

Design & Access Statement to accompany planning application to 77 Castle Road, London NW1 8SU

Conversion of Maisonette into 4 self contained flats

Overview of the property & existing uses

The statement is concerned with an application for Planning Permission for Change of Use and alterations to an existing Maisonette at 77 Castle Road London NW1 8SU. This statement sets out to explain the application and the design proposals according to local authority guidances and requirements.

The property is within the Camden Town with Primrose Hill ward but is not a Listed Building nor within a conservation area.

Site, property & surrounding area

The Site context is Urban, number 77 Castle Road is a Public House at the junction with Hadley Street. There are no landscaped areas or trees at present and none are proposed. The surrounding area is predominantly residential with a mix of houses and purpose built flats. The property is next to a railway viaduct and there is a commercial use within the viaduct arches.

The property is believed to have been built circa 1870 as a Public House with the landlords maisonette on the upper floors. and is traditional brick construction with stucco rendered decorative panels to the 2 street facades. The roof is flat, this itself is likely a replacement of an original pitched 'London' roof set behind parapet roofs. The ground floor footprint extends to the footways and is slightly larger than the upper floors, there is a small yard at the rear.

The location site has a PTAL rating of 6a, which in terms of access to public transport is classed as excellent.

Current building uses & relevant planning history

The use of the building is A4 drinking establishment with the upper floors presumed to be an ancillary use. The only previous planning permissions for the building is for a minor single storey rear extension to accommodate storage and larger toilets was granted in 1987 case number 8700927. The current building appears to be as originally built.

The reason for the proposed change is that the ground floor pub is now a managed operation in line with trends in the licensed property business, the Landlord no longer exists in the traditional sense and there is no need for living accommodation on site. Furthermore, the maisonette is a large property and has proven impossible to rent with commercial agents advising that smaller units will be viable.



View of 77 Castle Road from Castle Road looking West



View of 77 Castle Road from Hadley Street looking South



View of 77 Castle Road from Hadley Street looking North

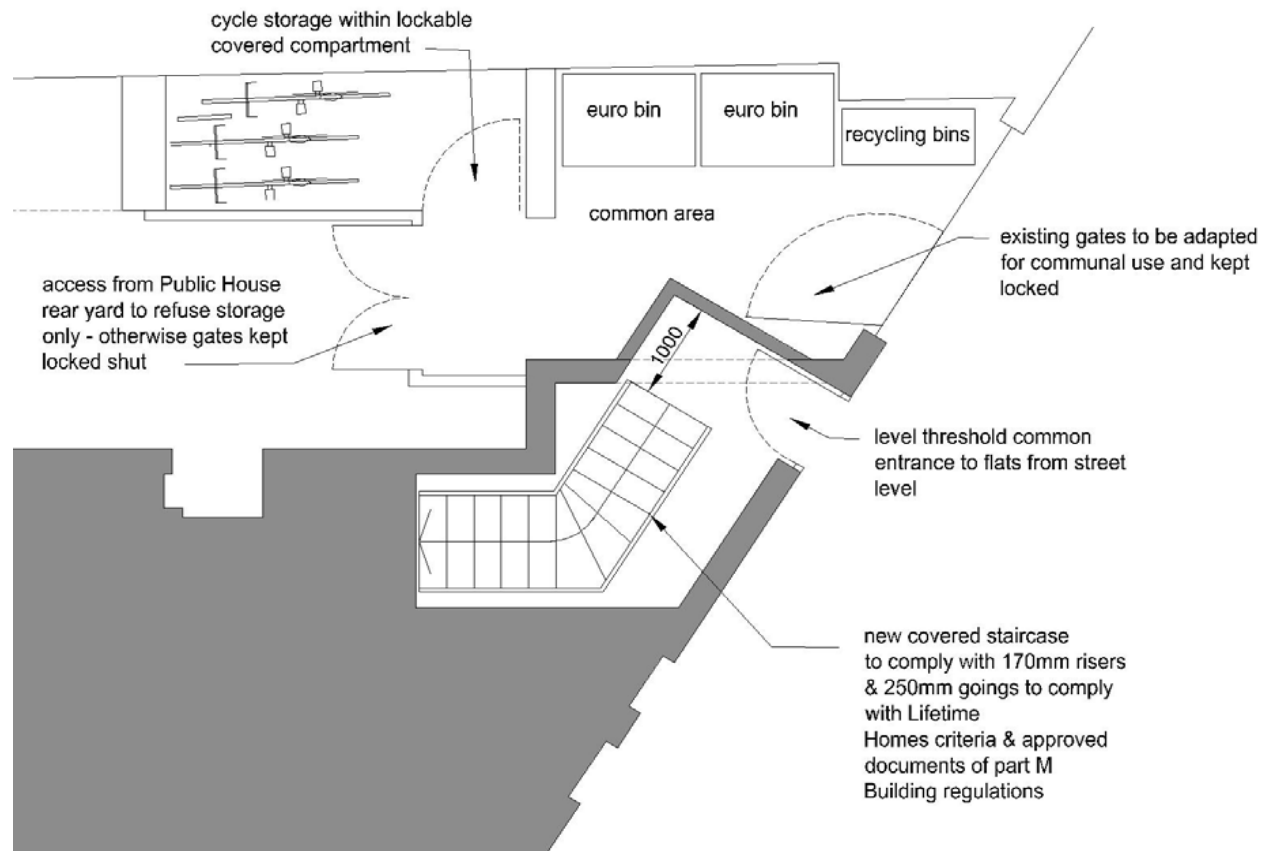


View of 77 Castle Road from Castle Road looking East

The Proposals

The proposed scheme achieves 4 1 person, 1 bedroom accommodation units and these exceed the floor space requirements of 32 sq metres as per guidance in Camdens residential design standards in document CPG2. Each flat in the development has a net floor area of 33 sq metres.

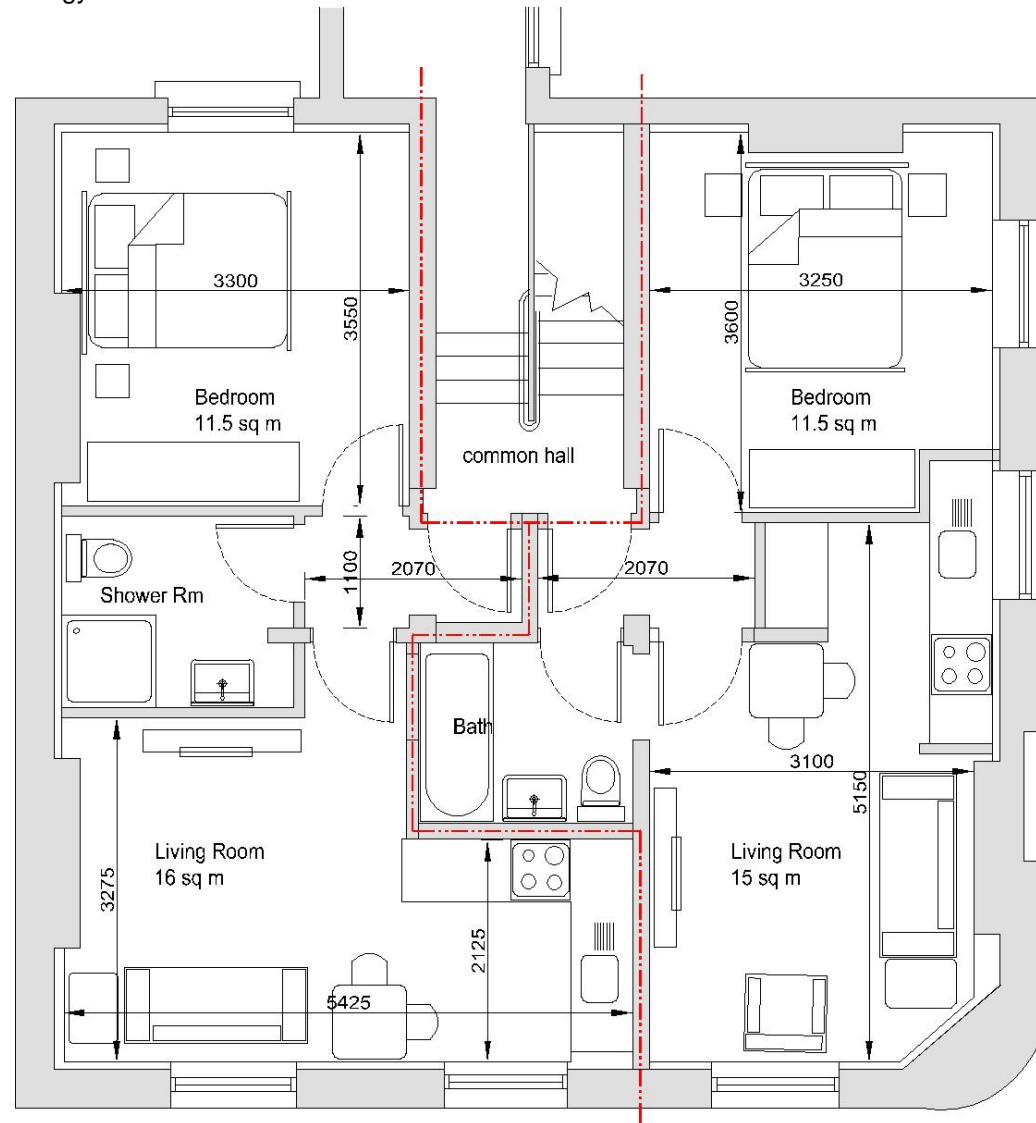
The development for change of use is largely confined to internal alterations to the first and second floor where the existing Maisonette is converted. There are minor alterations to the Ground Floor level in order to achieve an independent entrance and staircase access to the First floor level. The intention is to keep these as discreet as possible and the walkway across the flat roof at first floor shall be virtually undetectable from the Street. There shall be provision for cycle and refuse storage for the new flats adjacent to the new entrance.



The proposed entrance scheme

CS6 Providing Quality Homes

Essentially this proposal would appear to be in line with strategy which has identified a need for greater housing supply within the borough. The proposal provides 4 new units which comply with the current guidances for housing quality. The present building provides 1 unit which does not comply. Furthermore, the new units which although compact, will be of excellent build quality, secure and energy efficient.



The proposed floor plan & flat layouts

CS11 Promoting Sustainable and efficient travel

The location for this development has a PTAL rating of 6a, this is an excellent rating for access to public transport and therefore ideally suited to inner city residential accomodation. It is assumed that the development will be subject to car free policy and that a S106 agreement shall be entered into with the developer as a conditon of any permission.

CS13 Tackling Climate Change through promoting higher environmental standards.

The key points in compliance with this strategy lie in the fact that this is essentially the recycling of a redundant property to create 4 new residential units, the energy consumed in implementing this is therefore very low. Furthermore, the occupational energy usage will also be minimised as the construction will call for current building regulation standards of thermal insulation to be installed. The new property will therefore use very little energy in occupation as heat loss shall be minimal, lighting will be at current building regulation standards and private car use shall not be permitted.

This standard comparies extremely well with the current use of the building which does not have to comply with any standards and is by virtue of its current condition, extremely inefficient.

CS14 Promoting High Quality Places and Conserving our Heritage

Although not currently in a conservation area, the property is an original building in quite original condition, and is an important feature of a historic street pattern. Whilst many of the adjoining buildings are more modern, the street pattern remains as the 19th century development, with 2 and 3 storey terraced houses, the corner pub would have been an important building within this setting. This development does not propose any external changes aside from a revised entrance on a side elevation, the character of the building will be entirely retained and the new use shall ensure its maintainance.

CS17 Making Camden a safer place

This development will contirbute on 2 levels to this strategy, firstly by effectively reusing a redundant building it will be no longer vulnerable to unauthorised use or occupancy, and secondly that the level of security within the building shall comply with safer by design standards.

CS18 Dealing Our Waste and Encouraging Recycling

The development does not aim to be self sufficient in terms of waste management but as a inner city site shall benefit from regular communal waste and recycling collections, space for which shall be provide for in the common areas.

DP5 Homes of different sizes

The development provides 4 virtually identical unit sizes and is not attempting to offer a mix within itself. This is due entirely to the confines of converting an existing building, the footprint does not lend itself to splitting into other shapes as it would be too large for a 2 or 3 person unit, and not large enough for a 4 person unit. However, given the context of the development is already in a mixed environment of flats and houses, it may be considered that the additions are suitable in a local context rather than wirthin the property itself.

DP6 Lifetime Homes & Wheelchair Homes

The development recognises the provision for Lifetime Homes and the proposal incorporates the following solutions to the 2010 revision of the Lifetime Homes criteria.

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| 1. Car Parking Width: | Not relevant as no on site parking included |
| 2. Access from Car Parking : | Not relevant as no on site parking included |
| 3. Approach Gradient: | There are no ramped approaches |
| 4. External Entrances: | External Entrances are illuminated, covered and have a level threshold. |
| 5. Communal Stairs: | The new external stairs will be fully compliant with building regulations requirement M1, the existing internal staircases shall be altered to gain Building regulation approval |
| 6. Doorways & Hallways | Internal doorways and Hallways are compliant with the guidance dimensions |
| 7. Circulation Space: | The layouts of the flats will accommodate wheelchair turning at key points such as the Dining and Living areas, although these units are not required nor intended to be fully wheelchair accessible and some compromise of full requirements. |
| 8. Living Rooms: | All of the units are single storey and level, the permanent living room is therefore at entry level |
| 9. Entrance level Bed space: | All of the units are single storey and level, the permanent bedroom is therefore at entry level. |
| 10. Entrance level WC & shower: | All of the units are single storey and level, the WC and shower/ bath facility is therefore at entry level. |
| 11. Bathroom & WC walls: | Bathroom & WC walls will be of construction robust enough to take ligature or fixings points to assist movement |
| 12. Stair or through floor Lift: | Not relevant as all of the proposed units are single storey |
| 13. Potential for fitting hoists: | All ceilings are the structural ceilings and not suspended layers and will therefore be capable of accommodating potential ligature or fixings points to assist movement |
| 14. Bathroom design: | All bathrooms within the flats are on the same level as the living and sleeping accommodation and provide adequate space for ambulant disabled movement. |
| 15. Window heights: | All window heights shall have opening operation to comply with lifetime home standard, and building regulations and are configured to allow views out from a seated position within a living space. |
| 16. Service Controls: | All service control and switches are to be positioned in accordance with the lifetime homes scheme and part M building regulations |

DP18 Parking Standards and limiting the Availability of Car Parking

The policy to promote car free development is recognised and it is not anticipated that the new dwelling will have access to residents car parking permits. It is anticipated that a S106 legal agreement shall be entered into with the local authority to preserve this policy.

DP24 Securing High Quality Design

The proposals are considered to be neutral in this aspect as alterations to the building externally are limited to a minor change to the Entrance. However, the development will ensure a new lease of life for the building and it will be decorated externally and maintained: this will undoubtedly be more attractive than the neglected vacant building currently existing.

DP26 Managing the Impact of Development on Occupiers and Neighbours

The new development shall not create any additional overlooking, light, noise or odour pollution.

Proposed area schedules.

The existing & proposed areas are as follows:

	<i>as existing</i>	<i>as proposed</i>
Site Area & Yard	250 sq metres A4 use	250 sq metres A4 use
Ground Floor	160 sq metres A4 use	160 sq metres A4 use
First Floor	75 sq metres C3 use (maisonette)	64 sq metres C3 use (2 x 1 bed flat)
Second Floor	75 sq metres C3 use (maisonette)	64 sq metres C3 use (2 x 1 bed flat)

Conclusion

This application is primarily concerned with the effective use of a existing building.

It satisfies fundamental needs for additional marketable housing as identified by Camden in the current local development plan and core strategy.

It ensures the maintainence of a historic building.

It provides good quality, comfortable, secure and sustainable homes for 4 people.

We therefore consider the proposals to be a positive contribution to the borough and the immediate environment.