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East Heath Car Park, Hampstead Heath

City of London Corporation

Design & Access Statement
Prepared by LUC
March 2012

Project Title: East Heath Car Park, Hampstead Heath

Client: City of London Corporation

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Planning & EIA
Design
Landscape Planning
Landscape Management
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Mapping & Visualisation

LUC LONDON
43 Chalton Street
London NW1 1JD
T 020 7383 5784
F 020 7383 4798
london@landuse.co.uk

Offices also in:
Bristol
Glasgow
Edinburgh



Land Use Consultants Ltd
Registered in England
Registered number: 2549296
Registered Office:
43 Chalton Street
London NW1 1JD
LUC uses 100% recycled paper

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1 Design & Access Statement

Introduction

- 1.1 This Design and Access Statement accompanies an application for planning permission to extend the car park and improve the layout of parking arrangements to increase parking capacity at East Heath, Hampstead Heath. The proposal also involves landscape works to improve the appearance of this part of the Heath.
- 1.2 The Statement responds to the requirement set out in the *Town and Country Planning (Development Management Procedure) (England) Order 2010* for planning applications to be accompanied by a Design and Access Statement. It follows guidance set out by CABE¹ and the Urban Design Group².
- 1.3 Reference is made throughout the Statement to drawings that form part of the full planning application package. The full list of submitted drawings and documents is set out in **Table 1.1**.

Background

- 1.4 Hampstead Heath is a 275 hectare open space owned and managed by the City of London Corporation. The vision for Hampstead Heath, as set out in the Hampstead Heath Management Plan³, can be summarised as follows:

¹ Design and Access Statements: How to write, read and use them. CABE, 2006

² Design and Access Statements Explained. Urban Design Group, 2008

³ Hampstead Heath Management Plan: Towards a Plan for the Heath 2007-2017, City of London in association with LUC, November 2007

'Our vision is of a beautiful and accessible piece of the countryside in the city, a place with a rich mosaic of habitats, a diverse landscape and a wealth of historic and natural resources...

Table 1:1: Schedule of Drawings

Drawing	Description
5266.01	Location Plan (Red and Blue Line Boundary)
5266.02	Existing Site Layout Plan
5266.03	Proposed Site Layout Plan
5266.04	Proposed Layout: Detailed Areas
5266.05	Proposed Sketch Illustration
164.01.00	Tree Constraints Plan

The general atmosphere of the Heath is the feature valued most by its users – providing the opportunity to experience the quiet enjoyment of nature in this encapsulated countryside. The Heath's main users are those who come for informal activity, especially walking. From the heights that afford the vantage spots to take in views, the ground drops away into a series of folds that contain a wonderful variety of spaces; some are intimate enough to achieve a sense of isolation or of being in the countryside, in others it is possible to enjoy an informal game without intruding on adjacent quiet spaces. On the lower ground there are substantial remnants of a managed agricultural landscape, where hedge-lines create another set of enclosed spaces. These areas are now managed less

intensively to encourage a diversity of habitats where wildlife flourishes’.

- 1.5 The South End Green and East Heath approaches to Hampstead Heath have evolved in response to a range of diverse issues in recent years. This has resulted in a range of management solutions, which have not been appropriately co-ordinated and have consequently had a negative impact on the function and appearance of the area.
- 1.6 This is acknowledged in the Management Plan for the Heath, which sets out a number of aspirational goals to meet identified management objectives. Specifically, aspirational goal B16 states:
‘Review and, as necessary, enhance the entrance to East Heath (Lower Fairground and South End Green) and its facilities. There is a perception that this busy entrance to the Heath is unattractive and does not provide the appropriate welcome to visitors.’
- 1.7 The car park at East Heath provides a valuable parking facility for visitors to Hampstead Heath. The car park is popular and is generally full by mid-morning, meaning that visitors typically have to wait for a space to become available or choose to leave the car park and look for a parking space elsewhere.
- 1.8 However, there are a number of practical/functional issues associated with the East Heath car park that need to be addressed notably:
- Maximising parking capacity through efficient use of the space
 - Surface material is uncomfortable for pedestrians and tends to erode
 - The location of barriers and pay points
 - The car park is for visitors of the Heath, however it is known there are other regular users
 - The location of recycling bins owned/operated by Camden Council that do not form part of the waste collection strategy for the Heath
- 1.9 The accretion of street furniture, management measures and clutter has accumulated in response to specific issues rather than

being designed ‘in context’. Cumulatively, these measures have resulted in a semi-urban appearance, which conflicts with the wider vision for the Heath.

- 1.10 This is exacerbated by the Fair, which comes to East Heath three times a year. This is a long-established use of the site and is regulated by an agreement between the City of London Corporation (CoL) and the Showman’s Guild. A number of individual pitches come together for the event on the existing car park and the adjoining Fairground site, which requires the following actions for each event:
- De-mounting of the car park fence (requiring heavy lifting and swinging machinery) and temporary storage elsewhere on-site
 - Closing of the car park for eight days per event to facilitate the arrival and set-up of the showmen
 - Overseeing the arrival of the showmen and large pieces of equipment via the upper gate (causing traffic disruption on the highway)
 - Digging up of existing aggregate surface to lay temporary cables that run power from imported generators to the rides etc.
 - Re-levelling of aggregate, collection and re-erection of the car park fence after each event
- 1.11 Not only does this result in a loss of staff time for those who work at the Heath, it also creates regular disruption to the unstable car park surface material and results in a loss of revenue from the operation of the car park. The East Heath is also used by Zippo’s Circus once a year. Although this event does not occupy the car park, it does have an impact on the site as a result of heavy vehicles and temporary infrastructure.
- 1.12 A strategic review of the East Heath has therefore been commissioned by CoL, which identifies the need to address the issues associated with the car park and to assess how the set-up and operation of the fairground can be more appropriately managed in the long-term, in line with the Hampstead Heath Management Plan.

The Proposed Scheme

- 1.13 To maximise parking space in the car park, a realignment of the site is proposed to make it rectangular in shape, increasing the overall footprint from an area of 2,538 sqm to 2,970 sqm to provide an additional area of 430 sqm (17% increase). This approach will minimise encroachment onto the Lower Fairground site, with no additional space required at the north of the site and a maximum of seven metres at the south. This will provide up to an additional 40 parking spaces (see drawing 5266.01).
- 1.14 Parking bays are to be marked with markers in the ground, along with marks on the fences, to encourage improved parking and to maximise the spaces available. The markers will be kept to the minimum required to ensure this does not detract from the rural feel of the Heath.
- 1.15 Mixed hedges will be planted along the fence line bordering the car park to the south and west, adjacent to East Heath Road and South End Green, to both screen the site and enhance the rural quality of the area. The layout of adjacent footpaths will be improved and re-surfaced in a material that is more in keeping with the character of the surrounding area.
- 1.16 Further discussions are required with the Showman's Guild to seek to avoid the closure of the car park during the Fair. A strategic review the fairground site as a whole is required to enable a means to accommodate the rides and sideshows at locations within the fairground site. With this in mind, this application for planning permission relates only to the proposed extension of the East Heath car park and associated landscape works.

The Design Process

- 1.17 Local character and circumstances have been key influences over the design and location of the proposed scheme. The following sections describe the planning and physical context of the site and how these influenced design. The planning policy context is also referred to where appropriate.

Consultation

- 1.18 It is important to involve key stakeholders in the design process to ensure that user needs are met and to understand and respond to any concerns. The key stakeholders in this scheme are the City of London Corporation, the London Borough of Camden and users of the Heath. All have been involved in the formulation of the proposals.

The London Borough of Camden

- 1.19 A pre-application advice request was submitted to the London Borough of Camden to seek advice on the acceptability of the proposed scheme in planning terms. Written pre-application advice was received from David Peres Da Costa, Planning Officer, in a letter dated 7th December 2011 (ref. CA/2011/ENQ/06859).
- 1.20 In summary, the key points raised in the letter were:
 - Overall, the proposed alterations are considered acceptable
 - Details of how trees are to be protected during implementation of the scheme should be submitted with a planning application
 - A storm water mitigation scheme should also be submitted as part of a planning application

Planning and Physical Context

- 1.21 Hampstead Heath is a 275 hectare open space owned and managed by the City of London as a registered charity. It comprises a mosaic of habitats including woodland, grassland, scrub and open water. Close to the centre of London, it is one of the most important areas for recreation in the capital.



Figure 1.1: Hampstead Heath

- 1.22 Hampstead Heath is located in the London Boroughs of Camden (230 hectares) and Barnet (45 hectares). The City of London became the custodian of Hampstead Heath in 1989, taking over from the London Residuary Body which had managed the Heath following the abolition of the Greater London Council in 1986. The adjacent 45 hectare Kenwood Estate, including Kenwood House, is owned and managed by English Heritage.
- 1.23 East Heath is located in the south-west corner of Heath. The car park is accessed via East Heath Road and is bound by the Fairground site immediately to the north-east, Pryors Field to the north, South End Green to the south and Preachers Hill to the west.
- 1.24 The footprint of the car park is 2,538 sqm and can currently accommodate between 100 and 110 vehicles, depending upon the way in which cars are parked. There is currently no bay markings indicated on the ground, which often leads to over-generous spaces being taken up by vehicles. Cars are parked in three rows; one along each of the sides and a row across the centre of the car park.

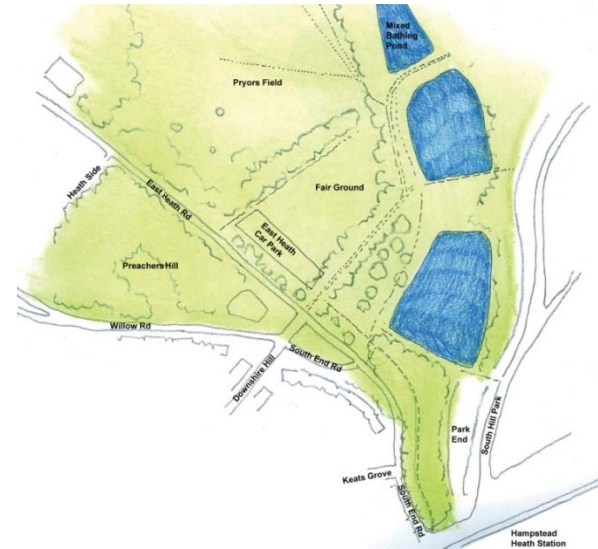


Figure 1.2: Site Location

- 1.25 Hampstead Heath is designated as Metropolitan Open Land (MOL) and strategic open space in the London Borough of Camden Core Strategy. There are two conservation areas immediately adjacent to East Heath: Hampstead Conservation Area to the south and west, and South Hill Park Conservation Area to the east.

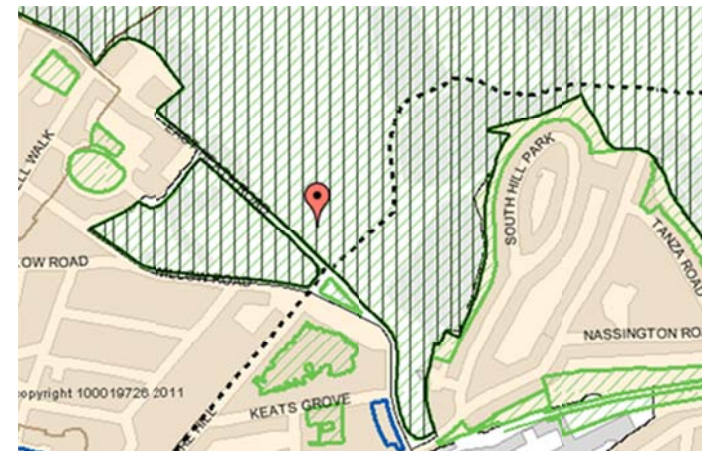


Figure 1.3: Camden Proposals Map

The Development Plan

- 1.26 The adopted development plan that covers Hampstead Heath comprises policies in the London Plan (the Spatial Development Strategy for Greater London), the London Borough of Camden Core Strategy DPD, adopted in October 2010, and the Development Policies Document, adopted in November 2010.

The London Plan

- 1.27 The replacement London Plan was adopted in July 2011 and sets out the spatial development strategy for Greater London, providing a framework for the development and use of land over the next 20-25 years. It provides a strategic, London-wide policy context within which boroughs should set their local planning policies. It replaces the previous version of the London Plan published (consolidated with alterations since 2004) in March 2008 and forms the regional level (London wide) development plan.

London Borough of Camden

- 1.28 The London Borough of Camden is currently preparing its Local Development Framework (LDF), which sets out the Council's spatial strategy and its policies and proposals for development and other uses of land. The LDF is made up of a portfolio of Development Plan Documents (DPDs), including the Core Strategy, which was formally adopted in November 2010. The Core Strategy is now a key statement of policy that the Council considers when determining planning applications.
- 1.29 Camden also adopted its Development Policies Document in November 2010, which sets out detailed planning criteria policies that are used in determining planning applications in conjunction with policies included in the Core Strategy.
- 1.30 Camden adopted their Unitary Development Plan (UDP) in June 2006. However, all but one of the policies in the UDP has now been replaced by the LDF and therefore no longer form part of Camden's development plan.

- 1.31 With this in mind, the proposed scheme will be appraised against the Core Strategy and the Development Policies Document.

- 1.32 The key planning policy considerations that relate to the East Heath car park are:

- Metropolitan Open Land and Open Space
- Transport and Traffic
- Cultural Heritage
- Impact on Trees
- Biodiversity
- Amenity Considerations
- Flood Risk

Metropolitan Open Land & Open Space

- 1.33 Hampstead Heath is identified in Camden's Core Strategy as being Metropolitan Open Land (MOL) and open space. Core Strategy Policy CS15 *Protecting and Improving Our Parks and Open Spaces and Encouraging Biodiversity* seeks to protect open spaces, including Metropolitan Open Land. Policy CS15 also states that the Council will preserve and enhance the historic, open space and nature conservation importance of Hampstead Heath and its surrounding area by:
- working with the City of London, English Heritage and Natural England to manage and improve the Heath and its surrounding areas;
 - protecting the Metropolitan Open Land, public and private open space and the nature conservation designations of sites;
 - seeking to extend the public open space when possible and appropriate;
 - taking into account the impact on the Heath when considering relevant planning applications;
 - protecting views from Hampstead Heath and views across the Heath and its surrounding area;

- improving the biodiversity of, and habitats in, Hampstead Heath and its surrounding area, where opportunities arise.

- 1.34 The existing car park was originally opened more than 40 years ago and its layout has subsequently been upgraded in recent years. The car park, along with the Fairground site, largely defines the character of the East Heath and creates a 'semi-urban' appearance in a predominantly rural setting.
- 1.35 An extension to the East Heath car park is in keeping with the existing use of the site and will not result in an adverse change to the character of this part of the Heath. Rather, the proposed scheme seeks to enhance the character of the site by improving the visual appearance of the car park. This will be achieved by providing mixed hedge planting along the southern and western edges of the site to 'soften' the car park and help integrate it within the wider historic landscape. Hedge planting will also serve to screen the car park in views from the East Heath Road and the South End Green approach. Improvements to the layout and surface treatment of the surrounding footpath network will also help enhance the character and appearance of the area.
- 1.36 Given the scale and nature of the proposed scheme, along with existing screening of the site, the proposals will not be visually intrusive and will not have a negative impact on the open nature of the Heath. Impacts on nature conservation and biodiversity are considered in the following sections of this Statement.
- 1.37 It is therefore considered that the proposed scheme is in keeping with the open character of the Heath and is deemed an appropriate open land use in MOL. The proposed scheme complies with Policy CS15.

Transport and Traffic

- 1.38 Core Strategy Policy CS11 *Promoting Sustainable and Efficient Travel* seeks to promote the delivery of transport infrastructure and the availability of sustainable transport choices in order to reduce the environmental impact of travel and relieve pressure on the Borough's transport network. In making private transport more sustainable, Policy CS11 seeks to restrict new public parking and promote the re-use of existing car parks, where appropriate.
- 1.39 Development Policy DP19 *Managing the Impact of Parking* states that the Council will seek to ensure that the creation of additional car parking spaces will not have negative impacts on parking, highways or the environment.
- 1.40 As mentioned above, the car park at East Heath provides a valuable parking facility for visitors to Hampstead Heath. However, the car park is popular and is full by mid-morning, meaning that visitors typically have to wait for a space to become available or choose to leave the car park and look for a parking space elsewhere, often in surrounding residential areas.
- 1.41 It is therefore proposed that the car park is realigned to make more efficient use of parking space. This will increase the overall footprint by 430 sqm (17% increase) to provide an additional 40 parking spaces (see drawing 5266.03).
- 1.42 The car park is currently accessed from a single, two-way junction off East Heath Road with the ability for vehicles to enter and leave the site in both a northerly and southerly direction.
- 1.43 Car park ticketing records, taken in June 2011 by the City of London Corporation, indicate that the peak time for use of the car park is between 10am and 2pm. This is outside of the normal morning and evening peak hour traffic flows on the highway network. These records also indicate that, during this peak period, there are around 35 vehicles per hour entering the car park and parking. During these peak times, there are vehicles that enter the car park and leave without finding a space to park. Therefore, the actual number of vehicles entering the car park is anticipated to be higher than this.
- 1.44 East Heath Road is not a strategic highway route, nor is it a bus route. Therefore, taking into account the parking records, an increase of 40 parking spaces is not considered to have an impact on strategic issues associated with capacity of the network. Similarly, it is considered that the existing access into/out of the car park, including sightlines for vehicles, is sufficient to accommodate an additional 40 parking spaces without harming highway safety.
- 1.45 The proposed scheme also involves works to improve the surrounding footpath network for pedestrians accessing the Heath

via East Heath Road and South End Green, and provides additional cycle parking for 10 bikes.

- 1.46 The proposed scheme will therefore improve parking facilities available to visitors of the Heath, reducing the need for off-site parking whilst maintaining the safe operation of the highway network, in accordance with policies CS11 and DP19.

Cultural Heritage

- 1.47 Core Strategy CS14 *Promoting High Quality Places and Conserving Our Heritage* requires development of the highest standard of design that respects local context and character, and preserves and enhances heritage assets and their settings including, amongst other things, conservation areas and archaeological remains.
- 1.48 Development Policy DP25 *Conserving Camden's Heritage* states that the Council will not permit development outside of a conservation area that causes harm to the character and appearance of that conservation area. With regards to archaeology, Policy DP25 states that the Council will protect remains of archaeological importance by ensuring acceptable measures are taken to preserve them and their setting, including physical preservation, where appropriate.
- 1.49 Although the East Heath car park and Fairground site is not located within a Conservation Area, it sits in between two conservation areas: Hampstead Conservation Area to the south and west, and South Hill Park Conservation Area to the east. Careful consideration has therefore been given to the landscape of the site, which has in-turn influenced the overall design approach to the car park extension. The proposed scheme will improve the visual appearance of the car park through the provision of new boundary planting that is both sensitive to, and reflective of, the character and setting of the site. Mixed hedge planting including hawthorn, blackthorn, field maple, dog rose, guelder rose and holly is proposed along the southern and western edges of the site to help 'soften' the car park and integrate the 'semi-urban' character of the site within the wider historic landscape.
- 1.50 As noted above, the existing car park was established more than 40 years ago and, together with the Fairground site, largely

defines the character of the East Heath. The proposed scheme is an extension of a long-established use and will not detract from the character of the Heath. Rather, it will integrate the existing car park with the wider landscape more sensitively, improving the overall appearance of the East Heath, in line with the objectives of the Hampstead Heath Management Plan.

- 1.51 It is considered that the proposed works to the car park 'sit' carefully within the wider historical context of the Heath, which defines the setting of the adjoining conservation areas. The proposed scheme therefore complies with policies CS14 and DP25.
- 1.52 The proposal site is not located within an archaeological priority area. Notwithstanding this, trial pits were dug and inspected in August 2011 to determine existing ground conditions. This confirmed there is approximately 150mm Type 1 crushed stone aggregate which makes up the existing car park surface. Below this there is 100-300mm of made ground/fill attributed to demolition rubble from the Second World War deposited on the car park and Fairground site. Below this is natural London clay. No archaeological features were identified.
- 1.53 Excavation works to a depth of approximately 600-700mm will be required for the car park extension. However, given that the site is not within an archaeological priority area and that trial pits revealed no archaeological features, it is considered the proposed works will not affect any remains of archaeological importance in line with policies CS14 and DP25.

Impact on Trees

- 1.54 Core Strategy Policy CS15 *Protecting and Improving Our Parks and Open Spaces and Encouraging Biodiversity* seeks to protect trees and promotes the provision of new trees and vegetation.
- 1.55 The proposed scheme will not result in the loss of any trees in and around the proposal site.
- 1.56 An Arboricultural Impact Analysis Report (AIA) has been prepared based on a detailed appraisal in the form of a tree survey of 35 individual trees or tree groups adjacent to the East Heath car park (see **Appendix 1** of this Statement). The report considers the health and safety of these trees under their current growing

conditions and assesses the likely impacts of the proposed improvements to the car park, all measured against the guidance set out in *BS5837: 2005 - Trees in relation to construction – Recommendations*.

1.57 In summary, the AIA concludes that the proposed scheme can be achieved without unacceptable adverse impacts on retained trees subject to the adequate separation of retained trees from construction works (as set out in the AIA and the Tree Constraints Plan – see drawing 164.01.00), as well as protective fencing to the standards set out in *BS5837:2005- Trees in relation to construction - Recommendations*.

1.58 Given the proposed tree protection measures outlined above, the proposed scheme complies with Policy CS15.

Biodiversity

1.59 Core Strategy Policy CS15 *Protecting and Improving Our Parks and Open Spaces and Encouraging Biodiversity* seeks to protect and improve sites of nature conservation and biodiversity, in particular habitats and biodiversity identified in the Camden and London Biodiversity Plans in the Borough.

1.60 The main features of nature conservation interest in Hampstead Heath are identified in the Hampstead Heath Management Plan as including:

- Approximately 800 identified veteran trees
- More than 180 recorded species of birds
- Breeding bird species including kingfisher, reed warbler and sparrowhawk
- More than 300 recorded species of fungi
- More than 30 significant ponds
- The West Heath sphagnum bog is one of only a very few examples of this habitat in London.

1.61 None of these features are located in close proximity to the area of the existing car park and Fairground site and will not be affected by the proposed scheme given the nature and scale of the proposals.

1.62 The proposals involve improvements to the East Heath car park site. This site is predominantly hard standing made up of demolition rubble, with a sparse covering of grass on the Fairground site. The site also hosts the Fair three times a year and the circus once a year, causing considerable disruption to the site and its immediate surroundings. The site is therefore considered to be of little value in terms of nature conservation and biodiversity interest.

1.63 The proposed works seek to improve the biodiversity value of the site by introducing a mixed hedge to the western and southern boundary of the car park including hawthorn, blackthorn, field maple, dog rose, guelder rose and holly. No habitats will be lost or disturbed as a result of the proposed development.

1.64 Overall, it is considered that the proposed works will have a positive impact on the biodiversity value of the Fairground site and will ensure there is no unavoidable damage to nature conservation interests. The proposed scheme therefore accords with Policy CS15.

Amenity Considerations

1.65 Development Policy DP26 *Managing the Impact of Development on Occupiers and Neighbours* seeks to protect the amenity of occupiers and neighbours by promoting a high standard of development that does not cause harm to the living conditions of adjoining properties in relation to impacts on outlook, amount of daylight and sunlight, privacy and levels of noise, fumes and vibrations.

1.66 The nearest residential properties to the East Heath car park are approximately 100 metres to the south-west on Downshire Hill and Willow Road. Given the distance of the proposed scheme from these properties, it is considered that there is unlikely to be any adverse impacts relating to amenity, outlook and levels of noise.

1.67 Although the car park is partially screened by a row of trees flanking the south and west of the car park facing onto East Heath Road, the car park is relatively exposed when entering the Heath from South End Green. With this in mind, the existing tree line will be reinforced by a new hedge row approximately 1m in height. This will provide a natural form of screening and insulation,

reducing the visual impact of the car park from the East Heath Road and the South End Green approach. It is therefore considered that the proposed scheme complies with Policy DP26.

Flood Risk

- 1.68 Core Strategy Policy CS13 *Tackling Climate Change Through Promoting Higher Environmental Standards* requires development to prevent or mitigate local surface water and down-stream flooding, especially in areas up-hill from, and in, areas known to be at risk from surface water flooding. Development Policy DP23 *Water* requires developments to reduce the pressure on the combined sewer network and the risk of flooding by:
- incorporating water efficient features and equipment and capturing, retaining and re-using surface water and grey water on-site;
 - limiting the amount and rate of run-off and waste water entering the combined storm water and sewer network through sustainable urban drainage methods to reduce the risk of flooding.
- 1.69 Surface water run-off is currently shed to either side of the car park where it permeates naturally into soft ground. Rainwater run-off from the proposed car park will be treated in the same way as existing.
- 1.70 It was advised in the pre-application advice received from Camden that a storm water mitigation scheme be submitted as part of this planning application for the car park extension (refer to paragraph 1.20 above). The City of London Corporation is currently in the process of preparing a Surface Water Management Strategy and flood mitigation plan for the entire Heath. It is therefore felt that surface water mitigation for the East Heath car park should not be considered in isolation to the wider management strategy for the Heath. Mitigation measures set out in the Surface Water Management Strategy, once completed, will be applied to the operation of the East Heath car park.
- 1.71 The City of London Corporation is in consultation with the London Borough of Camden, and recent meetings have taken place. The City are providing Camden with detailed information regarding

compaction and absorption data, which form part of the City's highly detailed hydrological studies which took place in 2006 and 2010. This data will enable Camden's consultants to model flood risk and the City will continue to liaise with Camden's Engineers' and consultants in terms of measures to mitigate surface water flooding issues.

Consideration of Alternatives

- 1.72 No alternative locations were considered for the scheme, as the area immediately adjoining the existing car park to the south and east (i.e. the Fairground site) is the only viable location for the car park extension.

Social and Economic Context

- 1.73 The key social influences on the location and design of the scheme are the need to maximise parking capacity for visitors of the Heath by making more efficient use of existing space. The key economic influences are the need for the City of London Corporation to increase the number of chargeable parking spaces in order to provide a means of managing the car park, and other facilities on the Heath, more effectively.

Use

- 1.74 The East Heath car park will continue to function as a 'pay and display' car park for visitors of Hampstead Heath. A strategic review of the Fairground site as a whole will be undertaken by the City of London Corporation to determine how the Showman's Guild can continue to put on the Fair at the Heath without the need to close the car park.

Amount

- 1.75 The proposed works to the East Heath car park will provide 40 additional parking spaces. The total number of parking spaces available at the site will increase from 100-110 to 140, including

five disabled bays. Ten cycle spaces will also be provided as part of the scheme.

Layout

- 1.76 The following parameters have influenced the layout of the proposed scheme:
- A small increase in car park footprint is likely to be more acceptable, but a large increase would not
 - Need to redefine the boundary to provide a more efficient shape that will increase the capacity with minimum increase of overall surface area
 - The proposals should concentrate on elements within the Hampstead Heath ownership boundary and resist amendments that affect the Public Highway
 - The principles established in the Hampstead Heath Management Plan should be followed
- 1.77 Diagonal/echelon parking layouts were considered for the re-organisation and extension of the car park. However, whilst this arrangement can work in tight urban/built contexts, it does not work in the sort of arrangement that exists at East Heath and would require greater land-take to achieve the required capacity.
- 1.78 With this in mind, a number of layout options were considered for the car park, as shown in **Figure 1.4** below.

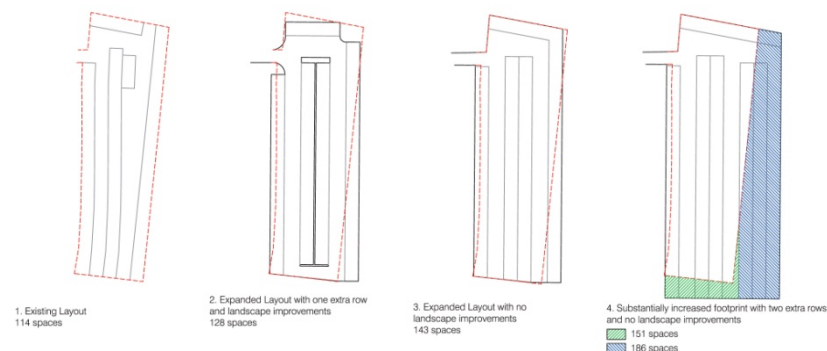


Figure 1.4: Layout Options

- 1.79 A combination of options 3 and 4 was taken forward as the preferred option, providing four rows of parking utilising a proportion of the space coloured green and blue in option 4. The proposed layout is shown in **Figure 1.5** below and on drawings 5266.03 and 5266.04.

Scale

- 1.80 The re-organisation and extension of the car park will increase the overall footprint from an area of 2,538 sqm to 2,970 sqm, providing an additional area of 430 sqm (a 17% increase) for new parking spaces (see drawing 5266.03).

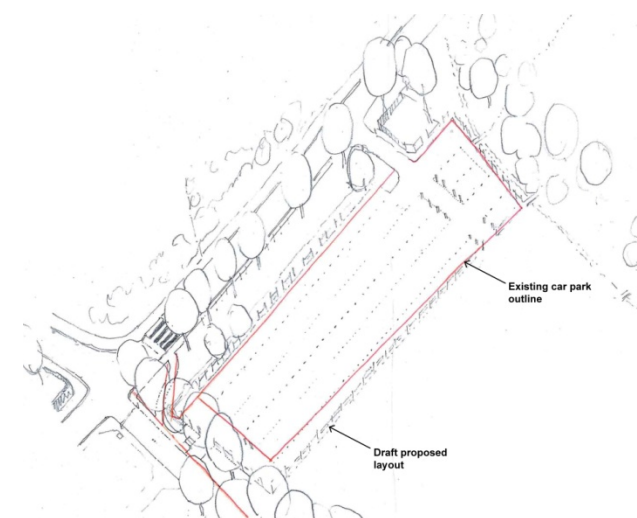


Figure 1.5: Proposed Site Layout

Appearance

- 1.81 The surface treatment for the proposed car park extension will be formed of crushed stone and Type 1, as per the existing car park.

Macadam surfacing will run through the most heavily trafficked areas, by the main entrance ways into the car park. The parking spaces will be delineated with markers in the ground along with marks on the boundary fences. A fine gravel surface is proposed for the resurfacing of the footpaths. The western edge of the car park will be enclosed with new Sussex timber fencing, which is more in keeping with the countryside character of the Heath (see drawings 5266.03 and 5266.04).

Landscape

- 1.82 Mixed hedge planting is proposed around the southern and western boundaries of the car park to minimise visual impact as visitors approach the Heath from East Heath Road and South End Green. The hedge will include hawthorn, blackthorn, field maple, dog rose, guelder rose and holly. Existing timber fencing around the car park will be retained and supported by new fencing. The layout of the footpath network around and through the car park will be improved and resurfaced with fine gravel (see drawings 5266.03 and 5266.04).

Access

- 1.83 The East Heath car park is currently accessed from a single, two-way junction off East Heath Road, with the ability for vehicles to enter and leave the site in both a northerly and southerly direction. Access into the re-organised car park will remain as

existing, with the main ingress/egress from East Heath Road. Access into the car park will continue to be controlled by gates, which will be closed when the car park is not open.

- 1.84 Pedestrian access into the car park will be via the entrance to the south of the car park, and access from the car park on to the Fairground site will be via a new gate to the north-east of the car park extension.
- 1.85 Five disabled parking bays will be provided in the south-west corner of the car park. Ten cycle parking spaces will also be provided adjacent to the southern entrance of the car park.

Conclusion

- 1.86 The proposed scheme constitutes a well-considered proposal that will make a very positive contribution to the requirements of the City of London Corporation and visitors of Hampstead Heath. It will make more efficient use of an existing space for parking and will provide much needed additional facilities to meet demand. An extension to the East Heath car park is in keeping with the existing use of the site and will enhance the character of the site by improving the visual appearance of the car park.
- 1.87 Overall, the proposal meets important and demonstrable needs, complies with regional and local planning policy and responds positively to the context and character of its surroundings. Planning permission should be granted.

Appendix 1

Arboricultural Impact Analysis, Tree Survey & Tree Constraints Plan