. The proximity to traffic lends the site as a heavily urbanised setting if not for the quieter roads that lead off it.

View of neighbouring church east of site



The consistency of architectural style within this sub-area is not matched by consistency in character of the three constituent roads. The Finchley Road is a busy and noisy environment and quite unlike the roads that lead off it. The houses lining the eastern side of this road provide a fine backdrop to a comparatively poor environment.

### 252 Finchley Road Historic Scheme

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## 3.0 Site Setting



View of neighbouring residential property west of site



View of opposite retail and residential buildings on Finchley Road



Photograph of Mansion building next to St. Andrew's Church



View looking east down Finchley Road

### Historic Appraisal

These notes have been derived from the Borough of Camden's website and aim to demonstrate our alignment with the council's thoughts in relation to this area.

Finchley Road has been an important route into and out of London since it was built in the 1830s. An unusual inn, built on one of its major junctions, was even the inspiration for the name of the surrounding area, Swiss Cottage.

It still retains a slightly bohemian feel - a throwback to the 1930s, which saw refugees from mainland Europe arrive in large numbers. These included psychologist Sigmund Freud, whose home is now a museum dedicated to his life and work and whose statue by Oscar Nemon can be found nearby on the corner of Belsize Lane and Fitzjohn's Avenue.

### Context Appreciation

The Redington/ Frogna Conservation Area occupies an area of sloping land to the west and south west of the historic centre of Hampstead Village. It forms a well-preserved example of a prosperous late 19th century and Edwardian residential suburb. The houses are predominantly large detached and semi-detached and display a variety of formal and free architectural styles typical of the last 19th and early years of the 20th centuries. On the whole, these are built in red brick with clay tiled roofs, occasional areas of tile hanging and render and many of them have white painted small pane windows. Mature trees and dense vegetation form the dominant features of the street scene in many of the "Avenues" and "Gardens" of the Conservation area. In addition, the rear gardens, many of which are sizeable, make a contribution to the area's verdant quality. The gardens also contribute to the ecological balance of the area.

Of great significance to the area's character are its contours and slopes causing numerous views and vistas and giving emphasis to many of the buildings.

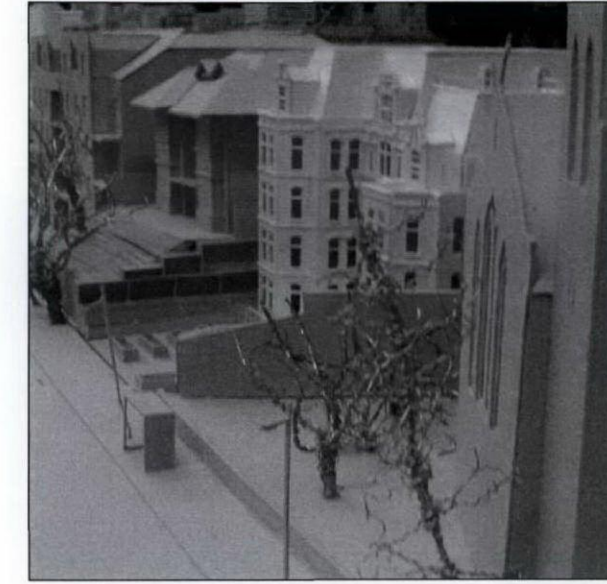
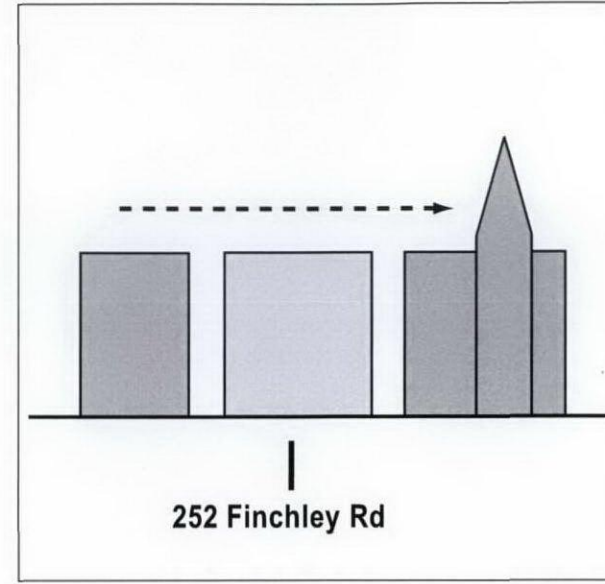
To the back of **252 Finchley Road** is Bracknell Gardens. Laid out and developed in 1905, it features a mix of Free style and neo-Georgian houses designed by eminent architects such as C.H. Saunders and Willaim A. Burr. Bracknell Gardens rises slightly from its northern and southern ends, at the junctions with Heath Drive and Frogna Lane respectively.

Bracknell Gardens features two architectural themes. The lower, southern stretch of the road is characterised by groups of semi-detached two and three storey Edwardian houses with prominent street facing gables. Whilst different designers and builders have used a variety of materials, including tile hanging, render and yellow and orange brickwork, there is a consistency of character. The upper, northern section of Bracknell Gardens has larger neo-Georgian houses of later origin. These give this section of the road a more open character which has more in common with Greenaway Gardens than the southern part of the road.

To the right of **252 Finchley Road** lies Frogna Lane. The three storey late Victorian red brick houses towards the eastern end of the road (Nos. 15-19), and the neo-Georgian houses at Nos. 9-13 Frogna Lane and Bracknell Gate are well set back from the road and their main contribution to the character and appearance of the street is the original boundary wall which is partly constructed from "lava" bricks- misfired bricks from the local brickfields. At the junction with Finchley Road is the listed United Reform St Andrew's Church c1902-4, by Pite and Balfour. In coursed, snecked rusticated rubble with Bath stone dressings and slated roofs, it forms an entrance to the Conservation Area.



## 4.0 Design Approach



### Introduction

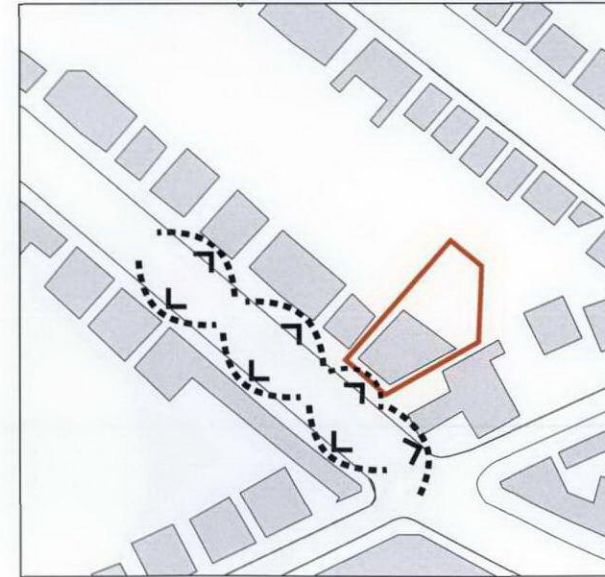
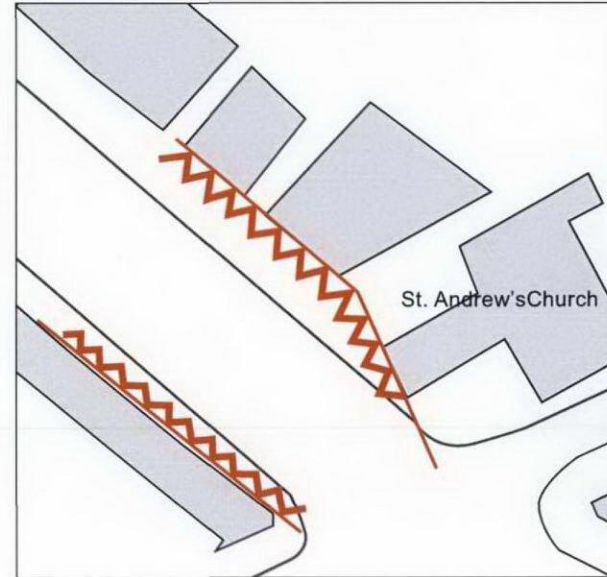
These diagrams illustrate the essential points in the analysis of the site which have been determining in the design approach and layout of the proposal for 252 Finchley Road.

### Site Geometry

- The majority of sites along Finchley Road are rectangular
- 252 Finchley Road is an irregularly shaped site
- 252 Finchley Road follows the boundaries of the site and the building opens up to the landscape at the back of the site

### Scale

- Finchley Road is lined with four to six storey buildings - there is a 5 storey apartment block and a tall listed church
- 252 Finchley Road respects the scale of the surrounding buildings - it is no higher than the existing ridge height and keeps itself subservient to the church



### Edges

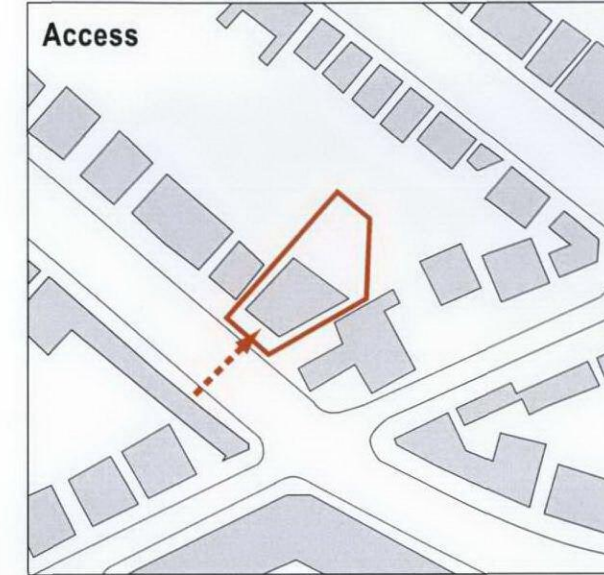
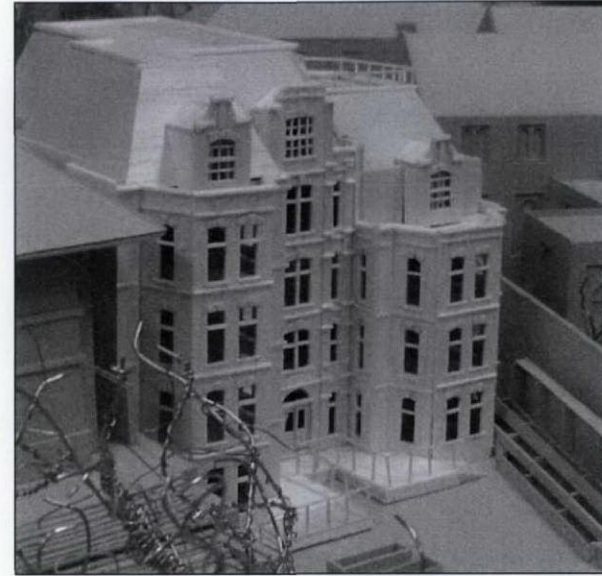
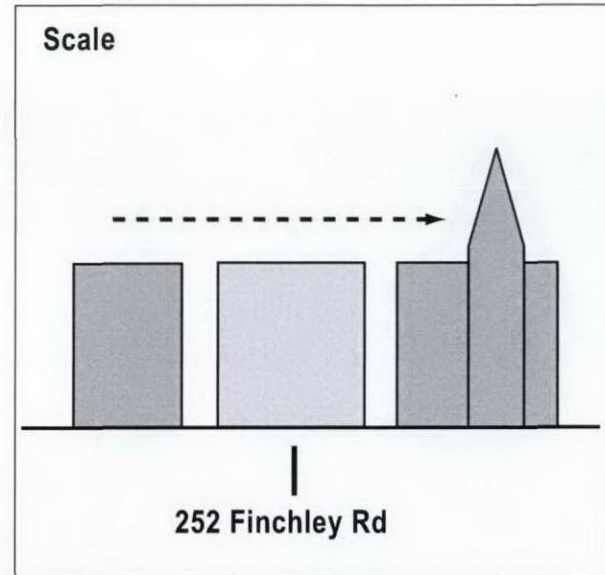
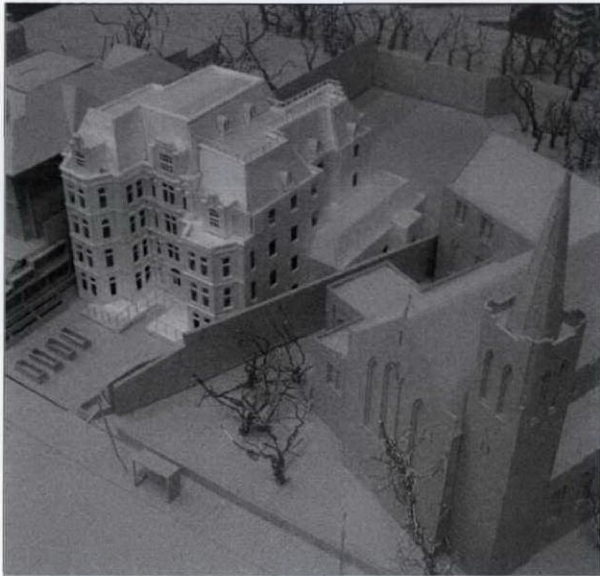
- The buildings along Finchley Road are aligned with a straight edge towards the road
- St. Andrew's Church is set back with an angle to the road
- These building lines create a space in front of the St. Andrew's Church

### Acoustics

- The dominant lines of building mass along Finchley Road make the sound bounce back and forth between the buildings
- The fact that 252 Finchley Road and St. Andrew's Church have edges set back from the road, breaks up the sound and minimizes the impact of noise



# 5.0 Design and Access Statement



## Design Approach

**252 Finchley Road** or White Lodge is a substantial size dwelling. Architecturally, it is an eclectic mix of styles and it is quite difficult to ascertain the true period of its construction due to the oddities. It appears to be a building built in the 1930s with new extensions through the ages. It is not of any particular merit and would be described as a neutral building, neither contributing nor detrimental to the setting.

The character of the surrounding buildings is mixed and characterised by a variety of building sizes and uses. The site is surrounded by buildings of varying heights. Mostly Victorian with predominantly London stock bricks, they are medium rise buildings between 4 to 6 storeys with strong road frontages.

The proposed development of **252 Finchley Road** comprises the demolition of the 5 storey dwelling with a basement, some inhabitable roof space and the erection of a 5 storey residential building of 14 units including basement car park accessed directly into Finchley Road.

On the basement level, waste and recycling bins, bicycles, storage and car-parking will be available. On ground level, there will be provision of an appropriate landscaping design to complement the building and create a sense of openness and outlook through innovative landscape architecture. All occupants of the apartment building will have shared access to a landscaped communal garden at the rear of the property backing onto Bracknell Gardens.

**252 Finchley Road** is designed to respect the dominant building line within a street, and the general rhythm of the facades, conforming to the height and alignment of the existing frontage. It is subservient to the surrounding buildings, most notably the Listed St Andrew's Church.

## Layout

**252 Finchley Road** unifies the different dominant building lines set up by the Church as well as the buildings along Finchley Road. The building follows the geometry of the site and its arrangement allows for the creation of a unique landmark building designed to complement and enhance the setting in which the building sits.

## Scale

Finchley Road is lined with four to six storey buildings. Across from the site is a tall 4 storey building. Two doors down is another tall 5 storey apartment block. Next door is a tall listed church.

The proposed development respects the scale of the surrounding buildings. It is no higher than the existing ridge height and keeps itself subservient to the church. The massing takes into consideration the proportions and layouts of adjacent properties. The proposal is of a scale that harmonises with its surroundings.

Window proportions and scale in particular have been sized to match the existing buildings. This maintains the rhythm of the streetscape. On the rear elevation and side elevations will maintain the scale, proportion and detailing. Materials have been chosen to match adjoining properties to complement the scale of the existing elements and complement the setting.

## Use

The planning history of **252 Finchley Road** reveals that it was converted to three flats in the past. The use has not changed and is still a residential building. The proposed use will maintain the site purely for residential purposes. This is compatible to existing surrounding uses as the buildings near the site are mostly medium rise residential developments such as the four storey mansion block across from the site.

## Amount

The scheme proposes 14 units. The flat areas are as detailed below:

Flat 01	124 m <sup>2</sup>	(3 beds)
Flat 02	167 m <sup>2</sup>	(4 beds)
Flat 03	63 m <sup>2</sup>	(1 bed)
Flat 04	73 m <sup>2</sup>	(2 beds)
Flat 05	85 m <sup>2</sup>	(2 beds)
Flat 06	82 m <sup>2</sup>	(2 bed)
Flat 07	59 m <sup>2</sup>	(1 bed)
Flat 08	55 m <sup>2</sup>	(1 bed)
Flat 09	63 m <sup>2</sup>	(1 bed)
Flat 10	62 m <sup>2</sup>	(1 bed)
Flat 11	118 m <sup>2</sup>	(3 beds)
Flat 12	95 m <sup>2</sup>	(2 beds)
Flat 13	96 m <sup>2</sup>	(3 beds)
Flat 14	94 m <sup>2</sup>	(2 beds)

The residential density for the site is 101 u/ha. The scheme therefore does not contravene the London Plan's density guidelines for 'urban' setting residential developments. The accommodation proposed is appropriate to the site due to its proximity to transport links + convenient presence access to shops + services. Furthermore, it holds its own presence as a building within a dense conurbation.

## Landscaping

The mature vegetation surrounding the site will be respected and there will be no detrimental impact to any key surrounding arboriculture elements in the site. The rear garden will be allowed access by all flats. There will be provision of an appropriate landscaping design to complement the building and create a sense of openness and outlook through innovative landscape architecture. The front entrance will be paved with new complementary hard wearing floor materials and lighting added for security.

## Appearance

**252 Finchley Road** adopts the appearance of traditional buildings. Designed to appear as part of the historic fabric, the approach is to blend itself with the other traditional buildings along the street as well as the conservation area. Two large bays project themselves to maximise sunlight into the development. This approach echoes other victorian mansions along Finchley Road. The materials used will be red brick.

## Access

The development has been designed to comply with approved Document Part M of the Building Regulations to embrace best practice and to enhance everyone's experience of living or visiting the residential development.

In accordance with Policy H7 of the adopted UDP all the residential units are built to Lifetime Homes Standards. Two of the units are accessible to wheelchair users and the remaining units can easily be adapted for potential residents who are wheelchair users. Good levels of external lighting and internal lighting in communal areas will assist the visually impaired as will the use of colour contrast between floors and wall surfaces, between doors and adjoining wall and between fittings and fixtures and the surfaces against which they are seen.

The entry pad and phone and letter boxes will be located so that they are suited to use by wheelchair users as well as ambulant residents, tenants and visitors.

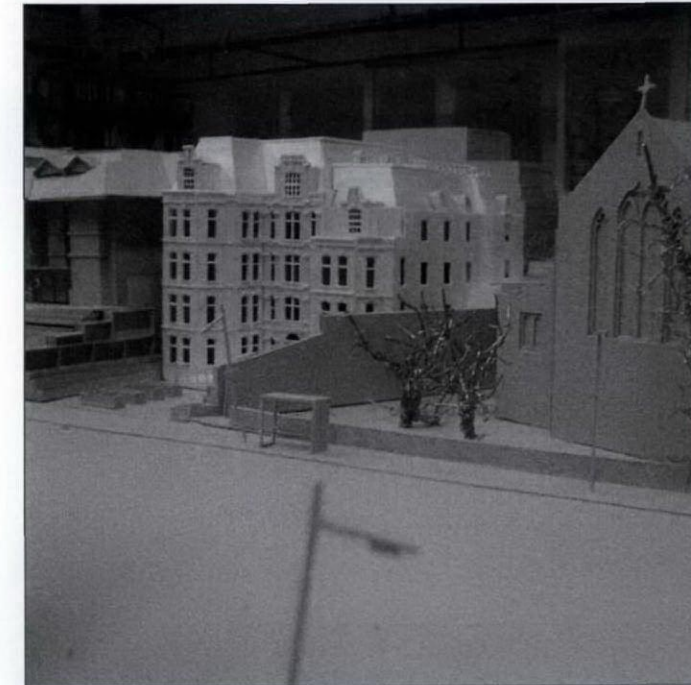
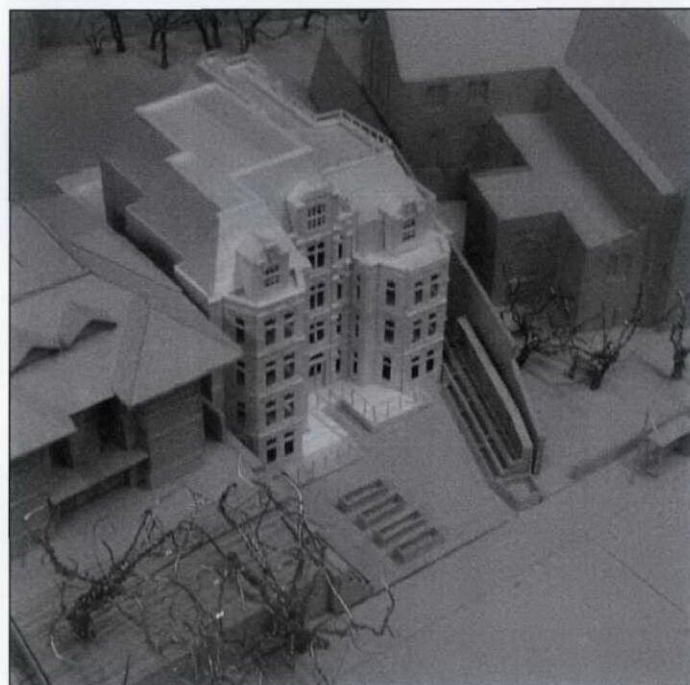
The flats are well connected with every sort of transport: Finchley underground station and overground station and abundance of bus stops within a 5 minute walk.

The location of the site ensures the proximity to all kinds of services and amenities, like booksellers, food shops, police service, banks, pharmacies, restaurants and so on.

Inclusive access is considered for the building with lifts to all floors.



## 5.0 Design and Access Statement



### 5.0 Design and Access Statement

### 252 Finchley Road Historic Scheme

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**Photographs of the Finchley Road model at development stage**  
The proposed building is a 3-dimensional object that is completely massed into the surrounding site. This can be clearly seen in the model photographs and we ask the Design Team to carefully review the architecture photographs which demonstrates how the architectural proposal enriches the street fabric through the rhythm of the fenestrations and the projecting bays.

PARRITT LENG

Architecture & Sustainable Design  
Construction & Management  
Marketing & Communications

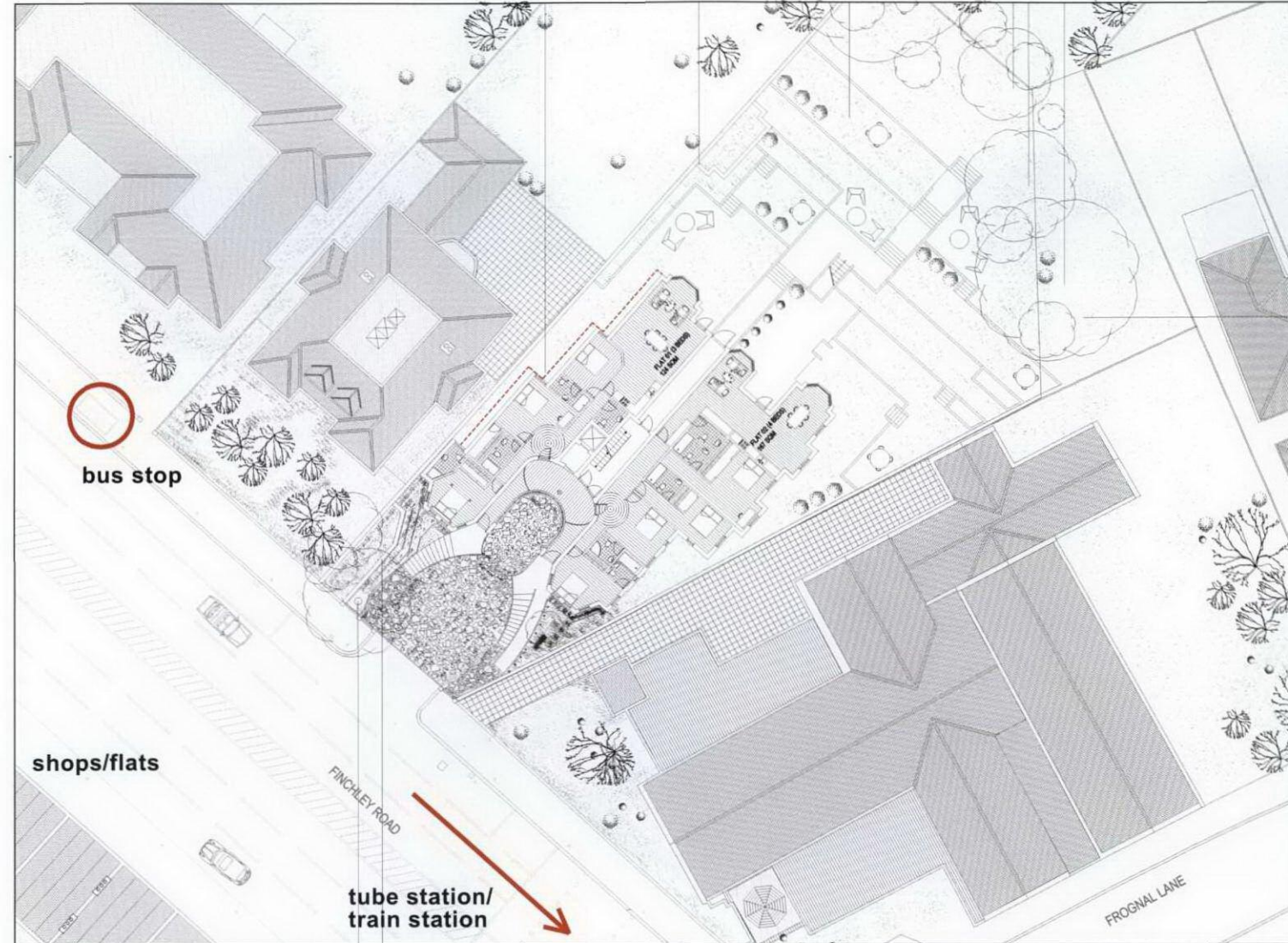
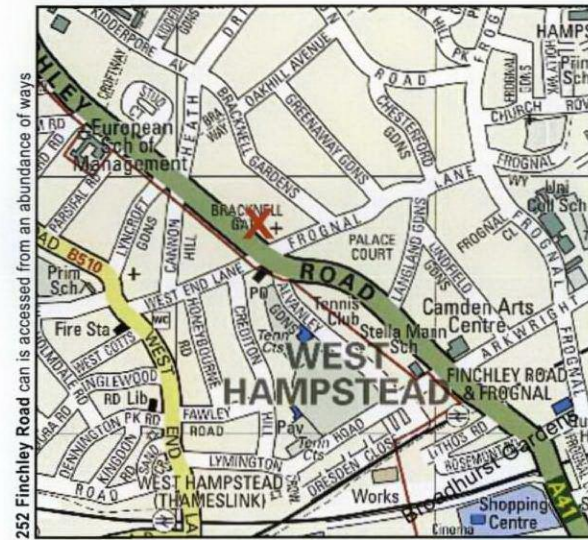
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## Existing Situation

The planning application site is located on the east side of Finchley Road in the London Borough of Camden.

## Local Highway Network

### Finchley Road

Finchley Road (the A41) forms part of the strategic Road Network and has a Red Route status in the vicinity of the site. Finchley Road runs two-way to/from central London and the north west via the Home Counties and the West Midlands. In the vicinity of the site, Finchley Road is subject to a speed limit of 30mph with 3 southbound lanes in the immediate vicinity of the site (a bus lane and two others for general traffic). Footways are wide and well maintained in the vicinity of the site and pedestrian crossing facilities are available in the form of pelican crossings and/or pedestrian phases within signal controlled junctions. Given the strategic importance of the road, Finchley Road caters for significant volumes of traffic throughout the day. In the vicinity of the site, Finchley Road is predominantly residential in nature with some substantial buildings. The Red Route restrictions which Finchley Road is subject to, are enforced Monday to Saturday between 07:00 and 19:00.

## Servicing

Typically, premises along Finchley Road do not have dedicated off-street servicing facilities and, as such, have to undertake their servicing requirements to/from the street in accordance with the Regulation Orders applicable in the vicinity.

## Public Transport

The planning application site is well served by public transport, being within easy walking distance of Finchley Road Station, Finchley Road & Frognal Station and a number of bus routes served by dedicated bus stops on Finchley Road.

## Underground Services

Finchley Road Station is served both by the Metropolitan and Jubilee Lines. The Metropolitan Line provides direct links to/from Central London (Baker Street, King's Cross, Liverpool Street and Aldgate) and suburban destinations, such as Amersham, Chesham, Uxbridge and Watford. The Jubilee Line provides regular services between Stanmore (in North West London) to Stratford (in East London), via Wembley Park, Baker Street, Waterloo, London Bridge and Canary Wharf amongst others.

## British Rail Services

Finchley Road & Frognal Station is served by Silverlink services. Silverlink operate services between Richmond (to the southwest) and North Woolwich (to the southeast). These services operate on an approximate 15 minute frequency Monday to Friday during the day.

## Bus Services

A number of bus services proceed straight past the site and/or in close proximity to the site. The relevant services are summarised below:

Route 13 - operates between Aldwych and Golders Green with an approximate daytime frequency of a bus every 5-12 minutes in either direction.

Route 82 - operates between North Finchley and Victoria with an approximate daytime frequency of a bus every 6-12 minutes in either direction.

Route 113 - operates between Edgware and Oxford Circus with an approximate daytime frequency of a bus every 5-12 minutes in either direction.

Route 187 - operates between the Central Middlesex Hospital and Finchley Road (Sainsbury's) with an approximate daytime frequency of a bus every 12 minutes in either direction.

Route 268 - operates between Finchley Road (Sainsbury's) and Golders Green with an approximate daytime frequency of a bus every 12 minutes in either direction.

Route C11 - operates between Archway and Brent Cross Shopping Centre with an approximate daytime frequency of a bus every 10-12 minutes in either direction.

## Section Summary

The site is located in close proximity to Finchley Road and Finchley Road & Frognal Stations and, in addition, is served by numerous bus services which proceed past the site. Finchley Road is well served by retail and leisure/entertainment facilities, which provide the full range of shops and services that potential future residents of the planning application scheme would be likely to require.



## DESIGNING AND ADAPTING FOR WHEELCHAIR ACCESS

From initial design discussions for unit 10, the guidelines found in the Wheelchair housing design guide have been apparent and considered in every stage of design development.

The sections entitled 'Living Room', 'Bathroom' and 'Bedroom' give a detailed account of particular design aspects concerning the layouts that are evident in Unit 10. Aspects which have been considered in creating an ergonomically tailored apartment for a wheelchair user.

### Building Regulations

Designing for Accessibility is part of a new generation of design guides jointly published by CAE and RIBA Enterprises. The design guidance is based on relevant legislation, acknowledged good practice, good design, and, where possible, empirical research. The CAE/RIBA Enterprises series will assist service providers, employers and education providers seeking to fulfil their duties under the Disability Discrimination Act 1995 whilst helping those responsible for developing buildings (architects and designers)

### PART M

Part M of the regulations sets minimum legal standards for access and use of buildings by all building users, including disabled people. Since a requirement for access was first introduced in 1985, there have been a number of changes to and extensions in the scope of access regulations. The most recent – and most radical – revision came into effect on 1 May 2004. Whereas previously Part M was concerned with "access for disabled people", now the requirement (for non-domestic buildings) is simply that:

#### Access and use

"Reasonable provision shall be made for people to gain access to and use the building and its facilities"

This does not apply to any part of a building that is used solely to enable the building or any service or fitting within the building to be inspected, repaired or maintained.

### Designing for the Bathroom

The way in which the bathroom has been laid out we have made sure there is enough space for the inclusion of a 'disability bath'. Below are three examples of bath types that could be implemented.



### LIVING ROOM

The way in which the Living Room has been designed means it intrinsically provides space for; furniture and for a wheelchair user to approach it, circulate, transfer to seating, and approach and operate doors, windows and equipment.

- A path width larger than the minimum requirement of 1000mm (A)
- A pull out work top is apparent which when in use creates an 'L-shaped' workspace in the kitchen area (B)
- A considerate kitchen layout which enables the user to easily access all equipment and controls from one wheelchair position (C)

### BATHROOM

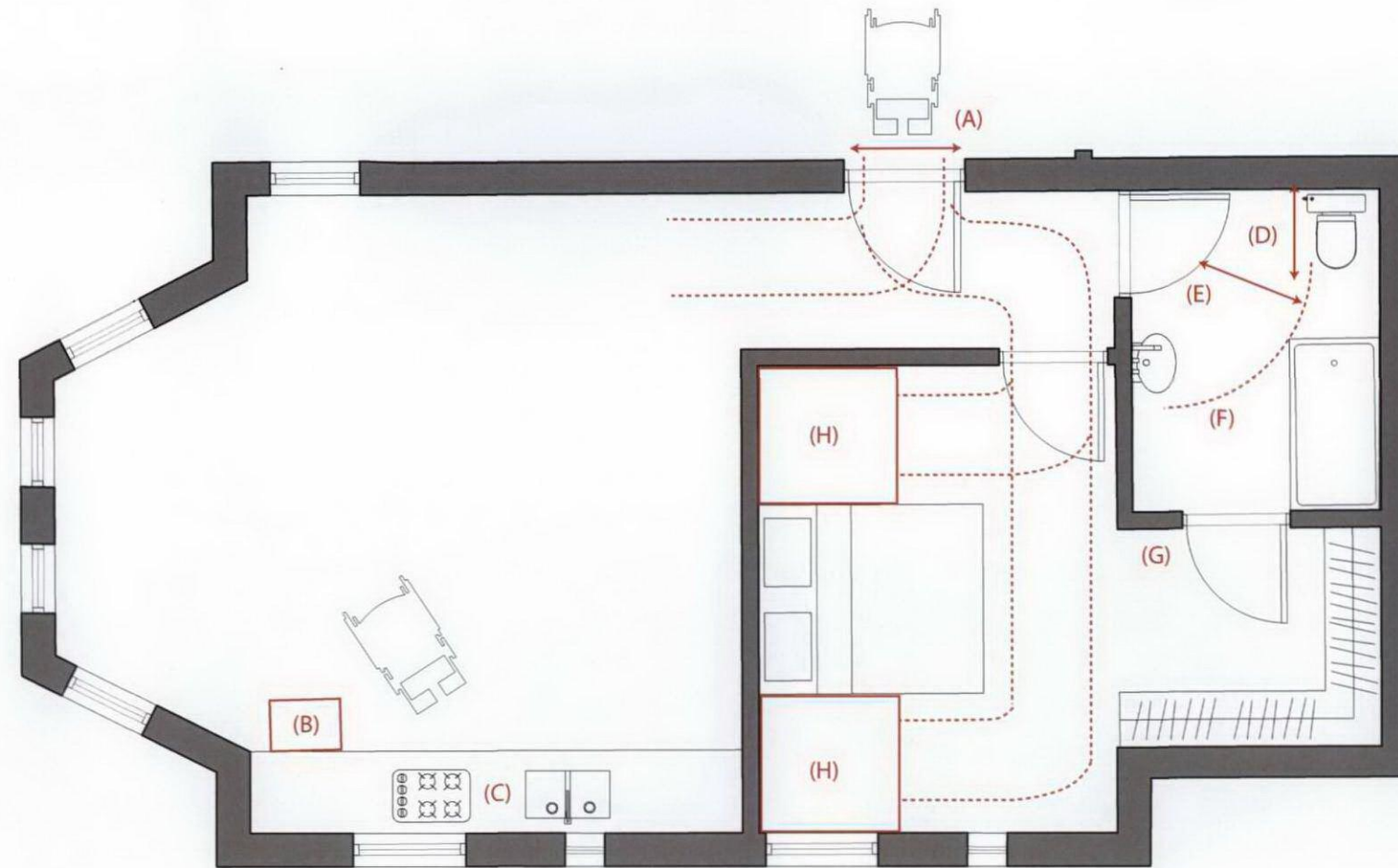
In housing design terms the design of the bathroom is the key to enabling independence and dignity for disabled people. The ability to manage most if not all toilet and bathing functions without assistance is highly desirable and is the foundation of 'independent living'. In the design for adaptation of Unit 10, we have incorporated the following design aspects to suit the needs of a disabled person.

- A 750mm (minimum) projection of WC for lateral transfer from wheelchair (D)
- A 1100mm (minimum) between inward-opening door swing and WC (E)
- The WC has an uninterrupted space for a range of transfers
- A considerate layout which maximises the range of operations possible from one wheel chair position (F)

### BEDROOM

Ensuring that there is space in the unit 10 bedroom to accommodate the normal range of bedroom furniture, and for the wheelchair-using member of the house hold to enter, approach and transfer to beds, approach and use other furniture and operate windows was paramount in the design of this development.

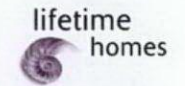
- The potential for a 'Cradle-hoist' to be added to allow easy access to the bathroom is included (G)
- A clear space of 1200 x 1200mm is included either side of bed (shown by boxes of red circles) (H)
- A layout which positions the windows in a way that gives the user a view from the bed position (I)





# 7.0 DDA Compliancy, Lifetime Homes Standards and Wheelchair Housing

## All residential accommodation meets lifestimes homes standards



Lifetime Homes

When considering new housing development and conversion proposals, the council will seek to ensure it is built to Lifetime Homes Standards, providing homes which are accessible and capable of easy adaptation to meet the needs of people with disabilities.

252 Finchley Road conforms to the layout and design standards as set out in the Lifestyles Homes targets in reference to provision of homes which are accessible and can be easily adapted to meet the needs of a person with disabilities. We have achieved a scheme that is currently 100% adaptable in terms of Lifetime Homes Standards.

ELEMENT OF ACCESSIBILITY	Flat 01	Flat 02	Flat 03	Flat 04	Flat 05	Flat 06	Flat 07	Flat 08	Flat 09	Flat 10	Flat 11	Flat 12	Flat 13	Flat 14
Approach to the house (allowing for a vehicle parked in front of the property).	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Gradient approach.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Entrance. All entrances should be: Illuminated, have level access over the threshold and have a covered main entrance.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Doorway and corridor width.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Communal Stairways.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Where flats are reached by a lift.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Turning spaces. There should be a turning space in dining areas and living rooms and adequate circulation space for wheelchair.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Living room : Should be at entrance level.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
There should be a wheelchair accessible entrance level WC with drainage provision allowing a shower to be fitted.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Walls in bathrooms and toilet should be capable of taking adaptations such as handrails.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
The design should provide for a reasonable route for a potential hoist from a main bedroom to the bathroom.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
The bathroom should be designed to incorporate ease of access to the bath, WC and wash basin.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Living room window glazing should begin at 800mm or lower and window should be easy to open/operate	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓
Switches, sockets, ventilation and service controls should be at a height useable by all.	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓



7.0 DDA Compliancy, Lifetime Homes Standards and Wheelchair Housing

252 Finchley Road Historic Scheme

December 2010

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