

# Construction Management Plan (Highway Measures)

Site; Hunter's Lodge, 5 Belsize Lane, Hampstead, London NW3 5AD  
September 2011 - DRAFT

## Introduction

This Construction Management Plan (CMP) outlines how construction work will be carried out and how this work will be serviced (eg delivery of materials, set down and collection of skips), with the objective of minimizing traffic disruption and avoiding dangerous situations and minimizing the impact on the local amenity.

The plan covers both the demolition and construction phases of development. Details of the CMP for Hunter's Lodge relate directly to the scale, type and location of the proposed development and they assess the impact on transport and on the local amenity including road user amenity.

The CMP should be read in conjunction with the attached Site Layout Plan (as shown in Appendix). This shows how self contained the site is and explains how we intend to manage key issues such as parking, loading/offloading, storage, access, excavation etc etc

## Construction Management Plan - Key Issues

As a key element of the CMP, we outline here how the following issues have been considered and where necessary, their impacts mitigated;

Note; the term "vehicles" below refers to all vehicles associated with the implementation of the proposed development eg demolition, site clearing, delivering of plant, materials and construction, staff parking etc

a) A brief description of the site, surrounding area and development proposals for which the CMP applies

The site is located in a residential area of Hampstead on Belsize Lane and is at approximate National Grid Reference 527006, 185240 with the post code being NW3 5AD. The proposed development comprises of the demolition of an existing 1928 garage block, it's replacement with a 3 storey extension and the construction of a lightweight glass garden room facing onto a new patio. The development is "semi basement" involving some minimal excavation to the new garden level.

b) Proposed start and end dates for each phase of construction

The start date of the project will depend totally on a planning approval date. However, at this stage, it is assumed that the project will start in December 2011. In broad terms, the project will be divided into various construction phases as follow;

- mobilization and site preparation and delivery of all equipment (starts 1/12/11)
- removal and set aside of topsoil (starts 1/12/11)
- demolition of existing 1928 extension and removal of waste from site (5/12/11)
- support of remaining buildings, boundary wall (retained section) and protection of trees
- piling works in preparation for site excavation and new foundations (starts 19/12/11)
- site excavation and removal of clay for preparation of new foundations (starts 2/1/12 to 30/1/12)
- forming of in situ reinforced concrete foundations and steel framework for garden room (starts 30/1/12 to 5/3/12)
- complete new build structure of the 3 storey extension, garden room, and boundary wall, including roof, waterproofing and all insulation (starts 5/3/12 to 2/4/12)
- complete all structural glass works to garden room and windows to extension (start 2/4/12 to 16/4/12)
- internal services and finishes (starts 16/4/12 to 29/6/12)
- garden and patio landscaping (starts 28/5/12 to 29/6/12)

c) The proposed working hours within which vehicles will arrive and depart

Monday to Friday 8.00 to 17.00. Saturday 8.00 to 13.00 only in exceptional circumstances

d) The access arrangements for vehicles

Access to the site will be via the two existing Belsize Lane crossovers - the main access being through double gates in the location of the existing garage block (which is to be demolished). As and when absolutely necessary, parking spaces in the public road could be suspended by obtaining the appropriate permits

e) Proposed routes for vehicles between the site and the Transport for London Road Network (TLRN) ([http://www.tfl.gov.UK/assets/downloads/TFL\\_Base\\_Map\\_Master.pdf](http://www.tfl.gov.UK/assets/downloads/TFL_Base_Map_Master.pdf) – see map attached in Appendix)

From Belsize Lane follow A502 through to Golders Green to the A406 (Transport for London Road Network – North & West area) and on to transfer stations or landfill sites

f) Typical sizes of all vehicles and the approximate frequency and times of day when they will need access to the site, for each phase of construction

The largest vehicle requiring access to the site will be one that is able to carry a maximum load of 20 tonnes net. These vehicles will be for delivery of materials, removal of excavated materials, delivery of plant and generally for the use of the contractors. Typically, these vehicles will operate between 8.00 and 15.00 and will take no longer than 20 minutes to load and offload, during which time they will be fully supervised by a Banksman. The estimate number of loads for the excavation phase will be 40, with a maximum of 4 loads per day. The estimated number of loads for the concrete for the foundation phase will be 10 over the period of 1 week. Regular deliveries of materials would be expected on an average of once per week during the duration of the project, mainly in the mornings. While delivering, the vehicles will be parked in the road area between the two sets of residents parking located on the boundary of Hunter's Lodge. We would look to avoid any large deliveries or excavations during the St Christopher school run which are typically between 8.30 and 9.00 in the mornings and between 15.00 and 15.30 in the afternoons.

g) Swept path drawings for any tight maneuvers on vehicle routes to the site

There are no tight maneuvers and Banksman will be available all the time

h) Details (including accurate scaled drawings) of any highway works necessary to enable construction to take place

There are no highway works necessary to enable the construction to take place. However, there will be works to the public pavement to remove the crossover and reinstate the public footpath when all building works are completed

i) Parking and loading arrangements of vehicles and delivery of materials and plant to the site

See sections on "access arrangements" and "vehicle arrangements" above. Parking will be available on the site and loading and offloading will be conducted on the street side within a maximum of 20 minutes

j) Details of proposed parking bays suspensions and temporary traffic management orders

There will be no temporary traffic management orders and it is very unlikely that we will have to suspend any residents parking bays during the works. If required, we would

only look to suspend half the parking bay to the corner of the site and none along the front elevation.

k) Proposed overhang (if any) of the public highway (scaffolding, cranes etc)

None are required for this development. There may be a requirement during the finishing phases for scaffolding to the front elevation of the new extension - in this event, a safe pedestrian walkway would be formed under the scaffolding and there would be no overhang into the public highway

l) Details of hoardings required or any other occupation of the public highway

Hoardings will be erected to the height of the existing boundary wall, on the site boundary, hence not occupying the public highway

m) Details of how pedestrian and cyclist safety will be maintained, including any proposed alternative routes (if necessary), and any Banksman arrangements

Banksman will attend all loading and unloading to ensure safety and free flow of pedestrians and cyclists

n) Details of how traffic associated with the Development will be managed in order to reduce congestion

Given the frequency of vehicles associated with the construction works (see section f above), we do not expect any traffic congestion on Belsize Lane

o) Details of any other measures designed to reduce the impact of associated traffic (such as the use of construction material consolidation centers)

The contractor will be encouraged to consider all measures to reduce the impact of traffic associated with the construction works. As an example of this, they will use part of the site as a construction material consolidation centre and any earth excavated during the foundations phase will be stored in a skip on site, ready for rapid collection by grab lorries

p) Details of how any significant amounts of dirt or dust that may be spread onto the public highway will be cleaned or prevented

Should any debris be spilled onto the road or public pavements, this will be swept up and the public highway cleaned as required. The hoardings constructed around the whole site will help to prevent the spread of dust to neighbouring gardens and onto the public highway

q) Details of consultation on a draft CMP with local residents, business, local groups and Ward Councillors (who, how, comments, amendments to plan etc)

As a relatively small domestic project, in a reasonably quiet residential road, no consultation with the local residents has yet taken place. However, all three of the Ward Councillors are aware of the proposals and have full access to this CMP. Should the scheme receive planning permission, then neighbours will be contacted and consulted on the CMP in the manner recommended by the planning authority

r) Details of any Construction Working Group that will be set up, addressing the concerns of surrounding residents, as well as contact details for the person responsible for community liaison on behalf of the developer, and how these details will be advertised to the community

See details of the consultation process above. The Belsize community is a very tight knit one and much of the communication between residents is done by email and via the Ward Councillors

s) Details of any schemes such as the "Considerate Contractors Scheme" that the project will be signed up should form part of the consultation and be notified to the Council. The contractor will follow the "Guide for Contractors Working in Camden"

See details of the consultation process above

t) Details of other construction sites in the local area and how your CMP takes into consideration the cumulative effects of construction local to your site

Due to the relatively small scale of the works, it is not considered that this project will adversely affect or be affected by other construction sites in the area. This site is in a residential area, with good access to the arterial routes, so we believe this construction project will have limited impact. We believe that there are no other construction site in Belsize Lane or Wedderburn Road current at this time

u) Any other relevant information with regards to traffic and transport

No other relevant information at this time

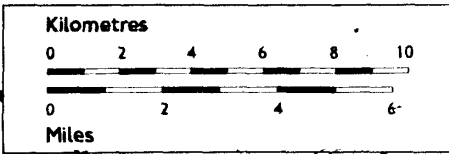
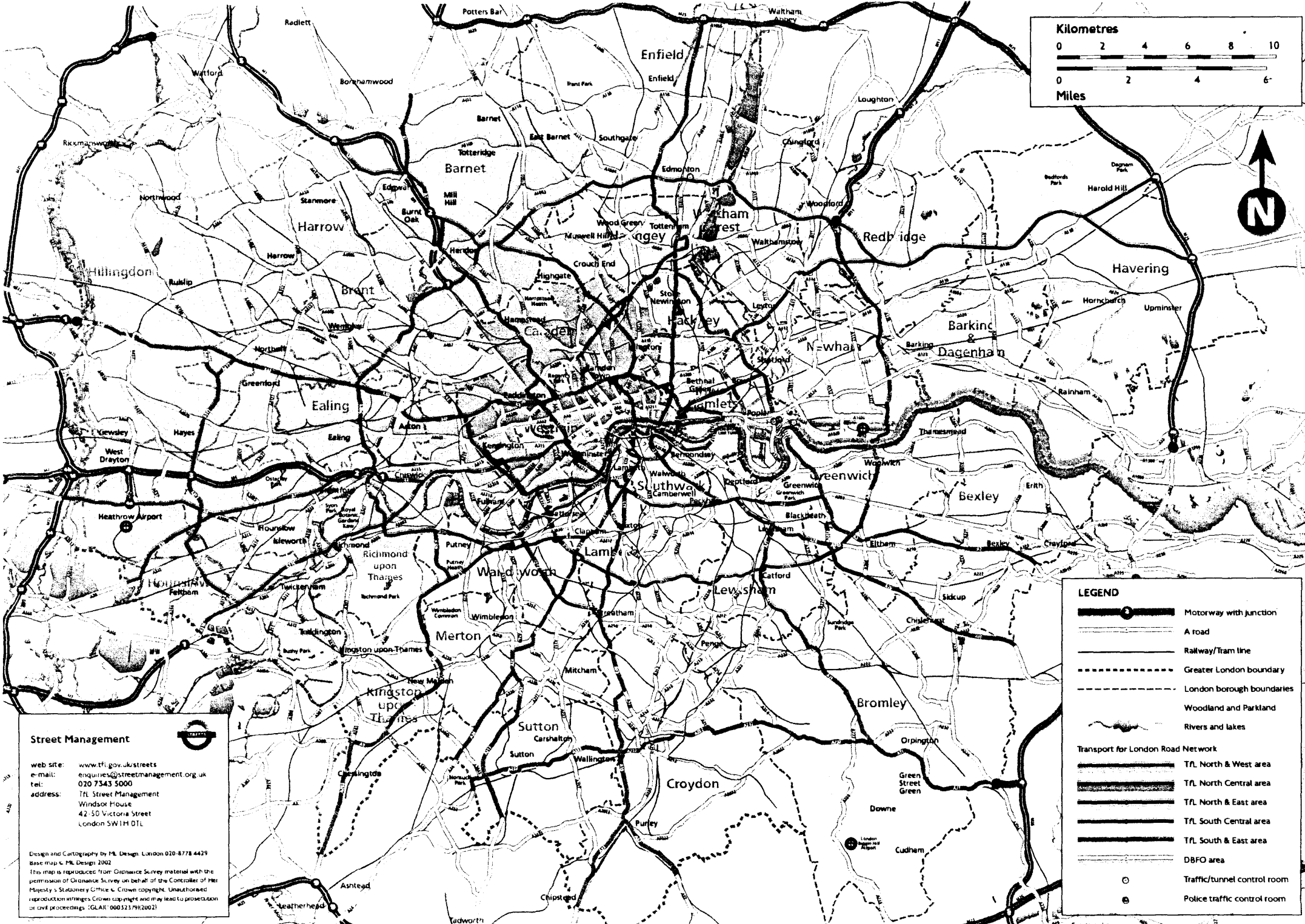
v) The CMP should also include the following statement;

We agree to include the following statement in our CMP

"The agreed contents of the Construction Management Plan must be complied with unless otherwise agreed with the Council. The project manager shall work with the Council to review this Construction Management Plan if problems arise in relation to the construction of the Development. Any future plan must be approved by the Council and complied with thereafter."

## APPENDIX

- i) TLRN Base Map
- ii) Construction Management Plan – Site Layout Drawing



**Street Management**

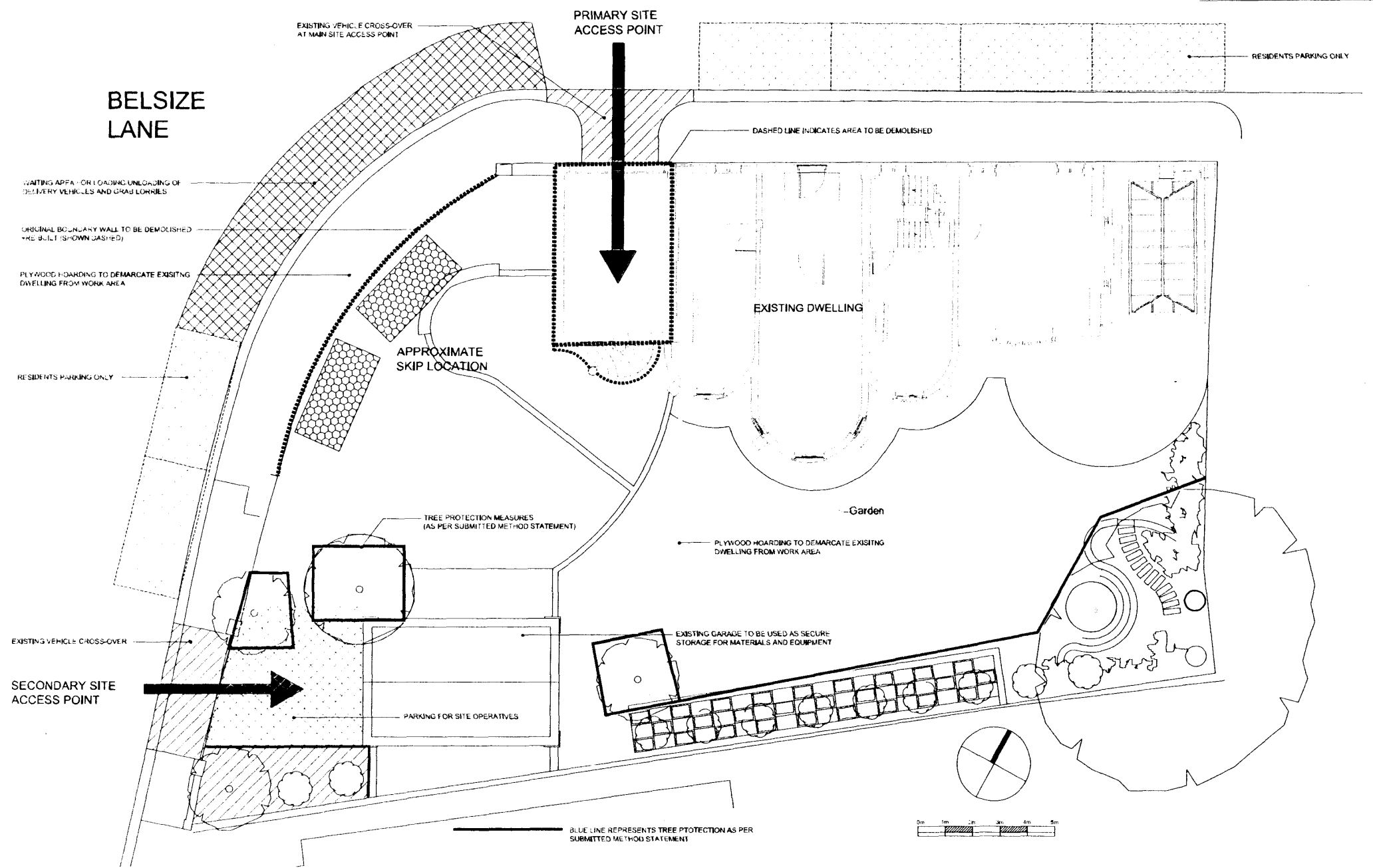


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**LEGEND**

- Motorway with junction
- A road
- Railway/Tram line
- Greater London boundary
- London borough boundaries
- Woodland and Parkland
- Rivers and lakes
- Transport for London Road Network**
- TfL North & West area
- TfL North Central area
- TfL North & East area
- TfL South Central area
- TfL South & East area
- DBFO area
- Traffic/tunnel control room
- Police traffic control room



Hunters Lodge, Belsize Lane, London

Drawing: Construction Management Plan

Scale: Not to scale Drawn: MW

Status: Planning Date: Sept. 2011

Ref: 1007 - 150 Revision: A

# CONSTRUCTION MANAGEMENT PLAN

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