



ADL Traffic Engineering Ltd

9 Coldbath Square, London, EC1R 5HL

Tel: 020 7278 8844

Fax: 020 7278 8810

Email: cad@adltraffic.co.uk

**SERVICE MANAGEMENT PLAN
PROPOSED STUDENT ACCOMMODATION
BRITANNIA STREET
LONDON**

Watkin Jones Group
Llandygai Industrial Estate
Bangor
Gwynedd
LL57 4YH

ADL/AJM/2252/31A

May 2012

REPORT CONTROL

Document: Service Management Plan
Project: Britannia Street
Client: Watkin Jones
ADL Reference: 2252

Document Checking

Primary Author	Mike England	Initialled: ME
-----------------------	--------------	----------------

Contributor	Amol Pisal	Initialled: AP
--------------------	------------	----------------

Review by	Allan J Mendelsohn	Initialled: AJM
------------------	--------------------	-----------------

Issue	Date	Status	Checked for Issue
1	18.05.12	Draft	AJM
2	30.05.12	Draft	AJM
3	31.05.12	Final	AJM

This report and its contents are copyright ADL Traffic Engineering Ltd and their clients.
This report shall not be reproduced without written permission.

CONTENTS

	Page No
1.0 INTRODUCTION	
1.1 Purpose of Report	1
1.2 Discussions with Local Authority	1
2.0 SITE AND SURROUNDING AREA AND ROAD NETWORK	
2.1 Site and Surrounding Area	2
2.2 Road Network	3
2.3 Parking/Loading Restrictions	3
3.0 PROPOSED DEVELOPMENT	
3.1 Proposed Development	5
3.2 Servicing Traffic Generation	5
3.3 Servicing Arrangements	7
4.0 SERVICING MANAGEMENT PLAN	
4.1 Loading Procedures	8
4.2 Goods Deliveries	8
4.3 Loading Slots	9
4.4 Refuse/Recycling	9
5.0 CONCLUSIONS AND RECOMMENDATIONS	10

APPENDICES

1.0 SITE LOCATION PLAN

2.0 SITE AND SURROUNDING AREA

3.0 ARCHITECT'S SITE LAYOUT

4.0 SERVICING: AUTOTRACK ANALYSIS

1.0 INTRODUCTION

1.1 Purpose of Report

- 1.1.1 ADL Traffic Engineering Ltd have been appointed by Watkin Jones to address the traffic and transportation issues with regard to the above site.
- 1.1.2 The site is located at 15 - 27 Britannia Street and is on the junction of Britannia Street and Wicklow Street. The site is currently a disused three storey block with a gfa of 4,796m². The proposal is to develop approximately 226 student accommodation units with ancillary uses to include residential accommodation (2 x one-bed studio flats), performance space/gallery and an office suite.
- 1.1.3 This Service Management Plan considers the proposals for servicing the site by delivery and refuse collection vehicles and should be read in conjunction with the Transport Assessment.

1.2 Discussions with the Local Authority

- 1.2.1 Pre-application discussions were undertaken between the developer and the London Borough of Camden, whereby the following was identified:

“There are yellow lines around the site which could be used for servicing, in particular, Britannia Street is wide enough for small vehicles to stop and still allow vehicles to pass. Yellow lines allow loading for up to 40 minutes. Servicing on-street could be acceptable depending on the amount of servicing trips generated by this development. The applicant will need to demonstrate that servicing on-street would be acceptable in their Transport Statement. A Service Management Plan (SMP) should be included in draft form and this will be secured by S106.”

- 1.2.2 This Statement is presented to the Highway Authority as part of the transportation submission. Should there be comments or changes required then they can be noted and incorporated in subsequent revisions of this Statement.

2.0 SITE AND SURROUNDING AREA AND ROAD NETWORK

2.1 Site and Surrounding Area

- 2.1.1 The site is located in the London Borough of Camden to the southeast of London Kings Cross St Pancras. A site location map is provided as Appendix 1.0.
- 2.1.2 The site is situated to the east of the A501 Grays Inn Road, and is bounded by Britannia Street to the north and Wicklow Street to the southern and western sides. On the eastern side of the site the railway line runs from north to south. A site and surrounding area plan is provided as Appendix 2.0.
- 2.1.3 The surrounding area comprises of a mix of uses including offices, accommodation, residential, retail units and a pub. The site which is currently vacant was formerly occupied by several different companies and used as office and conference space.
- 2.1.4 The site comprises the buildings of no.15 Britannia Street and buildings 17-27 Britannia Street, London, WC1.
- 2.1.5 The existing buildings are user class B1 with the exception of the ground floor of 17 Britannia Street which has a permission for B1/D1 gallery/exhibition space.
- 2.1.6 The existing building has a total gross internal area of 4,796 sq. m. This is comprised of:
- Basement – 710 sq. m
 - Ground Floor – 1,577 sq. m
 - First Floor – 1,274 sq. m
 - Second Floor – 1,235 sq. m
- 2.1.7 It is considered that the existing building would have been serviced on street mainly from either Britannia Street and Wicklow Street as this is legally permitted as outlined in paragraph 2.3.5 below.

2.2 Road Network

- 2.2.1 Britannia Street runs in an east to west direction perpendicular to the A501 Grays Inn Road. It is a dual carriageway road, subject to a 30mph speed limit, with footways on either side and benefits from street lighting. To the east of the site Britannia Street connects to the A201 Kings Cross Road and to the east with the A501 Grays Inn Road.
- 2.2.2 At either end of Britannia Street there is a right only exit onto Grays Inn Road and Kings Cross Road, which is a northbound one-way street.
- 2.2.3 The site can also be accessed from Wicklow Street, which passes the site on its southern and western sides and is subject to one way flow in a northbound direction.

2.3 Parking/Loading Restrictions

- 2.3.1 Both Britannia Street and Wicklow Street are subject to restrictions, with single yellow lines present within the vicinity of the site.
- 2.3.2 There are single yellow lines on both sides of Britannia Street for much of its length, with parking bays situated at various locations. Britannia Street and Wicklow Street are designated as a 'Controlled Zone' with no waiting permitted between the hours of 08:30-18:30 Monday to Friday and 08:30-13:30 on a Saturday (maximum stay 2 hours).
- 2.3.3 There are a mix of on street parking bays along Britannia Street and Wicklow Street, which include spaces for resident permit holders, disabled users and pay and display (up to a maximum of 2 hours). In addition there are Car Club spaces situated on Britannia Street and 'hospital only' parking and motorcycle parking on Wicklow Street.
- 2.3.4 There are restrictions in place on Britannia Street to large goods vehicles.

2.3.5 Firstly, there is a restriction to vehicles greater than 18 tonnes in weight between the following hours:

- Monday to Friday - Midnight to 07:00 and 21:00 to Midnight
- Saturdays - Midnight-07:00 and 13:00-Midnight
- Sunday - At any time

2.3.6 Secondly, there is an overnight restriction on Britannia Street for buses and vehicles over 5 tonnes in weight between the hours of 18:30-08:00 hours.

2.3.7 With reference to the LB Camden website and in accordance with the restrictions above vehicles are able to load/unload directly from Britannia Street, subject to the following:

“Where no restrictions apply (as indicated by the blips)loading/unloading can take place on single or double yellow lines, for an unlimited amount of time from the end of controlled hours or 6:30pm, (whichever comes earlier) through to 11am the next day, provided loading and unloading is continuous.

After 11am and until the end of controlled hours or 6:30pm (whichever comes earlier), where no restrictions apply heavy goods vehicles can load or unload on single yellow lines for up to 40 minutes provided loading and/or unloading is continuous.

The loading/unloading time for cars and light goods vehicles such as small vans, will be restricted to 20 minutes from 11am until the end of controlled hours.

Please note that if your loading/unloading activity is not continuous you will be considered to be parked.”

2.3.8 A plan showing the yellow line markings and parking locations in place is included as Appendix 2.0.

3.0 PROPOSED DEVELOPMENT

3.1 Proposed Development

3.1.1 The proposal involves the redevelopment of a vacant office building into a six storey development, but varies across the site. It comprises student residential accommodation, residential development (2x one-bed units studio flats), office floorspace, performance space/gallery and ancillary facilities. The proposed development is expected to be utilised by students enrolled at the University of the Arts, particularly those studying at the new Kings Cross-Central Saint Martin's Campus.

3.1.2 A copy of the architect's ground floor layout and accommodation schedule is provided in Appendix 6.0. The breakdown of this scheme along with its associated facilities are summarised as follows:

Table 3A Schedule of Student Accommodation

Floor	Cluster Student Units (12.5m ²)	Student Studios (17m ²)	Disabled Student Studios (24m ²)	Total
GF	6	10	6	22
ML	-	4	-	4
1F	35	12	3	50
2F	37	12	3	52
3F	37	9	2	48
4F	30	5	2	37
5F	-	13	-	13
Total	145	65	16	226

3.1.3 The proposed student accommodation development is proposed for occupation from September 2014.

3.2 Servicing Traffic Generation

3.2.1 It is considered that as the proposal is seeking to provide predominantly student accommodation, with the commercial/employment floor area of the ancillary uses there would be no material difference in the number of service deliveries generated by the proposed use compared to the existing permitted development.

3.2.2 It is considered that the deliveries associated with the ancillary uses would mainly occur during the daytime 09:00 – 17:00 hours and consist of deliveries of:

- Office suppliers (stationary, water cooler deliveries etc.);
- Couriers.

3.2.3 These deliveries would mainly be undertaken by LGV's/transit vans. It is expected that the number of deliveries would equate to approximately 1 vehicle (i.e. 2 two-way movements) per day.

3.2.4 The student accommodation and residential units would be likely to be serviced by the following:

- Home shopping;
- Goods deliveries (TVs, furniture).

3.2.5 Experience suggests that student accommodation generates a relatively low demand for servicing, with occasional deliveries of basic food and drink supplies, stationary and parcels. In addition, there will be an occasional requirement for servicing by contractors. It is likely that the proposed student accommodation would be likely to generate circa 3-4 deliveries per day and that the majority of delivery activity would be undertaken by small to medium sized vehicles.

3.2.6 These deliveries are likely to occur during the evening 18:00 – 21:00 hours and are therefore unlikely to coincide with the other deliveries. It is considered that the student accommodation would generate a level of service deliveries per day, which would not be considered material.

3.2.7 In light of the above, it is reasonable to conclude that servicing activity generated by the proposed uses will have no material impact on the operation or environmental condition towards the local highway network.

3.2.8 With regards to the frequency of the refuse and recycling collections, these are expected to take place as part of the existing weekly/fortnightly collections, on the same days as those which already occur. Given the previous office use at the site it is considered that there will not be a significant change to existing or previous collections.

3.2.9 The student refuse and recycled waste will be stored in the ground floor refuse store located at the eastern end of the site on Britannia Street. Carey Jones Chapman Tolcher Architects have advised that the refuse and recycled waste for the two residential studio flats will be stored in the cycle store located at the southeast end of the site on Wicklow Street adjacent to the Performance/Gallery space as this cycle store is big enough to accommodate the refuse and recycled waste bins associated with the residential development.

3.2.10 Full details of the refuse/recycling strategy are set out within the Design and Access Statement prepared by Carey Jones Architects.

3.3 Servicing Arrangements

3.3.1 The practical servicing solution is that delivery vehicles would stop on the single yellow lines along Britannia Street and Wicklow Street where servicing is permitted and goods would be transferred or if necessary trolleyed round to the various access points.

3.3.2 As per the existing use the site will require servicing by refuse trucks and for delivery of consumables, laundry, etc which will be undertaken on-street. The largest vehicles expected to access the site will be a 10m rigid lorry and an 11m refuse truck (26 tonnes). The swept path analysis for these vehicles is illustrated in Appendices 4.1 to 4.4.

3.3.3 Britannia Street is served by both Gray's Inn Road and King's Cross Road. Whilst vehicles could journey to and from the site via various parts of the surrounding highway network, the following routes would be the most acceptable;

- From North or East:
City Road, Penton Rise, King's Cross Road, Acton Street, Gray's Inn Road into Britannia Street
- From South:
Gray's Inn Road into Britannia Street
- From West:
Euston Road, Pentonville Road, King's Cross Road, Acton Street Gray's Inn Road into Britannia Street

4.0 SERVICING MANAGEMENT PLAN

4.1 Loading Procedures

- 4.1.1 A receptionist/security service will be appointed to manage the office facilities. Part of this person's duties will be to operate as the service manager and they will take overall responsibility for compliance with the service management plan and furthermore, will be responsible for overseeing the deliveries and delivery drivers specifically to ensure the operation and proposals laid out herein are adhered to.

4.2 Goods Deliveries

- 4.2.1 Loading would only be permitted on Britannia Street/Wicklow Street. Goods which require daily deliveries would seek to avoid the morning and evening peak hour traffic 08:00 – 09:00 and 17:00 – 18:00 hours.
- 4.2.2 Vehicles will not be permitted to park on Britannia Street/Wicklow Street unless loading or unloading goods or materials and must not be left unattended at any time.
- 4.2.3 It would be the services manager's responsibility to arrange delivery times so that if more than one delivery per day is required, the timings of these deliveries do not coincide.
- 4.2.4 Goods will be moved directly (where necessary by trolley) from the vehicle to the site. It is likely that the vast majority of deliveries will be small and the loads carried by the vehicle driver. At no time will delivery boxes be permitted to obstruct the footway on Britannia Street or Wicklow Street.
- 4.2.5 In the case of building evacuation or emergency, any vehicles on Britannia Street/Wicklow Street may be required to stop delivery and leave, it may be necessary in such instances to reschedule deliveries.
- 4.2.6 Users transferring goods between Britannia Street/Wicklow Street and the access will be required to use rubber wheeled trolleys to minimise noise, disturbance and damage to surfaces.

4.3 Loading Slots

- 4.3.1 Regular suppliers would be allocated a strict delivery time. Delivery time is the start of a 20 minute 'window' which enables the driver to off load/collect a consignment from Britannia Street/Wicklow Street.
- 4.3.2 The service manager will be responsible for allocating delivery slots and compiling a monthly delivery schedule. The service manager will undertake best endeavours to rearrange deliveries that coincide if there is more than one regular supply delivery on any one day. The Service Manager will undertake best endeavours to arrange deliveries to avoid the peak hour traffic flows on Britannia Street (08:00 – 09:00 and 17:00 – 18:00 hours).
- 4.3.3 Deliveries that miss an agreed booking slot due to traffic delays or any such reason outside their control will only be permitted if there is a free slot and they will not coincide with another delivery. If there is already a delivery arranged then they will not be permitted to unload until the next available slot becomes available.
- 4.3.4 A contact name must be obtained by the service manager before goods are turned away to arrange an alternative delivery time.
- 4.3.5 Suppliers must be advised of the importance of adhering to delivery times at the time the order is placed to avoid wasted trips.

4.4 Refuse/Recycling

- 4.4.1 The development intends to store the student refuse and recycled waste in the ground floor refuse store located at the eastern end of the site on Britannia Street. The refuse will be available to be collected from Britannia Street. Carey Jones Chapman Tolcher Architects have advised that the refuse and recycled waste for the two residential studio flats will be stored in the cycle store located at the southeast end of the site on Wicklow Street adjacent to the Performance/Gallery space as this cycle store is big enough to accommodate the refuse and recycled waste bins associated with the residential development.

5.0 CONCLUSIONS AND RECOMMENDATIONS

- 5.1 ADL Traffic Engineering Ltd have been appointed by Watkin Jones to undertake this Servicing Statement with regard to the redevelopment of the site at 15 - 27 Britannia Street.
- 5.2 The site is a disused office block with a gfa of 4,796m². There is no off site servicing bay for the existing development therefore it is considered that the offices would have been serviced on street from Britannia Street and/or Wicklow Street, which would be similar to other developments within the vicinity of the site.
- 5.3 The London Borough of Camden Council have already implemented parking and loading restrictions in the vicinity of the site. The site is located in CPZ CA-D which restricts parking Monday to Friday 08:00 – 18:30 hours and Saturday 08:30 – 13:30 hours. A mix of on-street parking bays exist for resident parking, pay and display, car-club and hospital only parking, within the vicinity of the site.
- 5.4 In addition, there are restrictions in place on Britannia Street to large goods vehicles, as outlined within Section 2.
- 5.5 Loading is permitted at anytime along the site frontage on Britannia Street/Wicklow Street as this will remain unaffected by the revisions being implemented by the Council.
- 5.6 The proposal is for 226 student accommodation units and residential accommodation (2 x one-bed units studio flats). Ancillary uses such as performance space/gallery, office space, refuse store, cycle parking, plant and storage will also be provided on site.
- 5.7 It is considered that the number of deliveries to the development proposal will be minimal and that the practical loading solution would be for vehicles to stop on Britannia Street/Wicklow Street.

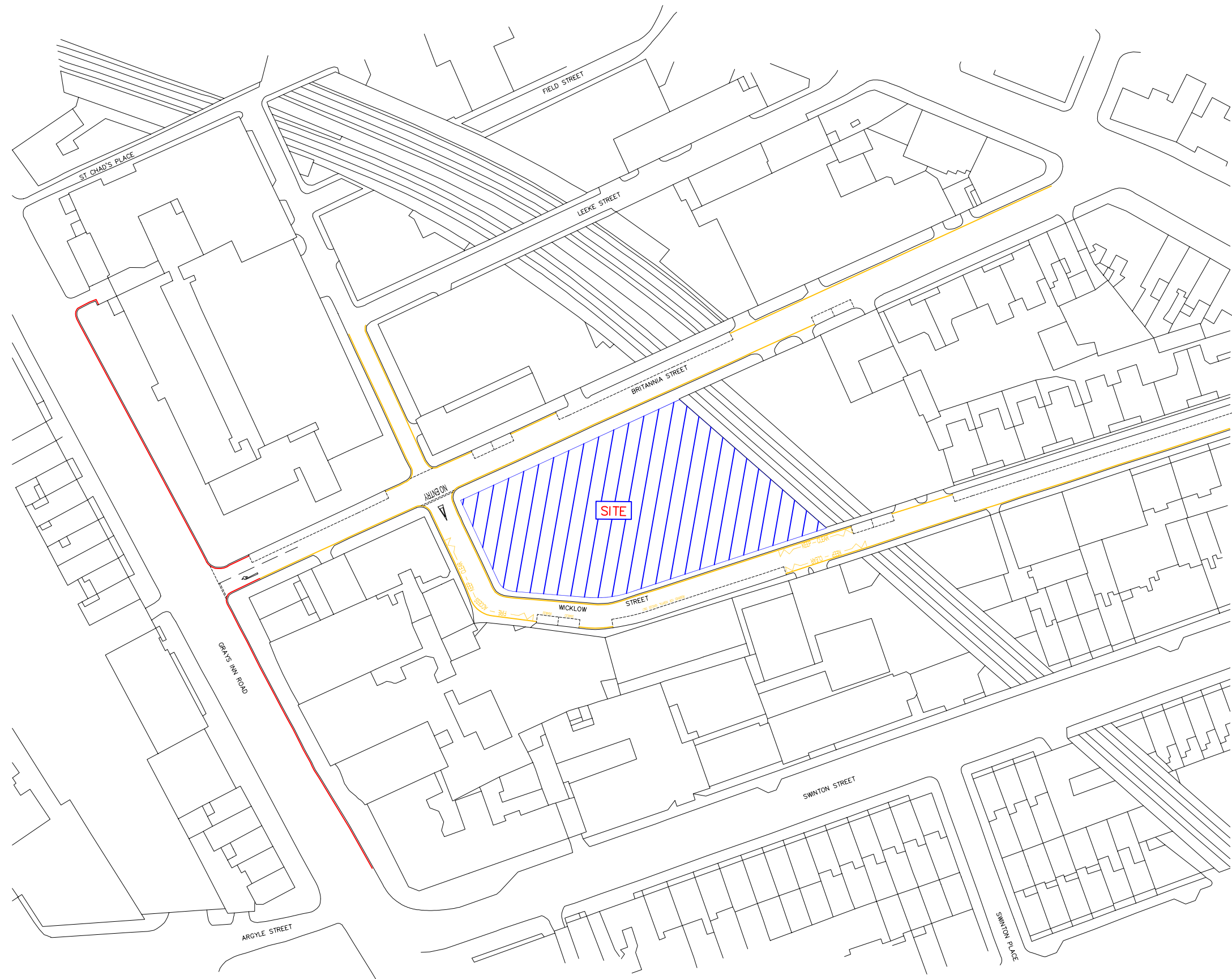
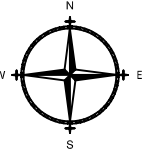
- 5.8 A servicing management plan is outlined including the responsibilities of the receptionist of the student accommodation who will also form a security function for the office suite(s).
- 5.9 This Service Management Plan outlines the responsibilities of the receptionist of the student accommodation who will also form a security function for the development.
- 5.10 The agreed contents of this Servicing Management Plan must be complied with unless otherwise agreed with the Council. The building occupiers shall work with the Council to review this Service Management Plan from time to time when necessary. Any future revised plan must be approved by the Council and complied with thereafter.
- 5.11 There are therefore no servicing reasons for refusing this application. Servicing on street from Britannia Street/Wicklow Street is considered to provide a viable and acceptable solution.

APPENDIX 1.0

SITE LOCATION PLAN

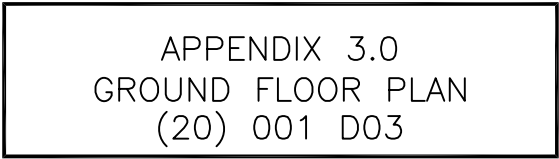


SITE AND SURROUNDING AREA

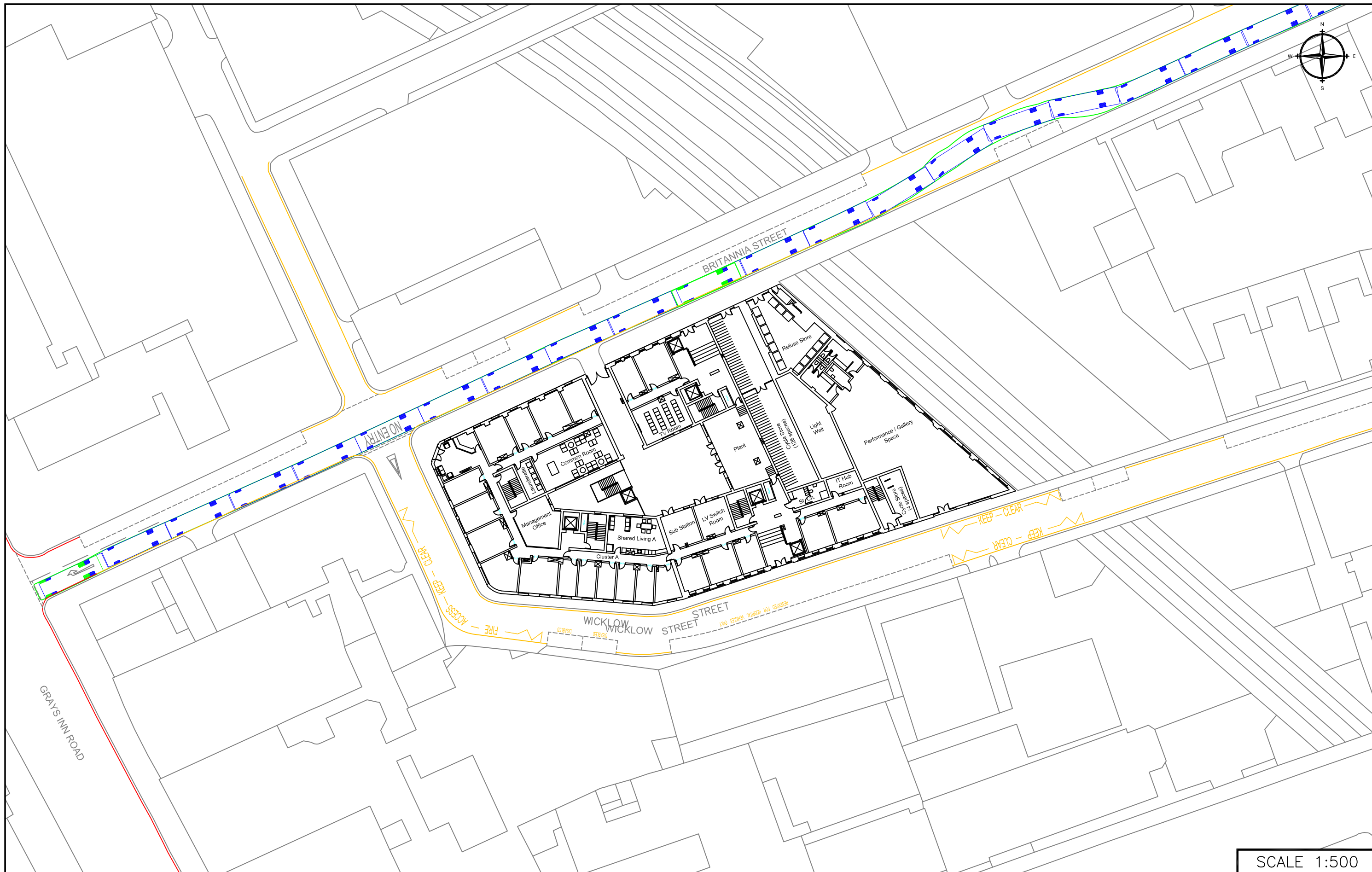


SCALE 1:1000

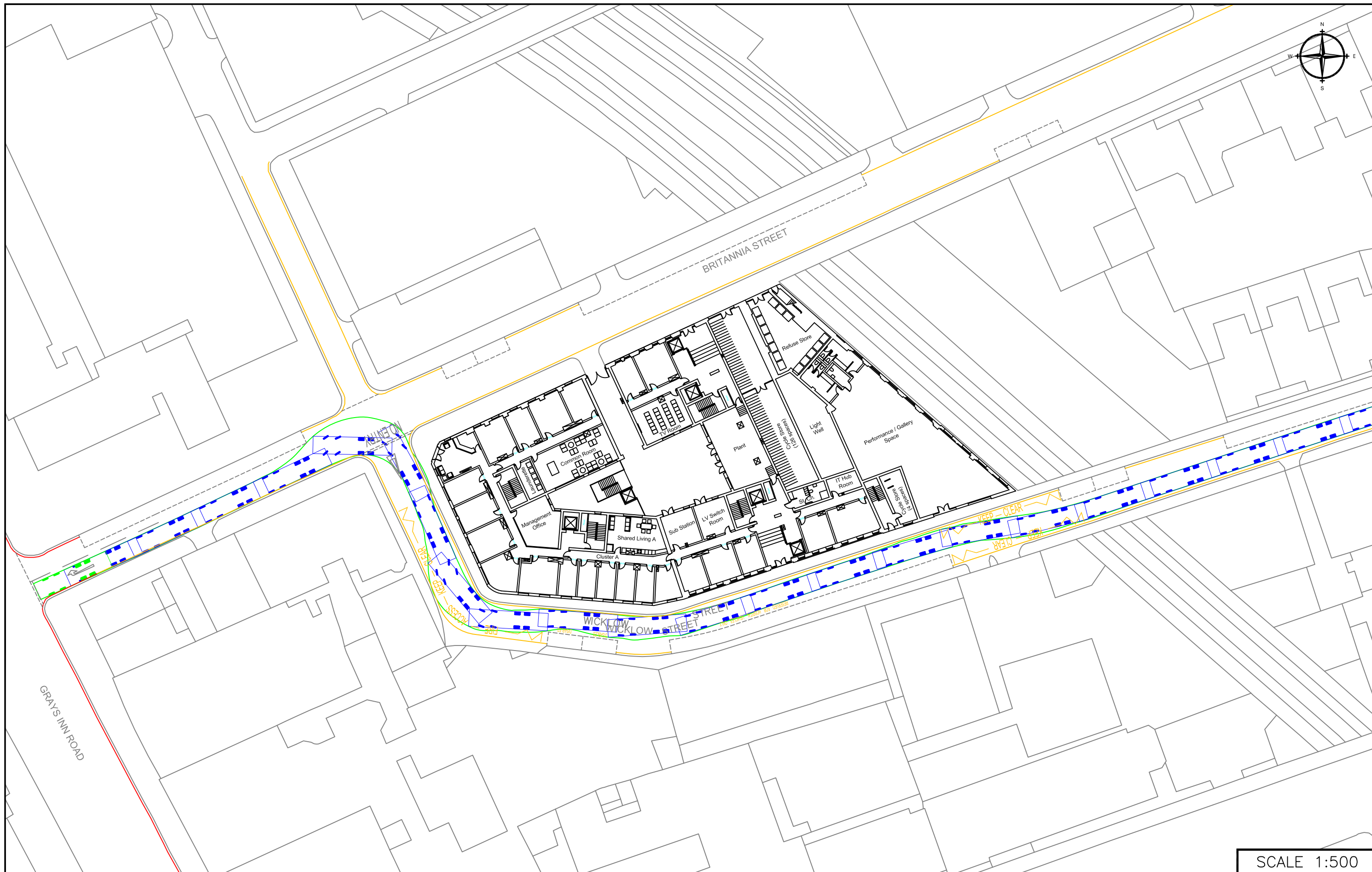
ARCHITECT'S SITE LAYOUT



SERVICING: AUTOTRACK ANALYSIS

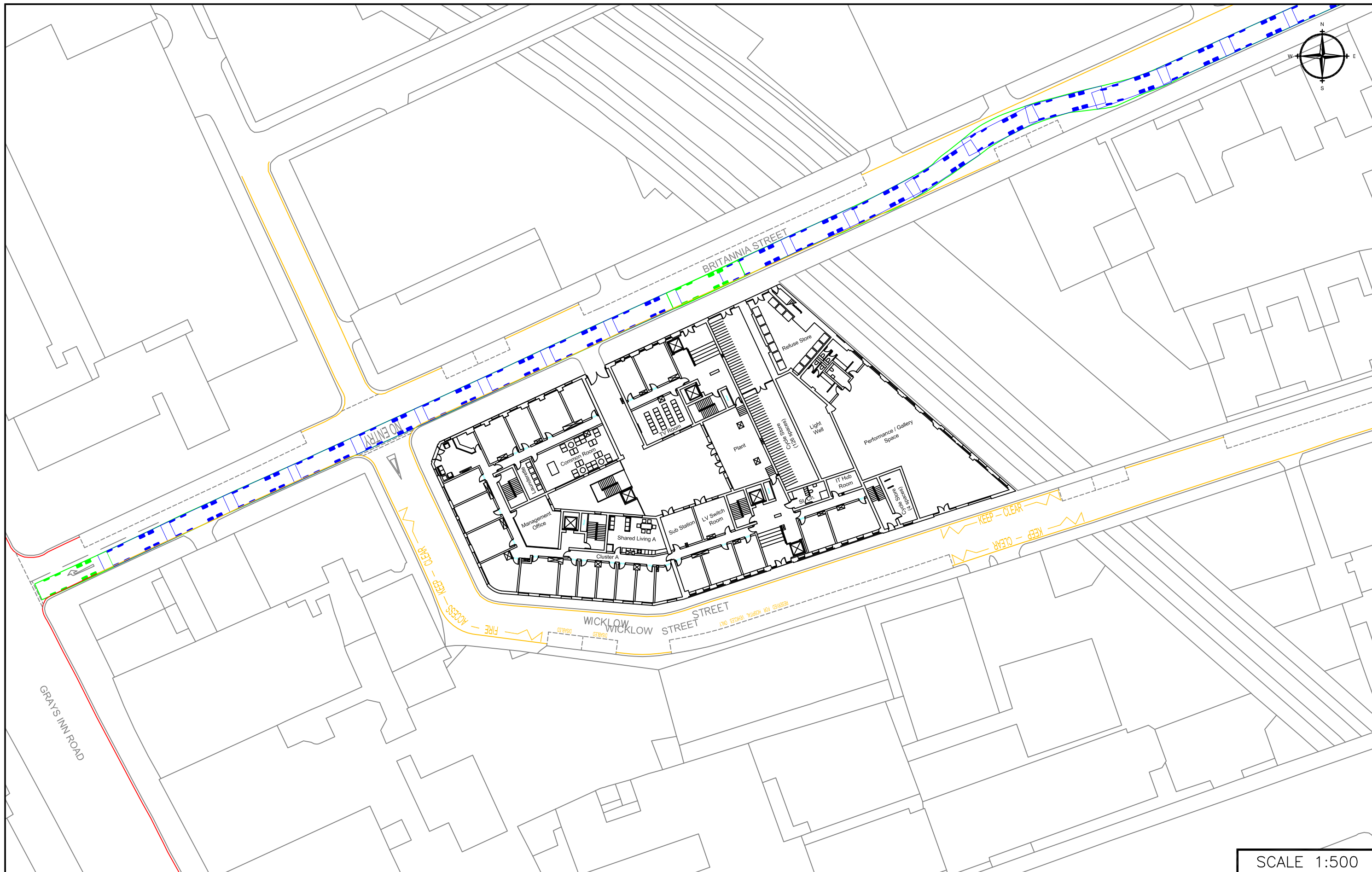


APPENDIX 4.1b
TRACK ANALYSIS: 10.0m RIGID
BRITANNIA STREET (20) 001 D03



SCALE 1:500

APPENDIX 4.2a
TRACK ANALYSIS: 11.347m REFUSE
VEHICLE WICKLOW STREET (20) 001 D03



SCALE 1:500

APPENDIX 4.2b
TRACK ANALYSIS: 11.347m REFUSE
VEHICLE BRITANNIA STREET (20) 001 D03